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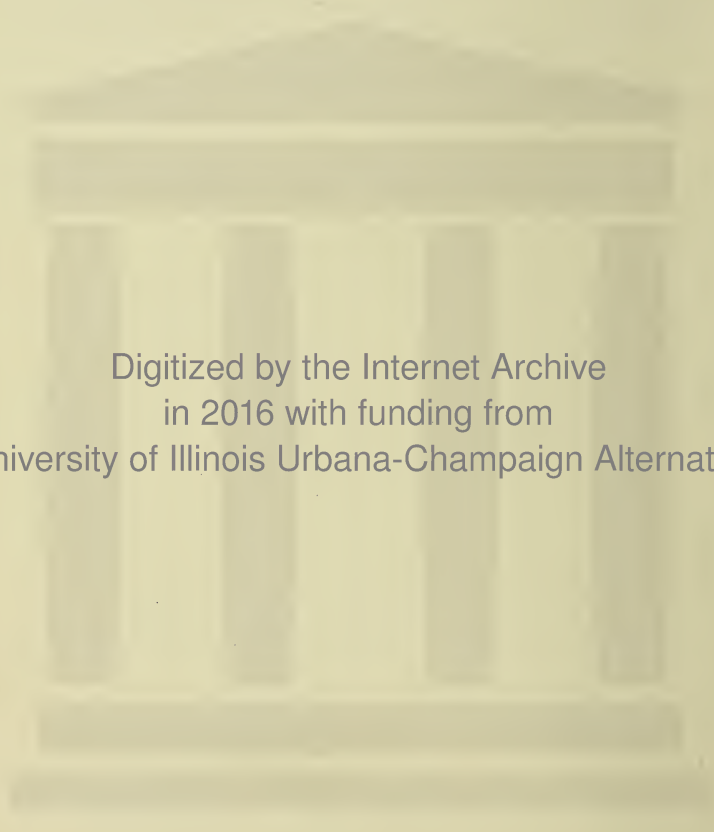
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THE
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OF
TARIFF AND TRADE NOTICES
AND
MISCELLANEOUS COMMERCIAL INFORMATION.

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[No. 114.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

The improvement in the state of employment has been maintained during December, the slight increase reported in the percentage of unemployed members of trade unions at the end of the month being entirely accounted for by the Christmas holidays, which always make the December returns less trustworthy than those for other months as an indication of the state of the labour market. In spite of the indirect effects of the continued stoppage in the engineering trade, the percentage of unemployed is considerably lower than for the corresponding month of 1894.

In the 88 trade unions, with an aggregate membership of 398,258, making returns, 19,121 (or 4·8 per cent.) are reported as unemployed at the end of December, compared with 4·2 per cent. in November, and with 7·7 per cent. in the 67 unions, with a membership of 367,796, from which returns were received for December 1894.

Coal Mining.—Coal miners continue to be well employed. It is shown, from returns relating to 882 pits, employing 256,398 workpeople, that these pits worked an average of 4·88 days per week during December, compared with 5·08 days in November, and 4·97 days in December 1894. The average for last month, however, is reduced by the Christmas holidays.*

Iron Mining.—At 113 iron mines, employing 13,432 workpeople, the average number of days worked during December (including the Christmas holidays) was 5·57 days per week, as compared with 5·74 days in November, and 5·85 days in December 1894.*

* The Christmas holidays were not included in the period on which the figures for December 1894 were based.

In the *Pig Iron Industry* the improvement in employment continues. The number of furnaces in blast at the end of the month owned by 109 ironmasters making returns was 324, or two more than in November and 15 more than in December 1894. The number of workpeople employed at these furnaces was 20,880, an increase of 208 over November last, and of 903 over December 1894.

Returns furnished by 173 employers respecting *Steel and Tinplate Works, Puddling Furnaces, and Rolling Mills* showed that they had 62,316 workpeople employed at the end of the month, as compared with 60,014 workpeople at the end of December 1894, an increase of 2,302. Returns from 37 tinplate manufacturers give the number of their mills in operation at the end of the month as 180, being 16 less than at the end of November last, and 11 more than at the end of December 1894.

Employment in the *Engineering* and kindred trades has continued good (except for the stoppage in the North), the percentage of unemployed union members being 5·0, compared with 4·7 in November. In December 1894 the percentage reached 9·7.

In the *Shipbuilding* trades, the percentage of unemployed union members at the end of the month was 10·5, compared with 10·2 in the previous month and 16·6 in December 1894.

On the whole, the *Building* trades, though showing the usual seasonal decline, are well employed for the time of year. The percentage of unemployed in unions making returns for December is 3·8, compared with 2·1 for November and 5·7 for December 1894.

Some seasonal falling off in employment has taken place in the *Furnishing* and *Woodworking* trades. The percentage of unemployed in unions making returns is 4·4, compared with 2·9 in November and 6·3 in December 1894.

The *Printing* and *Bookbinding* trades are not so busy as in November, though quite up to the average for the season. The percentage of unemployed union members is 4·1, compared with 2·5 in November and 4·5 in December 1894. Employment in most branches of the *Paper* trade has improved, and the percentage of unemployed union members has fallen from 6·2 to 3·1.

The *Tailoring* trade, both in the ready-made and bespoke branches, is dull in most districts.

Employment in the ready-made *Boot and Shoe* trade has improved in the most important districts, but in other centres it is dull. The bespoke trade is reported dull.

The *Leather* trades are scarcely so well employed as in November, and the percentage of unemployed union members in December was 4·1, compared with 2·3 in the former month and 7·6 in December 1894.

In the *Cotton* trade spinners have been better employed in most districts, but employment for weavers continues slack. The *Woollen* and *Worsted* trades continue well employed. Employment

in the *Hosiery* trade is still good. The *Silk* trade is busy in most districts.

Information respecting 447 textile mills, employing about 72,000 women and girls, shows that there has been an improvement in the state of employment, 91 per cent. being in mills giving full employment, compared with 87 per cent. among those for whom returns were received in November. The improvement occurred in the spinning branch of the cotton trade.

Trade Disputes.—The number of disputes commencing in December was 37, as compared with 38 in November and 43 in December 1894. Thirteen disputes took place in the mining and quarrying industries, 7 in the building trades, 5 in the textile trades, 2 each in the clothing and engineering and ship-building trades, 1 in the metal trades, 1 in connection with dock labour, and 6 in a miscellaneous group of industries. In 34 of the 37 disputes, 3,931 workpeople were involved, an average of 115 persons per dispute, as compared with 321 in the previous month. Thirteen old disputes, 12 of which involved 1,819 workpeople, were settled during the month, and at the end of the month 11 new and 26 old disputes, directly involving 7,954 workpeople, were known to be still unsettled.

Changes in Wages.—Of the 16,500 workpeople affected by changes in rates of wages reported during December, 11,500 received increases and 5,000 sustained decreases. The total number affected in the corresponding month of last year was 7,000 (increases, 1,500, decreases 5,500). The changes reported last month include an advance to the iron ship-builders on the Clyde and at Belfast, to take effect from the beginning of February, and increases in the rates paid to 2,270 blast furnacemen in the Lancashire and Cumberland district, and to 2,500 glass bottle makers in Yorkshire. Blast furnacemen's wages in the Cleveland and Durham district have, however, been reduced 2 per cent.

TRADE OF THE UNITED KINGDOM FOR 1895.*

From the "Monthly Accounts relating to Trade and Navigation of the United Kingdom" it appears that the total value of the imports from foreign countries and British Possessions for the twelve months ending 31st December 1895 was 416,688,000*l.*, as against 408,345,000*l.* for the same period of 1894, or an increase of 8,343,000*l.* This increase is seen principally in manufactured articles of which the value of the imports in 1895 was 6,701,000*l.* in excess of that for the same period of 1894, the principal articles contributing to this increase being leather, iron manufactures, linen yarn, silk broad stuffs, silk ribbons, and woollen cloths and stuffs. Articles of food and drink show an increase

* See also Statistical Tables, pages 113 to 125.

of 1,487,000*l.*, raw materials for industries and for textile and other manufactures an increase of 1,170,000*l.*, and oils an increase of 605,000*l.*

With regard to the exports of British and Irish produce and manufactures, the figures show still more satisfactory results, the value for the year 1895 having been 226,169,000*l.*, as against 215,824,000*l.* in 1894—an increase of 10,345,000*l.* Exports of yarns and textile fabrics have increased in value 5,399,000*l.* during the year; machinery and millwork, 1,010,000*l.*; metals and articles manufactured therefrom, 928,000*l.*; and other manufactured or partly manufactured goods, 3,955,000*l.*

With regard to the exports of foreign and colonial merchandise, the value for 1895 amounted to 59,971,000*l.*, being 2,009,000*l.* in excess of that for the previous year.

OFFICIAL ENQUIRIES UNDER THE BOILER EXPLOSIONS ACT.

A formal investigation was held at Newcastle-under-Lyme, on the 11th and 12th ultimo, into the circumstances attending the explosion of a locomotive boiler, belonging to a colliery company at Madeley Heath, on the 1st November last, whereby two persons were killed.

The locomotive, made in 1875, was purchased by the company in 1890, and it was thereafter examined periodically by an assistant to the company's engineer. In 1894 the fire-box was found to be thin on the left-hand side, and a patch was put on it. In October 1895 the locomotive was retubed and the fire-box examined, when some stays were renewed, but the boiler was not subjected to hydraulic pressure after these repairs. The locomotive was then taken out of the shed, and within a few hours the boiler exploded.

The Court found that the explosion was due to a portion of the plate on the right-hand side of the fire-box having become so wasted as to be unable to withstand the pressure to which it was subjected, and to the fact that the heads of the stays had so wasted that the thin plate was practically unsupported. They were of opinion that the fire-box had not been properly examined in December 1894, nor in October 1895, and that the neglect of the assistant to their engineer had contributed to the explosion. For this neglect the Court held the company responsible, and they were ordered to pay 25*l.* towards the costs and expenses of the investigation.

A formal investigation was held at the Council House, Birmingham, on the 11th ultimo, into the circumstances attending the explosion of a boiler at Liverpool Street, Birmingham, on the 18th October last.

The boiler, one of the Root type, consisted of 12 rows of tubes, seven in each row, screwed at their ends into cast-iron headers.

These headers were connected by cast-iron connections, and they were fitted with caps, which were secured to the headers with two bolts, the square heads and square shanks of which fitted into slots on the headers.

On the 18th October the joint of a cap on a header at the back end of the boiler was leaking; the bolts were screwed up upon two occasions, and, about 1 p.m., the engine driver was about to screw up the bolts again when one of them came out of the slot and the cap was partly blown out; steam escaping therefrom so severely scalded the engine driver that he died in the hospital the same afternoon.

The Court found that the explosion was due to the right-hand bolt slipping out of its position through not being properly fitted and bedded in the slot provided for its reception. They were of opinion that the bolts should not have been tightened whilst the boiler was under steam, but they exonerated the owners and their engineer from blame.

A formal investigation was held at Wishaw on the 23rd and 24th instant, into the circumstances attending the explosion of a boiler at the Excelsior Iron Works on the 27th November, whereby two persons were killed and five others were injured.

The boiler, one of the Lancashire type, was upwards of 20 years of age. Its management was entrusted to the engineer of the works, by whom it was examined every three weeks.

On the 27th November, when the boiler was being worked at a pressure of about 45 lbs. per square inch, it exploded. The right-hand flue collapsed and ruptured at the angle bar connecting it to the back end plate and at the circumferential seam between the first and second rings near the front end.

The Court found that the explosion was due to grooving at the flue attachment; they did not consider that the owner was justified in trusting to the inspection merely of his own employé, without taking other precautions to ensure that the boiler was being worked under safe conditions.

The Court ordered the owner to pay the sum of 60*l.* towards the costs and expenses of the investigation.

GOVERNMENT CONTRACTS.

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply (1) waggons, (2) 100 ft. spans, (3) brake-vans, &c.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered for waggons at that office by *two* o'clock p.m. on Tuesday the 21st January 1896, and for 100 ft. spans, brake-vans, &c., by *two* o'clock p.m. on Tuesday the 28th January 1896, after which times no tender will be received.

Admiralty.—A communication has been received at the Board of Trade from the Contract Department of the Admiralty, stating that tenders will be received until noon on the dates named below for the under-mentioned articles, for which forms of tender, containing conditions of contract and all particulars, may be obtained on personal application at the Contract Department, Admiralty, or by letter addressed “Director of Navy Contracts, Admiralty, Whitehall, S.W.” Full-size patterns of the articles may be seen, and small samples of most of them obtained, at the Admiralty pattern rooms, 72, Great Queen Street, Lincoln’s Inn, London, W.C., and at the Chambers of Commerce at the places specified against the items, on application to the Secretary, from whom forms of tender may also be obtained, viz. :—

Dates for
Receipt of
Tender.

Buttons, stained bone	-	17,400 gross	} Birmingham	-	-	} 21st January
„ white metal	-	19,500 „		-	-	
„ gilt, &c.	-	2,200 „		-	-	
Worsted, blue	-	25,000 lbs.	} Bradford, Cleckheaton, Dewsbury, Glasgow, Halifax, Leeds, and Leicester	-	-	
„ white, &c.	-	800 „		-	-	
Flannel, No. 1	-	700,000 yards	} Cleckheaton, Halifax, Huddersfield, Leeds, Manchester, and Roch- dale	-	-	
Blankets	-	44,500 No.		-	-	
Bunting	-	255,000 yards	} Bradford, Halifax, Hud- dersfield, and Leeds	-	-	
Cloth, blue, No. 1	-	75,000 „		-	-	
„ No. 4	-	20,000 „	} Bradford, Dewsbury, Halifax, Huddersfield, and Leeds	-	-	
Serge, blue, No. 1	-	120,000 „		-	-	
„ No. 2	-	670,000 „		-	-	
„ No. 3	-	30,000 „		-	-	
Navy Tartan	-	7,000 „	} Belfast and Leicester	-	-	
Comforters	-	18,000 No.		-	-	
Jerseys	-	22,500 „	} Dewsbury, Halifax, Hud- dersfield, Leeds, and Manchester	-	-	
Stockings	-	12,000 pairs		-	-	
Socks	-	140,000 „		-	-	
Woollen drawers	-	55,600 „	} Glasgow, Leeds, and Manchester	-	-	
Fearnought	-	83,500 yards		-	-	
Kersey	-	10,300 „		-	-	
Caps, blue cloth	-	8,000 No.	} Barnsley, Belfast, Dun- dee, and Leeds	-	-	} 28th January
Duck covers for caps	-	4,000 „		-	-	
Hessen	-	95,000 yards	} Barnsley, Belfast, Dun- dee, and Leeds	-	-	
Linen, bleached, &c.	-	58,400 „		-	-	
„ black	-	15,000 „		-	-	
Osnaburgh	-	175,000 „		-	-	
Towelling	-	119,000 „		-	-	
Duck	-	600,000 „	} Barnsley, Belfast, Dun- dee, Leeds, and Man- chester	-	-	
Drill, white cotton	-	60,000 „		-	-	
Jean, blue	-	150,000 „	} Barnsley and Manchester	-	-	
	-			-	-	

Dates for
Receipt of
Tender.

Canvas sail cloth (Royal Navy)	-	815,000 yards	Belfast, Dundee, Glasgow, and Leeds		
Canvas sail cloth (merchant)	-	334,000 "			
Canvas for seamen's bags	-	90,000 "			
Coal sack cloth	-	144,000 "			
Hammock cloth	-	268,000 "	Belfast, Bristol, Dundee, Glasgow, and Leeds		
Twines	-	440 cwt.			
Threads	-	13,560 lbs.	Belfast, Glasgow, and Leeds		
Biscuit bagging	-	65,500 yards	Dundee		
Jute thread	-	18,500 lbs.			
Hair, curled	-	129,500 "	Glasgow and Leeds		28th January
Waterproof union packing paper.	-	80,000 yards	Glasgow, Halifax, and Manchester		
Calico	-	121,000 "	Glasgow and Manchester		
Silesia, black	-	20,000 "			
Cotton waste, packing, &c.	-	8,700 cwt.			
Sponge cloths	-	91,000 No.			
Shirting	-	250,000 yards	Manchester		
Small wares (bindings, gimps, webbing, &c.)	-	See Form of its Tender.			
Tape, Dutch	-	506,000 pieces			
" white	-	45,000 "			
" blue worsted	-	35,000 "			
Combs, horn	-	18,000 No.			
" ivory	-	12,000 "			

Manufacturers only will be accepted.

War Office.—Tenders will be received by 12 o'clock noon on 20th January 1896 for the supply of:—

Razors and cases,
Clasp knives,
Table knives and forks,
Carving knives and forks,

from date of acceptance of tender until 31st December 1898.

Patterns and specifications can be seen and forms of tender obtained at the Cutlers' Hall, Sheffield. Forms of tender can also be obtained at the War Office.

TENDERS FOR CONSTRUCTION OF MAIN SEWER AT BILBAO.

Her Majesty's Consul at Bilbao has advised the Secretary of State for Foreign Affairs that the Corporation of that city are prepared to receive tenders up to noon on January 30th, 1896, for the construction of a portion of the main sewer, the engineer's estimate for which is about 5,738*l*. Plans, estimates, specifications, and form of tender for this work may be inspected at the Sanitation Department of the Bilbao Town Hall.

BOARD OF TRADE AWARD FOR GALLANTRY.

The Board of Trade have awarded a silver medal to José Manoel Valente, a Portuguese signalman, in recognition of his services in assisting to rescue the crew of the S.S. "Braganza," of Liverpool, which was wrecked outside the harbour of Leixoes, Portugal, on the 15th November last.

COMMUNICATION WITH THE MAPLIN PILE LIGHTHOUSE.

A notification has been received at the Board of Trade from the Postmaster General to the effect that, in accordance with the recommendation of the Royal Commission, electrical communication has now been established with the Maplin Pile Lighthouse.

CERTIFICATE OF ALTERATION OF THE SEVERN FISHERY DISTRICT.

The following is a copy of a certificate issued by the Board of Trade on the 9th December 1895, for the alteration of the Severn Fishery District:—

Salmon and Freshwater Fisheries Acts, 1861 to 1892.

Whereas the limits of the Fishery District of the River Severn were defined by a certificate under the hand of one of Her Majesty's Principal Secretaries of State, dated the 2nd day of September 1882:

And whereas the Board of Conservators of the said district have duly made application to the Board of Trade to grant a certificate enlarging and altering the limits of the said district:

And whereas certain objections have been made to the application, and the Board of Trade have caused inquiries to be held into such objections with the exception of those which relate to the inclusion within the said district of any portion of the county of Warwick:

And whereas it is desirable that a certificate founded on the application should be granted without delay, but that the questions which relate to the county of Warwick should be postponed for further consideration, without prejudice to any of the parties concerned:

Now, therefore, the Board of Trade, by virtue of the powers conferred upon them by the Salmon and Freshwater Fisheries Acts, 1861 to 1892, and otherwise, do by this certificate enlarge and alter the limits of the said fishery district so that the limits of the said River Severn and of the fishery district of the said river shall include all such estuaries, rivers, streams, brooks, lakes, ponds, canals, dykes, cuts, drains, channels, watercourses and waters, and all such portions of the sea and sea-coast as lie within the following limits, namely, a line commencing at the bench

mark 25·8 placed on the sea-wall on the western bank of the mouth of Cone Pill, in the county of Gloucester, and thence drawn true east until it meets the boundary between the parishes of Woolaston and Aylburton in the said county, thence seawards along such boundary until it meets the line of ordinary low water on the western edge of Lydney Sands, thence in a south-westerly direction along the general line of ordinary low water on the western edge of Lydney, Sheperdine, and Beacon Sands, successively, to the south-westernmost extremity of Beacon Sands, thence true south-east until it reaches the line of ordinary low water on the western edge of Oldbury Sands, thence in a south-westerly direction along the general line of ordinary low water on the western edge of Oldbury and Slimeroad Sands successively until it reaches the south-western extremity of Slimeroad Sands, thence straight towards the north-eastern extremity of the Lower Bench until it meets the deep-water line of the channel dividing the said Lower Bench from the Upper Bench, thence successively along the deep-water line of such channel and of the channel dividing Northwick Oaze and the English Stones from the Dun Sands until it meets the deep-water line of the main channel of the estuary of the River Severn or Bristol Channel, near the Dumplings, thence along such deep-water line through the Shoots until it meets a straight line drawn from the seaward extremity of the western bank of Collister Pill, in the county of Monmouth, to the site of the Avon Battery, in the county of Gloucester, thence along such line, to the site of the said Avon Battery, thence along the western and southern boundary of the said site to the road leading through Avonmouth to Shirehampton, thence along the southern side of such road until it reaches the natural watershed between the River Avon on the one hand and the rivers and streams flowing into the eastern side of the estuary of the River Severn or Bristol Channel north of the Avon Battery on the other hand, thence successively along such watershed and along the natural watershed of the River Severn until it reaches the natural watershed between Cone Pill aforesaid on the one hand, and the rivers and streams flowing into the western side of the said estuary south of Cone Pill on the other hand, thence along such last-named watershed in a south-easterly direction until it reaches the Great Western Railway near Woolaston Station, in the county of Gloucester, and thence straight to the bench mark 25·8, where it began :

Provided that this certificate shall not operate to include within the said district any portion of the geographical counties of Warwick, Oxford, Northampton, or Leicester, without prejudice, however, to so much of the above-recited application as relates to such counties, or to any notices given or objections made in respect thereto, or to the grant of any further certificate thereunder—the limits of the said river and fishery district as above defined being more particularly shown on a map marked S. 2387/1895, and deposited herewith with the clerk of the county council of Worcester.

And the said river and fishery district as hereby enlarged and altered shall be for all purposes a river and fishery district within the meaning of the Salmon and Freshwater Fisheries Acts, 1861 to 1892.

Signed and sealed by order of the Board of Trade this ninth day of December 1895.

(Signed) A. D. BERRINGTON,
An Assistant Secretary to the
said Board.

CERTIFICATE OF ALTERATION OF THE WYE FISHERY DISTRICT.

The following is a copy of a certificate issued by the Board of Trade on the 9th December 1895 for the alteration of the Wye Fishery District :—

Salmon and Freshwater Fisheries Acts, 1861 to 1892.

Whereas the fishery district of the River Wye was formed and its limits were defined by a certificate under the hand of one of Her Majesty's Principal Secretaries of State, dated the 18th day of January 1866 :

And whereas the limits of the said district were altered and further defined by like certificates, dated respectively the 17th day of December 1866 and the 18th day of February 1869 :

And whereas the Board of Conservators of the said district have duly made application to the Board of Trade to grant a certificate altering the limits of the said district :

And whereas certain objections have been made to the application, and the Board of Trade have caused an inquiry to be held into such objections :

And whereas it is desirable that a certificate founded on the application should be granted :

Now, therefore, the Board of Trade, by virtue of the powers conferred upon them by the Salmon and Freshwater Fisheries Acts, 1861 to 1892, and otherwise, do, by this certificate, alter the limits of the said fishery district so that the limits of the said River Wye and of the fishery district of the said river shall include all such estuaries, rivers, streams, brooks, lakes, ponds, canals, dykes, cuts, drains, channels, watercourses and waters, and all such portions of the sea and sea-coast, as lie within the following limits, namely, a line commencing at the bench mark 25·8 placed on the sea-wall on the western bank of the mouth of Cone Pill, in the county of Gloucester, and thence drawn true east until it meets the boundary between the parishes of Woolaston and Aylburton in the said county, thence seawards along such boundary until it meets the line of ordinary low water on the western edge of Lydney Sands, thence in a south-westerly direction along the general line of ordinary low water on the western

edge of Lydney, Sheperdine, and Beacon Sands, successively, to the south-westernmost extremity of Beacon Sands, thence true south-east until it reaches the line of ordinary low water on the western edge of Oldbury Sands, thence in a south-westerly direction along the general line of ordinary low water on the western edge of Oldbury and Slimeroad Sands, successively, until it reaches the south-western extremity of Slimeroad Sands, thence straight towards the north-eastern extremity of the Lower Bench until it meets the deep-water line of the channel dividing the said Lower Bench from the Upper Bench, thence, successively, along the deep-water line of such channel and of the channel dividing Northwick Oaze and the English Stones from the Dun Sands until it meets the deep-water line of the main channel of the estuary of the River Severn or Bristol Channel, near the Dumpings, thence along such deep-water line through the Shoots until it meets a straight line drawn from the site of the Avon Battery, in the county of Gloucester, to the seaward extremity of the western bank of Collister Pill, in the county of Monmouth, thence along such line to the said western bank, thence straight to the parish church of Undy, thence along the natural watershed between the streams which flow into Magor Pill on the one hand and the streams which flow into Caldicot, Styne, West and Collister Pills on the other hand to the point in Wentwood at which such watershed meets the natural watershed of the River Wye, thence along such last-named watershed until it reaches the natural watershed between Cone Pill on the one hand and the rivers and streams flowing into the western side of the estuary of the River Severn or Bristol Channel south of Cone Pill on the other hand, thence along such watershed in a south-easterly direction until it reaches the Great Western Railway near Woolaston Station, in the county of Gloucester, and thence straight to the bench mark 25·8, where it began—the limits of the said river and fishery district being more particularly shown on a map marked S. 2388/1895, and deposited herewith with the clerk of the county council of Hereford.

And the said river and fishery district as hereby altered shall be for all purposes a river and fishery district within the meaning of the Salmon and Freshwater Fisheries Acts, 1861 to 1892.

Signed and sealed by order of the Board of Trade this ninth day of December 1895.

(Signed) A. D. BERRINGTON,
An Assistant Secretary to the
said Board.

LOAN EXHIBITION OF ENGLISH FURNITURE AND FIGURED SILKS OF THE 17TH AND 18TH CENTURIES.

A communication has been received from the Secretary of the Department of Science and Art, stating that the Lords of the Committee of Council on Education have directed the formation,

at the Bethnal Green Museum, of a loan collection of examples of English furniture and figured silks of the 17th and 18th centuries, which will be opened in April 1896, in the galleries recently occupied by the National portraits which are now removed to Trafalgar Square.

In order to ensure the formation of a thoroughly representative collection, the assistance has been invited of a number of gentlemen eminently qualified by their knowledge to advise with regard to these branches of industrial art, which still form, as they did in the past, the main handicrafts of the East of London.

The interest taken on the continent in English 18th century furniture is shown by the fact that commissions have recently come from Germany and Austria to inspect the examples of the works of Chippendale, Sheraton, and Hepplewhite in the South Kensington Museum, and to secure authentic specimens, or good copies, for the Commercial Museums of Strasburg and Vienna.

Recent changes of fashion have caused a demand for 18th century patterns in figured silks, the manufacture of which formerly gave employment to 60,000 weavers in the East of London, and the designs of the early days of George III. are now being revived or adapted. Possessors of dresses and costumes made of these silks will be invited to lend them for exhibition, and as 18th century trade pattern books—which attach to cut examples of the silks the names of weavers and merchants—will be included, comparisons between the designs in these samples and the patterns in the old costumes will probably lead to greater accuracy in dating the English figured silks already in the possession of the Museum.

The Loan Collection will remain open for six months; and, in addition, the Bethnal Green Museum will be enriched by the generous loan by Sir Wollaston Franks of his collection of European porcelain, and by the Chantry Bequest collection of pictures lent by the Royal Academy.

II.—IMPORTS OF HAY INTO THE UNITED KINGDOM.

With reference to previous notifications in the "Board of Trade Journal," the following statement of the quantity of hay imported into the United Kingdom from each country during the month of December and the twelve months ended 31st December 1895, together with the corresponding figures for 1894, has been prepared from particulars supplied to the Board of Trade by Her Majesty's Customs:—

Countries.	Month of December		Twelve Months ended 31st December	
	1895.	1894.	1895.	1894.
	Tons.	Tons.	Tons.	Tons.
Russia, North - - - -	—	—	298	27,625
" South - - - -	—	—	—	997
Sweden - - - -	29	—	31	—
Norway - - - -	543	36	1,601	2,969
Denmark - - - -	403	151	2,668	5,042
Germany - - - -	1,208	18	5,198	1,929
Holland - - - -	4,171	496	24,267	13,540
Belgium - - - -	942	52	6,236	3,887
France - - - -	1,879	376	16,379	6,206
Spain - - - -	—	—	—	364
Italy - - - -	—	—	—	125
Greece - - - -	—	—	—	25
Turkey, European - - -	—	—	—	476
" Asiatic - - - -	—	—	25	542
Tripoli - - - -	—	—	—	3
Tunis - - - -	—	—	—	275
Algeria - - - -	240	225	7,090	3,951
Cape of Good Hope - -	—	—	—	1
Victoria - - - -	—	—	10	3
New South Wales - - -	—	—	13	—
New Zealand - - - -	—	—	1	22
Canada - - - -	431	1,958	16,899	28,711
Newfoundland - - - -	—	—	6	23
United States: America -	424	4,369	36,176	152,423
Chile - - - -	—	—	217	4,070
Uruguay - - - -	—	—	2	—
Argentine Republic - -	77	27	1,678	805
Total - - - -	10,347	7,708	118,795	254,214

The total quantity of hay imported into the United Kingdom in 1893 amounted to 263,050 tons, and in 1894 to 254,214 tons.

III.—AUTHORISED GAS UNDERTAKINGS.

The usual Parliamentary Returns relating to authorised gas undertakings in the United Kingdom have recently been issued. The return relating to undertakings belonging to companies is for the year ended 31st December 1894, and that relating to the gas undertakings of local authorities is for the year ended 25th March 1895.

Particulars are given in the returns with respect to 627 undertakings, 199 of which belong to local authorities and 428 to companies. The number of undertakings belonging to companies is the same as in the previous year's return, while the number of those owned by local authorities has increased by seven.

The grand total of the two returns and the increase or decrease in the various items during the year are shown in the following table:—

TABLE showing the TOTAL of the LOCAL AUTHORITIES' and COMPANIES' RETURNS.

—	Number of Authorised Undertakings included in Return.	Amount Authorised.	Paid up and Borrowed.
COMPANIES' RETURN - - -	428	£ † 55,583,190	£ 44,798,877
LOCAL AUTHORITIES' RETURN - -	199	† 28,068,793	24,624,554
TOTAL - - - -	627	83,651,983	69,423,431
Increase during the year - -	7	1,150,335	1,768,224
Decrease during the year - - -	—	—	—

† Share and loan capital authorised.

‡ Loan authorised.

—	Receipts.	Expendi- ture.	Tons of Coal Carbonised.	Number of Cubic Feet of Gas Made.
COMPANIES' RETURN - - -	£ 13,006,219	£ 9,110,485	7,128,126	72,610,842,498
LOCAL AUTHORITIES' RETURN - -	6,402,046	4,681,035	4,191,175	41,684,838,142
TOTAL - - - -	19,408,265	13,791,520	11,319,301	114,295,680,640
Increase during the year - -	578,960	—	299,009	3,513,892,239
Decrease during the year - - -	—	303,698	—	—

	Number of Cubic Feet of Gas Sold.	Length of Gas Mains in Miles.	Number of Consumers.	Number of Public Lamps Lighted.
COMPANIES' RETURN - - -	66,977,906,792	14,557	1,266,943	295,787
LOCAL AUTHORITIES' RETURN - -	38,183,083,467	8,753	1,257,274	217,069
TOTAL - - -	105,160,990,259	23,310	2,524,217	512,856
Increase during the year - - -	3,274,619,129	484	72,632	5,630
Decrease during the year - - -	—	—	—	—

The above summary indicates the continued prosperity of the gas industry in the United Kingdom during the year 1894-5. The expenditure decreased 303,698*l.*, while the revenue increased 578,960*l.* At the same time there was a large increase in the number of tons of coal carbonised, the quantity of gas made and sold, and the number of consumers. The capital invested in the gas undertakings is now nearly 69½ millions—an increase during the year of over 1¼ millions.

The two following tables show the average receipts, expenditure, and gross profit per 1,000 cubic feet of gas sold by companies and local authorities respectively during the last five years :—

COMPANIES.

Year.	Receipts.	Expenditure.	Profit.
	<i>s.</i> <i>d.</i>	<i>s.</i> <i>d.</i>	<i>s.</i> <i>d.</i>
1890	3 9·21	2 10·54	0 10·67
1891	3 9·88	2 11·70	0 10·18
1892	3 9·63	2 10·32	0 11·31
1893	3 10·21	2 9·82	1 0·39
1894	3 10·60	2 8·64	1 1·96

LOCAL AUTHORITIES.

Year.	Receipts.	Expenditure.	Profit.
	<i>s.</i> <i>d.</i>	<i>s.</i> <i>d.</i>	<i>s.</i> <i>d.</i>
1890	3 5·31	2 7·06	0 10·25
1891	3 5·05	2 7·90	0 9·15
1892	3 3·81	2 7·29	0 8·52
1893	3 5·01	2 8·09	0 8·92
1894	3 4·24	2 5·42	0 10·82

The gas undertakings of local authorities and companies are, however, carried on under such obviously different conditions that the comparative value of these and other figures in the respective returns is somewhat affected.

The following table reveals some of the differences in the two classes of undertakings:—

	Average Number supplied by each Undertaking of		Average Number supplied per Mile of Main of		Average Annual Consumption of Gas by	
	(1.) Private Consumers.	(2.) Public Lamps.	(1.) Private Consumers.	(2.) Public Lamps.	(1.) Private Consumers.	(2.) Public Lamps.
Companies -	2,960	691	87	20	c. f. 48,803	c. f. 17,399
Local Authorities -	6,318	1,091	144	25	27,797	14,903

The quantity of gas made per ton of coal carbonised continues to slightly increase, but there is not sufficient information in the returns to show to what extent these figures are affected by the increasing use of materials other than coal in the manufacture and enrichment of gas. The figures for the last five years are subjoined:—

Year.	Average Number of Cubic Feet of Gas made per Ton of Coal Carbonised.	
	Companies.	Local Authorities.
1890	10,155	9,877
1891	10,034	9,098
1892	10,138	9,882
1893	10,157	9,871
1894	10,186	9,946

During the year the price of gas was reduced by about 79 companies and 38 local authorities, and was raised by 4 companies and 2 local authorities. In 1893 the price was reduced by about 37 companies and 21 local authorities, and increased by about 35 companies and 29 local authorities.

Companies' Return.—The total amount of share capital paid up on 31st December 1894 was 37,059,200*l.*, of which sum 8,352,043*l.* (including premiums amounting to 2,637,019*l.*) had been raised under the Auction Clauses.

One hundred and fifty-six companies paid a dividend of 10 per cent. on their original capital, and 44 paid dividends varying from 10 $\frac{1}{2}$ to 20 $\frac{1}{2}$ per cent., mostly under the operation of the sliding scale; 21 companies paid no dividend—two less than in the previous year. Of these 21, only five undertakings were worked at a loss, the aggregate loss being 264*l.*

The lowest price charged for gas under the operation of the sliding scale was 1*s.* 9*d.* per 1,000 cubic feet at Plymouth, the company paying a dividend of 12 $\frac{1}{4}$ per cent. on their ordinary capital. At Newcastle the price was 1*s.* 10*d.* with a discount of 10 per cent., and the dividend paid under the sliding scale was 9 $\frac{3}{4}$ per cent.

The highest price charged was 6s. 8d. at Menai Bridge, where the undertaking belongs to a private proprietor.

The highest dividends paid under the sliding scale were $16\frac{1}{2}$ per cent. by the Harrogate Gas Company, $15\frac{1}{2}$ per cent. by the South Metropolitan Gas Company, and 14 per cent. by the Wandsworth and Putney, Croydon and Ashton Gas Companies. The Kingston-upon-Hull Gas Company paid $20\frac{1}{8}$ per cent., but not under the sliding scale.

The following table of dividends paid by the companies upon their original capital shows an increase in the average rate of dividend from 7l. 18s. 1d. to 8l. 2s. per cent. ;—

Dividend paid on Original Capital.	No. of Companies.				
	1890.	1891.	1892.	1893.	1894.
No dividend	10	15	18	23	21
$\frac{1}{2}$ per cent.	—	—	—	1	—
1	2	1	2	1	1
$1\frac{1}{4}$	—	—	—	—	2
$1\frac{1}{2}$	—	1	—	—	—
$1\frac{3}{4}$	1	—	—	—	—
2	4	2	4	2	4
$2\frac{1}{4}$	—	2	—	1	—
$2\frac{1}{2}$	3	2	—	3	3
$2\frac{3}{4}$	—	—	—	1	—
3	4	4	6	6	1
$3\frac{1}{4}$	—	—	1	—	—
$3\frac{1}{2}$	5	4	1	4	2
$3\frac{3}{4}$	—	1	—	3	—
4	3	8	7	15	17
$4\frac{1}{4}$	1	—	—	—	—
$4\frac{1}{2}$	4	2	8	4	4
$4\frac{3}{4}$	—	1	—	1	—
5	29	32	30	27	32
$5\frac{1}{4}$	—	2	—	—	2
$5\frac{1}{2}$	3	3	2	4	3
$5\frac{3}{4}$	—	—	—	—	1
6	17	11	15	18	10
$6\frac{1}{4}$	—	1	—	—	—
$6\frac{1}{2}$	1	2	1	1	2
$6\frac{3}{4}$	—	—	—	1	—
6	2	1	3	4	2
$6\frac{1}{2}$	—	—	—	—	1
$6\frac{3}{4}$	—	—	1	—	—
7	19	19	18	15	17
$7\frac{1}{2}$	—	—	—	1	—
$7\frac{1}{4}$	—	—	2	1	3
$7\frac{1}{2}$	25	22	24	21	17
$7\frac{3}{4}$	1	—	1	2	—
8	40	37	35	30	35
$8\frac{1}{4}$	1	1	1	2	1
$8\frac{1}{2}$	1	—	1	2	1
$8\frac{3}{4}$	4	7	7	9	5
$8\frac{1}{2}$	2	3	1	5	3
$8\frac{3}{4}$	—	—	1	—	—
9	13	13	15	19	22
$9\frac{1}{4}$	2	3	3	—	2
$9\frac{1}{2}$	2	3	3	5	2

Dividend paid on Original Capital.	No. of Companies.				
	1890.	1891.	1892.	1893.	1894.
9 $\frac{3}{4}$ per cent. - - - - -	—	—	1	3	2
10 " - - - - -	155	158	153	142	156
10 $\frac{1}{8}$ " - - - - -	—	—	—	—	1
10 $\frac{1}{4}$ " - - - - -	—	—	—	1	—
10 $\frac{1}{2}$ " - - - - -	3	4	5	3	2
10 $\frac{3}{4}$ " - - - - -	—	—	1	—	—
10 " - - - - -	—	—	—	1	—
10 $\frac{3}{4}$ " - - - - -	3	2	2	3	2
11 " - - - - -	11	7	6	4	7
11 $\frac{1}{20}$ " - - - - -	—	—	—	1	1
11 $\frac{1}{4}$ " - - - - -	1	1	1	1	2
11 $\frac{1}{2}$ " - - - - -	—	—	—	1	—
11 $\frac{1}{5}$ " - - - - -	7	9	8	8	9
11 $\frac{3}{4}$ " - - - - -	1	1	—	—	—
11 $\frac{7}{8}$ " - - - - -	—	—	1	—	—
12 " - - - - -	4	4	5	5	5
12 $\frac{1}{4}$ " - - - - -	2	2	2	2	1
12 $\frac{1}{2}$ " - - - - -	2	1	1	1	2
12 $\frac{3}{4}$ " - - - - -	1	—	—	—	—
12 $\frac{5}{8}$ " - - - - -	1	2	3	1	1
13 " - - - - -	5	4	3	3	2
13 $\frac{1}{4}$ " - - - - -	—	—	—	—	1
13 $\frac{3}{5}$ " - - - - -	—	1	—	—	—
13 $\frac{1}{2}$ " - - - - -	—	—	1	—	1
13 $\frac{3}{4}$ " - - - - -	2	2	1	1	1
14 " - - - - -	3	3	3	3	3
14 $\frac{3}{5}$ " - - - - -	1	—	—	—	—
15 " - - - - -	1	1	—	—	—
15 $\frac{1}{2}$ " - - - - -	—	1	1	1	1
16 " - - - - -	1	1	1	1	1
19 " - - - - -	—	1	—	—	—
20 $\frac{1}{8}$ " - - - - -	1	1	1	1	1
27 " - - - - -	—	—	1	—	—
32 $\frac{1}{2}$ " - - - - -	1	1	—	—	—
Gas not yet supplied - - -	4	5	4	3	2
Private undertakings - - -	7	7	7	6	8
Total - - -	416	422	429	428	428
Average dividend of 405 com- panies, 1890 - - - - -	8l. 9s. 4 $\frac{3}{4}$ d. per cent. 8l. 7s. 10 $\frac{3}{4}$ d. " 8l. 3s. 11 $\frac{3}{4}$ d. " 7l. 18s. 1d. " 8l. 2s. 0d. "				
Average dividend of 410 com- panies, 1891 - - - - -					
Average dividend of 418 com- panies, 1892 - - - - -					
Average dividend of 419 com- panies, 1893 - - - - -					
Average dividend of 418 com- panies, 1894 - - - - -					

Local Authorities' Return.—The lowest prices per 1,000 cubic feet of gas were 1s. 10d. and 2s., discount 2d., charged by the Widnes Corporation.

The highest price was 7s. per 1,000 cubic feet, charged by the Tain Gas Commissioners.

In 5 cases the expenditure exceeded the receipts, as compared with 10 cases in the previous year; and in 35 cases the

excess of receipts over expenditure was insufficient to provide for the payment of interest, &c., as compared with 54 similar cases in the previous year.

The eight local authorities shown in the following table made a profit of over 20,000*l.* after payment of interest, &c. Eight other local authorities, viz., Stockport, Southport, Bradford, Birkenhead, Oldham, Blackburn, Wigan, and Rochdale, made a profit of over 10,000*l.* after payment of interest, &c.

Town or City.	Net Profit.	Price charged to Private Consumers.	How Profit was applied.
Manchester -	£ 90,438	2 <i>s.</i> 6 <i>d.</i>	54,044 <i>l.</i> in aid of rates, and 36,394 <i>l.</i> to contingent fund.
Bolton - -	38,251	3 <i>s.</i> 2 <i>d.</i> , discount 6 <i>d.</i>	14,773 <i>l.</i> to reserve fund; 23,478 <i>l.</i> in aid of rates.
Leeds - -	33,718	2 <i>s.</i> 4 <i>d.</i> , discount 2½ and 5 %	31,188 <i>l.</i> to previous deficiencies; 2,530 <i>l.</i> to reserve fund.
Nottingham -	30,842	2 <i>s.</i> 2 <i>d.</i> to 2 <i>s.</i> 6 <i>d.</i>	Not yet applied.
Birmingham -	30,448	2 <i>s.</i> 3 <i>d.</i> to 2 <i>s.</i> 7 <i>d.</i> , discount 5 %	To city improvement rate.
Salford - -	29,513	2 <i>s.</i> 3 <i>d.</i> and 2 <i>s.</i> 4 <i>d.</i>	6,593 <i>l.</i> to depreciation funds; 22,920 <i>l.</i> to relief of rates.
Leicester -	26,898	2 <i>s.</i> 4 <i>d.</i>	To general district rate.
Belfast -	24,308	2 <i>s.</i> 9 <i>d.</i> , discount 2½ to 20 %	To capital account, &c.

IV.—AGRICULTURAL RETURNS OF GREAT BRITAIN, 1895.

The following preliminary statement, showing the estimated total production of wheat, barley, and oats in the years 1895 and 1894, with the acreage and estimated average yield per statute acre in Great Britain, was issued by the Board of Agriculture on the 13th December last :—

WHEAT.

	Estimated Total Produce.		Acreage.		Estimated Yield per Acre.		Average of the Ten Years 1885-94.
	1895.	1894.	1895.	1894.	1895.	1894.	
England - -	<i>Bushels.</i> 35,120,459	<i>Bushels.</i> 56,087,603	<i>Acres.</i> 1,339,806	<i>Acres.</i> 1,826,626	<i>Bushels.</i> 26·21	<i>Bushels.</i> 30·71	<i>Bushels.</i> 29·35
Wales - -	951,505	1,420,082	44,036	56,470	21·61	25·15	23·34
Scotland - -	1,104,293	1,665,116	33,641	44,866	32·83	37·11	35·82
Great Britain -	37,176,257	59,172,801	1,417,483	1,927,962	26·23	30·69	29·32

BARLEY.

	Estimated Total Produce.		Acreage.		Estimated Yield per Acre.		Average of the Ten Years 1885-94.
	1895.	1894.	1895.	1894.	1895.	1894.	
England - -	<i>Bushels.</i> 58,092,197	<i>Bushels.</i> 61,194,073	<i>Acres.</i> 1,837,550	<i>Acres.</i> 1,766,142	<i>Bushels.</i> 31·61	<i>Bushels.</i> 34·65	<i>Bushels.</i> 33·07
Wales - -	2,996,676	3,347,992	111,886	111,572	26·78	30·01	28·03
Scotland - -	7,562,072	7,753,001	216,543	218,057	34·92	35·55	35·26
Great Britain -	68,650,945	72,295,066	2,166,279	2,095,771	31·69	34·50	33·02

OATS.

	Estimated Total Produce.		Acreage.		Estimated Yield per Acre.		Average of the Ten Years 1885-94.
	1895.	1894.	1895.	1894.	1895.	1894.	
England - -	<i>Bushels.</i> 78,645,010	<i>Bushels.</i> 88,289,392	<i>Acres.</i> 2,045,477	<i>Acres.</i> 1,978,312	<i>Bushels.</i> 38·45	<i>Bushels.</i> 44·63	<i>Bushels.</i> 40·58
Wales - -	7,654,066	9,012,652	242,198	250,866	31·60	35·93	32·53
Scotland - -	35,849,552	38,160,887	1,008,388	1,024,223	35·55	37·26	35·60
Great Britain -	122,148,628	135,462,931	3,296,063	3,253,401	37·06	41·64	38·21

V.—REGULATIONS FOR THE SALE OF MARGARINE IN VARIOUS COUNTRIES.

The German Official "Reichs-Anzeiger," in its issue of November 22nd last, published the provisions of a Bill, laid before the Reichstag, for the regulation of the sale of butter, cheese, lard, and substances produced in imitation of these. With reference to this new Bill, the "Reichs-Anzeiger" published some interesting details with regard to similar regulations in other countries.

In *Denmark* the law originally in force on this subject was that of April 1, 1885; but after a short time this law was replaced by a considerably more stringent one—that of April 5, 1888. This in turn was replaced by the law of April 1, 1891, which is still in force. Its provisions are as follows:—Every person intending to manufacture or sell margarine has to inform the police officials of such intention. Margarine may only be sold or kept in vessels which are specially marked and easily distinguishable from ordinary butter-casks. Places where margarine is sold must be so inscribed, and places for the sale or production of margarine must be separated from those where butter is sold or manufactured. Margarine may contain at most 50 per cent. of butter-fat, and is not to be of a deeper yellow colour than is indicated on a scheme of colours published by the Minister of the Interior. Oleomargarine and margarine cheese may only be bought or sold when specially packed in receptacles distinctly marked with the designation of the goods. In contract notes, bills of lading, accounts, and the like, which relate to the delivery of margarine, oleomargarine, and margarine cheese, these goods must be designated by their proper names. Special officials are entrusted with the duty of seeing that the provisions of the law are complied with, and these officers have access to all places where margarine, oleomargarine, or margarine cheese is manufactured, kept, or offered for sale; they have also the right to take samples and inspect the books of the firm. The Minister of the Interior is further empowered to prohibit, if necessary, the exportation of margarine and margarine cheese from Denmark.

In *Belgium* the trade in artificial butter was regulated by a Royal Decree of 10th December 1890, the provisions of which were made considerably more stringent by a Royal Decree of 11th March 1895. In virtue of the latter, only such quantities of milk or cream as are absolutely indispensable may be used in the manufacture of margarine, and in no case may the latter contain more than 5 per cent. of butter-fat or show a deeper shade of yellow than is indicated on a table of colours. The regulations also prescribe for the marking of places where margarine is sold, the vessels

or packages in which it is preserved, offered for sale, or transported, for the use of the cube-shape, in cases where the margarine is manufactured in definite shapes, and for the designation of the goods as "margarine" in accounts, &c. It is also forbidden to preserve or offer for sale margarine and natural butter in the same room.

In *Sweden* the trade in margarine and artificial butter was first regulated by a Royal Decree of October 2, 1885, the provisions of which were replaced by a decree of considerably wider scope, dated October 11, 1889, which, as in the case of Denmark and Belgium, provides that shops, vessels, and packages must bear the inscription "margarine," and that this word is expressly to be used in accounts, bills of lading, &c. Manufacturers of margarine must give notice when commencing operations, and the manufacture is controlled by a service of inspectors, who have the right of access to all parts of the factories at any time, and whose duty it is to see that only pure and innocuous ingredients are used in the manufacture of margarine.

In *France* the law of March 14, 1887, is in force, according to which artificial butter may only be traded in under the names "margarine," "oleomargarine," or "graisse alimentaire." This law, however, has proved to be insufficient to suppress frauds in the butter trade, and it is the intention of the Government to introduce more stringent provisions after the model of those in force in Denmark. A bill with this object, providing, amongst other things, for a regular State control of margarine factories, was laid before the Chamber of Deputies by the Government on July 20, 1894.

The trade in margarine has also lately been regulated for the first time in *Italy*, *Holland*, *Portugal*, and *Russia*.

According to the Italian law of July 19, 1894, the inscription "burro artificiale" or "margarina" must be stamped on each piece of artificial butter; similarly vessels and packages containing such butter must be plainly so marked, and the exterior of shops where margarine is sold must bear an inscription to that effect. The composition of artificial butter must be stated in accounts, bills of lading, &c., as well as on receptacles and packages. It is prohibited to colour margarine in order to make it resemble butter.

The Dutch law of June 23, 1889, provides that the wrapping, or where there is none the margarine itself, must be marked with the word "margarine" or "surrogaat," and empowers the police officials to visit at all times shops where butter is sold for the purpose of taking samples.

In *Portugal*, under the provisions of the law of April 12, 1892, a tax of 80 reis is levied on every kilo. of artificial butter manufactured, whilst foreign artificial butter is liable to an import duty of 400 reis per kilo., and oleomargarine to one of 250 reis.

In *Russia* the law of 8th (20th) April 1891 prohibits the colouring of artificial butter to make it resemble butter, as well as the mixing of the two; butter and margarine may not be sold

in the same sale-room. Vessels containing margarine, as well as shops where it is sold, must be so inscribed. The importation of margarine from abroad is not allowed, and in *Finland* the manufacture is entirely prohibited.

In *Norway* the legislation existing before the German law of July 12, 1887, remains unaltered. The Norwegian law of June 22, 1886, provides that artificial butter may only be kept in vessels bearing the word "margarine," and that in every shop where it is sold an inscription to that effect must be put up.

The manufacture and sale of oleomargarine and artificial butters in the United States are regulated by an Act passed in 1886, the principal sections of which are as follows:—

Sec. 1. That for the purposes of this Act the word "butter" shall be understood to mean the food product usually known as butter, and which is made exclusively from milk or cream, or both, with or without common salt, and with or without additional colouring matter.

Sec. 2. That for the purposes of this Act certain manufactured substances, certain extracts, and certain mixtures and compounds, including such mixtures and compounds with butter, shall be known and designated as "oleomargarine," namely: All substances heretofore known as oleomargarine, oleo, oleomargarine-oil, butterine, lardine, suine, and neutral; all mixtures and compounds of oleomargarine, oleo, oleomargarine-oil, butterine, lardine, suine, and neutral; all lard extracts and tallow extracts; and all mixtures and compounds of tallow, beef-fat, suet, lard, lard-oil, vegetable-oil, annatto, and other colouring matter, intestinal fat, and offal fat made in imitation or semblance of butter, or when so made calculated or intended to be sold as butter or for butter.

Sec. 3. That special taxes are imposed as follows:—

Manufacturers of oleomargarine shall pay 600 dols. Every person who manufactures oleomargarine for sale shall be deemed a manufacturer of oleomargarine.

Wholesale dealers in oleomargarine shall pay 480 dols. Every person who sells or offers for sale oleomargarine in the original manufacturer's packages shall be deemed a wholesale dealer in oleomargarine.

Sec. 6. That all oleomargarine shall be packed by the manufacturer thereof in firkins, tubs, or other wooden packages not before used for that purpose, each containing not less than ten pounds, and marked, stamped, and branded as the Commissioner of Internal Revenue, with the approval of the Secretary of the Treasury, shall prescribe; and all sales made by manufacturers of oleomargarine and wholesale dealers in oleomargarine shall be in original stamped packages. Retail dealers in oleomargarine must sell only from original stamped packages, in quantities not exceeding ten pounds, and shall pack the oleomargarine sold by them in suitable wooden or paper packages, which shall be marked and branded as the Commissioner of Internal Revenue, with the approval of the Secretary of the Treasury, shall prescribe. Every

person who knowingly sells, or offers for sale, or delivers, or offers to deliver any oleomargarine in any other form than in new wooden or paper packages as above described, or who packs in any package any oleomargarine in any manner contrary to law, or who falsely brands any package or affixes a stamp on any package denoting a less amount of tax than that required by law, shall be fined for each offence not more than 1,000 dols. and be imprisoned not more than two years.

Sec. 7. That every manufacturer of oleomargarine shall securely affix, by pasting on each package containing oleomargarine manufactured by him, a label on which shall be printed, besides the number of the manufactory and the district and State in which it is situated, these words: "Notice.—The manufacturer of the oleomargarine herein contained has complied with all the requirements of law. Every person is cautioned not to use either this package again or the stamp thereon again, nor to remove the contents of this package without destroying said stamp, under the penalty provided by law in such cases." Every manufacturer of oleomargarine who neglects to affix such label to any package containing oleomargarine made by him, or sold or offered for sale by or for him, and every person who removes any such label so affixed from any such package, shall be fined 50 dols. for each package in respect of which such offence be committed.

Sec. 10. That all oleomargarine imported from foreign countries shall, in addition to any import duty imposed on the same, pay an Internal Revenue tax of 15 cents per pound, such tax to be represented by coupon stamps, as in the case of oleomargarine manufactured in the United States. The stamps shall be fixed and cancelled by the owner or importer of the oleomargarine while it is in the custody of the proper Custom-house officers; and the oleomargarine shall not pass out of the custody of said officers until the stamps have been so affixed and cancelled, but shall be put up in wooden packages, each containing not less than 10 pounds, as prescribed in this Act for oleomargarine manufactured in the United States, before the stamps are affixed; and the owner or importer of such oleomargarine shall be liable to all the penal provisions of this Act prescribed for manufacturers of oleomargarine manufactured in the United States. Whenever it is necessary to take any oleomargarine so imported to any place other than the public stores of the United States for the purpose of affixing and cancelling such stamps, the collector of Customs of the port where such oleomargarine is entered shall designate a bonded warehouse to which it shall be taken, under the control of such Customs officer as such collector may direct; and every officer of Customs who permits any such oleomargarine to pass out of his custody or control without compliance by the owner or importer thereof with the provisions of this section relating thereto, shall be guilty of a misdemeanour, and shall be fined not less than 1,000 dols. nor more than 5,000 dols., and imprisoned not less than six months nor more than

three years. Every person who sells or offers for sale any imported oleomargarine, or oleomargarine purporting or claimed to have been imported, not put up in packages and stamped as provided by this Act, shall be fined not less than 500 dols. nor more than 5,000 dols., and be imprisoned not less than six months nor more than two years.

Sec. 11. That every person who knowingly purchases or receives for sale any oleomargarine which has not been branded or stamped according to law shall be liable to a penalty of 50 dols. for each such offence.

Sec. 13. That whenever any stamped package containing oleomargarine is emptied, it shall be the duty of the person in whose hands the same is to destroy utterly the stamps thereon; and any person who wilfully neglects or refuses so to do shall for each such offence be fined not exceeding 50 dols., and imprisoned not less than 10 days nor more than six months. And any person who fraudulently gives away or accepts from another, or who sells, buys, or uses for packing oleomargarine, any such stamped package shall for each such offence be fined not exceeding 100 dols., and be imprisoned not more than one year. Any revenue officer may destroy any emptied oleomargarine package upon which the tax-paid stamp is found.

Sec. 15. That all packages of oleomargarine subject to tax under this Act that shall be found without stamps or marks as herein provided, and all oleomargarine intended for human consumption which contains ingredients adjudged, as hereinbefore provided, to be deleterious to the public health shall be forfeited to the United States. Any person who shall wilfully remove or deface the stamps, marks, or brands on packages containing oleomargarine taxed as provided herein shall be guilty of a misdemeanour, and shall be punished by a fine of not less than 100 dols. nor more than 2,000 dols., and by imprisonment for not less than 30 days nor more than six months.

By an Act of 1890 the regulations have been revised, certain new regulations having been promulgated by the Commissioner of Internal Revenue for the better carrying into effect of the Act of 1886, chiefly in regard to the keeping of books and rendering of returns and to the time at which the special taxes are due.

In October 1895, further regulations as to the marking, branding, labelling, &c., of tins or other inner package devices of manufacturers' original packages of oleomargarine for export were issued by the Commissioner of Internal Revenue, by which it is enacted that no trade mark, label, brand, picture-illustration, or other advertising or descriptive device will be permitted to be used upon the tins or other inner package devices of manufacturers' original packages of oleomargarine for export which in anywise conceal the Government brand "oleomargarine," required under Section 16 of the Act of August 2, 1886, to be branded upon *all* packages of oleomargarine for export.

No such trade mark, label, brand, picture-illustration, or other advertising or descriptive device shall be placed upon the tins or

other inner package devices in such juxtaposition to the Government brand "oleomargarine" as to overshadow or confuse that notice as to the nature of the contents of the tins or other inner package devices. Nor shall it be permitted to use upon such tins or other inner package devices any trade mark, label, brand, picture-illustration, or other advertising or descriptive device, which in anywise indicates or is calculated to induce the belief that the contents are anything else than oleomargarine.

The Government brand "oleomargarine" must stand conspicuously by itself.

No trade mark, label, brand, picture-illustration, or other advertising or descriptive device prohibited as above for use upon the tins or other inner package devices, shall in any other form be inclosed in the manufacturers' original package containing the tins or other inner package devices.

The use of the word "butterine" in branding tins or other inner package devices of oleomargarine is prohibited by law. Section 2 of the Act of August 2, 1886, prescribes that "butterine" shall be known and *designated* as "oleomargine."

Under the foregoing instructions it will be seen that the use of any trade mark, label, brand, picture-illustration, or other advertising or descriptive device, representing a cow or dairy farm, or in any other form indicating the contents of the tins or other inner package devices to be a product of the dairy, or calculated to induce the belief that it is such product, is inadmissible.

VI.—INDUSTRIES OF THE PIRÆUS.*

From the following statement concerning the industries of the Piræus, it will be seen that Greece is now making some efforts to render herself independent of foreign imported manufactures.

At the Piræus there are seven weaving and spinning factories, the annual production of which is—coloured cotton tissues, 2,150,000 metres; unbleached cotton tissues, 1,650,000 metres; white cotton yarn, 950,000 metres; coloured cotton yarn, 350,000 metres. This industry works 2,050 looms and 51,000 spindles, and employs 3,400 workers (1,400 men, 2,000 women). A workman's wage is from 2 to 5 drachmas (1s. 7 $\frac{3}{4}$ d.—4s. at par value) per day, and a woman receives from 1 to 3 drachmas (9 $\frac{1}{2}$ d. to 2s. 4 $\frac{3}{4}$ d.).

There is only one manufactory of cloth at the Piræus, with an annual output of about 240,000 metres of stuff, of a value of 1,650,000 francs (72,000*l.*), and employing 150 men and 280 women, at work on 50 looms.

The products of all these factories are for the most part for home consumption; in 1894, however, Turkey imported cotton yarn and tissues to the value of 380,600 francs (15,000*l.*).

Fourteen steam flour mills are established at the Piræus, with an average daily production of 415,000 kilogs of flour. The motive power employed is 1,010 horse-power, and the mills give work to 350 men, the grain being imported chiefly from Russia and Turkey, though Thessaly furnishes a certain amount.

Five large distilleries and over 40 minor stills manufacture spirits and liqueurs. This industry has greatly extended since 1893, on account of the failure of the currant market in Europe and the consequent plethora of that product in Greece. Openings have been found in Turkey, Egypt, Roumania, and Servia for the sale of brandy, and in 1894 quantities to the value of 1,700,000 francs (68,000*l.*) were exported. These spirits enter much into competition with French brandies, and are even tending to replace them among the middle classes of the above-named countries.

In addition to these distilleries, five breweries turn out a common kind of beer, for local consumption only.

Five ironworks and dockyards for the construction and repairing of vessels, and for the making of machines, boilers, objects in iron and copper, and hydraulic presses are in activity, employing 700 workmen and a force of 400 horse-power, and doing an annual business of 3,500,000 francs (140,000*l.*). Some of their products, such as steam engines, boilers, and agricultural instruments, are exported to Turkey.

The remaining principal industries are two tanneries, 20 chair manufactories, eight small copper foundries, one nail factory, using German iron wire, two steam saw-mills, one dynamite factory, turning out about 400 tons per annum; besides candle, match, straw hat, brush factories, &c.

* Report of the French Consul at the Piræus.

VII.—INTERSTATE TARIFFS IN MEXICO.*

The amendments to the Mexican constitution proposed by Finance Minister Limantour, having for their object the abolition of the interstate tariff duties known as the *alcabala*, now before the Mexican Congress, have been strongly urged by President Diaz. The text of the amendments is as follows:—

First. Section 3 of Article 3 of the Federal Constitution is hereby amended and enlarged in the following form:—

The States shall not—

- (3.) Coin money, issue paper money, stamps, or stamped paper.
- (4.) Tax passage of persons or effects through their territory.
- (5.) Prohibit or tax, directly or indirectly, the introduction or exit from their territory of domestic or foreign merchandise.

(6.) Impose on the circulation or consumption of domestic or foreign merchandise duties or taxes of a nature to be collected by internal Custom-houses, or to require the opening or examination of packages, or to necessitate the transportation of goods under fiscal documents.

(7.) Issue or maintain laws or fiscal enactments implying distinction as to taxation or requirements, based on the places of origin of domestic or foreign merchandise, and whether the distinction be enforced by way of protection to local production or by way of favour to products coming from a given locality.

Second. Article 124 of the Constitution is amended in the following form:—

“It is the exclusive privilege of the powers of the Union to tax merchandise on its importation or exportation, or passage in transit through the national territory, as also to regulate at all times, and even to prohibit for reasons of public safety or policy, the circulation in the interior of the Republic of any kind of merchandise, whatever may be the place of its origin.”

These reforms shall come into force on the 1st day of July, 1896.

These proposed amendments seek the overthrow of an evil centuries old, fixed in popular habit and prejudice, and backed by powerful official interests.

If pushed (and there is reason to believe they will be), they may provoke a struggle which will determine Mexico's place in modern progress and be decisive of the national supremacy. If passed and enforced, trade with Mexico will be greatly increased.

Mexico is a tariff-taxed country. Indeed, the extent to which the tariff system prevails and the time it has endured ought to be a complete and convincing test of its merits. Intro-

* Based on a recent report of the Consul-General for the United States at Nuevo Laredo (Mexico).

duced in 1578, it is about the only political institution which has survived in all its crudity the country's innumerable crises.

Besides a national tariff, every one of the 27 States, the Federal District, and Territories, and almost every municipality has each its separate tariff, on the principle, perhaps, that what is good for the whole ought to be good for each of its parts. These different tariffs are of all kinds and conditions. There are the high, the low, the tariff for revenue only, for revenue with incidental protection, the out-and-out tariff for protection of home industries, and others whose character is unclassified.

The foreign merchant learns them all by hard experience. At every national port of entry he must stand and deliver to the nation, at every state boundary he must pause to pay tribute to the state, and each municipality he enters levies new exactions. In this latter respect, however, he is no worse off than the native Mexican. He, too, encounters the tariff if he seeks a market beyond the narrowest local limits.

In fact, lesser Mexican communities have frequently sought this ready relief from self-taxation by setting themselves up, for tariff purposes, as distinct territories (*distintos suelos*), and there were at one time thus established 276 tariff districts. Each of those districts reasoned that it could make the other 275 pay its taxes, and so set its *alcabala*, or tariff tax, accordingly.

But no people can profit by mutual imposition. The 276 tariff districts preyed on each other with obvious results. Traffic and revenue shrank, industry was dwarfed, enterprise crushed. The evil grew so manifest that there was a general outcry against it, and leading statesmen urged its abolition. Then began the movement which is now culminating in the proposed constitutional amendments.

As early as 1824 there was adopted a national constitution which guaranteed liberty of commerce. This, it was thought, would put an end to the *alcabala*. It did not; the system was defiantly maintained. On October 14, 1846, Congress, by formal enactment, declared the *alcabala* abolished, but threats of revolution forced its re-establishment. On June 14, 1848, it was suppressed in the Federal district and territories, but in a few short months Santa Anna restored it. On November 24, 1855, another general law was passed prohibiting it, but the law was repealed before the date set for its enforcement. The new constitution of 1857 reasserted the exclusive right of the Federal Government to regulate foreign and domestic commerce. With the empowering sanction of this fundamental law, an Act was passed providing that from and after January 1, 1862, the *alcabala* should for ever cease. But on April 14, 1862, the Act was repealed, the *alcabala* re-established, and the duties under it doubled. In 1869 and 1877 other measures were promulgated, only to prove equally abortive. The *alcabala* continued.

In the belief that the error of all previous legislation had been the insufficient time given to the several States to adjust their

fiscal systems, it was enacted in 1881 that from and after December 1, 1884, there should be no *alcabala*. This was subsequently extended to December 1, 1886, by which date both the law and the spirit of reform which created it seem to have died of inanition. In 1890, a national convention was held to consider the question. It consisted of delegates from all the States, duly empowered. Their deliberations lasted until April 1, 1891, when it was solemnly agreed that the *alcabala* was an evil and that the whole system of interior Custom-houses should be done away with. But nothing came of it; the state and local tariffs were levied and collected as usual.

It is claimed that in a few of the States the *alcabala* had been practically abandoned. But the first State to which the credit is given is the State of Tamaulipas, and so far from giving up the system or even advancing in the line of reform, Tamaulipas has retrograded, for, while nominally renouncing the *alcabala* duties for the State itself, the tax, with all its objectionable features, has been transferred to the several municipalities.

For example, in the schedule of the *alcabala* duties of the municipality of Nuevo Laredo in the State of Tamaulipas there are 51 separate classes of articles subject to a tariff duty ranging from 6 cents per yard on cassimeres to 5 dols. per 1,000 on cigars; and at the end of the classified list (so that nothing may escape), there is an omnibus clause to the effect that all articles not specified shall pay 25 cents. per 100 kilograms.

Nuevo Leon, which is another State named as free from the *alcabala*, has a system as oppressive as Tamaulipas, and the Federal district itself, the seat of government, where, if anywhere, reform ought to be manifest, leads all the rest in the severity of its tariff taxes, collecting annually from a single article (pulque) over 400,000 dols.

Despite the consensus of opinion of the best men of all parties, despite the determined efforts of 70 years, the *alcabala* continues to be the main source of State and municipal revenue.

With so many different tariff schedules, utterly devoid of uniformity, each shaped by special local circumstances, each changing from time to time with the whim of local law-makers, the trials of the merchant who attempts any extended business in Mexico may be imagined. Every State legislature, in annual session, is prone to revise the *alcabala* of the State. Every municipal council, at its weekly session, may propose changes in the *alcabala* of the district.

The ultimate passage of the proposed amendments, and their subsequent enforcement, seems to be certain. The fate of similar movements in the past is no criterion now; the Mexico of to-day is a new creation. The *alcabala* belongs to the period of the ox-cart and the pronunciamiento; it is out of place with steam, electricity, and a virile national authority.

VIII.—THE IMPORTATION OF MANUFACTURED ARTICLES INTO THE ARGENTINE REPUBLIC.

Manufactures of all kinds and raw materials to be worked up form the most considerable item in the import trade of Argentina, says the "Handels-Museum" for November 28, 1895. In the years 1887 to 1891 these articles were imported to the average annual value of 21,180,000 dollars gold, representing 18 per cent. of the total imports; in 1892 their value was 30,610,000 dols., or 34 per cent. of the total; in 1893, 27,810,000 dols. gold, or 29 per cent.; and in 1894, 25,860,000 dols. gold, equivalent to 28 per cent. of the total of the imports. The explanation of the low percentage for the five years 1887 to 1891 is that in consequence of the large imports of materials for railway and other construction, the total importation during this period exceeded that of the following years by more than 25 per cent. On the other hand, however, the imports of made-up articles, &c., in 1891 was only about half the usual figure, so that the average for the five years was only 21,180,000 dols. gold.

The following detailed examination of these statistics is taken from the "Buenos Aires Handels-Zeitung":—

The statistics show that there was a very considerable increase in the imports of cotton and woollen yarn in 1894 as compared with 1893, but, on the other hand, a similar decrease in raw cotton is noted, the imports for 1892 being about 150,000 kilogs., of a value of 80,000 dols. gold, and for 1894 about 36,000 kilogs. of a value of 9,000 dols. gold. A decline is also noticeable in the imports of common sewing thread, whilst in the case of silk thread there is a steady and not inconsiderable increase; in fact, this is the only article of French origin which does not show a falling off. The decreased imports of the different kinds of binding twine and the increase in pita hemp point to an increased manufacture of twine in the interior.

The importation of silk ribbons has fallen off, whilst the other classes show an increase, especially in goods coming from Germany and England.

Packing-cloth shows an increase of about 25 per cent. since 1892, which is distributed amongst all the exporting countries except Belgium. Imports of canvas are fairly stationary.

Tissues of pure cotton show a decrease of about 40 per cent. for 1894 as compared with 1892; this results from the fact that the imports of this article were extremely small in 1891, amounting to less than \$6,000,000, and consequently unusually large shipments were made in 1892.

Similarly in 1891, the imports of pure woollen cloth were below the average, but these reached their highest point in 1893, in comparison with which 1894 shows a decrease. Although tissues of pure cotton have fallen off, there is a considerable increase in tissues of silk and wool mixed.

There was also a decrease in 1894 as compared with 1893 in the importation of lace, edgings and tulles, especially from France and England. Imports of plush, covers of all kinds, and small wares show an increase. The augmentation in the imports of tow and rope wares, especially from England, is very marked. Imports of wicks for lamps and candles have fallen off, whilst raw materials for these have risen from 32,300 kilogs. in 1893, to 100,800 kilogs. in 1894.

The following table shows the share of the various countries in the import trade of Argentina in manufactured articles:—

	1894.	1893.	1892.	1894.	1893.	1892.
	Thousand Pesos Gold			Per Cent.		
England - - -	13,560	14,507	18,222	52·4	52·2	59·5
Germany - - -	3,260	3,201	3,444	12·6	11·5	11·2
France - - -	2,985	3,117	2,532	11·5	11·2	8·3
Belgium - - -	1,954	2,579	2,043	7·6	9·3	6·7
United States - - -	735	1,273	956	2·8	4·6	3·1
Italy - - -	1,832	1,634	1,638	7·1	5·9	5·4
Spain - - -	74	155	247	0·3	0·5	0·8
Uruguay - - -	602	587	1,143	} 5·7	4·8	5
Other Countries - -	862	760	392			
Total - - -	25,864	27,813	30,617	100	100	100

The value of the imports has steadily fallen in the three years under comparison, and with the exception of Italy each country shows a decrease, except that in 1893 the imports from the United States were greater than in 1892; but not only did this increase entirely disappear in 1894, but the figures for that year are considerably below those for 1892. On the other hand, the total weight of the imports in 1894 shows an increase over that in 1893, the figures for 1893 being about 10 per cent. less than in 1892.

The importation of the more valuable articles was greater in 1893 than in the past year, a fact which is shown in the statistics by the higher average value per ton of the imports from all countries except Italy and the United States. The articles which have principally contributed to this increase are those of pure silk and wool. The chief countries which export these, namely, England, Germany, and France, show the largest increases in this respect; the increase for France, which sends about two-thirds of the total silk imports, being about twice as great as those of its two competitors.

The growth of the importation of cheaper articles, apparent in the statistics of imports in 1894 from England, Germany, and France, is the cause of the falling off already remarked in the value of this group of the imports in spite of the not insignificant increase in the quantity. Figures which have already been published as to portions of the year 1895 indicate that this decrease of value and increase of volume is further accentuated.

IX.—IMMIGRATION AND COLONISATION IN PERU.*

Few countries have passed as many laws on immigration and colonisation as has Peru, and yet the one object of them all, viz., *white* immigration, has never been wholly attained.

Badly seconded by the majority of immigration agents, whose aim was generally to enrich themselves at the expense of the immigrants and of the Treasury, and the Government itself, chiefly by reason of its internal troubles, not being in a position to grant sufficient protection to immigrants on their arrival in its territories, Peru has found herself compelled to issue special laws in favour of colonisation and immigration. It is doubtless a very rich country; it possesses immense tracts of land which can be successfully cultivated, and large metalliferous deposits which can be profitably exploited; but under the present unstable order of affairs, and with the Government not offering the necessary guarantees, foreign capital will not be found to open up its wealth.

The climate of Peru is on the whole a good one. On the coast it is temperate and the rainfall slight, except in the northern parts; in the mountainous district, where rain falls abundantly at the commencement of the year, the high table-lands are cool and the valleys temperate; while in the eastern part of the country, or real tropical zone, the heat is regular and the rainfall heavy. Lima, the capital (population 100,000), enjoys a most agreeable climate. At La Merced, a district of the Chanchamayo valley in the department of Junin—one of the most fertile regions of Peru from an agricultural point of view—the mean temperature is 23° C.

From 1850 to 1876, during which period Peru was continually occupied with the question of immigration and colonisation, the results, as far as Europeans are concerned, were as follows:—About 1,000 immigrants landed during the three years 1850–53; 3,000 in the period 1872–76. These figures do not include 600 Germans, who arrived in two parties, one in 1857, the other in 1868, forming the colony of Pozuzo, the only flourishing European colony actually existing at present in Peru.

According to official returns, the above constitutes the entire white immigration into Peru for the space of the 26 years referred to.

Chinese immigration, or, to put it more accurately, the Chinese Treaty, lasted 25 years, from 1849 to 1874, the year in which the last batch of coolies arrived. During this time it is estimated that from 80,000 to 100,000 Chinese entered Peruvian territory, and in the opinion of many people, this Chinese immigration has done much for Peru. It has solved the question of domestic service, filled the valleys and the farms of the coast with an industrious

* From a Report of M. J. Welters, Consul-General Chargé d'Affaires for Belgium in Chili, Peru, and Bolivia.

population, carried an industrial initiative into the most distant parts of the mountainous and even of the tropical region, introduced several minor industries, and reduced the price of everything all round. In 1874 a treaty, known as the Treaty of Tientsin, was concluded between Peru and China, which is still in force, and regulates, amongst other questions, that of Chinese immigration.

Negroes are scattered throughout Peru, but their laziness, dishonesty, and the facility with which they give themselves up to all kinds of disorderly conduct, especially since the emancipation, render them a very difficult class of workers to manage. The earliest negro slaves were brought in 1529, the latest during the first quarter of the century; but slavery was finally abolished in 1855.

The law of 1849 was the first immigration law worthy of the name. It granted a bounty of 30 sols (sol = 2s.) per immigrant to every immigration agent, the colonists to be between the ages of 10 and 40, and in batches of not less than 50 at a time. These colonists were exempted from military duty and taxes for a period of 10 years.

This law, not having brought about the desired results, was abrogated in 1853.

A sufficient knowledge was not first obtained as to the habits, morality, and fitness of the emigrants. The latter, according to an Official Report of 1853, either died in large numbers during the voyage, or else gave way to acts of insubordination, which had a bad ending. These results of a forced and defective system have been the cause of the complete disappearance of European immigration. The report added that between February 26, 1850, and July 5, 1853, 3,932 colonists arrived, composed of 2,516 Chinese, 320 Irish, and 1,096 Germans, which had cost the Peruvian Treasury altogether 117,960 sols (11,796*l.*) in bounties alone.

Since 1853 several immigration laws have been promulgated, but none have met with any success on the chief point desired in Peru, namely, the advent of Europeans.

At present immigration and colonisation are regulated by a law passed in October 1893.

Peru has need principally of agriculturists and miners, but there are two reasons which may be urged against the advisability of European working men seeking to improve their position by coming to this country. In the first place, the Peruvian working man, having very few needs, receives a very small wage, and, under these conditions, it is difficult, if not impossible, to give foreigners wages much higher than the natives. Another reason is that the work of the fields and mines having been always carried out exclusively by negroes, mulattoes, Chinese, and Indians, the white working man would not find himself amid surroundings to which he could easily become accustomed.

This would not be the case when it is a question of starting new enterprises, in which the white element stands alone or predominates largely. Under this heading would come the German colony at Pozuzo, situated in the department of Junin,

at the junction of the rivers Pozuzo and Huancabamba, and formed by the banding together of a certain number of families of the same nationality for the purpose of devoting themselves to agriculture. Such a system constitutes real colonisation, and not the simple working together of men of different races in enterprises already started. At a level of about 800 metres above the sea, it extends along the banks of the above-named rivers for about 17 kiloms., producing under excellent conditions coffee, tobacco, cocoa, rice, vanilla, sarsaparilla, indigo, quinine, various fruits and vegetables, and, above all, cotton and the sugar-cane; in spite of a mean temperature of about 31° C., the climate is said to be healthy.

When Peru shall have recovered from the effects of late struggles and finally embarked on a period of peace, it is stated that those working men and artizans who have special knowledge of a trade may find good openings for the exercise of it. Reference is more especially made to stone-cutters, sugar-workers, cooks, coopers, masons, carpenters, machinists, &c. Master workmen, establishing themselves on their own account, might probably do good business. A good master carpenter, for example, might find plenty to do in Lima, Arequipa, and other towns; but a man setting up on his own account should have a reserve of at least 200 to 300 sols (20% to 30%) for preliminary expenses. With 3,000 sols (300%) a small farm can be established.

With reference to the law of 1893 cited above, it will be seen that it affords protection rather to those who arrive with the determination of staying as farmers—in a word, it is rather a colonisation than an immigration law.

Article III. states that immigrants have the right—1. To be lodged and kept at the expense of the Government for the first seven days after their arrival. 2. To the free entry of all their personal property and clothes, one gun, farm implements, and the tools of their trade in the proportion fixed by Government.

Article IV. states that colonists, besides those advantages named in Art. III., have the right to—1. A free third-class passage on the vessel which brings them to Peru. 2. A certain number of acres of land which the Government shall allot them. 3. Free conveyance from the port of disembarkation to the ground allotted them. 4. Free maintenance at Government expense for three months at the colony. 5. Exemption from taxes of all kinds for five years. 6. One free grant of such tools and farm implements as the Government may think fit.

Simple immigrants who are not intending colonists must have, therefore, money enough to live on till they shall have found occupation.

In conclusion, the Belgian Consul says that carpenters and cabinet-makers can earn 6 sols (12s.) per day, and masons 4 to 5 sols (8s. to 10s.). Engineers, often wanted on farms, can earn about 150 sols (15%) per month. A foreign workman can live in Peru on 1.20 sol (nearly 2s. 5d.) per day; but after a certain time, when he has come to know the country better, he will find 80 centavos (about 1s. 7d.) per day sufficient.

X.—JAPANESE COMPETITION WITH THE BOMBAY COTTON INDUSTRY.*

To Bombay merchants the most important feature of the present industrial development of Japan is undoubtedly the progress being made in cotton manufacture. Notwithstanding the natural dislocation of trade resulting from the perturbed political atmosphere, the cotton-spinning companies of Japan realised very encouraging results during the first half of the present year, the earnings in some cases being as high as 15 per cent. This favourable return is attributed to the increased price of yarns owing to speculative purchases in view of the revival of trade after the war, the gradual rise in the price of raw cotton, and the diminished cost of coal. The returns recently received show that the total number of spindles was only a single unit short of five lakhs and 19 thousand, or rather more than one-fifth of the nominal number for the entire Bombay Presidency. The average period during which the mills were working in the month was 27 days, and the average time the mills were kept open per diem was 22½ hours. The Far Eastern mills resemble those of British India in the class of work manufactured, which do not come into competition with the high counts of Lancashire. The total number of hands employed was 9,068 men and 28,494 women—a disproportion which should be carefully noted in Bombay, and which is to be accounted for by the fact that, while the average rate of pay for the male operative is 18·05 cents, that of the women is 9·83 cents.

New mills are being erected in Japan, and there are not wanting indications of similar enterprises being undertaken in China when circumstances are favourable. Whether Chinese or Japanese capital is employed in the erection of these mills, it is quite certain that their working will largely, if not entirely, be superintended by the Japanese. The Chinese is a very steady-going and industrious workman, but he has no head for the organisation of a large industry, and would be utterly at a loss in managing a mill. He will, therefore, call in the aid of his Eastern neighbour, and the mills will be worked under Japanese supervision, but with Chinese labour—a combination ensuring very cheap production, against which the Bombay mill-owner will find it hard to compete. To the cotton merchants in India the outlook is less serious, as there are not many parts of China where the soil is adapted for cotton growing excepting of a very coarse kind, and in Japan the area available for cultivation is necessarily mainly absorbed in the production of rice and other food-stuffs, to meet the demands of a growing population. It may, therefore, be

* From an article in the "Bombay Gazette."

safely assumed that the Far East will continue to indent largely on India for its supplies of raw cotton unless Japan is reached by the American markets on equally good terms.

This being so, it is rather disquieting to learn that a line of steamers is shortly to be established to run between Kobe and Yokohama and Mexico, connecting with the Tehuantepec railroad, and thus with the Gulf or Cotton States. The line will be worked under the auspices of the Osaka Cotton-Spinning Association for the express purpose of bringing the raw material direct from America, and it is stated that definite promises have been obtained of a subsidy from the Japanese Government. Besides cotton, it is expected that iron will be brought from a mine in the northern part of Mexico, which, on the authority of an analysis made by an English firm, is said to contain ore of superior quality. It is believed that paying return cargoes to Central America of Japanese staples will be forthcoming after the line has been in working order for some time, but the main object in view appears to be the direct importation of cotton. It is clear, therefore, that the prospects of the Bombay cotton trade with the Far East are the reverse of cheering, and it behoves the local merchants to give very earnest attention to the situation, with a view to seeing how far it is possible to legitimately counteract the existing tendency. Another threatened industry in which Bombay is interested, inasmuch as it is financed in that Presidency, is the export of Persian opium to Formosa, to supply the wants of the Chinese inhabitants, who number about two millions. The importation of opium to Japan is strictly forbidden, and the Mikado's Government have, after careful consideration, decided that the prohibition shall also apply to the recently acquired island of Formosa, on the ground that the Japanese on the island might adopt the opium habit. The result of this interdiction will be the cessation of a trade financed in Bombay, the total value of which is considerable.

XI.—THE FOREST WEALTH OF CANADA.*

The natural resources of Canada are her forests, her farms, her fisheries, and her minerals.

In Canada, in the various industries depending for their existence upon the supply of wood, there is an invested capital not far from 100 million dollars, and an annual wage list of over 30 million dollars, with an output valued at over 110 million dollars.

In addition there are the railways, which are dependent on the wood supply for railway ties and dimension timber, and in whose freights the lumber carried figures as nearly one-fifth of the total freight carried; the canals, of whose freights the products of the forest constitute two-fifths of the total freight carried; the mines, which require wood for shoring purposes; the ships which, themselves made of wood, find in the exports of the forest the materials for the full cargo, without which freight and rates on goods carried must be higher, nearly one-quarter of the exports of home production being products of the forest; the leather industry, which depends upon nature's supply of tannin secreted in the bark of trees; the lucifer match industries; those varied industries which depend in part upon wood, such as agricultural implements, edged tools, &c.; and the practically new industry of pulp making, which within 10 years has sprung up into an industry with nearly three million dollars of invested capital and over one million dollars of annual output.

The value of forest products consumed per capita may be estimated approximately. The value of forest products, calculated from the census returns of 1891, was 80,071,415 dols. For the fiscal year 1890-91 the imports of wood articles amounted to 3,132,516 dols., while for the same period the exports were 27,707,547 dols., leaving for consumption in Canada 55,996,384 dols., or a value of 15.59 dols. a head. With respect to the quantity used the census returns show an aggregate of 2,045,073,072 cubic feet as the total cut of the year. About 30 per cent. of this is exported, leaving 1,431,551,150 cubic feet for the annual home consumption. This is equal to 296.2 cubic feet per head of the population. The chief of the Forestry division of the United States department of Agriculture, estimates that the per capita consumption of the United States is about 350 cubic feet annually.

The ownership of Canadian forests is for the most part vested in the Provincial Governments, including the provinces of Ontario, Quebec, New Brunswick, and British Columbia, which grant licences to the lumbermen.

In the province of Manitoba, in the territories and in the Railway Belt of British Columbia (40 miles wide by 500 miles

* From a report in the statistical year book of Canada for 1894.

long), the Dominion Government, filling the place of the provincial governments, owns the Crown lands and their forests.

In Nova Scotia there is no system of timber licences, the trees being sold with the land, and not much timbered Crown lands remaining. This is also the case with Prince Edward Island.

In the settled portions of the provinces the woodlands are in the hands of private owners, but contain comparatively little that can be classed as forest, though the census returns indicate that about one-third of the occupied land is in woodland and pasture, possibly leaving one-fourth for woodland.

In the United States, notwithstanding the length of time during which attention has been directed to forestry, an exact census of forest area in existence has never been made. The area covered with wood growth is less than 500,000,000 acres. If all the land area, not known to be treeless or in farms, were under forest, the acreage would not exceed 850,000,000, but the lower figure is probably more nearly correct.

The same statement may be made respecting Canada. From some persons there are affirmations that there is not more than ten years' supply. From others there are declarations that the supply in the forests is sufficient to last 100 years, possibly 200 years.

The Assistant Commissioner of Crown Lands of Ontario points out that "while the department could give the area of the unsold lands of the Crown, all of which are covered, to a greater or less extent, with various kinds of timber, as this is a wooded province, it is quite an impossibility to estimate the quantities of timber upon the 90 million acres representing that unsold area."

The forest wealth of the country is very great. The forests formerly extended in an almost unbroken stretch from the Atlantic Ocean to the head of Lake Superior, a distance of 2,000 miles. The great plains of the North-West have always, within the memory of man, been sparsely timbered, but on the Pacific slopes of the Rocky Mountains down to the shores of the ocean there are mammoth trees that can compare favourably with the growth of any region on the globe. From the earliest days of its occupation by the French the forest wealth of the country washed by the St. Lawrence engaged the attention of the Government of France, who saw therein vast resources available for their naval yards. They drew from these forests large numbers of masts and spars, and issued stringent regulations for the preservation of the standing oak. When the country was ceded to Great Britain but little attention was paid at first to its vast timber supply, owing to the fact that almost the whole of the Baltic trade was carried in British bottoms, and that the timber of Europe provided an unfailing and convenient return freight for the shipping thus engaged. When, however, the troubles of the Napoleonic era commenced, and especially when the continental blockade was enforced, the timber supplies of the Baltic becoming

uncertain and insufficient, attention was directed to the North American colonies, with the result of increasing the quantity of timber which reached Great Britain from 2,600 tons in 1800 to 125,300 tons in 1810, and to 308,000 tons in 1820.

The following figures will show the development of the trade for the last four years :—

			Tons.
1891.	Exported to the United Kingdom	-	1,051,091
1892.	„ „ „	-	1,406,350
1893.	„ „ „	-	1,255,773
1894.	„ „ „	-	1,381,816

A noticeable feature in the returns is the steady decline in the quantity of square timber exported to England and the increasing quantity of sawn or manufactured wood. The decrease in hewn timber sent from Canada in 1894 compared with 1893 was 39·3 per cent., and the increase in sawn lumber for the same year was 4·3 per cent. This is in favour of the Canadian forests, as timber in the square involves great waste, and the débris left in the forest increases the danger from fires.

The products of the forest and manufactures from them supply a large proportion of the exports of Canada. In 1894 the value of forest products exported was 28,173,813 dols., as against 28,212,552 in 1893.

The chief customers for these exports are the United Kingdom and the United States.

The value of the exports of forest products to the United Kingdom in 1894 was 12,332,726 dols., as compared with 14,606,759 dols. in 1893. The United States took timber in 1894 to the value of 13,903,586 dols. (*11,426,462 dols. in 1893*).

A marked feature of the export to the United States is the great increase in the number of pine saw-logs taken from Canada.

In the fiscal year 1893, according to the Trade and Navigation Returns, these logs amounted to 127,000,000 feet, B.M., of the value of 1,056,355 dols., and in 1894 to 277,947,000 feet, of the value of 2,359,951 dols.

The wood-pulp industry with a rapid growth has attained large proportions. By the census of 1891 the product of pulp-wood was 261,155 cords, but as it was then recorded for the first time there can be no comparison with the previous decades. Pulp-mills did not appear in the census of 1871; by that of 1881 the invested capital amounted to 92,000 dols., the wages to 15,720 dols., and the product to 63,300 dols.; by the census of 1891 the invested capital had increased to 2,900,907 dols., the wages to 292,099 dols., and the value of the product to 1,057,810 dols.

There has been a correspondingly large increase in the exports of wood for pulp. The article did not appear in the Customs returns of 1889. In 1890 its export was valued at 80,005 dols.; in 1891 at 188,198 dols.; in 1892 at 219,548 dols.; in 1893 at 386,092 dols.; and in 1894 at 393,260 dols. The export of wood-pulp also made great strides, amounting in 1890 (its first appearance

in the returns) to 168,180 dols.; in 1891 to 208,619 dols.; in 1892 to 355,303 dols.; in 1893 to 455,893 dols.; and in 1894 to 547,217 dols.

Three things are necessary to the successful development of the manufacture of pulp—suitable wood, extensive water power, and cheap labour. All the elements indispensable to the success of pulp manufacture are to be found in Canada, besides particular additional advantages. The immense forests of coniferous trees contain a practically inexhaustible supply of the different kinds of wood required in this line of manufacture. They are, moreover, of a superior quality and very much sought after by the manufacturers of the United States, as is seen in the yearly increasing demand. With regard to quality and quantity, Canada is as well situated as Norway and Sweden, if not better.

If the price obtained in England is taken as a criterion, Canadian wood produces better pulp than that of Norway and Sweden, for in 1893 Canadian pulp was sold in England at an average of 24.80 dols. a ton, as against 20.77 dols. for the Scandinavian product.

XII.—AUSTRALASIAN STATISTICS FOR 1894.*

From the usual tables of statistics for the seven colonies lately issued by the Victorian Statistical Department, the following information is taken:—

The first table shows the population, debt, revenue, and expenditure of each of the colonies. In population there is shown an increase of 85,462 souls, as compared with the estimate at the end of 1893 for the whole of the colonies. An increase in the total amount of public debts for the colonies to the extent of 3,168,229*l.* may be noted as a small accession of indebtedness compared with recent years. The aggregate revenues of the whole of the colonies was less in 1894 than in 1893 by about 300,000*l.*, and the expenditure decreased to the extent of 1,500,000*l.*, so that the year's deficit has been reduced from 1,839,000*l.* in 1893 to 588,000*l.*

	Estimated Population on 31st Dec. 1894.	Public Debt on the 31st Dec. 1894.	Public Revenue.	Public Expenditure.
		£	£	£
Victoria - - -	1,179,103	46,804,382	6,716,814	7,310,246
New South Wales - -	1,251,450	56,451,369	9,300,711	9,178,706
Queensland - - -	445,155	30,639,534	3,358,302	3,291,528
South Australia - -	347,720	22,306,500	2,496,544	2,526,787
Northern Territory -	4,682		62,872	120,783
Western Australia -	82,072	3,417,339	863,680	755,564
Tasmania - - -	157,456	7,779,145	696,795	789,805
New Zealand - - -	686,126	40,386,964	4,288,716	4,398,930
Total - - -	4,153,764	207,785,233	27,784,434	28,372,349

The second table refers to the rate per head of population of public revenue, taxation, expenditure, and debt. Many persons are quite unaware of the fact which this table discloses, namely, that in taxation, both direct and indirect, and in public indebtedness, the colony of Victoria shows the lowest rate per head of all the colonies, with the single exception that indirect taxation in Tasmania is less per head than in Victoria.

Colony.	Total Revenue.	Taxation.	Per Head of the Population.	
			Expenditure.	Debt.
	£ s. d.	£ s. d.	£ s. d.	£ s. d.
Victoria - - -	5 14 5	2 2 6	6 4 6	39 13 11
New South Wales - -	7 10 4	2 3 6	7 8 4	45 2 2
Queensland - - -	7 13 1	3 2 5	7 10 1	68 16 7
South Australia - -	7 4 9	2 4 1	7 6 6	63 6 0
Northern Territory -	13 2 7	7 4 11	25 4 5	
Western Australia -	11 12 4	6 0 3	10 3 3	41 12 9
Tasmania - - -	4 9 4	2 13 9	5 1 3	49 8 1
New Zealand - - -	6 6 3	3 7 8	6 9 6	58 17 3
Total Australasia -	6 15 3	2 11 2	6 18 1	50 0 6

In the next table details are given of values of imports and exports for each of the colonies. A reference to the figures shows that the imports of the seven colonies have fallen from 53,258,007*l.* in 1893 to 48,817,685*l.* in 1894, a decrease of roughly 4,440,000*l.* Exports have also fallen off from 65,754,126*l.* in 1893 to 62,900,055*l.* in 1894, namely, a decline to the amount of 2,850,000*l.* The value of imports per head of population fell from 13*l.* 4*s.* 6*d.* in 1893 to 11*l.* 17*s.* 7*d.* in 1894, and that of exports from 16*l.* 6*s.* 7*d.* in 1893 to 15*l.* 6*s.* 1*d.* in 1894.

Colony.	Imports, 1894.		Exports, 1894.	
	Total Value.	Per Head of Population.	Total Value.	Per Head of Population.
	£	£ s. d.	£	£ s. d.
Victoria - - -	12,470,599	10 12 4	14,026,546	11 18 10
New South Wales - -	15,801,941	12 15 5	20,577,673	16 12 7
Queensland - - -	4,337,400	9 17 9	8,795,559	20 1 0
South Australia - -	6,226,690	18 1 1	7,301,774	21 3 6
Northern Territory -	98,945	20 13 3	227,009	47 8 1
Western Australia -	2,114,414	28 8 9	1,251,406	16 16 7
Tasmania - - -	979,676	6 5 8	1,489,041	9 11 0
New Zealand - - -	6,788,020	9 19 11	9,231,047	13 11 10
Total - - -	48,817,685	11 17 7	62,900,055	15 6 1

The last table shows that the total land under cultivation in 1894 was 9,800,954 acres, which shows a fair increase from 9,670,131 acres in 1893. Wheat produced gives a decrease from 42,034,000 bushels in 1893-4 to 37,306,000 bushels in 1894-5. Cattle increased from 12,555,000 in 1893 to 13,301,425 in 1894, and sheep also increased from 119,247,000 to 121,171,000.

Colony.	Total Land under Cultivation.	Gross Produce of Wheat.	Live Stock.	
			Cattle.	Sheep.
	Acres.	Bushels.		
Victoria - - -	2,980,299	11,445,878	1,833,900	13,180,943
New South Wales - -	1,669,999	7,041,978	2,465,411	56,977,270
Queensland - - -	284,552	545,185	7,012,997	19,587,691
South Australia - -	2,758,304	13,618,062	423,602	7,267,642
Northern Territory -	—	—	237,229	67,552
Western Australia -	145,877	170,401	187,214	2,132,311
Tasmania - - -	575,286	872,000	177,038	1,727,200
New Zealand - - -	1,386,637	3,613,037	964,034	20,236,829
Total - - -	9,800,954	37,306,541	13,301,425	121,171,438

XIII.—THE SHIPPING TRADE OF NEW SOUTH WALES.*

The records of the shipping trade of New South Wales, although carefully kept by the various governors, are not available prior to the year 1822, the earlier registers relating to the colony's statistics having disappeared. There are in existence, however, numerous manuscript returns which record the shipping trade of the earlier years, and these will doubtless be published at some future date.

The rate of increase has been fairly constant, though checked occasionally by the occurrence of bad seasons or the failure of prices in the European markets. The following table shows the quantity and tonnage of shipping arriving and departing from New South Wales during the year 1890, and for the four succeeding years :—

Year.	Entered.		Cleared.	
	Number.	Tonnage.	Number.	Tonnage.
1890	2,889	2,413,247	2,777	2,348,625
1891	3,021	2,821,898	3,100	2,872,338
1892	2,960	2,804,549	3,067	2,842,635
1893	2,914	2,590,371	2,916	2,602,957
1894	3,272	2,859,968	3,291	2,878,586

The trade of the colony is, to a very large extent, carried on under the British flag, the deep-sea trade being in the hands of shipowners of the United Kingdom, and the coasting trade in local hands. There has, however, been a notable increase in foreign shipping since 1881. In 1870, the British shipping entered and cleared amounted to 1,333,410 tons, while in 1894 it was 5,191,971 tons, showing an increase of 3,858,561 tons. The foreign shipping amounted to 128,352 tons in 1870, and 546,583 tons in 1894, being an increase of 418,231 tons.

The advent of the French line of Messageries Maritimes in 1883, and of two lines of German steamships some four years later, has tended to increase the amount of the foreign trade of the colony ; but, nevertheless, the tonnage under the British flag in 1894 was 90·5 per cent. of the whole. The annexed table shows the respective amounts of British and foreign tonnage for the last five years :—

Year.	British.	Foreign.	Total.
	Tons.	Tons.	Tons.
1890	4,156,963	604,909	4,761,872
1891	4,902,607	791,629	5,694,236
1892	4,977,850	669,334	5,647,184
1893	4,704,300	489,028	5,193,328
1894	5,191,971	546,583	5,738,554

* Based on a Statistical Report by Mr. T. A. Coghlan, Government Statistician.

Of the tonnage set down as British, the larger portion is owned or registered in the colonies. In 1870, out of 1,333,410 tons of shipping entered and cleared under the British flag, 964,718 tons, or 72·3 per cent., were Australasian; in 1880, out of 2,259,924 tons entered and cleared, 1,499,236 tons, or 66·3 per cent., represented colonial shipping; while in 1894 the colonial shipping amounted to 3,056,260 tons, out of a total of 5,191,971 tons, or 58·9 per cent.

Compared with the figures just given, the tonnage of the foreign vessels trading with New South Wales will appear small. Taking the year 1894, for which the inward and outward tonnage for the principal nationalities are given below, France stands first, then Germany, then America. Scandinavia is the only other nation whose carrying trade with the colony is important.

					Tonnage.
France	-	-	-	-	180,698
Germany	-	-	-	-	157,029
U.S. America	-	-	-	-	138,963
Sweden and Norway	-	-	-	-	33,819
Italy	-	-	-	-	9,605
Other nations	-	-	-	-	26,469
Total					<u>546,583</u>

As might be expected from the fact that over one-fourth of the tonnage which arrived in the Colony in 1894 was in ballast, as against nearly one-thirty-seventh of the clearances, the average value of freights outward was higher than those inward. Thus the average value of the goods imported by sea was 4*l.* 18*s.* per ton, while that of the goods exported was 5*l.* 6*s.* 8*d.*, or 8·8 per cent. higher. If, however, allowance be made for shipping in ballast, the mean value of the exports was only 6*l.* 1*s.* 6*d.*, or 21·9 per cent. lower than the imports, which averaged 7*l.* 15*s.* 6*d.* per ton. New South Wales exports have, generally speaking, a high value per ton, and the fact that, when vessels entering and leaving in ballast are excluded, they have a lower average value than the imports is due to the large amount of coal exported, the quantity for 1894 having been 2,125,125 tons weight, equivalent to 1,442,506 tons measurement, out of a total of 2,878,586.

Of the tonnage engaged in the outward trade of the Colony, 13·5 per cent. went to the United Kingdom, though in value, the exports were 51·8 per cent. of the total. The exports by sea to Victoria and the other Australasian colonies amounted to 54·1 per cent. of the whole, while in value they reached only 19·6 per cent. As regards the other British possessions, the tonnage of vessels carrying goods from New South Wales to their ports amounted to 6·3 per cent. of the whole, but the value only reached 2·6 per cent. In the case of foreign countries, the

tonnage in 1894 amounted to 26·1 per cent. and the value to 26 per cent.

The progress of the shipping trade of Sydney has been very uniform, the increase from the year 1860 being at an average rate of about 6 per cent. per annum. The vessels registered as entering the port of Sydney considerably exceed in tonnage those clearing. To account for this difference it is only necessary to state that vessels leaving Sydney for Newcastle for the purpose of shipping coal are reckoned as departures from Newcastle, and not from Sydney. For this reason the clearances of Newcastle uniformly exceed the arrivals, as will be noticed in the subsequent table. The practice of clearing vessels at both ports at one time obtained, but has been abandoned for many years, and vessels are now cleared at the port which they last leave. The following statement shows the shipping entered and cleared at both Sydney and Newcastle for the last five years :—

Year.	Sydney.		Newcastle.	
	Entered.	Cleared.	Entered.	Cleared.
	Tons.	Tons.	Tons.	Tons.
1890	1,644,589	1,356,632	625,398	842,180
1891	1,934,556	1,535,306	704,306	1,140,536
1892	1,896,677	1,594,108	729,167	1,024,318
1893	1,902,265	1,585,857	524,844	842,363
1894	1,959,435	1,612,179	721,931	1,035,930

No other seaport of the Colony can be compared with either Sydney or Newcastle, though for some time Wollongong has maintained a trade of some consequence, especially in coal; and of late years the importance of Eden, Twofold Bay, has increased. In 1894 the vessels which entered Wollongong direct from places outside the Colony were all in ballast, and totalled 59,107 tons; while at Eden the shipping inwards amounted to 27,126 tons and landed goods to the value of 618£. The shipping outwards from Wollongong had an aggregate tonnage of 91,451, and from Eden 46,225, the value of the export trade at these two ports being respectively 56,917£. and 15,819£.

The years 1883 and 1884 were marked by great activity in the construction of both sailing and steam vessels, 50 sailing and 52 steam vessels having been built in 1883, whilst 39 sailing vessels and 64 steamers were built in the subsequent year. Trade then became less active, and the industry showed a tendency to die out. In 1888 it had fallen lower than in any of the preceding years, and there has been little improvement since, the tonnage of sailing vessels built during 1894 being only 466, and of steamers 463, in all 929. The number of vessels built in each of the last five years was :—

Year.	Sailing Vessels.		Steamers.		Total.	
	No.	Tons.	No.	Tons.	Vessels.	Tonnage.
1890	9	394	6	697	15	1,091
1891	27	573	13	767	40	1,340
1892	27	782	10	317	37	1,099
1893	11	563	7	236	18	799
1894	6	466	8	463	14	929

No reliable data have, so far, been procurable as to the number and tonnage of vessels built abroad for the New South Wales local trade, but the number must be considerable, and such vessels form an import of large value, altogether lost sight of in the Customs returns. Some idea, however, of the large number imported may be gathered from the registration of the tonnage of other than New South Wales built vessels, which for the past 10 years has averaged 4,794 tons sailing and 3,735 tons steam per annum.

The accommodation provided, both by the Government and by private enterprise, for the fitting and repairing of ships is equal to the requirements of the trade of the Colony. At Sydney there are three graving docks, five floating docks, and three patent slips. At Newcastle there are two patent slips, besides which there are other docking and building yards in different parts of the Colony for the convenience of coasters and small craft.

For natural facilities for shipping Sydney stands unrivalled. The water deepens abruptly from the shores, so that the largest vessels may be berthed alongside the wharves and quays.

The coast of New South Wales is well provided with light-houses, the number at the end of 1894 being 23, besides which there were 17 leading lights and light-ships for the safety of harbour navigation. The cost of erecting the lighthouses was 199,467*l.*, in addition to which a sum of 32,106*l.* was spent on lanterns, making a total expenditure of 231,573*l.* on construction and equipment. The South Solitary Island lighthouse cost the largest sum to construct, namely, 31,259*l.*, exclusive of the amount paid for the lanterns. The Macquarie revolving light, on the South Head of Port Jackson, is one of the most powerful lights in the world, and is visible 25 miles at sea.

XIV.—NEW TARIFF BILL OF THE UNITED STATES.

A despatch, dated 27th December last, has been received at the Foreign Office from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, transmitting copy of the text of the Tariff Bill, entitled "A Bill to temporarily increase "revenue to meet the expenses of Government and provide "against a deficiency," which passed the House of Representatives on 26th December last. The Bill imposes an additional duty on almost all articles of import now dutiable, with the exception of sugar; certain classes of wools, now on the free list, are assessed with a duty equal to 60 per cent. of that imposed by the McKinley Tariff Act; and woollen goods are assessed with an additional duty of 60 per cent. of the McKinley rate over and above the *ad valorem* duty now levied upon them.

The text of the Bill referred to is as follows:—

"Be it enacted by the Senate and House of Representatives of the United States of America in Congress assembled, That from and after the passage of this Act, and until August 1, 1898, there shall be levied, collected, and paid on all imported wools of classes 1 and 2, as defined in the Act herein-after cited, approved October 1, 1890, and subject to all the conditions and limitations thereof, and on all hair of the camel, goat, alpaca, and other like animals, except as herein-after provided, and on all noils, shoddy, garnetted waste, top waste, slubbing waste, roving waste, ring waste, yarn waste, and all other wastes composed wholly or in part of wool, and on woollen rags, munge, and flocks, a duty equivalent to 60 per centum of the duty imposed on each of such articles by an Act entitled 'An Act to reduce 'the revenue and equalise duties on imports, and for other 'purposes,' approved October 1, 1890, and subject to all the conditions and limitations of said Act; and on all wools and Russian camel's hair of class 3, as defined in said Act approved October 1, 1890, and subject to all the conditions and limitations thereof, there shall be levied, collected, and paid the several duties provided by such Act approved October 1, 1890. And paragraph 279 of Schedule K, and also paragraph 685 in the free list of an Act entitled 'An Act to reduce taxation, to provide revenue for the Government, and for other purposes,' which became a law August 27, 1894, are hereby suspended until August 1, 1898.

"Sec. 2. That from and after the passage of this Act, and until August 1, 1898, there shall be levied, collected, and paid on all imported articles made in whole or in part of wool, worsted, or other materials described in section 1 of this Act, except as herein-after provided, 60 per centum of the specific pound or square-yard duty imposed on each of such articles by an Act entitled 'An Act to reduce the revenue and equalise duties on

imports, and for other purposes,' approved October 1, 1890, and subject to all the conditions and limitations thereof, in addition to the *ad valorem* duty now imposed on each of such articles by an Act entitled 'An Act to reduce taxation, to provide revenue for the Government, and for other purposes,' which became a law August 27, 1894; and on carpets, druggets, bockings, mats, rugs, screens, covers, hassocks, bedsides, art squares, and other portions of carpets or carpeting, made in whole or in part of wool, the specific square-yard duty imposed on each of such articles by said Act approved October 1, 1890, and subject to all the conditions and limitations thereof, in addition to the *ad valorem* duty imposed on such articles by said Act which became a law August 27, 1894.

"Sec. 3. That from and after the passage of this Act, and until August 1, 1898, there shall be levied, collected, and paid on all imported lumber and other articles designated in paragraphs 674 to 683, inclusive, of an Act entitled 'An Act to reduce taxation, to provide revenue for the Government, and for other purposes,' which became a law August 27, 1894, a duty equivalent to 60 per centum of the duty imposed on each of such articles by an Act entitled 'An Act to reduce the revenue and equalise duties on imports, and for other purposes,' approved October 1, 1890, and subject to all the conditions and limitations of said last-named Act; but pulp wood shall be classed as round unmanufactured timber exempt from duty: Provided, that in case any foreign country shall impose an export duty upon pine, spruce, elm, or other logs, or upon stave bolts, shingle wood, pulp wood, or heading blocks exported to the United States from such country, then the duty upon the lumber and other articles mentioned in said paragraphs 674 to 683, inclusive, when imported from such country, shall be the same as fixed by the law in force prior to October 1, 1890.

"Sec. 4. That on and after the passage of this Act, and until August 1, 1898, there shall be levied, collected, and paid on all the imported articles mentioned in Schedules A, B, C, D, F, G, H, I, J, L, M, and N of an Act entitled 'An Act to reduce taxation, to provide revenue for the Government, and for other purposes,' which became a law August 27, 1894, a duty equivalent to 15 per centum of the duty imposed on each of said articles by existing law in addition to the duty provided by said Act of August 27, 1894: Provided, that the additional duties imposed by this section shall not in any case increase the rate of duty on any article beyond the rate imposed thereon by the said Act of October 1, 1890, but in such case the duty shall be the same as was imposed by said Act: And provided further, That where the present rate of duty on any article is higher than was fixed by said last-named Act, the rate of duty thereon shall not be further increased by this section, but shall remain as provided by existing law."

XV.—CUSTOMS TARIFF OF NEW ZEALAND.

By the Customs and Excise Duties Act of 18th October 1895, the Customs tariff of New Zealand has been revised, the duties in many cases being altered.

The following is a table of the duties now in force, including the modifications introduced under the above law :—

Table of Duties.

Note.—The headings of the respective classes in this Table and in the Table of Exemptions are used solely for convenience of classification, and are not in any way to affect the articles specified therein, or be construed to indicate the material of which any such article is made.

The word “iron” includes steel, or steel and iron combined.

Neither steam engines, nor parts of steam engines, nor steam boilers (land or marine), are included in the expression “machines” or “machinery” in either this Table or the Table of Exemptions.

No.	Articles.	Rates of Duty.	
	CLASS I.—FOODS AND ARTICLES FOR HUMAN CONSUMPTION.	s. d.	
1	Almonds, in the shell - - - -	Per lb.	0 2
2	Almonds, shelled, n.o.e. - - - -	„	0 3
3	Bacon and hams - - - -	„	0 2
4	Biscuits, ships', plain and unsweetened - -	Per cwt.	3 0
5	Biscuits, other kinds - - - -	Per lb.	0 2
6	Boiled sugars, comfits, lozenges, Scotch mixtures, and sugar candy, including internal packages -	„	0 2
7	Candied peel and drained peel - - - -	„	0 3
8	Capers, carraway seeds, catsup, cayenne pepper, chillies, chutney, curry-powder and paste, fish-paste, gelatine, isinglass, liquorice, olives -	20 % <i>ad val.</i>	
9	Chocolate, confectionery, and all preparations of chocolate or cocoa :—		
	In plain trade packages - - - -	Per lb.	0 3
	In fancy packages, or in small packages for retail sale - - - -	20 % <i>ad val.</i>	
10	Confectionery n.o.e., including internal packages -	Per lb.	0 2
11	Fish, dried, pickled, or salted, n.o.e. - -	Per cwt.	10 0
12	Fish, potted and preserved - - - -	Per lb., or package of that reputed weight, and so in proportion for packages of greater or less reputed weight	0 2
13	Fruit, fresh, viz. :—		
	Apples, pears, plums, cherries, peaches, nectarines, medlars, apricots, quinces, tomatoes - (No duty exceeding $\frac{1}{4}d.$ per lb. to be levied on apples and pears from 14th July to 31st December.)	Per lb.	0 1
	Currants, raspberries, gooseberries, blackberries, and strawberries - - - -	„	0 0 $\frac{1}{2}$
	Lemons - - - -	„	0 0 $\frac{1}{2}$

No.	Articles.	Rates of Duty.
CLASS I.—FOODS AND ARTICLES FOR HUMAN CONSUMPTION— <i>cont.</i>		s. d.
14	Fruits, dried - - - - -	Per lb. 0 2
15	Fruits, preserved in juice or syrup - - -	25 % <i>ad val.</i>
16	Fruit-pulp, and partially preserved fruit, n.o.e. - -	Per lb. 0 1½
17	Fruits preserved by sulphurous acid - - -	" 0 1
18	Glucose - - - - -	" 0 1
19	Honey - - - - -	" 0 2
20	Jams, jellies, marmalade, and preserves - -	Per lb., or package of that reputed weight, and so in proportion for packages of greater or less reputed weight 0 2
21	Jellies concentrated in tablets or powder - -	Per lb. 0 4
22	Maizena and cornflour - - - - -	" 0 0¼
23	Meats, potted or preserved - - - - -	20 % <i>ad val.</i>
24	Milk, preserved - - - - -	25 % <i>ad val.</i>
25	Mustard - - - - -	Per lb. 0 2
26	Nuts of all kinds, except cocoanuts - - -	" 0 2
27	Oysters, preserved - - - - -	Per lb., or package of that reputed weight, and so in proportion for packages of greater or less reputed weight 0 2
28	Pearl barley - - - - -	Per cwt. 1 0
29	Peas, split - - - - -	" 2 0
30	Pickles - - - - -	Per Imp. gallon 3 0
31	Provisions n.o.e. - - - - -	20 % <i>ad val.</i>
32	Rice and rice-flour - - - - -	Per cwt. 6 0
33	Rice, undressed and dressed in bond - - -	" 4 0
34	Salt, except rock-salt - - - - -	Per ton 10 0
35	Sardines, including the oil - - - - -	Per lb. 0 2
36	Sauces - - - - -	Per Imp. gallon 4 0
37	Spices, including pepper and pimento, unground -	Per lb. 0 2
38	Spices, including pepper and pimento, ground -	" 0 4
39	Sugar - - - - -	" 0 0½
40	Treacle and molasses - - - - -	" 0 0½
41	Vegetables, fresh, dried, or preserved - -	20 % <i>ad val.</i>
42	Vinegar, table, not exceeding 6·5 per cent. of acidity* - - - - -	Per gallon 0 6
CLASS II.—TOBACCO.		
43	Cigarettes, not exceeding in weight 2½ lb. per 1,000 - - - - -	Per 1,000 17 6
	And for all weight in excess of 2½ lb. per 1,000 - - - - -	Per oz. 0 6
44	Cigars - - - - -	Per lb. 7 0
45	Snuff - - - - -	" 7 0
46	Tobacco - - - - -	" 3 6
47	Tobacco, unmanufactured, entered to be manufactured in the Colony, in any licensed tobacco manufactory, for manufacturing purposes only, into tobacco, cigars, cigarettes, or snuff - -	" 2 0

* Vinegar exceeding 6·5 per cent. of acidity to be treated as acetic acid.

No.	Articles.	Rates of Duty.	
CLASS III.—ALCOHOLIC BEVERAGES, AND MATERIALS FOR MAKING SAME.		s.	d.
48	Ale, beer of all sorts, porter, cider, and perry, the gallon, or for six reputed quart bottles, or 12 reputed pint bottles - - -	Per gallon	2 0
49	Cordials, bitters, and liqueurs - - -	Per liquid gallon	16 0
50	Hops - - -	Per lb.	0 6
51	Malt - - -	Per bushel	2 0
52	Rice malt - - -	Per lb.	0 1
53	Solid wort - - -	"	0 6
54	Spirits and strong waters, the strength of which can be ascertained by Sykes' hydrometer - (No allowance beyond 16·5 under proof shall be made for spirits or strong waters of a less hydrometer strength than 16·5 under proof.)	Per proof gallon	16 0
55	Spirits and strong waters, sweetened or mixed, when not exceeding the strength of proof -	Per liquid gallon	16 0
56	Spirits and strong waters in cases shall be charged as follows, namely:— Two gallons and under, as two gallons; over two gallons and not exceeding three, as three gallons; over three gallons and not exceeding four, as four gallons; and so on for any greater quantity contained in any case.		
57	Spirits or strong waters, mixed with ingredients in any proportion exceeding 33 per cent. of proof spirit, and although thereby coming under any other designation, excepting patent or proprietary medicines, or tinctures and medicinal spirits otherwise enumerated -	"	16 0
58	Wine, Australian, containing not more than 35 per cent. of proof spirit verified by Sykes' hydrometer, the gallon, or for six reputed quart bottles, or 12 reputed pint bottles	Per gallon	5 0
59	Wine, other than sparkling and Australian, containing less than 40 per cent. of proof spirit verified by Sykes' hydrometer, the gallon, or for six reputed quart bottles or 12 reputed pint bottles - - -	"	6 0
60	Wine, sparkling - - -	"	9 0
CLASS IV.—NON-ALCOHOLIC BEVERAGES, AND MATERIALS FOR MAKING SAME.			
61	Aërated and mineral waters and effervescing beverages - - -	20 % <i>ad val.</i>	
62	Chicory - - -	Per lb.	0 3
63	Chocolate - - -	"	0 3
64	Cocoa - - -	"	0 3
65	Coffee, essence of - - -	20 % <i>ad val.</i>	
66	Coffee, raw - - -	Per lb.	0 2
67	Coffee, roasted - - -	"	0 5
68	Syrups, lime- or lemon-juice, sweetened, raspberry vinegar - - -	25 % <i>ad val.</i>	
69	Tea - - -	Per lb.	0 4
CLASS V.—DRUGS, MEDICINES, CHEMICALS, AND DRUGGISTS' SUNDRIES.			
70	Acid, acetic, n.o.e., containing not more than 30 per cent. of acidity - - - For every additional 10 per cent. of acidity or fraction thereof - - -	" "	0 1½ 0 0½

No.	Articles.	Rates of Duty.
CLASS V.—DRUGS, MEDICINES, CHEMICALS, AND DRUGGISTS' SUNDRIES— <i>cont.</i>		
71	Acid, tartaric - - - - -	Per lb. s. d. 0 1
72	Baking-powder, yeast preparations, and other ferments - - - - -	20 % <i>ad val.</i>
73	Chemicals n.o.e., including photographic chemicals and glacial acetic acid - - - - -	20 % <i>ad val.</i>
74	Cream of tartar - - - - -	Per lb. 0 1
75	Drugs and druggists' sundries and apothecaries' wares, n.o.e. - - - - -	20 % <i>ad val.</i>
76	Essences, flavouring, spirituous, until 1st of February 1896 - - - - -	15 % <i>ad val.</i>
	Essences, flavouring, spirituous, after 1st of February - - - - -	Per liquid gallon 16 0
77	Essences, flavouring, n.o.e. - - - - -	15 % <i>ad val.</i>
78	Eucalyptus oil, in bulk or bottle - - - - -	20 % <i>ad val.</i>
79	Glycerine, refined - - - - -	20 % <i>ad val.</i>
80	Opium - - - - -	Per lb. 40 0
81	Patent medicines - - - - -	40 % <i>ad val.</i>
82	Proprietary medicines, or medicaments (1) bearing the name of the proprietor on label or package; (2) bearing a prefixed name in the possessive case; (3) n.o.e., prepared by any occult secret or art - - - - -	40 % <i>ad val.</i>
83	Saccharine, except in the form of tabloids or tablets - - - - -	Per oz. 1 6
84	Sarsaparilla - - - - -	25 % <i>ad val.</i>
85	Soda, carbonate and bicarbonate - - - - -	Per cwt. 1 0
86	Soda, crystals - - - - -	” 2 0
87	Tinctures and medicinal spirits of any recognised pharmacopœia, containing more than 50 per cent. of proof spirit - - - - -	Per lb. 1 0
88	Tinctures and medicinal spirits of any recognised pharmacopœia, containing less than 50 per cent. of proof spirit - - - - -	” 0 6
CLASS VI.—CLOTHING AND TEXTILE GOODS.		
89	Apparel and ready-made clothing, and all articles n.o.e., made up wholly or in part from textile or other piece goods - - - - -	25 % <i>ad val.</i>
90	Apparel made by British or foreign tailors, dress-, mantle-, or jacket-makers, to the order of residents in the Colony, and intended for the individual use of such residents, whether imported by the residents themselves or through an importing firm - - - - -	40 % <i>ad val.</i>
91	Blankets - - - - -	20 % <i>ad val.</i>
92	Collars and cuffs, of paper or other material - - - - -	25 % <i>ad val.</i>
93	Cotton counterpanes - - - - -	20 % <i>ad val.</i>
94	Cotton piece-goods, to include turkey twills, dress prints (hardspun and plain-woven), where the invoice value does not exceed 4d. the yard and cotton piece-goods n.o.e. - - - - -	10 % <i>ad val.</i>
95	Cotton piece-goods, namely, tapestry, cretonnes; chintz, art crêpe, and serges; velveteens, velvets, and plushes, all kinds; damasks, moquette, sateens, linenettes, crepons, crimps, zephyrs, ginghams, turkey twills, prints, printed cottons, piques, vestings, quiltings and marcelles, muslins of all kinds, nets, window-nets Hollands, curtains and blinds, diapers, ticks including coloured Belgian, towellings, laces - - - - -	20 % <i>ad val.</i>

(To be continued.)

XVI.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

RUSSIA.

Tariff Classifications.

Note.—Poud = 36 lbs. avoirdupois. Russian Pound = .902 lb. avoirdupois. Rouble (Gold) = 3s. 2d.

Despatches, dated 27th November and 17th December last, have been received at the Foreign Office from Mr. W. E. Goschen, Her Majesty's Secretary of Embassy at St. Petersburg, stating that by a recent order of the Department of Customs the following classification of goods has been notified:—

1. Parts of machines and apparatus liable to duty jointly with machines, shall be recognised as imported with the latter, though cocks, injectors, oil-cups, whistles, mono and other meters, pipes, and belting of various materials may be packed separately, and if it shall be certified by an expert that these articles are immediately adjusted for mutual connection with the separate parts of the machines and constitute a necessary and an essential portion of the machines themselves.

2. Ground wine stone to be cleared under section 112 (duty, R. 1.50, gold, per poud gross).

3. The accidental presence of an inconsiderable quantity (not exceeding 2 per cent.) of woollen rags in ordinary rags shall not make the whole parcel dutiable as woollen rags. Such article, with the small admixture of woollen rags above mentioned, shall be admitted free of duty under section 176, letter *a*, point 1. In each case, however, the Board of Customs shall determine whether the rags are dutiable or otherwise.

4. Pineapples in juice, in hermetically closed tin cases, and containing a larger quantity of sugar than the ripe fruit in its natural state, and other improving condiments, to be cleared under section 24, point 1. (Duty, R. 9.60, gold, per poud.)

Watch fittings of every kind, not put together, to be cleared under section 171, point 6. Duty, 50 copecks gold per Russian pound.

Cotton tissues, bleached and raw, with designs or stripes executed by the introduction of a few additional coloured silken threads, to be cleared under corresponding points of section 187,

with an addition of 50 per cent., in accordance with Note 2 to sections 187 and 188. Duty from 35 copecks gold per pound to R. 1·35, gold, plus 50 per cent.

Musical boxes, playing one, two, three, or more tunes, wound by a key or not wound by the hand, to be cleared under section 172, point 4, notwithstanding that in form or dimension they may be meant merely for the amusement of children. Duty, 10 copecks gold per Russian pound, weight of cases included.

Mercery articles made of copper, copper alloys, cast-iron, iron, steel, tin, lead, and zinc, gilt or silvered, but without admixture of other metals, to be cleared under section 149, point 3. Duty from 65 copecks, gold, plus 30 per cent. to R. 1·30, gold, plus 30 per cent. per Russian pound.

RUSSIA—ROUMANIA.

The Navigation of the Pruth.

A despatch, dated December 2 last, has been received at the Foreign Office from Mr. W. E. Goschen, Her Majesty's Secretary of Embassy at St. Petersburg, enclosing translation of the text (taken from the "Journal de St. Petersburg") of a Convention concluded between Russia, Austria-Hungary and Roumania on March 2, 1895, containing certain modifications of the stipulations of December 15, 1866, with regard to the navigation of the Pruth.

The above text may be seen on application at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, S.W., between the hours of 11.0 and 5.0.

SWEDEN.

Tariff Changes.

With reference to a note on pp. 426-27 of the "Board of Trade Journal" for April 1895, a despatch has been received at the Foreign Office from Sir Spenser St. John, Her Majesty's Minister at Stockholm, enclosing copy and translation of the Decree referred to in the above notice, from which it appears that the increased rates of duties on cement, copper wire, electric cables, and electric lamps were not to come into force till the 1st January 1896, and that the articles on which the higher rates were to come into force on the 4th April last were the following:—

Articles.	Weight.	Amount.
Bread:—		
Finer small breads, biscuits, cakes, ginger-breads, and several such sorts which cannot be classed under confectionery, the weight of their inmost coverings included - - - -	Per 1 kilo.	öre. 30
Other sorts - - - -	„ 1 „	6·5
Grain:—		
Of corn. <i>V. corn.</i>		Kroner öre.
Of rice and rice-meal - - - -	„ 100 kilos.	6 50
Other sorts - - - -	„ 1 kilo.	17
Flour:—		
Of corn. <i>V. corn.</i>		
Of arrowroot and other vegetables which cannot be classed under corn nor be referred to as medicines - -	„ 100 kilos.	6 50
Rice, unshelled or paddy - - -	„ 100 „	3 70
Rice, grain or flour. <i>V. grain.</i>		
Corn:—		
Unground:—		
(a.) Rye, wheat, corn, maize, with peas and beans - - - -	„ 100 „	3 70
(b.) Oats and vetch - - - -	Free.	—
(c.) Malt, also crushed - - - -	„ 100 kilos.	5 00
(d.) Other sorts not specified—ground -	„ 100 „	3 70
Flour and grain of all sorts - - -	„ 100 „	50

BELGIUM.

Tariff Modifications.

A despatch, dated 30th December last, has been received from the Foreign Office, inclosing extract from the “*Moniteur Belge*” of the 29th December last, from which it appears that by Royal Decree the following articles of the Belgian Customs tariff have been transferred from the category of “*Mercerie*” and “*Quincaillerie*” to that of “*Produits divers pour l’industrie*,” and will pay a duty of 5 per cent. *ad valorem*, viz.:—Mountings for fans, and metal-work in small pieces, spangles, &c., used in the making of dress ornaments.

“*Le Journal des Tarifs et Traités de Commerce*” states, that by virtue of three decrees of the 9th November last the following articles are transferred from the category of *Mercerie et Quincaillerie* and classed as *Produits divers pour l’Industrie*, and pay a duty of 5 per cent. *ad valorem*, viz.:—

1. Cork wheels for glass polishing. 2. Capsules of tin lead, or of tin and lead alloy, for the fastening of bottles, pots, or other recipients. 3. Metal accessories (buckles, hooks, sliding rings of umbrellas, and hooks, little chains, handles, angle tips, little ornaments, &c.) for fixing on saddlery, trunks, and portmanteaux, or similar travelling objects.

FRANCE.

Tariff Decisions.

The following tariff decisions have recently been published by the French "Direction Générale des Douanes":—

French colonial sugars exported from the colonies to the mother-country are entitled to an allowance for waste (in manufacture) equivalent to the average of the excess yield obtained by the home manufacturers during the preceding sugar year. This average having been 21·73 per cent. during the season 1894-95, an allowance for waste of 21·73 per cent. will be granted to French colonial sugars exported from the colonies to the mother-country from 1st September 1895 to 31st August 1896.

The construction bounty of the law of 30th January 1893 cannot be granted for ships constructed by private firms for the State or for public departments.

A duty of 50 centimes per ton on Algerian phosphates of lime is levied at the time of exportation.

SWITZERLAND.

Duty on Raisins.

According to the "Moniteur Officiel du Commerce," the Federal Council on the 3rd December last arrived at the following decision with regard to the duty on raisins, to come into force immediately:—

The note to 398*a* of the Swiss Customs Tariff is suppressed, and replaced by the following—

Note to 396.—Currants and raisins in the bunch, not of table quality, as well as all raisins (except Malaga raisins) the packing of which does not comply with the regulations of 398*a*, must pay the duty of 20 francs, according to No. 396, and are additionally liable to a monopoly tax of 4.20 frs. per quintal.

Note to 398a.—Malaga raisins, however packed, pay 3 francs per quintal, the same as all other sorts of raisins, not denominated under No. 396, provided that these latter be imported in boxes or drums of a gross weight not exceeding 5 kilogs.

PORTUGAL.

Decree concerning the Importation of Foreign Grain.

The "Bolletino di Notizie Commerciali" publishes the following decree of 14th November last, taken from the official "Diario do Governo":—

Art. 1. The decree, bearing the dates of 26th September 1893 and 12th February 1895, remains in force till 31st July 1896, with modifications specified in the following articles:—

Art. 2. The importation of foreign grain, as authorised by this decree, is fixed for the current agricultural year at 140,000,000

kilogs., 135,000,000 of which are for bread and 5,000,000 for alimentary pastes.

Art. 3. The amount of grain to be distributed among the factories, windmills, and water mills, entered on the register, will be regulated according to tables attached to this decree.

Art. 4. The import duty on foreign grain is fixed at 20 reis per kilog., without any additional duty whatsoever.

Art. 5. In addition to the amount referred to in Article 2 above, the military bakeries are authorised to clear as much as 1,000,000 kilogs. through the Customs for the manufacture of bread for the army up to 31st July 1896.

PORTUGAL—WEST AFRICAN POSSESSIONS.

Regulations for Facilitating Trade.

The Portuguese "Diario do Governo" for the 20th of November publishes two Royal decrees, by which trade in the Portuguese Possessions of Western Africa is facilitated. The former of these decrees further extends, in the colonies of Cape Verde, Guinea, San Thomé and Príncipe, Ambriz, as well as Loanda with Benguella and Mossamedos, the exemptions from tonnage dues granted to certain categories of ships. These dues are no longer levied on:—

- a. Warships of any nationality.
- b. Pleasure boats, the property of legally formed and recognised companies.
- c. Trading vessels, which by close contracts, or by international agreements and special legislation, are freed from the payment of the said dues.
- d. Portuguese and foreign ships, exclusively employed in fisheries, sloops, undecked vessels of any size, coasting vessels under 20 tons.
- e. Vessels entering, not for trade, but under stress of weather.
- f. Vessels that enter and clear in ballast, and vessels entering with cargo that clear in ballast, in order to be repaired in another port, even though it be a foreign port, and thence to return to reload their cargo.
- g. Ships which only enter to renew their provisions, such as steamers to take in coal; and ships of all kinds:—
 - h. If they enter and leave without loading or discharging cargo, or if, after loading or discharging, they are considered unseaworthy and are destined to be broken up.
 - i. If they merely bring shipwrecked persons, prisoners, or such persons at the request of the Consul or other officials.
 - j. If they enter the port merely in order to take goods from ships that have been obliged by stress of weather or other causes to discharge them.
 - k. If they are only carrying bullion or specie.

In the colony of Cape Verde, moreover, release from tonnage dues continues to be granted to ships which only take in or dis-

charge small packages, even when these are liable to duty, provided such packages from any one vessel shall not exceed 10 cbm. in bulk, and also such ships as take on or put off not more than four passengers each.

The second decree ordains that in the colony of Cape Verde a ship shall for the future no longer be considered as trading at a port if it only renews its supply of food or other necessities of life for the use of those on board, or carries on small barter which does not exceed 50,000 reis, or takes on board or lands travellers. In such cases the limit of the duty payable to the Custom-house officers shall be 1,000 reis (in accordance with No. 2 of Table II. in the Customs tariff of April 16, 1892), instead, as has hitherto been the case, of 9,000 reis (in accordance with No. 1 of the said table).

SPAIN—CUBA.

[Regulations for Goods shipped to Cuba]

A despatch, dated 16th December last, has been received at the Foreign Office from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, enclosing copy and translation of a Royal Order, published in the Official Gazette of the 9th December last, stating the requirements which must hereafter be observed in the designation of goods sent to Cuba. The measure is stated to have been rendered necessary to prevent fraud caused by uncertainty of the terms employed in ships' manifests.

The translation of the above Royal Order, which is addressed to the Governor-General of the island of Cuba, is as follows:—

In view of an official communication received from the Intendencia General de Hacienda of that island, No. 2,358 of 20th October last, in which he informs me of an order he had issued with the object of preventing the concealment of goods subject to the highest duties, by vaguely entering their class and kind in the manifest.

In view of numbers 3 and 5 of Article 40 of the Customs ordinances in force:

Considering that, for the convenience of the public and for the legitimate interests of the Treasury, the order of the Intendencia is based on valid grounds, inasmuch as the vagueness employed in the manifests in the designation of goods has given rise to great abuse—verification being rendered difficult and fraud being facilitated; taking into consideration, at the same time, the mode of procedure observed in cases 3 and 5 of Article 40 of the Ordinances in force:

Considering, in effect, that in these cases and in said precepts it is prescribed that the manifests should indicate the class and kind of the goods, that packages containing yarns, tissues, alcohols, cinnamon, cocoa, opium, tea, hats, and boots and shoes should be entered in the manifest separately and not included in other packages containing miscellaneous goods, and that should a

package contain various goods and any of those mentioned in the preceding paragraph, the class and weight of the latter must be given in detail in the manifest.

Considering that these precepts of the Ordinances have not proved sufficient to prevent confusion in the entering of goods in the manifests, owing, no doubt, to a liberal interpretation of the same, not in harmony with the spirit with which they were framed, and which is that of clearly expressing the class and number of the goods, and that packages of the most important goods with respect to Customs duties should not be mixed with others.

Considering that to prevent misunderstandings in bonâ fide commerce it is necessary to issue instructions of a general character to put a stop to this state of affairs, which is the source of continual claims and a pretext for the commission of fraud, giving expression to the determination of the Intendencia in this sense.

His Majesty the King, and in his name the Queen Regent of the Kingdom, has resolved as follows:—

1. That goods shall not be designated by names other than those employed in the tariff in force.

2. That the same generic name shall not be employed to designate goods appearing in different numbers of the tariff.

3. That packages containing yarns, tissues, alcohols, cinnamon, cocoa, opium, tea, hats, and boots and shoes be declared separately in the manifest, and that under no circumstance should any other goods be mixed with them in the same package.

4. That any infringement of the disposition of the two preceding precepts shall be punished conformably to case 2 of Article 149 of the Customs Ordinances in force.

5. That the strictest compliance in every respect of the precepts contained in Article 40 and following be observed, under pain of the penalties prescribed in the Ordinances.

6. That the order of the Intendencia of October 18th last should be considered as amplified in the sense expressed above, the object of which is approved.

ITALY.

Importation of Animal Products.

A report, dated December 10 last, has been received at the Foreign Office from Mr. Charles A. Payton, Her Majesty's Consul at Genoa, transmitting copies and translations of two Ordinances recently communicated by the local authorities at Genoa concerning the importation of ham, bacon, and other preserved swine-flesh, and that of cattle, sheep, and various animal products. It will be observed that in Ordinance No. 11 Great Britain is not included in the countries from which the importa-

tion of preserved swine-flesh is permitted. The following is the text of the translations of the Ordinances referred to:—

Inland and Maritime Health Ordinance No. 11.

Art. 1. The prohibition of the importation into the kingdom of swine-flesh salted, smoked, or in other way preserved, remains in force against all countries except the following : Austria-Hungary, the German Empire, Switzerland, Denmark, France, and the United States of America.

Art. 2. Such flesh arriving from the above-mentioned countries will have to be accompanied by a sanitary certificate of origin to be issued by the competent local authorities, and to be countersigned by the Italian Consul or consular agent, residing or having jurisdiction in the place from which the flesh is sent.

Maritime Health Ordinance No. 13.

Art. 1. The prohibition is maintained in force of the importation of—

(a.) Cattle and sheep from the following countries:—

European and Asiatic Turkey.

Island of Cyprus.

Egypt.

Bombay.

Russian ports in the Black and Azoff seas.

Bulgaria.

Greece.

Somaliland.

Zanzibar.

(b.) Cattle from Malta, from whence the importation of sheep is permitted, on condition of their being submitted to a medical examination at the port of destination, at the expense of the parties interested.

(c.) Swine from European and Asiatic Turkey, from Cyprus, Egypt, and the United States of America.

Art. 2. From all countries against which the prohibition of importation of cattle and sheep exists, the introduction into the kingdom of salted hides will be forbidden, with the exception of the island of Malta.

Art. 3. With regard to Asiatic Turkey, Somaliland, and Zanzibar, the prohibition of importation of untanned hides, unwashed wool, bones, horns, hoofs, &c., and of other bovine and ovine products, is maintained in force.

Art. 4. From all other States the introduction into the kingdom of cattle and of the above-mentioned products will be permitted, on the condition that both the animals and such products be accompanied by the sanitary certificate of origin to be issued by the competent local authorities, and to be countersigned by the Italian Consul, or consular agent having jurisdiction in the district from which the animals or products are originally sent.

Art. 5. Salted guts and wool, washed or calcined, can be freely imported into the kingdom, whatever may be their place of origin

Classification of "Locomobile" and "Locomotive."

A despatch, dated December 10th last, has been received at the Foreign Office from Sir Francis Clare Ford, Her Majesty's Ambassador at Rome, enclosing explanation received from the Italian Ministry for Foreign Affairs of the terms "locomobile" and "locomotive," in relation to tariff classification. It would appear that a "locomobile" is a steam-engine which cannot be propelled along a road by its own steam power, but which has to be dragged along by horses, &c.; whilst a "locomotive" is a steam-engine which can be propelled along a road (or rails) by means of steam power communicated to its wheels from the engine itself. Both railway "locomotives" and road "locomotives" are thus comprised in a single category, and it has been recently decided that ploughing machines of the Fowler system are to be assigned to the same category as road "locomotives."

UNITED STATES.

Disinfection of Hides.

A despatch, dated 12th December last, has been received from the Foreign Office, inclosing copy of a circular issued by the United States Treasury Department, containing the following revised regulations for the disinfection of hides:—

"Representations having been made to this Department that the process promulgated in its letter of November 14, 1895, for the disinfection of hides of neat cattle intended for shipment to the United States, is attended with injury to the hides, the following method, which is intended solely for the protection of cattle in this country, and which is suggested by the Department of Agriculture, is adopted hereby, and will be required in all cases of shipments of such hides, when not dry-salted or arsenic-cured, from the countries of Europe, Asia, Africa, Australia, and South America, viz.:—

"Dry hides which have been salted or arsenic-cured may be accepted as having been disinfected by the process of curing, and need not be submitted to any further treatment. Dry hides which have not been salted or arsenic-cured should be disinfected. Disinfection with sulphur dioxide may be accepted in case a room is provided which can be tightly closed, and also in case the bundles of hides are undone and each hide suspended separately from the ceiling in such a manner that there may be free circulation of the sulphur fumes, and that all parts of the surface may be acted upon. There should be at least 4 lbs. of sulphur burned to each 1,000 cubic feet of air space, and the room should be kept closed and the hides subjected to the sulphur dioxide for six

hours ; or the dry hides may be immersed in a 4 per cent. solution of carbolic acid, or a 1 to 1,000 solution of bichloride of mercury, until they are thoroughly wet with the disinfectant. Fresh or moist hides, whether salted or not, should be disinfected by immersion in a 5 per cent. solution of carbolic acid, or a 1 in 1,000 solution of bichloride of mercury.

“ It is further directed that hides of neat cattle, other than dry-salted or arsenic-cured, the product of the countries above named, will require disinfection as above whenever they shall be shipped *via* the ports of any other country ; and that hides, other than dry-salted or arsenic-cured, the product of any country not named above, if transhipped and actually landed at ports in any of the countries named, will require disinfection.

“ It should be understood that the regulations herein provided do not in any way modify or affect any regulations concerning disinfection issued under the quarantine laws of the United States.”

Proposed Duty on Bottles.

A despatch, dated December 20 last, has been received at the Foreign Office from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, stating that it has been suggested to Congress that Customs duties should be imposed in the United States on bottles, which are now admitted free.

It is stated that malt and aerated liquors are largely exported to the United States from Belfast in bottles chiefly imported from England and Germany, and he recommends that restrictions should be imposed on bottles from those two countries.

BRAZIL.

Revised Tariff.

A telegram, dated January 3 last, has been received at the Foreign Office from Mr. Phipps, Her Majesty's Minister at Rio Janeiro, stating that the revised tariff of Brazil doubles all primitive duties, suppressing 50 and 60 per cent. ; additional duties to take effect for goods shipped before January 1. Generally this raises duties by one-third, but there are certain specific duties raised or reduced. Fancy cottons are raised 44 per cent. No differential duties on British commerce are recognisable.

BRITISH INDIA.

Tariff Decisions.

Information has been received from the India Office with reference to the following modifications in the Customs tariff of British India :—

Cotton umbrellas in No. 111 of Schedule IV, of the Indian Tariff Act are to be assessed to duty *ad valorem*.

Detonators should be treated as included in "all other descriptions" of explosives specified in No. 10 of Schedule IV., and charged duty at 5 per cent. *ad valorem*.

The import duty on goods received by foreign parcel post at all ports in India, which cannot be delivered in India and are re-exported beyond the limits within which they are liable to Customs duty, will be refunded.

The duty on spikes (commonly known as dogspikes) of iron and steel, other than the spikes mentioned in No. 93 of Schedule IV. of the Indian Tariff Act, is reduced from 5 per cent. to 1 per cent. *ad valorem*.

All dyed or coloured cotton twist and yarn of count No. 20 and lower counts is exempted from the whole of the Customs duty in excess of $\frac{1}{2}$ per cent. leviable thereon on importation into British India.

Machinery (and component parts thereof), as defined in Article 14, which is intended for the manufacture of tea in all its stages, from the drying of the leaf to its packing for the market inclusive, is exempted from the import duty leviable thereon under No. 14 of Schedule IV. of the Indian tariff.

"Iron screws pay an import duty of 5 per cent. *ad valorem* under No. 13 of Schedule IV. of the Indian Tariff Act, 1894."

NEW SOUTH WALES.

Modifications in the Customs Tariff.

A communication has been received from the Agent-General for New South Wales stating that a Bill to amend the Customs tariff of that colony was introduced into the Legislative Council on the 10th of September 1895, and came into force on the 1st January last.

Paragraphs Nos. 4 and 5 of the Bill are as follows:—

4. Upon all goods specified or mentioned in Schedule A of the said "Customs Duties Act of 1892"* which are not specified or mentioned in Schedule A of this Act, and which are not specified or mentioned in the next following section of this Act, the respective duties authorised to be levied and collected by the said Act of 1892 shall continue to be levied and collected until and including the 31st December 1895, and no longer. And upon all goods liable under the said Act of 1892 to *ad valorem* duties, such duties shall be levied and collected until and including the said 31st December 1895 and no longer. And for the purpose of levying, collecting, and enforcing payment of all duties mentioned and referred to in this section, the provisions of the said Act of 1892 shall be applied, notwithstanding anything contained in the last preceding section of this Act.

5. The duties authorised to be levied and collected by the "Customs Duties Act of 1892,"* upon butter (which shall,

* A full statement of the duties under this Act was published in the Board of Trade Journal for February 1893. See pp. 186 *et seq.*

however, be levied at the rate of one penny per pound only), cement, plaster, and hydraulic lime, cheese, bacon, and ham, chicory, dandelion and taraxicum, roasted, ground, or mixed with any other article, chocolate, plain or mixed with any other article, chocolate creams, cornflour and maizena, cocoa, prepared, paste, or mixed with any other article, coffee, roasted, ground, or mixed with any other article (except coffee essence), fish, dried, preserved, or salt, iron, galvanised, in bars, sheet, or corrugated, galvanised wire netting, milk, condensed or preserved, sporting powder, shot, rice, salt, timber, dressed and undressed, doors, sashes, and shutters, respectively, shall continue to be levied and collected under the said Act until and inclusive of the 30th June 1896. And for the purpose of levying, collecting, and enforcing payment of all duties mentioned in this section, the provisions of the said Act of 1892 shall be applied, notwithstanding anything contained in the second section of this Act.

The modifications introduced in the tariff are incorporated in the following schedule :—

SCHEDULE A.
Permanent Tariff.

Article.	Amount of Duty.
	<i>s. d.</i>
Spirits.—On all kinds of spirits and spirituous compounds imported, and not otherwise enumerated - - - -	Per proof gall. 14 0
No allowance beyond 16·5 shall be made for the under-proof of any spirit of a less strength than 16·5 under proof.	
Case spirits.—Contents of two, three, four, or five gallons shall be charged—	
Two gallons and under as two gallons.	
Over two gallons and not exceeding three, as three gallons.	
Over three gallons and not exceeding four, as four gallons.	
Over four gallons and not exceeding five, as five gallons.	
Bitters, essences, fluid extracts, sarsaparilla, tinctures, medicines, infusions, and toilet preparations containing—	
Not more than 25 per cent. of proof spirit - - -	Per gallon 3 6
Not more than 50 per cent. of proof spirit - - -	7 0
Not more than 75 per cent. of proof spirit - - -	10 6
If containing more than 75 per cent. of proof spirit	14 0
If containing spirit over proof, to be charged as spirituous compounds - - - -	Per proof gall. 14 0
Methylated spirit - - - -	Per gallon 0 1
Perfumed spirits, perfumed waters, Florida water, and bay rum - - - -	Per liquid gall. 20 0
Wines, sparkling.—For 6 reputed quarts or 12 reputed pints - - - -	— 10 0
Other kinds - - - -	Per gallon 5 0
Other kinds.—For 6 reputed quarts or 12 reputed pints - - - -	— 5 0
Beer, ale, porter, spruce, or other beer, cider and perry—	
In wood or jar - - - -	Per gallon 0 6
In bottle - - - -	0 9
For 6 reputed quarts or 12 reputed pints - - -	0 9

Article.	Amount of Duty.	
Tobacco.—Delivered from ship's side or from a Customs bond for home consumption—manufactured, unmanufactured, and snuff	Per lb.	s. d. 3 0
Unmanufactured, entered to be manufactured in the Colony, at the time of removal from a Customs bond, or from an importing ship to any licensed tobacco manufactory for manufacturing purposes only into tobacco, cigars, and cigarettes	"	1 0
Sheepwash	"	0 3
Cigars and cigarettes (including wrapper in latter case)	"	6 0
Opium and any preparation thereof	"	20 0

Diminishing Duties.

Candles, per lb., or reputed package of that weight, and so in proportion for any such reputed weight, night lights, and stearine	"	0 1
From 1st July 1897	"	0 0½
From 1st July 1899	Free.	
Oil, kerosene, naphtha, and gasoline	Per gallon	0 6
From 1st July 1896	"	0 3
From 1st July 1897	Free.	
Oils, except linseed oil (raw or boiled), fish and seal oils, black whale, cocoanut, sperm, palm, and essential oils	Per gallon	0 6
From 1st July 1896	"	0 3
From 1st July 1897	Free.	

Sugar, Refined.	Sugar, Raw, and Solid Glucose.	Molasses and Treacle; Glucose, Liquid and Syrup.	
Per Cwt. s. d.	Per Cwt. s. d.	Per Cwt. s. d.	
6 8	5 0	3 4	
5 4	4 0	2 8	From 1st July 1897.
4 0	3 0	2 0	From 1st July 1898.
2 8	2 0	1 4	From 1st July 1899.
1 4	1 0	0 8	From 1st July 1900.
Free	Free	Free	From 1st July 1901.

Article.	Amount of Duty.	
Biscuits	Per lb.	s. d. 0 1
From 1st July 1898	"	0 0½
From 1st July 1900	Free.	
Confectionery (including cakes, comfits, liquorice, liquorice paste, lozenges of all kinds, cocoanut in sugar, sugar-candy, succades, and sweetmeats)	Per lb.	0 2
From 1st July 1898	"	0 1
From 1st July 1900	Free.	
Fruits, dried, candied, and prunes (exclusive of dates)	Per lb.	0 2
From 1st July 1898	"	0 1
From 1st July 1900	Free.	
Jams and jellies, per lb., or reputed package of that weight, and so in proportion for any such reputed weight	Per lb.	0 1
From 1st July 1898	"	0 0½
From 1st July 1900	Free.	
Preserves and canned fruits, fruits boiled, peel—drained, or dry	Per lb.	0 1
From 1st July 1898	"	0 0½
From 1st July 1900	Free.	

XVII.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

PROJECTED EXHIBITION OF ARTISTIC FURNITURE AT WARSAW.

A despatch, dated December 13 last, has been received at the Foreign Office from Mr. Henry Grant, Her Majesty's Consul-General at Warsaw, enclosing a letter received from the committee of the Warsaw "Museum of Handicrafts and Decorative Art," stating that an exhibition of artistic furniture is to be opened in Warsaw on the 1st April next, and enclosing a circular, of which the following is a translation:—

"The committee of the Museum of Handicrafts and Decorative Arts of Warsaw, with the object of developing industry, and at the same time educating the public taste by familiarising it with the principal features of articles of the best periods of construction, has decided to open, on the 1st April 1896, at Warsaw a special exhibition of artistic furniture, to include the 10 following sections:—

1. Gothic style.
2. Epoch of the renaissance in Poland and at Dantzic.
3. Italian renaissance.
4. French renaissance (Henry II. and Louis XIII.).
5. Style Louis XIV.
6. Style Louis XV.
7. Style Louis XVI.
8. French Directory and Empire.
9. Modern eclectic style.
10. Podhalian style (construction and decoration in wood as carried out in the Carpathians).

"The success of this undertaking depends on the support given to it by manufacturers and the public. The committee feel persuaded that all manufacturers whose goods may be classed in the above-named categories, of other countries as well as of our own, as well as amateurs having in their collections articles of these different epochs, will be willing to give their support by exhibiting, not only complete sets of furniture, but also accessory objects of art, such as draperies and other ornaments connected with the said furniture.

"As far as foreign exhibitors are more particularly concerned, the committee begs to inform them—

"1. That the Warsaw Museum hopes to be able to arrange that articles sent for the museum from abroad shall be admitted into Russia free of duty.

"2. That the space necessary for the exhibition of their goods is offered gratis to foreign exhibitors.

"3. That as far as artistic furniture is concerned, the towns of Warsaw and Lodz, with a collective population of 700,000

inhabitants in two great centres of commerce and industry, must be considered as important places of trade, where the furniture industry is yet but little developed. The foreign manufacturer has, therefore, every inducement to send specimens of his goods, with albums, catalogues, and price lists.

“ 4. That the Warsaw Museum will make a certain number of purchases as models for its collections, which will afterwards serve as a permanent advertisement for those firms who exhibited the goods.

“ 5. That the Warsaw Museum will defray the expenses of carriage of articles for exhibition from the station of Thorn (Railway of Western Prussia) to Warsaw.

“ Anyone who intends to take part in the exhibition is begged to forward to the ‘Musée des Metiers et Arts Decoratifs, 66, Rue Faubourg de Cracovie, Warsaw,’ a declaration specifying the articles available for exhibition, and adding, where possible, drawings or photographs, with dimensions and approximate weights of the various articles.

“ The committee will then make a selection of articles most in harmony with the general plan of the projected exhibition, and will forward the necessary instructions for their despatch.”

The circular is signed by the president of the museum, the president of the committee, and the general secretary.

Mr. Grant, in his covering letter, states that it would be a good opportunity for English manufacturers of high-class furniture to make themselves known in Poland, and, in his opinion, they would probably find many customers in that country, owing to the indifferent workmanship of the native manufacturers and to the exorbitant prices they charge for anything that requires skilled labour.

IMPORTATION OF SHODDY INTO SWEDEN.

A despatch, dated December 11 last, has been received at the Foreign Office from Sir Spenser St. John, Her Majesty's Minister at Stockholm, stating that the Swedish Government has acceded to the representations made to it for a modification of the existing regulations with regard to the sanitary certificates hitherto required on the importation of shoddy, and enclosing a copy and translation of the decree which has just been issued, in accordance with which these certificates will no longer be required on the importation of dyed shoddy.

The reason why an exception has been made with regard to uncoloured shoddy is that, in the process of manufacture dyed shoddy is subjected to so high a temperature as to exclude any possibility of the importation of germs of infection; and, further, it is stated that there is considerable difficulty in distinguishing between uncoloured shoddy and what is known in Sweden under the name of “lumpen”—a kind of rough felt.

THE TRADE OF GERMAN EAST AFRICA FOR 1894.

A despatch, dated 5th December last, has been received at the Foreign Office from Mr. M. Gosselin, Her Majesty's Secretary of Embassy at Berlin, enclosing copy of the last issue of the official "Deutsches Kolonialblatt," from which it appears that the total value of the imports into German East Africa during the year 1894 amounted to 2,913,317 dols., or 6,198,546 rupees 52 pesas. During the year under review the rupee averaged 1·15635 marks, so that the imports were valued at 7,167,689 marks 59 pfennigs, equal (at M. 20·30 to the pound sterling) to 353,080*l*.

The value of the exports amounted to 1,982,272 dols., or 4,217,600 rupees, equal, at the same exchange, to 4,877,021 marks 76 pfennigs, the equivalent in English money of 240,242*l*.

As compared with the statistics of 1893 the imports show an increased value of 125,000 dols.; the exports a decrease of 35,000 dols.

The "Kolonialblatt" points out that in 1893 the rupee averaged 1 mark 30 pfennigs, while in 1894 the average value fell to 1 mark 15 pfennigs. On converting, then, the above returns into German currency, the 1894 imports show a less value of 545,000 marks, the 1894 exports a less value of 703,000 marks, as compared with the German currency value of the previous year's returns.

Taking into consideration last year's commercial crisis, caused by the locusts plague and consequent famine, the official organ does not regard these figures as "unfavourable."

The export of food stuffs, for instance, show a notable falling off, that of rice alone showing a decrease of 105,000 dols. On the other hand, the value of rice imported was increased by 148,000 dols.

The purchasing power of the natives shows a corresponding decrease, notably in the falling off of the demand for cotton goods.

The following imports show an increase in 1894 as compared with the previous year:—Tools, petroleum, meal, coffee, tea, sugar, tobacco, butter, milk, cheese, wine, and beer.

On the other hand, all cotton goods, soap of all sorts, and gunpowder show a considerable decrease, the importation of gunpowder having fallen from 94,000 dols. to 476 dols.

Turning to the exports, the following articles show an increase:—Matting (*Flechtgraswaaren*), raw india-rubber, hippopotamus-teeth, rhinoceros horns, timber, cocoa-nuts, sesame, and tobacco.

SUPPLIES OF COAL FOR SPAIN.

A despatch, dated 11th December last, has been received from the Foreign Office, enclosing translation of a Royal Decree, published in the Madrid Gazette of December 5 last, and worded as follows:—

The Minister of Marine is authorised to purchase, without the formality of asking for tenders—

1. Cardiff coal for the use of vessels, for a period of two years, in the department of Cadiz, province of Algeciras, and Malaga, in conformity with para. 8, Art. VI. of the Royal Decree of February 27, 1852.

2. Spanish coal, for a period of two years, for coastguard vessels, workshops, tugs, dredgers, and other purposes, in connection with the arsenals, on observance of para. 7 of said article and decree.

NEW CURRENCY IN PORTO RICO.

A despatch, dated 10th December last, has been received at the Foreign Office from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, reporting that a Royal Decree has been published in the "Gaceta de Madrid" of the 9th December, forbidding the circulation in Porto Rico of Mexican dollars. A special coinage will be issued of Spanish dollars, corresponding exactly to the five-peseta piece at present in use. At the proper time a law will be introduced into the Cortes authorising the circulation of this coinage in Spain.

Gold coins from the national mint, and those which circulate in the Spanish dominions from foreign mints, will be a legal tender in Porto Rico with a premium of 20 per cent. over its nominal value.

The foreign coinage at present circulating in Porto Rico will be at once called in and melted down for recoinage, and coins of the legal currency corresponding to that circulating in the Peninsula will be given in exchange, and the necessary amount of bronze coinage will also be introduced.

Mexican dollars will be called in at the rate of 95 per cent. of their nominal value, or 95 cents of the new coinage will be given for each Mexican dollar. The notes created by the law of August 17 will come into use simultaneously with the new coinage at its full legal value of one dollar. The exchange will take place at various places in the island fixed by the Governor-General, six days after the publication of the decree in the island, and it will continue during eight days. During the four following days they will be exchanged at the central offices in the capital, at the same rate of 95 cents to the dollar, if presented in quantities of not less than 120 dollars; half of the money given in exchange will be in the new coinage, and half in the notes before mentioned.

After the periods thus fixed, Mexican dollars will not have legal circulation. They will only be admitted at the state caisses, at the rate of 55 per cent., for 90 days after the exchange has been closed at San Juan de Porto Rico.

The notes referred to will be redeemed as soon as the necessary delay in issuing the new coinage admits of it. It will be carried out at the full nominal value of one dollar for each note, and the

Government reserve the right of withdrawing the notes from circulation, and of giving silver in exchange immediately after giving 10 days' notice, and the operation shall take place in the capital during 15 days. The circulation of the notes is not to continue for more than three months from the date on which the calling in of Mexican dollars at San Juan de Porto Rico has been accomplished, if the number of Mexican dollars in Porto Rico does not exceed 10,000,000.

The Government will place in circulation in Porto Rico gold coinage representing the surplus resulting from the recoinage of the Mexican dollar after paying all expenses connected therewith. These coins will be issued by the state caisses (*cajas de estado*) in the proportion, and from a date fixed by the Government.

THE CATTLE AND MEAT TRADE OF BELGIUM.

A despatch, dated December 20 last, has been received from the Foreign Office, enclosing copy of a memorandum of Mr. G. R. de Courcy Perry, Her Majesty's Consul-General at Antwerp, on the subject of the cattle and meat trade of Belgium. According to this memorandum, in 1880, when the last agricultural statistics were taken, the number of cattle and sheep in Belgium amounted to 1,382,815 and 365,400 head respectively.

The figures quoted below show the exportation and importation of cattle and sheep and of fresh meat during the years 1891 to 1894 inclusive.

	Cattle.			
	1894.	1893.	1892.	1891.
	No.	No.	No.	No.
Importation - - -	55,653	120,810	68,607	104,913
Exportation - - -	1,184	1,102	3,249	4,781
Excess of importation over exportation.	54,469	119,708	55,358	100,132
Sheep.				
	No.	No.	No.	No.
Importation - - -	193,395	199,558	190,136	204,621
Exportation - - -	7	318	8,886	7,276
Excess of importation over exportation.	193,388	199,240	181,250	197,345
Fresh Meat.				
	Kilogs.	Kilogs.	Kilogs.	Kilogs.
Importation - - -	21,104,000	12,953,000	27,239,000	25,407,000
Exportation - - -	19,829,000	12,314,000	18,325,000	17,474,000
Excess of importation over exportation.	1,275,000	639,000	8,914,000	7,934,000

The average market prices of the different kinds of meat per kilogramme (living weight), were as follows, during the years 1889 to 1893 inclusive :—

—	1893.	1892.	1891.	1890.	1889.
	Centimes.	Centimes.	Centimes.	Centimes.	Centimes.
Bullocks - - -	80	86	89	86	83
Bulls - - -	71	73	76	70	66
Cows - - -	71	73	75	68	65
Calves - - -	96	106	100	98	96
Pigs - - -	91	94	84	89	85

The average price of mutton (slaughtered) was 1.23 francs per kilogramme in 1893, and 1.40 francs per kilogramme in 1892. Besides the fixed Customs duty of 4 centimes per kilogramme (living weight) on bullocks, bulls, and steers; of 5 centimes on calves that have not yet lost their milk teeth; of 3 centimes for cows and heifers; of 2 francs per head for ewes, wethers, and rams; and of 1 franc per head for lambs, all animals imported from abroad, either by sea or land, must undergo a sanitary inspection at the expense of the importer. A regulation which came into force on the 30th October last fixes the charge for this inspection at 2 francs per head for cattle showing symptoms which necessitate a special examination for tuberculosis, and 1 franc per head for the same animals if in a healthy condition, 50 centimes for calves of all descriptions under four months of age, and 5 centimes per head for sheep.

No exact information is obtainable respecting the amount of fresh meat consumed in Belgium, but it is estimated that about 500,000 cattle of all kinds are slaughtered in Belgium annually for home consumption.

BRUSSELS INTERNATIONAL EXHIBITION OF 1897.

A despatch, dated 27th December last, has been received from the Foreign Office, transmitting extract from the "Moniteur Belge," containing Royal decrees creating the Board of Administration of the International Exhibition which is to be held at Brussels in 1897.

The exhibition will comprise 14 sections:—1. Fine arts. 2. Social economy. 3. Hygiene. 4. Life saving. 5. Industrial and decorative arts. 6. Lighting and heating. 7. Electricity—traction. 8. Military art. 9. Industrial manufactures—materials, methods, and products. 10. Sporting material. 11. Exercises—popular games. 12. Temporary competitions in agriculture and horticulture. 13. Practical teaching, and the industries and handiwork of women. 14. Trade. Colonies.

TRADE IN BULGARIA.

The following circular has been addressed by the Foreign Office to the Chambers of Commerce of the United Kingdom on the subject of circulars addressed to them by the newly-instituted Chamber of Commerce at Rustchuk, according to which that body appears to arrogate the right of issuing permissions to trade to foreign subjects:—

“The Marquess of Salisbury has learnt that the Chamber of Commerce of Rustchuk, in Bulgaria, has addressed two circulars, dated respectively 3rd August and 29th November last, to the various Chambers of Commerce in this country regarding permissions to trade and applications as to the standing of Bulgarian traders. I am now directed by his Lordship to inform you that the functions contemplated by these circulars are in part beyond the powers of the Chamber. Bulgarian Chambers of Commerce may open registers of traders and of commercial travellers and agents, but such registration is purely voluntary, and Chambers have no authority either to give or to withhold permissions to trade.”

(Signed) GEORGE N. CURZON.

THE UNITED STATES DEPARTMENT OF LABOUR.

A copy of Bulletin No. 1 of the Department of Labour for 1895 has been received at the Foreign Office from the Government of the United States.

This publication contains an account of strikes and lock-outs in the United States, from January 1, 1881, to June 30, 1894, and in the United Kingdom, France, Italy, and Austria in recent years; an article on private and public debt in the United States; digest of recent reports of the Connecticut, Indiana, Michigan, Minnesota, Missouri, and Wisconsin bureaux of labour statistics; a digest of the report by Miss Collet on the statistics of employment of women and girls in England and Wales; and an article on employer and employé under the common law.

MARKET FOR BINDING TWINE IN KANSAS.

A communication, dated 2nd December last, has been received at the Foreign Office from Mr. A. J. Vansittart, Her Majesty's Consul at Chicago, stating that he understands there has been some correspondence between Kansas City and the Belfast Rope-works, Ireland, regarding the matter of binding twines, and that the Belfast Company are about to establish an agent in Kansas.

It is said that ten years ago no binding twine was used in the United States for the purpose of binding oats or wheat in the

field, but since that date large quantities of machinery have been patented for the purpose of binding grain, and it has become necessary to use twine for this purpose.

There appears to be no fibre grown in the United States that can be used for this object, and the trade has grown to such an extent that 150,000,000 lbs. were issued in the United States in 1895, and the quantity is increasing rapidly. There used to be a very heavy duty upon this class of goods, but they are now on the free list, and enter all the United States ports free of duty. The sale at Kansas City shows that one-tenth of the twine, namely, about 15,000,000 lbs., used in the country is handled from that point, and Mr. Vansittart suggests that it might be of interest and serviceable that the attention of the various twine and rope works in Great Britain be called to this, in order that they could open correspondence with the proper parties for placing the British goods upon that market.

Mr. Vice-Consul Burrough at Kansas City is making full inquiries in the matter, and would doubtless be in a position to give more explicit information to the parties interested if called upon to do so.

CENTRAL AMERICAN EXHIBITION AT GUATEMALA

A despatch, dated 18th November last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Consul at Guatemala, enclosing copy and translation of contract, together with photographic plans having reference to the Central American Exhibition, to be opened at Guatemala in the early part of 1897. The contractors have, it is said, purchased the buildings of the late Bordeaux Exhibition for the sum of one million francs, delivered at the port of San José, whence they will be conveyed to the site in Guatemala city at the expense of the Government; and the Construction Company, which is a limited liability company, with a capital of 60,000*l.* in 100*l.* shares, at present standing at 15 per cent. premium, expect to realise a good profit on the contract.

Besides the ordinary courts for exhibits, the exhibition is also to contain amusements of various kinds.

In connection with the exhibition a tramway, on the Deauville system, is also being constructed for a length of 3,800 metres in the outskirts of the city, which will tend to materially increase the value of the property along the route. This property is now being laid out in building plots, and water from the waterworks of Acatan (about six miles from the centre of the city), lately opened, is being conveyed along the route, so that the district is likely rapidly to become an important building estate.

The copy and translation of the contract, and the photographic plans, can be seen at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, between the hours of 11 and 5.

CONTRACT FOR NEW LOAN IN GUATEMALA.

Communications, dated 19th and 20th November last, have been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Consul at Guatemala, transmitting copies and translations of notices which have appeared in the official journal, "El Gualtemaltico," from which it appears that the Government of Guatemala has raised a new loan of 658,500*l.* to carry on the works of the Guatemalan Northern Railway (Guatemala City to Puerto Barrios on the Atlantic Coast), and for the service of the 4 per cent. external debt.

The contract for this new loan is with Messrs. Muller and Thomson, of Hamburg, and the interest is to be 10 per cent., guaranteed on the revenues of the Northern Railway, together with the tax of 20 per cent. in gold now paid by mercantile houses as importation dues. Should this fail to produce sufficient, the Government undertake to make up the difference.

ARRANGEMENT BETWEEN THE "PACIFIC MAIL STEAMSHIP COMPANY" AND THE "COLOMBIAN STEAMSHIP LINE."

With reference to a note on the above subject on p. 564 of the November number of the "Board of Trade Journal"—a report, dated 5th December last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Consul at Guatemala, stating that the arrangement come to between the "Colombian S.S. Line," and the "Pacific Mail S.S. Company" therein referred to has since been cancelled, and that there will be no change in the running of the respective companies.

XVIII.—GENERAL TRADE NOTES.

SHIPPING THROUGH THE SUEZ CANAL.

According to the "Canal de Suez" bulletin for the 12th December last, the number of ships passing through the canal during the month of September 1895 was 270, of a gross tonnage of 951,643 tons, 259 of which showed a tonnage of 912,461 tons. Of these vessels 191 were English (681,815 tons), 27 German (86,009 tons), 19 French (77,020 tons), 16 Dutch (42,087 tons), 5 Austrian (20,547 tons), 5 Italian (17,530 tons), the remainder being composed of 2 Russian, 2 Spanish, 2 Norwegian, and 1 Turkish vessel.

MANUAL OF INFORMATION FOR RUSSIAN STOCK HOLDERS.

A copy has been received at the Board of Trade, of a manual issued by the Russian Ministry of Finance, containing information for Russian Government Stock Holders as to the titles, the original amounts, the amount of capital on January 1, 1896, the dates on which the coupons fall due, &c., of the various Russian loans.

THE LEATHER TRADE OF RUSSIA.

The Russian leather trade is influenced by the foreign markets, especially the American. In 1895 there was a decrease in the importation of leather from abroad. The better supply of fodder also induced farmers to slaughter less cattle, to increase their depleted stocks. There is consequently a considerable demand for leather, according to the "Handels Museum," and prices have gone up, especially in the cases of sole leather and horse-skins, which were quoted at the Nijni-Novgorod market at from 1½ to 2 roubles apiece, whilst at the present time the price of horse-skins at Moscow is seven roubles each. The impending supply of leather from Siberia is attracting considerable attention. In Semipalatinsk, Orenburg, and Troizk a very active trade is done in horse-hides, sheep- and goat-skins. This year the leather is expected to be of a considerably better quality, as there was a better supply of fodder for the animals and fewer cases of disease.

THE PRODUCTION OF PIG-IRON IN RUSSIA.

According to the "Handels Museum" of the 12th December last, the quantity of pig-iron produced in Russia during the first six months of 1895 amounted to 49,800,000 pouds (about 800,000

tons). The amount produced during the whole of 1894 was 74,500,000 pouds (1,197,000 tons), the figures for 1893 being 64,900,000 pouds (1,043,000 tons). The statistics for the different districts where pig-iron is produced show that the production in the Ural was stationary, whilst that in the south and south-west districts shows an increase from 11,900,000 pouds in the first six months of 1894, to 16,600,000 pouds in the corresponding period of 1895. The Polish foundries, also, show a not inconsiderable increase. In view of the fact that new factories are continually being erected in South Russia, a further increase in the production of pig-iron is expected, and the total production for 1895 is estimated at 84,000,000 pouds (1,350,000 tons). The Russian importation of pig-iron, steel, and metal manufactures shows only a slight increase in 1895.

REFORM OF COMMERCIAL EXCHANGES IN RUSSIA.

In April 1895 the Russian Ministry of Finance appointed a committee to elaborate a scheme for the reform of commercial exchanges in Russia. The report which this committee has laid before the Government points to the introduction of extensive alterations in the Russian commercial exchanges. The following information as to the proposals which will shortly come into force has been communicated to the "Handels Museum":—

The exchanges are to be regarded as public institutions and placed under the control of the Trade and Manufactures Department of the Ministry of Finance. They are divided into three classes: those of St. Petersburg and Moscow are to be of the first class, and those of Warsaw, Rostow, Kiev, Odessa, Riga, and Libau of the second, whilst the third class will embrace all the remaining Russian exchanges. It is noteworthy that Nijni-Novgorod is included amongst the exchanges of the third class. The old regulations are to remain in force for exchanges of the third class, whilst reforms are to be effected in the cases of the first- and second-class. The exchanges are to be corporations accessible to merchants of every rank, whilst at present, apart from individual credit institutions, only merchants of the first guild can become members. New members may be received after the committee of the exchange has decided in secret session as to their suitability. But a member of an exchange can be entirely or temporarily suspended, if, for example, he fails to fulfil engagements resulting from business done on the exchange. The Minister of Finance has also the right to order the exclusion of any person from the exchange. A special register of transactions effected on the exchange is to be kept by the exchange notary under the supervision of the committee, who are held responsible for it. Transactions not recorded in the register are in themselves valid, but are not to be considered as "business done on the exchange." Up to the present the number of brokers on each exchange has been limited, but under the new regulations any member may act as a broker.

REVISION OF THE GERMAN COMMERCIAL CODE.

In connection with the reform of civil law in Germany a revision of the commercial code is also in contemplation. A draft of a new commercial code, which, according to the "Handels Museum," has been prepared in the Imperial Office of Justice, is to be considered by a committee consisting of legal authorities and representatives of trade and industry. In the consideration of those parts of the draft code which especially affect the interests of certain classes, particularly the provisions as to commercial assistants and agents, the committee is to have the assistance of representatives of the classes concerned.

PRODUCTION OF SUGAR IN GERMANY.

According to the German official statistics as to the production and taxation of sugar in the German Customs district, there were in operation in 1894-95 467 sugar manufactories, 405 of which were beet-root sugar factories, 56 were sugar refineries, and 6 were for extracting sugar from molasses. These factories, according to the "Handels Museum," used altogether 14,521,030 tons (metric tons of 2,204 lbs.) of beets, and produced 1,692,011 tons of raw sugar, as well as 992,136 tons of sugar of all kinds ready for consumption, the figures for the 1893-94 campaign being 1,270,508 tons of raw sugar and 819,629 tons of sugar ready for consumption. The quantity of beet-roots worked up was the largest ever known in Germany; the 1893-94 figures were 10,644,352 tons. The importation of sugar into Germany was insignificant, and remained at about the same level as in the previous year. On the other hand, considerably larger quantities of sugar were exported in 1894-95 than in the previous year, although the export of molasses has fallen off. In 1894-95, 609,663 tons of raw sugar and 392,857 tons of sugar ready for consumption were exported, as compared with 436,674 tons and 262,610 tons respectively in 1893-94. The consumption of sugar in Germany in the year 1894-95 was computed at 552,695 tons of refined sugar, *i.e.*, 10·7 kilogs. (23½ lbs.) per head of the population, which compares with 10·1 kilogs. in 1893-94 and 9·1 kilogs., the average of the last nine working years.

BRUSSELS AS A SEAPORT.

The "Monde Economique" states that a decree recently signed by the King of the Belgians will have the effect of transforming Brussels into a seaport.

There are but 19 miles of canal to make at a cost of about 1,480,000*l.* When finished, vessels of 2,000 tons will be able to come right up to Brussels.

IMPORTS OF SUGAR INTO TUNIS.

According to a French Consular Report the consumption of sugar in Tunis is steadily increasing. France and Austria are the chief exporting countries. Raw sugar is imported almost exclusively from France; in 1894 the imports were valued at 239,235 frs., of which 227,206 frs. came from that country. Refined sugar of French origin was imported to the value of 786,337 frs., whilst the imports from Malta, the point of transit for the Austrian product, were valued at 829,655 frs.

EXPORTATION OF OLIVES AND OLIVE OIL FROM GREECE.

According to the "Economiste d'Orient" the amount of olives exported from Greece was 2,875,394 ocques (ocque = 2·84 lbs. avoirdupois) in 1894, as against 968,420 in 1893, and 2,051,597 in 1892; 1,989,153 ocques were exported during the period January 1 to August 31, 1895.

Greek olives are chiefly sent to Russia, this country taking half of the total trade. Turkey, Roumania, and Egypt import also considerable quantities. The exportation of olive oil varies much from year to year. The amount for 1894 was 7,693,644 ocques, as against 2,972,322 in 1893, and 2,234,018 in 1892. 2,549,664 ocques were exported from January 1 to August 31, 1895. Russia and Turkey have been the principal customers for Greek olive oil, but from 1894 Austria and Italy commenced to take the lead. In 1894, Italy imported 2,186,005 ocques of Greek olive oil; Austria, 1,700,819 ocques, and Russia only 1,511,108 ocques.

BULGARIAN COMMERCIAL CODE.

The "Handels Zeitung" says that a projected Bulgarian commercial code to replace the Turkish code hitherto in force has been laid before the Chamber. The new law is based on the German code, and was elaborated by a committee of four members, who also made use of the Italian and Roumanian codes; it was revised and finally edited by another committee, consisting of seven members, with Dr. Stoilow, the head of the Ministry of Justice, as president.

BULGARIAN LAW AGAINST USURY.

According to the "Handels Museum" of November 28th last, the Bulgarian Chamber was, in its last session, occupied by the first reading of a usury law. This bill provides that a rate of interest in excess of 12 per cent. is punishable with imprisonment for a period not exceeding six months, and a fine of from 100 to

500 francs. Not only does this law forbid usury in the case of actual money loans, but by its provisions anyone who buys standing crops at 10 per cent. below their value according to prices ruling at the time is liable to the same punishment, as well as any person selling goods for a subsequent payment in produce who makes an excessive profit from his customer. Further, any person carrying on such transactions under the guise of written acknowledgments of debt, bills of exchange, &c., is liable to imprisonment up to two years.

BULGARIAN CHAMBERS OF COMMERCE.

The "Revue d'Orient" says that the Sophia Chamber of Commerce has just held a series of general meetings; the budget for the year 1896 has been voted, including 34,900 francs for expenses (4,900 francs more than in 1895), and the project for the reorganisation of industrial corporations was discussed at length; it was resolved that special exhibitions and competitions ought to take place every year under the patronage of the Government. At Varna and Philippopolis the Chambers of Commerce have also recommenced work; but at Rustchuk, on the other hand, the Chamber has not been able to meet owing to its members being too busy with other occupations, and the president has been obliged to beg the Government to convoke an extraordinary meeting for the month of March 1896.

LIGNITE IN ROUMANIA.

The United States "Engineering and Mining Journal" states that there is no true coal at present mined in Roumania, but there are several deposits of lignite or brown coal which are being worked. Recently it is said that arrangements have been made for more extensive exploration of the Pratozia district, where large deposits are known to exist, and where a small mine is already working at Philipesti.

TRADE OF ROUMANIA FOR 1894.

"L'Econometiste d'Orient" publishes the following figures with regard to the trade of Roumania for 1894:—

In that year the total imports from all countries amounted to 718,000 tons, of a value of 422,142,000 francs (16,886,000*l.*), as against 717,000 tons in 1893, valued at 430,490,000 francs (17,220,000*l.*). The principal countries from which Roumania imported in 1894 were Austria-Hungary, from which the imports were valued at 114,805,000 francs (4,592,000*l.*); Germany,

116,974,000 francs (4,679,000*l.*); Great Britain, 84,029,000 francs (3,361,000*l.*); and France, 34,219,000 francs (1,369,000*l.*).

The total amount of Roumanian exports in 1894 was 2,071,000 tons, of a value of 294,198,000 francs (11,768,000*l.*), as against 2,895,000 tons, valued at 370,652,000 francs (14,826,000*l.*) in 1893.

The principal countries receiving Roumanian merchandise in 1894 were: United Kingdom, 78,842,000 francs (3,154,000*l.*); Belgium, 61,359,000 francs (2,454,000*l.*); Germany, 58,084,000 francs (2,323,000*l.*); and Austria-Hungary, 42,838,000 francs (1,714,000*l.*).

THE PORT OF DÉDÉAGATCH (TURKEY).

The Belgian Consul at Adrianople, in a recent report to his Government, states that preliminaries are being arranged between the Oriental Railway Company on the one hand, and the Ministry of Public Works on the other, for the construction of a new harbour at Dédéagatch, at a cost of about 6½ millions of francs (260,000*l.*). The jetty would be very extensive, so as to permit of the mooring side by side of six boats at a time. Rails would be also laid down, and waggons could load and unload direct.

TURKISH RAILWAYS.

According to the "Journal de la Chambre de Commerce de Constantinople," the works for the construction of the railway from Eski-Chéhir to Konieh are being actively pushed forward. The embankments are completed as far as Konieh, and active endeavours are being made to finish a great portion of the work before the frosts. It is thought that the opening of the Konieh line may take place next June.

TURKISH FISHERIES.

The "Moniteur des Interêts Matériels" states that the revenue of the Turkish fisheries, after having fallen for two consecutive years, has shown a slight improvement, increasing from 40,057*l.* (Turkish) in 1893-94 to 43,037*l.* (Turkish) in 1894-95, or an increase of 2,980*l.* (Turkish). This result will calm the fears that have arisen as to the future of the fisheries in the neighbourhood of Constantinople. There is ground for hoping that the measures which have been taken to improve them will soon show practical results.

THE FINANCES OF MEXICO.

The "South American Journal," quoting from the "Mexican Herald," gives a short account of a speech in November last of the Finance Minister in the Mexican Chamber of Deputies.

The Minister is reported to have said that in the period that had elapsed of the fiscal year, the revenue had increased beyond expectations. The most important source of revenue, the Custom-house collections, had increased approximately at the rate of 300,000 dols. monthly in the months of July, August, September, and October. This gives at once an excess of 1,200,000 dols. of revenue over estimates. There had also been an increase in the receipts from the Stamp tax, but that increase only amounted to 300,000 dols., so that the total excess was 1,500,000 dols.

On the other hand, there was a slight diminution, with respect to estimates, in the yield of the new tax on gold and silver, the diminution in question being due to the fact that in forming the estimates no account was taken of the circumstance that many of the smelting companies had on hand a considerable quantity of ore ready for treatment, which could not be equitably subjected to the new tax, inasmuch as the companies had purchased the ore at a price previously agreed on, and that to oblige them to pay tax on such ore would have involved them in heavy loss. It was thus necessary to exempt the ore in question from the 2 and 3 per cent. taxes embodied in the present budget. The loss to the revenue from this source was estimated at 300,000 dols., the excess above referred to being reduced to 1,200,000 dols.

Turning to expenditure, it would be found that, in addition to those authorised by the budget, it had been necessary to incur other expenses, of which some had been authorised by Congress, while the approval of that body had still to be obtained for others.

The Vera Cruz harbour works require, under a special contract approved by the Legislature, a weekly expenditure of 11,000 dols., instead of 7,000 dols. as formerly. This increase of 4,000 dols. weekly involves an expenditure of 200,000 dols. in excess of the expenditure provided in the budget.

The reduction of postage rates has further modified the conditions under which the present budget was voted. The rate of interior postage on letters having been reduced one-half, it might have been supposed that the postal receipts, at least as regards internal postage, would also shrink to one-half, and as the yield of internal postage on letters is estimated at 800,000 dols., a curtailment in revenue of 400,000 dols. was to be expected, if the strict rule of mathematical proportion held good in such cases; but the considerable increase in the volume of correspondence would do something towards making good the deficit in the postal receipts.

The real increase in the revenue so far, as compared with the estimates formed last May during the discussion on the budget, amounted to between 700,000 dols. and 800,000 dols. In proposing a reduction in the tax on salaries, the Executive

naturally took into consideration the sum produced by that tax. In the preceding fiscal year that sum was in round numbers 1,100,000 dols. Forty per cent. of the amount in question represented 440,000 dols. or so, leaving a substantial margin in favour of the increase in revenue already referred to. It has, in point of fact, been observed as a fiscal year that the yield of the national revenue always shows a material decrease in the second half of the fiscal year as compared with the first half, and it has been thought advisable to restrict the reduction now proposed to 40 per cent.

STREET RAILWAYS IN THE UNITED STATES.

According to a recent number of the New York "Iron Age," Poor's Directory of Railway Officials, just issued, states that the total length of the street railway lines in the United States equals 13,176·38 miles, an increase of 3,514·32 miles over those in operation in 1891. Of this total, 13,176·38 miles, 409·40 miles are operated with steam dummies, 10,238·13 by electric power, 578·54 by cable, and 1,950·31 by animal traction. Of the 13,176·38 miles of street railways in operation, companies operating 378·38 miles failed to supply complete statistics of capitalisation either as to stocks, bonds, or both. The capitalisation of the remaining 12,797·50 miles is shown to be 520,745,823 dols. stock and 367,694,477 dols. bonds, an average of 40,691 dols. per mile of stock and 23,333 dols. per mile of bonds, equal in the aggregate to 69,024 dols. per mile of stock and bonds, as against 56,611 dols. per mile for steam railroads.

NEW LIGHTHOUSE ON THE CHILIAN COAST.

The "Chilian Times" announces that from the 1st of February next the lighthouses now in course of construction on Mocha Island will be lighted from sunset till sunrise. One of these lighthouses is situated on the east coast of the island a little to the south of Anegadiza Point, approximately in lat. 38° 22' 12" S., and long. 73° 53' 44" W.; and the other is situated on the west coast on the summit of Torrecillas prominence, approximately in lat. 38° 21' 22" S., and long. 73° 58' 6" W. The towers will be round, 8·7 metres in height, painted white; the superstructure will be painted green, and will be crowned with a copper cupola. The illuminating apparatus will be deeptric, of the 4th order, white light, the one on the east coast with double flashes each half minute, and the one on the west with single flashes each quarter minute. In clear weather the eastern one will be visible at a distance of 18 miles, and the western one at 15.

COMMERCIAL ENTERPRISE IN THE FAR EAST.

The following is extracted from a report by the Swiss Consul at Yokohama :—

Great alterations in the economic condition of China are apparently at hand. By the Treaty of Shimonoseki various new ports in the valley of the Yangtse have been opened to foreigners. At these places, the land for foreign settlements has already been marked off, and there is at present a Japanese official in Shanghai who is superintending the necessary arrangements for the formation of landing stages, bridges, roads, &c. Companies have been and are being formed by Europeans and Japanese to develop the raw materials of the country. Cotton mills and silk factories have been established, and shares are eagerly sought for. The expedition, sent out at great expense by the Lyons Chamber of Commerce to study the conditions of trade in East Asia, has, according to the Shanghai "Ostasiatische Lloyd," reached China. One half of the party has penetrated the interior by way of Tongking, whilst the other is ascending the Yangtsekiang to effect a junction with the former in Szechuan. This expedition, composed of 40 members, consists entirely of specialists, who are to investigate the problems of the commercial development of these provinces and the opening up of their immense wealth. In any case the expedition will cover ground which has not yet been worked by either Englishmen or Germans. It has also been proposed to send at once a similar expedition from England to the western and southern provinces, as well as Szechuan and Yunnan. This project is being taken in hand by the Blackburn Chamber of Commerce.

GERMAN ENTERPRISE IN CHINA.

The "London and China Telegraph" states that, according to Dalziel's Shanghai correspondence, a syndicate of German capitalists has made a highly important move by offering to purchase the arsenal dockyard at Foochow, as well as the arsenal ironworks at Han-yang, on the Yangtze Kiang, in the province of Hupeh. Dr. Stuebel, German Consul-General at Shanghai, is now at Foochow preparing the way for the transaction. Some time ago the Chinese authorities offered to sell the works in question to a rich Chinese, to be carried on on private instead of Government account as heretofore, but the offer met with no response. It is Herr von Brandt, formerly German Minister at Peking, who, on his recent return to China, submitted the present proposal to the Chinese Government.

DEVELOPMENT OF CHINESE OPIUM TRADE.

A recent issue of the "Pioneer Mail" states that an Indo-China steamer on her last trip to Canton, had in her cargo a consignment of 20 piculs of native opium transhipped at Shanghai.

Some years ago, native opium in large quantities commenced to come down the Yangtze. At first it was mostly smuggled, but later on regular shipments were made, doubtless on account of the larger quantity being sent and the difficulty of smuggling it. The shipments have steadily increased, until now there are considerable quantities coming down to supply the ports on the river and for distribution from Shanghai. This last is important as showing that the foreign drug is now about to meet a native competition in its stronghold, Canton and district, where it has always been in so much more favour than the native. If the new development is followed up, Szechuan opium for the South will simply pay freight per steamer from Ichang to Canton (being transhipped at Hankow and again at Shanghai) and the dues levied by the foreign Customs, instead of having to pay the costly overland carriage and the heavy squeezes of the multitudinous lekin stations *en route* to its destination.

NEW RAILWAYS IN JAPAN.

The "London and China Telegraph" states that a railway project of considerable importance is reported from Kyoto. The idea is to connect Tsuruga on the Japan Sea with Kyoto by a line along the western shore of Lake Biwa, that route being shorter than the existing Government road, which makes a detour along the eastern shore. The whole distance is 65 miles, and the estimated capital of the company, 3,200,000 *yen*. The projectors of the above line, among whom are found most of the names of the principal business men in Japan, have also applied for a charter to construct another line connecting the two cities of Kyoto and Osaka. This second road is to start from the Kyoto terminus of the above-mentioned Kyoto-Tsuruga line at Okazakicho, the site of the recent domestic exhibition. Thence passing through Fushimi, Yodo, Yawata, Mokigate, and Morikuchi, it will have its terminus at Noda in Osaka. This means that it would run at some distance to the east of the existing Government railway.

OPENING-UP OF ROUTES IN PERSIA.

According to the "Journal de la Chambre de Commerce de Constantinople," it is reported from Tehran that Mr. Felix Moral, a German subject, has obtained from the Persian Government the right of making a road from Tehran to the frontier in the direction of Baghdad, and of working a transport service on this road, as also a concession for 90 years for the construction of an electric railway from Tehran to villages situated 10 kiloms. to the north of that town.

JUTE, HEMP, AND WOOLLEN MILLS IN INDIA.

According to information received from the India Office, there were 28 mills working jute and one mill working hemp, at the end of 1894-95, and employing a daily average number of 75,157 persons, namely, 50,154 men, 14,725 women, 3,359 young persons, and 6,919 children. The mills contained 10,048 looms and 201,217 spindles. The nominal capital of the mills, worked by joint stock companies, is stated in the returns at a little over Rx. 3,680,000 (Rx. = tens of rupees); but the figures are not complete for some of the mills, no returns of capital having been furnished. Probably about Rx. 250,000 or Rx. 300,000 may be added to the total on account of these.

All but two of the mills are in Bengal, most of them in the vicinity of Calcutta, and one (a new mill opened in October last) in the French settlement of Chandernagore. Of the two not in Bengal, one (a small one) is at Vizagapatam (this being the mill which works hemp) and the other at Cawnpore.

The progress of the industry during the last five years is shown below:—

—	Mills.	Looms.	Spindles.	Persons employed.
	No.	No.	No.	No.
1890-91	27	8,204	164,245	62,739
1891-92	27	8,695	174,156	66,333
1892-93	27	8,976	181,172	67,291
1893-94	28	9,590	192,688	69,179
1894-95	29	10,048	201,217	75,157

There were five woollen mills at work at the close of 1894--one at Cawnpore, one at Bangalore, one at Dhariwal in the Panjab, and two in the city of Bombay. The number of looms and spindles in 1894 was 529 and 17,228, and the value of the out-turn is reported at about Rx. 185,000. The fabrics made at present consist mainly of blankets, great-coats for the police and native soldiery, and materials for servants' clothing. Serges and other cloths and hosiery are also made of excellent quality in the Cawnpore and Dhariwal mills, but the manufacture of superior goods is as yet comparatively limited.

PAPER MILLS IN INDIA.

According to information received from the India Office, there are ten paper mills—four in the Bombay presidency, four in Bengal, one at Lucknow, and one at Gwalior, which was worked for a short time only in the past year and then closed. Of the ten, three are private concerns in the Bombay presidency, one of which has not been at work for many years. The others have an aggregate nominal capital of Rx. 612,200. The fibrous materials used for making paper are chiefly rags, babui and moonj grasses,

straw, jute and hemp cuttings, and old jute bags and cloth. The quality of the papers made is now very good, and they have a large and increasing sale. Most of the white and blue foolscap and much of the blotting paper, note paper, and envelopes used in the Government offices is now obtained from the Indian mills. The total quantity of paper made in 1894 was about $34\frac{1}{4}$ million lbs.; ten years previously it was about 13 million lbs. The value of the out-turn in 1894 is reported at over Rx. 500,000. The number of persons employed is 3,544.

There are a number of small paper works, for the manufacture of what is known as country paper, scattered through most provinces, but of these petty industries no statistical information is available.

NEW RAILWAYS IN INDIA.

Information has been received at the India Office from the Public Works Department of the Government of India that Berhampore-Rambha section of the East Coast State Railway, a length of 29.5 miles, was opened for the public carriage of passengers on the 15th October 1895, and that the Nankan-Mohnyin section and the Katha branch of the Mu Valley State Railway, $73\frac{3}{4}$ and 15 miles long, respectively, were opened for the public carriage of passengers on the 21st October 1895.

THE PORT OF KURRACHEE.

The "Journal de la Chambre de Commerce de Constantinople" is of opinion that the port of Kurrachee threatens to become a serious rival to Bombay. According to the annual report of the Chamber of Commerce, the total import and export trade for 1894 reached a value of not less than 165 million of rupees. This increase of trade is largely due to the extensive importation of cotton tissues in anticipation of an increased duty on these products.

The exportation of corn has greatly exceeded that of other Indian ports, Calcutta having exported 12,500 tons, Bombay 78,000 tons, and Kurrachee nearly 500,000 tons.

The export trade in raw cotton has also sensibly increased, Antwerp received 18,100 bales; but Bombay still holds first place with 153,000 bales.

CANADIAN SHEEP TRADE.

The "Canadian Gazette" for November 28th last, states that it is surprising to what proportions the Canadian export sheep trade is growing. In 1893 the shipments from the opening of

navigation to November 13th were 1,240; last year that figure increased to no less than 131,741, and this season it has still further increased to 201,572. The North-West has had much to do with this gratifying growth, and sheep raising is now rapidly becoming one of its leading industries. The wool business has grown with the sheep interest, and this year it is estimated that not far from 800,000 lbs. of wool were produced in Manitoba and the Territories together.

THE ASBESTOS INDUSTRY OF CANADA FOR 1894.

According to the "Bollettino di Notizie Commerciali," the production of Canadian asbestos in 1894 amounted to about 8,600 tons. This figure shows a decided increase on the preceding year, and the export of the mineral proves that the demand for raw asbestos is greater than it has been for the past three years.

The Quebec Central Railway carried 7,318 tons of asbestos last year, an amount only exceeded in 1891, when it reached 7,774 tons.

Eight companies for the mining of asbestos were working in 1894, employing altogether about 800 men and boys.

About 1,000 tons were sent to the United States; the rest to various parts of Europe.

All exportations were made direct to manufacturers, and not, as before, to middlemen or agents.

The proprietors of the mines have adopted a system of only extracting enough of the mineral to carry out the contracts of their customers, so that there is now no danger of a glut in the market.

The value of the amount produced last year is roughly calculated at 100,000%.

MINING IN BRITISH COLUMBIA.

According to the "Monetary Times" of Toronto the reports of valuable tin finds on Texada Island, and of coal and gold finds in the immediate neighbourhood of Vancouver, must at present be deemed quite inadequately verified, though there is little doubt that the country, within a circumference of 50 miles from Vancouver, is more profitably mineralised than has hitherto been generally conceived. The prospect of a successful working of the gold claims about Alberni, Vancouver Island, seem, on the other hand, to be growing better, though nothing like certainty can yet be expressed on the point. The mica mines on the North Thompson seem likely to be worked profitably in the early future, a first shipment to Baltimore being now *en route* whilst the cianabar deposits at Savona, on Kamloops Lake, are

not being quite as energetically worked as they were—possibly from lack of sufficient capital. Reports come in almost daily of new finds of good traces of the precious metals on the coast and islands at some distance north of Vancouver, and likely prospects are also said to have been struck in the Harrison Lake region of New Westminster's wide district. Great developments are meanwhile being made in all parts of West Kootenay and some parts of the Okanagan country, and a very busy winter season of ore shipments is in the former case about to set in. Meanwhile the hydraulic gold mines of the Similkameen and Cariboo regions respectively are about to close down most of their operations for the year.

NEW LINE OF STEAMERS BETWEEN VANCOUVER AND NEW ZEALAND.

The "Auckland Weekly News" states that the agreement entered into between the Hon. J. G. Ward, representing the New Zealand Government, and James Huddart, representing Huddart and Co., stipulates that that firm are to maintain a four-weekly steamship service between the Colony and Vancouver. The contractors are to have the option of making either Wellington or Auckland the port of call in the colony, or may make either the terminal port. In case they make one of the Australian ports the terminal port, the subsidy is to be only 20,000*l.* per annum, but if either of the above-named ports is made the terminal port the subsidy is to be 30,000*l.* per annum. The voyage is not to exceed 22 days, and 30*l.* per day is to be paid every day over and above that period the voyage is prolonged. The contract is to take effect after the expiration of eight months from the date of the agreement, and the first steamship is to leave New Zealand not later than the 1st December 1896. The steamships are to be of a gross tonnage of 3,300 tons, are to have triple-expansion machinery, refrigerators capable of carrying not less than 11,000 carcasses of mutton, or an insulated cubic capacity of 20,000 feet for cold storage; they are to provide for carrying 130 passengers, and to be fitted with the electric light and all other comforts and conveniences. The contractors are to have the option of calling both at Suva and Honolulu. The usual provisions are included respecting the carriage of mails. The contract is to be in force for a period of ten years. It is also agreed that the contract is subject to a provision that on no steamship carrying mails shall coloured labour in the pay of the contractors be employed, but this is to be subject to a demand to that effect being made by the Postmaster-General. The agreement is subject to ratification by Parliament.

GOLD MINING IN BRITISH GUIANA.

According to the "South American Journal" of 23rd November last, Mr. E. P. Wood, the Commissioner of Mines for the Federal Government of the United States, has furnished some information as to the goldfields of British Guiana, which will be of special interest at present in view of the territorial claim of Venezuela, which, if conceded, would involve the absorption of the rich Barima district to Venezuela. It appears that there are five separate goldfields in the colony, of which that of Essequibo is the most important. These all lie up different rivers at a distance of from 100 to 300 miles from Georgetown, and as no roads have been formed through the dense forest land the way of getting to them is in boats, the rivers being navigable for a great distance inland. There is a royalty on all gold obtained of 3s. 9d. per ounce, which must be paid into the Colonial Treasury in Georgetown before the gold can be sold, and the following additional charges are levied:—For a prospecting licence, 2s. 1d. per month; for every grant of a mining claim, 8s. 4d. per month; and for every grant of a placer claim, 4s. 2d. per month. The total output of gold in 1894 was about 139,000 ounces, all obtained from alluvial washings. Reef claims are not being worked at present, only one company erecting machinery. The quartz is very rich, many surface samples assaying 50 ounces to the ton, and though these may have been fancy pieces, yet even where there is no visible gold they often give eight to ten ounces. The reefs are of all sorts and sizes; quartz very friable, and not cased quite as one would wish. "But still there has been so little done," writes Mr. Wood, "as yet, that one cannot form a very correct judgment as to their permanency. Everything would point to their being in an exceedingly rich reefing district, and the chief thing needed is a class of men that understand mining. The labourers will have to be educated up to the underground work, but their is no reason why they should not make good miners eventually."

TRADE OF THE CAPE OF GOOD HOPE.

A communication, dated December 9th last, has been received from the Agent-General for the Cape of Good Hope, enclosing extracts from the "Cape of Good Hope Government Gazette" of November 12 last, from which it appears that the value of the imports into the Colony for the ten months ended 31st October 1895, including specie, amounted to 15,680,000*l.*, as compared with 9,685,000*l.* for the corresponding period of 1894. With regard to the exports, the total value for the ten months ended October 31st, 1895, including specie, was 13,958,000*l.*, as compared with 11,549,000*l.* for a like period in 1894.

FLAX-GROWING FOR AUSTRALIA.

In a short article on the state of flax-growing in Canada, the "Agricultural Gazette of New South Wales" advocates an attempt at similar results in Australia. It appears that considerable success has attended the growth of flax both for seed and for fibre in different parts of Canada. The price realised by the flax-seed in 1894, the yield of which in Ontario is estimated at 10 bushels per acre, was from 4s. and upwards per bushel, the fibre being also valuable. In Manitoba nearly double the amount of seed is raised, but the fibre in that province has no value. The seed is sold to mills, where linseed oil is extracted, the residue, forming linseed-cake, being in large demand in Europe for cattle-feeding. Flax is often sown as a catch crop where it is too late to sow wheat, or on land newly broken by the simple process of harrowing. Not more than half a bushel of seed is used to the acre, experience having shown that by sowing it thinly the plant bushes out so as to obtain the largest possible amount of seed. As the seed is very small in size, one-half bushel to the acre is said to give to that area a larger number of grains to the acre than a bushel and a half or possibly two bushels of wheat. Where the object is to obtain fibre, it is recommended that the soil be not too rich, and that the flax should never be grown on the application of fresh manure. It is asserted that the richness of the soil in Manitoba accounts for the fibre not possessing the strength of that grown in Ontario, and the same fact is reported in many of the western United States, where hundreds of thousands of bushels of flax are grown for the seed alone, the fibre being found to be useless. It is stated that flax cannot be considered an exhausting crop with respect to soil, but the land requires to be kept perfectly clean. Clean land is the test insisted on, rather than richness of soil, the latter not being favourable to the growth of fibre, although conducive to large yields of seed. The crop is a rapidly-growing one, and, therefore, valuable where the seasons are short, or for an intermediate or catch crop.

There is no doubt that the seed could be grown successfully on the rich coastal lands of Australia and would pay handsomely. In addition to the value of the linseed oil, 5*l.* a ton is obtained for the cake in Canada, where winter feeding of cattle appears to receive very little more attention than in New South Wales. Then again, the fibre might be raised on some of the worn wheat-lands of the table-land districts. In view of the depressing reports of starving dairy cattle from all parts of New South Wales, some crops to keep them through the hard winter months, particularly in a dry season like that now being experienced, would appear not only reasonable but abundantly necessary.

INDUSTRIAL STATISTICS OF VICTORIA.

The "Australasian Trade Review" of October 14 last notices a return which has been issued by the Assistant Government

Statist, Mr. J. J. Fenton, detailing the number of hands and extent of other power employed in various industries in the colony of Victoria. A valuation of land, buildings, and machinery owned by factories is also given. The total number of hands employed in all factories in the year ended March 1895 was 41,273, of whom 8,224 were females. This shows an apparent increase over the return for the previous year (39,815), but as, for the first time, the return for 1894-95 includes clerks, overseers, carters, and labourers, it is probable that an actual decrease is indicated. The number of factories has decreased from 2,737 in 1893-94 to 2,675 in 1894-95, but the horse-power employed increased from 28,834 horse-power to 30,880. Machinery and plant was valued at 6,640,660*l.* in 1894-95 against 6,618,653*l.* in 1893-94, an increase of 22,007*l.* Lands in the occupation of factory owners in 1894-95 were estimated to be worth 2,626,230*l.*, as compared with 3,081,374*l.* in 1893-94, the difference being, no doubt, due to the valuation rather than to any actual decrease in the extent of land occupied. Buildings and improvements also show a considerable falling off in stated value, viz., from 3,841,084*l.* in 1893-94 to 3,629,910*l.* in 1894-95. It is interesting to note that the value of lands occupied by factories in 1891 was estimated at 6,604,332*l.*, and that of land and improvements at 4,497,382*l.* The number of hands employed in 1890 was 59,181.

Totals of the number of hands employed in some of the chief industries are as follows:—Printing, 3,526; iron foundries and engine-making, 3,945; coach-building, 1,475; boot factories, 3,734; clothing factories, 4,781; brewing, 1,007; tanning, &c., 1,539; sawmilling, 2,386; brick and pottery making, 964.

REGULATION OF FACTORIES IN VICTORIA.

According to the "Australasian Trade Review," a new Bill to amend the Factories and Shops Act has been drafted by the Victorian Government, in which there are some stringent conditions laid down which will cause changes in some trades. Any place where one Chinese works is constituted a factory, and such Chinese are forbidden to work before 7.30 a.m. and after 5 p.m., or after 2 o'clock on Saturdays, or at any hour on Sundays. It is also provided that all furniture made by Chinese shall be so stamped.

Another severe condition is that no person may make clothing, boots, &c., for sale outside a factory except under special permit, or except for the wear of the person ordering it. This clause will be one very difficult to enforce, and will probably require to be recast. Minimum rates of pay for making apparel are to be fixed by a factories board. A number of other provisions are made, among which are clauses to regulate the closing of shops and to fix the number of hours during which shop employes may work.

SHEEP IN NEW ZEALAND.

The full return of the stock department of New Zealand as to the number of sheep in the Colony on April 20 last has been published, and the "New Zealand Trade Review" gives the following figures showing the increase or decrease in the various provinces of the colony :—

District.	1895.	1894.	Increase (+) or Decrease (-).
Auckland - -	846,873	858,295	- 11,422
Hawke's Bay - -	4,217,232	4,286,986	- 69,754
Wellington-West Coast -	3,930,541	4,024,071	- 93,530
Marlborough-Nelson -	906,307	887,582	+ 18,725
Canterbury Kaikoura -	5,559,990	5,744,668	- 184,678
Otago - -	4,365,661	4,429,227	- 63,566
	19,826,604	20,230,829	Net - 404,225

Of the net decrease 229,519 occurs in the South Island and 174,706 in the North Island. The interim report showed a net decrease of 463,521.

As mentioned in our former remarks, the South Island sustained further heavy losses of sheep since the date of the above stock-taking, through the heavy snowstorms that occurred last winter.

The improvement in the price of wool will no doubt give a fresh impulse to sheep-breeding, and increase the demand for store sheep.

XIX.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Aberdeen.—At the last monthly meeting of the Aberdeen Chamber of Commerce, Baillie Edwards in the chair, Mr. George J. Shepherd, in seconding the motion, said that the circular with reference to the Paris Exhibition of 1900 had been addressed by the Prime Minister through the Associated Chambers of Commerce for the purpose of obtaining their opinion on the subject, and with a view of ascertaining if, in the event of Great Britain deciding to take part in the exhibition, it would be for the benefit of the commercial interests of the country. The committee had come to the conclusion that international exhibitions were for the benefit of the countries taking part, and they recommended accordingly. With regard to the metric system, they had resolved to adhere to the position they took up before on this question, and had decided that it would be a hardship if the system were adopted within a less period than two years. With reference to the question of commercial education, he thought that if the sub-committee took a firm grip of the difficulties which they had to encounter they would find that these difficulties would vanish.

The recommendations of the committee were all unanimously adopted.

Blackburn.—The quarterly meeting of the Blackburn Chamber of Commerce was held on December 2 last, Mr. Henry Harrison in the chair.

The President said it could not be denied that for some time past the cotton trade had been exceedingly unprofitable, a great many looms had been stopped, the spinning industry of Oldham had been seriously affected, and a number of mills had been closed. All these things were of the gravest importance to Lancashire, but he found the stagnation of trade extremely difficult to diagnose. With a view to the solution of the matter they might look back to last year, where their experience of the foreign markets and the low price of the raw material led to a great falling off in certain goods, and there was no demand from the great distributing centres in any part of the world. Another fact which had probably militated against trade was that the Indian community might have been early informed of the 5 per cent. import duty to be imposed. Stagnation in the cotton trade had set in since that time, but when they tried to get at the reasons of it, it seemed an almost impossible task. Looking at the question of prices, it was very probable that there would be a small American crop of cotton, and there would certainly be an advance in prices. Already they had

experienced an advance of $1\frac{1}{2}d.$ per lb. in American cotton, and the result was that it was utterly impossible for the consumer to give any orders, or at least what had been given had not kept the looms going. All this tended to stop the import of yarns and cloth into foreign markets, and to a great extent that explained the depression. They might, however, take a little comfort to themselves from the fact that the consumption of cotton in time would lead to a slight demand in the near future. He had no reason to hope, however, that there would be any great demand for cotton goods. In view of this it behoved them not to stand on those goods alone. The condition of affairs was so grave that it pointed to the need for the extension of markets and new railways in India. There appeared to be a variety of opinion as to how the railways of India should be enlarged and extended. So far as their chamber was concerned, they had always been of one opinion, and that was that it was for the State to carry out all extensions of railways there. It was the duty of Chambers like theirs to look ahead, but at the same time he saw no immediate sign of improvement. He hoped, however, that they would be able to rely upon their leaders in commercial enterprise.

The President also gave a report as to the recent negotiations between the Bradford and Blackburn Chambers of Commerce on the question of the commercial mission to China, and other matters referred to were Companies Amendment Acts, Indian import duties, and yarn contracts.

Bradford.—At a recent meeting of the Council of the Bradford Chamber of Commerce, Mr. T. A. Duncan in the chair, the secretary read a letter from the Elberfeld Chamber of Commerce with reference to differences between Elberfeld manufacturers and Bradford yarn manufacturers.

Mr. Steinthal said that the statement that there had been differences with regard to the reeling of yarns was quite correct. In former times, before the Bradford Conditioning-house was established, when there was any difference of the sort it was settled on the basis of the Elberfeld establishment; but since the Bradford institution was established local spinners and manufacturers had naturally looked to the Conditioning-house here as an authority. But there had been cases in which the two establishments had differed in the reeling of the same yarns, and a good deal of irritation and annoyance had been occasioned by this circumstance. He had had hanks of yarn reeled in his presence in the Bradford Conditioning-house, and had taken these same hanks to Elberfeld and had them reeled there in his presence, and different results had been obtained. On the average, the difference had been something like five yards to the hank, or 1 per cent. As a worsted yarn hank was 560 yards, and the spinner had a latitude of $2\frac{1}{2}$ per cent., or 14 yards, it had been certain for a long time that when the reduction of length fell below $2\frac{1}{2}$ per cent. the buyer should be entitled to claim the full difference between the actual length and the 560 yards. Naturally this difference arose

principally upon low qualities of yarns which sailed very near the wind, and there had been cases in which, whilst the Bradford Conditioning-house made the yarn come above the 546 yards, the Elberfeld establishment brought the length below the full allowance. In those cases the merchant fell between two stools, not being able to get any allowance from the local spinner and yet having to make allowance to the Elberfeld buyer. It seemed highly desirable, therefore, that they should be able to arrive at some uniform standard as between Bradford and Elberfeld. It seemed to him that the difference might be consequent on the fact that in the Bradford Conditioning-house each hank of yarn was reeled separately by hand, whilst at Elberfeld eight hanks were reeled simultaneously by machinery. Probably there was something in the contention of the Elberfeld Chamber that there was more uniformity of tension with the machine reeling. Another point which he might mention was the difference in the charges. At Elberfeld 40 hanks of two-fold 32s. yarn were reeled for 2 marks; in Bradford the charge was 10s. It must be a great advantage to have the reeling done more cheaply. He moved that the letter should be referred to the Conditioning-house Committee for investigation.

After some discussion of the matter the course suggested by Mr. Steinthal was adopted.

The secretary read a letter from the India Office with reference to the development of British trade with Thibet, and also one which had been received from the statistical office at the Customs-house stating, in reference to the request of the Bradford Chamber for a separate heading for mohair in the trade accounts of the United Kingdom, that directions had been given for compliance with the request, and the change would be made from the beginning of the present year.

Bristol.—The monthly meeting of the Bristol Chamber of Commerce was held on December 18 last, Mr. George H. Perrin in the chair.

Upon the motion of the President, seconded by Mr. M. Whitwill, jun., leave was granted for the following resolution to appear, in the name of the Bristol Chamber, upon the programme for the forthcoming London meeting of the Associated Chambers:—"That this association reaffirms its opinion that Her Majesty's Government should take action to secure an increased supply of well-trained British seamen, as by so doing they would enlarge the resources from which the Royal Navy might draw its reserves, an increase which is absolutely essential to preserve 'the command of the sea,' for the protection of our commerce and the food supply of the nation, in addition to insuring the naval supremacy of the British Empire."

Dundee.—A meeting of the Dundee Chamber of Commerce was held on December 24 last, Mr. J. C. Buist in the chair.

The president, in moving the adoption of the minutes, said the directors had considered the draft of the despatch which was

twice addressed by the Colonial Office to the Governors of Her Majesty's Colonies on the subject of trade between the different parts of the British empire, and also on the subject of competition with foreign producers. In reply to the request for suggestions from the Dundee Chamber, they expressed their satisfaction with the action of Mr. Chamberlain, and they hoped that good results might accrue from it. The only suggestion they felt called upon to make was that it would be interesting were the quantities of jute goods exported from India to the various colonies given. They had, however, since received a reply from the Colonial Office to the effect that such a return would unduly enlarge the scope of the inquiry, and that therefore it could not be made. It was just 12 months since the Dundee Chamber came to a resolution with regard to the Indian Factory Acts. They thus far were without an answer from the India Office, but the Bengal Chamber of Commerce had been good enough to send them a copy of their reply to the Bengal Government on the subject.

Glasgow.—The monthly meeting of the Glasgow Chamber of Commerce was held on 9th December last, Mr. Walter Duncan in the chair.

The minutes of foreign affairs were read. These referred to communications received from the Leeds Chamber with reference to the construction of railways in British Burmah, and also the memorial which the Chamber had sent to the Secretary for India in 1890, and the committee recommended that the Indian Government should be asked, with as little delay as possible, to give the matter of opening up communication between British Burmah and China by railway their attentive consideration. With reference to the memorial from Demerara, in regard to the sugar industry, asking the Chamber to use their influence with the Government, either to get the bounties given to sugar-makers from beetroot by foreign countries stopped, or that a counter-vailing duty should be placed on all foreign sugar imported into this country equal to the bounty which might have been obtained on export, the committee expressed their sympathy with the distressed sugar industry in the West Indies, but they could not recommend the Chamber to support the petition.

The Chairman stated that a communication had been received enclosing copies of a memorial which had been presented to the Secretary of State for India, stating that exporters of Turkey-red yarns to India from this country were under a disadvantage compared with Continental exporters in the matter of quantities contained in their respective bundles.

Mr. Graham said he understood that this was a real grievance, and that the Indian authorities had said they could not interfere.

The communication, as well as one relating to a measure about to be introduced into the French Chamber, and expected to affect the importation of coal into France, was remitted to the Foreign Affairs Committee.

Other matters referred to were—communication with the Fastnet Rock, the projected railways in Gold Coast Colony,

Indian merchandise marks, the Companies Act, and the inconveniences of Grangemouth Harbour.

Leeds.—The monthly meeting of the council of the Leeds Chamber of Commerce was held on December 20th last, Mr. E. Slater in the chair.

Several communications from the Associated Chambers of Commerce on the subject of the proposed Paris Exhibition of 1900 were announced. It appeared that the British Government had been invited by the French Government to take part in the exhibition, but Lord Salisbury, attaching great value to the views of the Chambers of Commerce of the United Kingdom as to the advantage offered by the exhibition to the commercial interests of the country, desired to know the opinion of the several Chambers.—Mr. Zossenheim expressed the opinion that, although exhibitions had become rather stale nowadays, it was the duty of the Government to take part in it.—Mr. J. W. Willans said that he was quite sure that if we were not represented there, our rivals would be in full force, and so ground would be lost; and he moved that the council was heartily in favour of the Government accepting the invitation. This was unanimously agreed to.

The subject of the Burmah-China railway was brought before the council. Mr. Calvert read a letter from the Foreign Office, in which Lord Salisbury acknowledged the receipt of a communication from the Leeds Chamber on the subject. Several letters, the secretary also stated, had been received from other Chambers, supporting the resolution passed at a previous meeting of the council. A report from Mr. Holt S. Hallett, which had been forwarded to the members of the council on the previous day, was then considered. The chairman said it was a most admirable report, and stated the whole case very clearly. It showed the necessity of their urging the Government to take immediate action if they intended to secure the trade in those regions. The report, it was further stated, would, when supplied with maps, which were not yet ready, be sent out to all the Chambers of the country, and the council agreed to forward the following resolution on the subject to the Associated Chambers:—"That as the connection by railway of a seaport in Burmah with South-West China at Ssumao is greatly required, in order to open out to British trade our new territories in the basin of the Mekong, and to enable British manufacturers to compete with those of France in Northern Siam and in South-West China, this association urges upon the Imperial and Indian Governments the necessity for such a railway leading to Ssumao, either from Moulmein, viâ Siam, or from Rangoon, viâ Karenni, to be undertaken or guaranteed by the Government; and also for permission to be obtained from the Chinese Government for the continuation of the railway through Ssumao into the Chinese dominions, on conditions not less favourable than those granted to the French for the continuation of their railways into China."

Leicester.—A meeting of the Leicester Chamber of Commerce was held on December 10 last, Mr. W. P. Viccars in the chair.

Some discussion took place on the desirability of England being officially represented at the Paris Exhibition of 1900, and, on the motion of Mr. Wates, the resolution of the Central Chamber in favour of it was endorsed.

Mr. Rowlett drew attention to the fact that a system had lately grown up in the hosiery and other trades of supplying machinery to beginners on the hire system, and it was productive of very great harm. He gave an instance within his own knowledge where a manufacturer was largely trusted on the strength of having a factory full of machinery, and when the creditors were called together it was found that scarcely any of the machinery was his own, but was held on the hire system. The result was that 1,000% paid on the machinery was lost to the creditors, as the property belonged to the maker till it was wholly paid for. That did great harm, by bringing about illegitimate competition, and it was desirable from all points of view that something should be done to make the system less easy. It was agreed that Mr. Rowlett should bring the matter before the meeting of the Associated Chambers in March next.

Liverpool.—A meeting of the committee of the African Trade Section of the Liverpool Chamber of Commerce was held on the 13th December last, Mr. John Holt presiding.

A further letter had been received from the Foreign Office, saying that an agreement has been arrived at with the French Government respecting the engagement of Kroo labourers in the British and French possessions on the West Coast of Africa, but that certain details in connection with the arrangement were still under discussion.

It was also resolved that the Marquis of Salisbury should be requested to receive a deputation, with a view to urging the necessity which exists for the establishment of a Customs Union between the Niger Coast Protectorate and the Niger Territories, or some modification of the charter of the Royal Niger Company, which will do away with the necessity for maintaining a preventive service on the frontiers common to the two Protectorates.

A report of recent proceedings of the Legislative Council of the Gold Coast was laid before the committee. The report stated that the revenue of the Colony in 1895 was likely to exceed the estimates by 27,000% ; that examination was about to be made by means of artesian wells for good water for the supply of the coast towns ; that surveys of the reefs of Accra, the bar of the Volta, and other examinations have been begun with a view of affording harbour accommodation and developing navigation ; that pending decision as to the most suitable harbour for railway purposes, it has not been possible to include in the estimates for 1896 any provision for railway expenditure ; that the roads are being improved ; that a telegraph line is being constructed between Cape Coast and Prahsu ; and that in the

matter of the establishment of a Kroo colony, a site for a Kroo village has been selected at Accra.

The other business consisted of correspondence relating to commercial statistics, South Africa; the drawback on spirits exported from Lagos to Porto Novo, and the proposed new railways at Lagos; and the annual medical report, Sierra Leone.

A further meeting of the African Trade Section was held on the 4th January last under the presidency of Mr. John Holt, at which a letter was read from Captain Denton, Acting-Governor of Lagos, dated 19th November, handing copies of a notice embodying roughly the inducements which he had caused to be put before the inhabitants of the different Kroo towns from which labour is obtained, in the hope that by such means they might be persuaded to form permanent settlements in Lagos. The inducements offered are sites for villages and free grants of land for agricultural purposes, gifts of coffee and other plants for cultivation, and a bonus in cash to each Krooman who bears a good character and produces a certificate of employment which covers at least two-thirds of the period of his stay in the colony on the completion of five years' continuous residence. It was further announced that the Government would defray the actual cost of passage from the Kroo coast to Lagos. It was resolved that a copy of the notice should be sent to Sir Claude Macdonald, who is endeavouring to form a Kroo colony in the Niger Coast Protectorate.

London.—A meeting of the Council of the London Chamber of Commerce was held on December 12 last, Mr. W. H. Williams in the chair. Mr. Sydney Morse, on behalf of a deputation representing a general meeting of members held on the 9th inst., submitted a report to the Council on the rating question, and stated the reasons for the recommendations therein contained. After suggesting certain reforms in the existing system of rating, the report urged that any proposed new legislation on rating matters should be carefully watched in the interests of members of the Chamber, and that the Council should urge upon the Government, in the strongest manner, the absolute necessity of legislation on the lines indicated. The chairman said the Council thanked the deputation for the extreme care they had taken in the matter, which would have their best consideration. The following resolution was eventually moved by Sir Albert K. Rollit, and seconded by Mr. A. J. Hollington:—"That the contents of "the report in general, and the advisability of passing the "Exemption of Machinery from Rating Bill, be urged upon the "attention of the Government, and, if necessary, by deputation." This was carried unanimously.

The secretary submitted ten suggested resolutions for the annual meeting of this association in March, of which the following seven were approved:—(1.) Secondary education (2.) Berne official vocabulary. (3.) Colonial development. (4.) Commercial attachés. (5.) Commercial representation on exhibi-

tion commissions. (6.) Companies' Acts amendment. (7.) Boards of conciliation.

Other subjects referred to were:—Spanish certificates of origin, Companies' Acts Committee, the Colonial Office in relation to colonial trade, and light railways.

Manchester.—An ordinary meeting of the Manchester Chamber of Commerce was held on 11th December last, Sir F. Forbes Adam in the chair.

In reply to representations of the Chamber which expressed the hope that the Secretary of State for Foreign Affairs would constitute the consular representative at Fez a salaried official, at liberty to devote his whole time to British interests there, a letter from the Foreign Office was read stating that a proposal in accordance with the wishes of the Chamber was now before Her Majesty's Treasury.

In compliance with the resolution of the Board of Directors at the meeting held on the 13th November, a letter had been addressed to the India Office urging the immediate construction of a line of railway from Mandalay to a point near the Kunlong Ferry on the Salween River, with extension hereafter, when practicable, to Talifoo, in preference to giving support, direct or indirect, to any other scheme having a somewhat similar object in view. In answer to this letter a communication from the India Office was read, in which it was stated that the resolution passed by the Manchester Chamber of Commerce had been duly noted by the Secretary of State, and that the construction of the Mandalay-Kunlong Ferry Railway would be proceeded with as soon as might be practicable.

A copy of a memorial to the Secretary of State for India from a number of Turkey-red dyers in the West of Scotland had been sent to this Chamber with a request for support to the prayer of the memorial. The memorialists complained of the illicit competition of certain foreign exporters to India of dyed yarn. By a notification four years ago of the Governor-General of India in Council, which under the above-named Act he was empowered to promulgate, the "count" or "number" of cotton yarn was thenceforward to be held to be an indication of length. The regulation was, however, being evaded by certain non-British dyers, who gave short lengths of hanks and marked the total number of yards on a ticket outside the bundle in an inconspicuous manner. For example, a bundle of 40's ought to give 327,600 yards to a bundle. But, although placing the mark "245,000 yards genuine number 40" outside the bundle, the exporter gave 82,600 yards short of the total minimum standard length, to the great prejudice of honest exporters. The memorialists' concluding prayer was that the attention of the Custom-house authorities in India might be drawn to the matter, and that, if need be, new or amended regulations might be issued which would prevent all possibility of the continuance of the deception. The memorial was referred to the Yarn Sectional Committee for consideration and report.

The letter ordered by the Board of Directors at its last meeting to be sent to the Foreign Office asking whether it were possible under the existing laws of Japan, or existing agreements with that country, to protect the property of English subjects in trade marks against fraudulent imitation in Japan, had been under the consideration of the Marquis of Salisbury. A reply was now read, of which the following is an extract:—"There is no protection for such rights at present, but Her Majesty's Government will represent any case of fraudulent imitation which may be brought under its notice to the Japanese Government, in the hope that they will take any steps that may be possible to put a stop to it."

Other matters under consideration were telegraph charges in the Far East, and the import into China of non-chopped piece-goods.

Morley.—A meeting of the Morley Chamber of Commerce was held on December 13 last, Mr. J. C. Watson in the chair. The proposed commercial mission to China was the principal subject that came up for discussion. It was stated that of all towns in woollen districts which should be interested in the opening up of China, Morley came first, because it made a cheaper class of goods than any other town.

Nottingham.—At a meeting of the Council of the Nottingham Chamber of Commerce held on January 6 last, Mr. J. Billyeald in the chair, the following resolution, after a long discussion, was adopted:—

"That the secretary write to the Foreign Office and ask that Her Majesty's Consuls and Vice-Consuls in China be instructed to supply the Chamber with particulars of textile goods supplied or required in respective districts, more particularly with reference to lace and hosiery, accompanied by samples and prices as far as practicable."

Other matters referred to were of local interest only.

Oldham.—The monthly meeting of the Oldham Chamber of Commerce was held on December 16 last, Mr. A. Emmott in the chair.

The Secretary reported that he had received a copy of the memorandum prepared by Mr. J. S. Black, of Her Majesty's consular service, on openings for British trade in north-east and north Siam. It points out that there is an opening for British trade in the Laotian provinces, where there would be a demand for cheap manufactured articles designed in imitation of the style and material there in vogue. Mr. Black collected various articles of silk and cotton clothing worn by the natives of the districts, and through the kindness of the Foreign Office those articles had been forwarded for the inspection of members of the Chambers of Commerce at the Central Office, in London.

The Secretary read a communication from the Foreign Office, inviting the opinion of the Chambers of Commerce as to the

expediency of the Government taking part officially in the International Exhibition to be held in Paris in 1900. On the motion of Mr. Marsland, seconded by Mr. Ingham, it was agreed to approve of the resolution of the Executive Council of the Associated Chambers of Commerce urging the Government to accept the invitation.

The Chairman made a long statement with reference to the Indian import duties and the interview obtained by a deputation with Lord George Hamilton on the subject. Companies Act Amendment and yarn contracts also came up for discussion.

Walsall.—A meeting of the Council of the Walsall Chamber of Commerce was held on the 23rd December last, Mr. W. E. Blyth in the chair.

The report of the Wolverhampton Chamber on foreign competition in the hardware trade was fully discussed, and subsequently it was decided to send a copy of the report to every member of the Chamber.

With regard to the Walsall inquiry as to foreign trade with special reference to the competition in the brush trade, it was found that the information to hand was old and imperfect in many details, and it was resolved to apply for further information.

Mr. Hill drew attention to the fact that the Government of New Zealand, finding apparently that their tax on English trade was not successful, were now requiring travellers to deposit 50%, as well as pay the duty on their samples, the object being to take the tax on trade out of the deposit. That meant that a traveller going into Auckland, for instance, would have to pay down about 100%. Of course it was promised to be returned when he left, but often it took months to get the money back. He moved that they should write to Mr. Chamberlain, the Colonial Secretary, drawing his attention to this, and asking him to take some steps in it. Hitherto he seemed simply to have sent their letters of complaint on to the New Zealand Government.

The President seconded the resolution, which was carried unanimously.

A communication having been received as to the Paris exhibition, the President reported that the General Purposes Committee had written that they would support any effort to obtain direct representation on the Royal Commission, but that as to sending goods, Walsall manufacturers found that doing so was detrimental to their trade, as it simply enabled foreigners to compete the better with them.

This letter was unanimously approved.

Reference was also made to the decimal system and certificates of origin for Spain.

XX.—RECENT TRADE BLUE BOOKS.

1. *Reports by Her Majesty's Representatives abroad on Bounties, other than those on Shipping and Navigation, paid by the State in the Countries in which they reside. Commercial No. 7 (1895). (C. 7897.) Price 3½d.*

This publication, which is in continuation of Parliamentary Paper, Commercial, No. 26 (1889), C. 5867, contains the replies of Her Majesty's representatives abroad to the following circular addressed by the Earl of Kimberley.

SIR, Foreign Office, February 19, 1895.

With reference to the circular of the 2nd May 1889, relative to bounties, I enclose a copy of a letter from the Board of Trade, and, in accordance with the request made therein, I have to request you to furnish me, as soon as convenient, with a report on bounties other than those on shipping and navigation.

I am, &c.,

(Signed) KIMBERLEY.

The enclosure referred to in the circular was as follows:—

Board of Trade, London,

SIR,

January 24, 1895.

I am directed by the Board of Trade to state, for the information of the Secretary of State for Foreign Affairs, that they have received an application from the Royal Commission on Agricultural Depression asking to be furnished with full details respecting any bounties which may be accorded either in foreign countries or British possessions on agricultural production, or on the export of agricultural produce. Since the issue of the Parliamentary paper "Commercial No. 26 (1889)," the Board have received only fragmentary information bearing on the question of the bounties given abroad; and in order to insure that the Royal Commission should be furnished with as accurate and recent details as possible, I am to ask you to be good enough to move Lord Kimberley, should he see no objection, to cause instructions to be issued with a view to obtaining reports from Her Majesty's representatives in various countries, explaining the present situation in each country with regard to the subjects dealt with in the above-mentioned paper, and also with regard to bounties on the production or exportation of sugar.

It would be a convenience both to this Board and to the Royal Commission if the information now asked for could be supplied as soon as possible.

I have, &c.,

(Signed) R. GIFFEN.

Replies were received from the Argentine Republic, Austria-Hungary, Baden (Grand Duchy of), Bavaria and Wurtemberg, Belgium, Brazil, Bulgaria, Chile, Colombia, Denmark, Egypt, France, Germany, Greece, Guatemala, Italy, Japan, Mexico, Montenegro, Morocco, Netherlands, Persia, Peru, Portugal,

Roumania, Russia, Saxe-Coburg and Gotha, Saxony, Servia, Siam, Spain, Sweden and Norway, Switzerland, Turkey, United States, Uruguay, Zanzibar.

2. *Rule of the Road at Sea. Report of the Committee appointed by the President of the Board of Trade on the Screening of Ships' Side-Lights, and Minutes of Evidence and Appendices. 1. Report. (C. 7908.) Price 2½d.*

This contains the report of the Committee appointed on the 18th March 1895 to consider and report—

1. Whether the rule laid down in the Order in Council of January 30th, 1893, is in accord with, and calculated to secure compliance with, the requirements of Articles 3 and 15 of the International Regulations for Preventing Collisions at Sea, as to the fixing and screening of the side-lights of vessels.

2. Whether it is desirable in the interests of safety, and in order to secure due compliance with the above-named articles, that any, and what, alterations should be made in the instructions issued by the Board of Trade to their surveyors in the matter.

In concluding their report, the Committee make the following recommendations which, are limited to the matters directly involved in the questions submitted to them:—

1. That the Order in Council of January 30th, 1893, be cancelled.

2. That in the case of oil lamps the forward edge of the screen, or chock on it, should be in a line parallel to the keel with the inside edge of the wick.

3. That in the case of electric lights there should be a similar screening in regard to the inside edge of the filament.

4. That the breadth of the wick or system of wicks of each oil lamp, and of the filaments in the case of the electric light, should be not more than 2 inches nor less than 1 inch, measured at right angles to the fore-and-aft line of the ship.

5. That the visibility of the green light should as nearly as possible be equalised to that of the red by the employment of higher candle-power; and that the masthead light should not be so powerful as to diminish the visibility of the side-lights.

3. *Local Government Board. Reports and Papers on the Port and Riparian Sanitary Survey of England and Wales, 1893-94, with an Introduction by the Medical Officer of the Local Government Board. (C. 7812.) Price 6s. 10d.*

This publication contains, in addition to the introduction by the medical officer, a general report on the sanitary survey of the coast line of England and Wales, 1893-94, by Dr. F. Barry; detailed reports with respect to port and riparian sanitary districts inspected, 1893-94; a report on certain circumstances connected with the Manchester Ship Canal, by Dr. R. Bruce Low; table showing the port sanitary districts, urban sanitary districts, and parishes of rural sanitary districts riparian to the several customs ports of England and Wales.

As an appendix the following are given:—(1) Form of report of sanitary inspection; (2) general cholera order of August 28th, 1890; (3) supplemental cholera order of September 6th, 1892; (4) specimen code of regulations made under section 125 of the Public Health Act, 1875; (5) specimen order constituting one of the riparian authorities of a Customs port the port sanitary authority for a combination of districts; (6) specimen order constituting a joint board the port sanitary authority of a combination of districts; (7) specimen order fixing a mooring station *outside* the district of a sanitary authority; (8) excerpt from a general order of the Board, dated the 19th June 1893, defining the duties of port medical officers of health; (9) excerpt from a general order of the Board, dated 19th July 1893, defining the duties of port inspectors of nuisances.

4. *Companies (Winding-up). Fourth General Annual Report by the Board of Trade under Section 29 of the Companies (Winding-up) Act, 1890. 453. Price 8½d.*

This is the fourth annual report on the various matters, administrative, financial, and judicial, under the Companies (Winding-up) Act, 1890. It includes a report and statistical tables, prepared by the Inspector-General in Companies Liquidation, furnishing particulars of the amount and character of company insolvency, and of the results of winding up, so far as ascertained during the year 1894, and a report by the Solicitor to the Board of Trade on legal proceedings conducted by him under the Act.

With reference to the financial results, the following account of receipts and expenditure has been prepared by the Treasury, and presented to both Houses of Parliament in pursuance of section 28 of the Act:—

ACCOUNT showing the RECEIPTS and EXPENDITURE in respect of Proceedings under the Act during the Year ended 31st March 1895.

Particulars of Receipts.	Amounts (Provisional Figures).	Particulars of Expenditure.	Amounts (Provisional Figures).
	£ s. d.		£ s. d.
Net amount received by the Inland Revenue Department for Stamps issued in respect of business under the Act (less estimated cost of collection and manufacture)	6,286 9 5	Salaries of officers† - -	25,627 17 9
Amount of fees received in cash (including fees received in the County Court Department of Her Majesty's Treasury)	17,722 6 5	Miscellaneous expenses -	3,588 6 1
Dividends on Funds invested under section 16 of the Act*	9,601 14 2	Rent, rates, fuel, light, &c. -	3,714 14 10
		Stationery - - -	1,555 10 5
Total - - -	33,610 10 0	Total - - -	34,486 9 1

* After deduction of interest paid out under section 18 of the Companies (Winding-up) Act, being 1,945*l.* 18*s.* 2*d.* in 1894-95.

† Including a sum of 4,500*l.* representing the services of officers of the Inspector-General in Bankruptcy, and a sum of 1,000*l.* representing the services of officers of other departments of the Board of Trade.

The receipts and expenditure for 1893-94, published in the last report, were subject to certain corrections. As finally audited, they were respectively 35,942*l.* and 33,809*l.* There has accordingly, in the year 1894-95, been a decrease in the receipts of 2,332*l.* and an increase in the expenditure of 677*l.* The decrease in the receipts arises mainly under the head of Dividends on Funds Invested. This was to be expected, as the large balances paid into the Companies Liquidation Account during the Australian bank crisis in 1893-94 tended to raise the revenue from investments to an abnormal amount. It is also to be noted that in 1894-95 a considerably larger amount (*i.e.*, 1,109*l.* in excess of the amount in the previous year) was credited to companies by way of interest under section 18 of the Act, and the revenue was correspondingly reduced. The expenditure shows very slight fluctuations as compared with the previous year. The largest difference is under the head of Miscellaneous Expenses. This is caused by increased expenditure under the head of Law Costs, Scrivenery, Postage, and Incidentals. It is to be observed that the expenditure has for the first time exceeded the receipts. The excess is small (876*l.*), and can be met out of the surplus previously accumulated without involving any charge on the general revenue of the country.

The total payments into and out of this account during the year and the four years ended 31st December 1894 were as follows:—

—	Under Section 11.		Under Section 15.		Total.	
	For the Year ended 31 December 1894.	For the Four Years ended 31 December 1894.	For the Year ended 31 December 1894.	For the Four Years ended 31 December 1894.	For the Year ended 31 December 1894.	For the Four Years ended 31 December 1894.
Payments in -	£ s. d. 985,882 7 2	£ s. d. 5,569,240 17 9	£ s. d. 84,542 10 6	£ s. d. 672,861 11 3	£ s. d. 1,070,424 17 8	£ s. d. 6,242,102 9 0
Payments out -	1,055,286 2 2	5,288,690 10 6	149,330 8 2	482,586 7 7	1,204,616 10 4	5,771,276 18 1
Balance on 31 December 1894. }	£ s. d. 280,550 7 3		£ s. d. 190,275 3 8		£ s. d. 470,825 10 11	

5. *Abstracts of the Returns made to the Board of Trade of Shipping Casualties which occurred on or near the Coasts or in Rivers and Harbours of the United Kingdom from the 1st July 1893 to the 30th June 1894; also of the Returns made to the Board of Trade during the Year 1893-94 of Shipping Casualties which occurred to British Vessels elsewhere than on the Coasts of the United Kingdom, and to Foreign Vessels on or near the Coasts or in Rivers and Harbours of British Possessions Abroad. With Charts and Appendices. (C. 7858.) Price 4s. 5d.*

This is the annual return relating to wrecks which is prepared and issued by the Marine Department of the Board of Trade. It shows that the total number of sea casualties recorded in 1893-94 (total losses and serious and minor casualties) was 7,217,

or 1,651 more than the number in 1892-93, and 189 more than the number in the preceding year.

The total losses and serious casualties together were 2,508 in number, which was 365 more than the number in 1892-93, but 291 less than the number in 1891-92.

The number of total losses was 584 (tonnage 200,622), which was higher by 232 as regards number, and by 45,860 as regards tonnage, than 1892-93; higher by 37 as regards number, and by 3,619 as regards tonnage, than 1891-92; and lower by 44 in number, and by 17,429 in tons, than the average for the 18 years.

The broad results of the tables are that 11,303 vessels of all descriptions (tonnage 3,924,925) belonging to the United Kingdom were totally lost in the 18 years ended June 1894. The average annual loss was 628 vessels (tonnage 218,051). The losses of sailing vessels fell from an average of 503 vessels (tonnage 128,948) for the 18 years to an actual loss of 430 vessels (tonnage 83,071) in 1893-94. The losses of steam vessels were 154 (tonnage 117,551), while the average for the 18 years was 134 vessels (tonnage 89,104). The number of sailing vessels lost was higher than in any year since 1887-88, whilst the sailing tonnage lost was lower than in any year on record except 1892-93. These fluctuations are partly accounted for by the great gale of the 16th to the 20th November 1893, which caused the loss of a large number of sailing vessels of small tonnage on the coasts of the United Kingdom. The number of steam vessels lost was higher than in any year on record except 1882-83 and 1884-85, and the tonnage higher than in any year except 1884-85.

During the last 18 years 5,165 wrecks and casualties to ships belonging to the United Kingdom have been attended with fatal results to 32,312 persons, of whom 28,552 were employed in the navigation of the vessels and 3,760 were passengers.

The average annual loss during the 18 years was 1,795 persons, consisting of 1,586 crew and 209 passengers; and the loss in 1893-94 was 1,649 persons, of whom 1,520 were crew and 129 passengers. These figures show a decrease of 66 in the number of seamen lost and a decrease of 80 in the number of passengers lost as compared with the average for the 18 years. The number of casualties (314) attended with loss of life was higher than in any year since 1883-84.

The average number of seamen lost in sailing vessels was 1,073 and of passengers 61, against 874 seamen and 91 passengers lost in 1893-94.

The average number of seamen lost in steamships was 514 and of passengers 148, against 646 seamen and 38 passengers lost in 1893-94.

XXI.—QUARANTINE NOTICES.

DENMARK.—Marseilles has been added to the list of ports included in the notice published in page 732 of the "Board of Trade Journal" for December last.

ITALY.—The Board of Trade have received, through the Secretary of State for Foreign Affairs, the following translation of Italian Sanitary Ordinances:—

In order to assimilate the measures in force against localities infected with cholera to the regulations published with regard to the administration of the Italian sanitary service, and inasmuch as the danger of cholera being imported into Italy from the countries and districts mentioned immediately below, is admitted to exist, viz., from European and Asiatic Turkey; the coasts of the Black Sea and the Sea of Azof; the Russian coasts of the Baltic Sea; and the Danube ports of Russia and Roumania, Morocco, and Egypt; and in view of the above-mentioned regulations respecting the sanitary service approved by Royal Decree, dated September 29, 1895, the Ministry of the Interior decrees as follows:—

Art. 1. The regulations contained in Article 97, &c., of the above-mentioned law will be applied to vessels coming from the said places.

Art. 2. For vessels coming from these places and intending to obtain free pratique in Italian ports, the medical visit and disinfection of dirty linen for personal or domestic use on board will take place in the first of such ports at which they touch.

At the other ports the medical visit and disinfection will be limited to persons and dirty linen for personal or domestic use which have been disembarked.

Art. 3. Mail steamers, which although touching at Italian ports proceed to foreign ports, can land passengers and goods in accordance with the regulations of Art. 91 of the above-mentioned law, without it being necessary for them to obtain a special authorisation from the prefect of the province in order to do so.

Art. 97. All ships will be subjected to a rigorous medical inspection of their passengers, and all their personal and domestic effects to disinfection, which are not found to be perfectly clean.

The doctors entrusted with visiting the ship will decide in individual cases if the clothes of the passengers and crew must also be subjected to disinfection.

Art. 98. Ships on which cases of cholera have been verified during the voyage, or which, on their arrival, have cases or suspected cases of this illness on board, will be sent to the

nearest quarantine station of Asinara, Poveglia, or Augusta, to undergo inspection and disinfection which, according to the previous Article and those other sanitary measures, shall have been determined upon from time to time by the Ministry of the Interior.

Ships falling into this condition during the voyage can steer direct for those quarantine stations before approaching any other Italian port, announcing the fact, if possible, by means of the semaphore.

Art. 99. These ships can be dispensed from proceeding to a quarantine station to undergo measures, in accordance with the preceding Article, when, although cases of cholera have been verified on the journey, no other cases have occurred for at least five days on board after the complete cure or death of the sick person, and when they have a doctor on board and a stove for disinfection, according to the regulations.

The sanitary measures prescribed under these circumstances will be applied in the port itself at which the ship touches, or in the neighbouring sanitary stations. In no case, however, must the bilge water of the ship (*L'acqua della sentina*) be emptied into the harbour if it has not first been disinfected according to the Ministerial instructions.

Art. 91. Ships which must submit to sanitary measures in accordance with the present regulations or ordinances in force, in order to be admitted to free pratique on account of their ports of departure, or of circumstances which have occurred during the voyage, will be able, without obtaining free pratique, to load and unload their goods, and even passengers, if they submit to all those measures of precaution which shall be considered necessary to avoid other contact with the shore.

In the case of the landing of passengers, a special permission must be obtained from the prefect, who will only grant it on the advice of the provincial doctor.

Passengers and cargo which are landed will be subject to the sanitary measures prescribed in their case.

Art. 92. The Minister of the Interior, in agreement with the Minister of Marine, will be able, under ordinary circumstances, to release ships from the obligation of presenting a bill of health on their arrival when they come from specified ports designated by special ordinance.

PORTUGAL.—Telegrams have been received from Her Majesty's representatives at Lisbon reporting that Rabat is declared infected with cholera from December 1 last. Pernambuco and other parts of the province free from yellow fever from the same date. Tangiers suspected of cholera from December 11 last. Austro-Hungarian ports free from yellow fever since December 1 last, and Alexandria infected with cholera.

SPAIN.—Tetuan and Tangier are now declared clean and free pratique granted to arrivals therefrom. Rabat is still considered foul. Quarantine is imposed on arrivals from Casa Blanca and Mazagan.

BULGARIA.—The Board of Trade have received, through the Secretary of State for Foreign Affairs, a copy of a despatch, dated December 14, 1895, from Her Majesty's Representative at Sofia, enclosing the following circular relating to sanitary precautions adopted in Bulgaria :—

1. Vessels arriving in Bulgarian ports from Egypt and Asia Minor which have undergone quarantine at Constantinople or some other Ottoman port, and those which, arriving from a non-contaminated port have had no case of cholera on board are admitted to free pratique.

2. Vessels which, when leaving an infected port have any case of cholera on board but not repeated for seven consecutive days will be subjected :

(a.) To medical inspection.

(b.) To disinfection of the soiled linen and wearing apparel of both passengers and crew, when the quarantine authorities consider such disinfection necessary.

(c.) The holds will be emptied, carefully disinfected, and refilled with pure water.

(d.) Travellers will be subjected to five days' medical supervision at home, and the crew to five days medical visits.

(e.) The crew will not be allowed ashore, except in cases of great necessity, when on duty.

3. Vessels which have had cases of cholera on board for seven days will be considered as infected and subjected to the following measures :—

(a.) The sick persons will be isolated in the lazaret ashore.

(b.) All other travellers will land, if possible, and be subjected to medical visit at the lazaret for five days.

(c.) Soiled linen and wearing apparel will be treated as in the preceding section.

4. The following articles from infected places are forbidden entry into Bulgaria :—

(a.) Soiled linen, old clothes, and soiled bed-linen, such as mattresses, blankets, &c.

(b.) Rags and fragments, which, not coming direct from a factory, have not been sent in bales bound with iron hoops and marked with the number and mark of the authorities of the place of origin. Generally speaking, all effects, new and used, coming direct from a factory are permitted entry into Bulgaria.

MALTA.—The prohibition of the importation of hams from Italy is now removed.

XXII.—POST OFFICE NOTICES.

INSURANCE OF PARCELS FOR FRANCE AND FOR OTHER
COUNTRIES VIÂ FRANCE.

Parcels sent by Parcel Post to France, Algeria, and Corsica, and viâ France to Italy, Switzerland, Egypt, Constantinople, and Smvrna, can now be insured for any sum not exceeding 20*l.*, under the usual conditions.

The insurance fee will be :—

For a sum not exceeding 12*l.* - - - - - 5*d.*

For a sum exceeding 12*l.* but not exceeding 20*l.* - 7½*d.*

Coin, articles of gold or silver, jewellery, lace, and objects of art, which have hitherto not been transmissible by Parcel Post to the countries mentioned above, may now be sent, provided that they are insured.

PARCEL POST TO BRITISH BECHUANALAND. REDUCTION OF
POSTAGE.

The Colony of British Bechuanaland having been annexed to the Cape Colony, the postage on parcels for British Bechuanaland is now the same as that on parcels for the Cape Colony, viz., 9*d.* for every pound or fraction of a pound up to 11 lb.

The Post Offices thus transferred to the Cape Colony are those of—

Bailey-Brith,	Keimoes,	Morokwen,
Devondale,	Kuruman,	Mosita,
Dry Hartz,	Mafeking,	Setlagoli,
Geluk,	Maribogo,	Taungs,
Genesa,	Maritzani,	Vryburg, and
Groot Chwaing,	Mier,	Zwart Modder.

No alteration has taken place in the postage on parcels for the Bechuanaland Protectorate or other parts of Rhodesia.

XXIII.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month and Twelve Months ended 31st December 1895, compared with the corresponding periods of the year 1894.

MONTH ENDED 31ST DECEMBER.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 31st December		Increase.	Decrease.
	1895.	1894.		
I. Animals, living (for food) - - -	£ 679,631	£ 516,765	£ 162,866	—
II. (A.) Articles of food and drink, duty free - - -	11,672,910	11,030,361	642,549	—
(B.) Articles of food and drink, dutiable - - -	2,328,222	2,259,925	68,297	—
Tobacco, dutiable - - -	299,622	229,748	69,874	—
III. Metals - - -	1,766,646	1,450,770	315,876	—
IV. Chemicals, dyestuffs, and tanning substances - - -	498,925	424,668	74,257	—
V. Oils - - -	791,793	648,264	143,529	—
VI. Raw materials for textile manufactures - - -	8,203,741	7,262,934	940,787	—
VII. Raw materials for sundry industries and manufactures - - -	3,423,617	2,780,024	643,593	—
VIII. Manufactured articles - - -	5,918,317	5,263,443	655,174	—
IX. (A.) Miscellaneous articles - - -	1,315,159	1,155,569	159,590	—
(B.) Parcel post - - -	68,022	48,289	20,633	—
Total Value - - -	36,967,505	33,072,480	3,897,025	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - -	68,630	59,532	9,098	—
II. Articles of food and drink - - -	898,792	860,486	38,606	—
III. Raw materials - - -	1,355,244	1,547,621	—	192,377
IV. Articles manufactured and partly manufactured, viz.:—				
(A.) Yarns and textile fabrics - - -	8,523,703	7,783,218	740,485	—
(B.) Metals and articles manufactured therefrom (except machinery) - - -	2,879,048	2,338,452	640,596	—
(C.) Machinery and millwork - - -	1,321,938	1,079,308	242,630	—
(D.) Apparel and articles of personal use - - -	839,063	678,571	160,492	—
(E.) Chemicals, and chemical and medicinal preparations - - -	669,570	652,448	17,122	—
(F.) All other articles, either manufactured or partly manufactured - - -	2,475,609	2,482,039	—	6,430
(G.) Parcel post - - -	147,941	119,210	28,731	—
Total Value - - -	19,179,538	17,500,585	1,678,953	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	5,027,446	4,827,861	199,585	—

I.—FOREIGN TRADE OF THE UNITED KINGDOM—*cont.*

TWELVE MONTHS ENDED 31ST DECEMBER.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Twelve Months ended 31st December		Increase.	Decrease.
	1895.	1894.		
	£	£	£	£
I. Animals, living (for food) - -	8,966,252	9,039,883	—	123,631
II. (A.) Articles of food and drink, duty free - -	140,125,616	139,450,469	675,147	—
(B.) Articles of food and drink, dutiable - -	25,007,513	24,285,525	811,988	—
Tobacco, dutiable - - -	2,337,783	3,472,256	—	134,473
III. Metals - - -	18,645,036	19,050,718	—	405,682
IV. Chemicals, dyestuffs, and tanning substances - -	6,558,249	6,319,574	238,655	—
V. Oils - - -	8,110,625	7,505,504	605,121	—
VI. Raw materials for textile manufactures - -	70,772,860	70,626,057	146,803	—
VII. Raw materials for sundry industries and manufactures - -	44,114,973	43,091,405	1,023,568	—
VIII. Manufactured articles - - -	75,625,242	68,924,636	6,700,606	—
IX. (A.) Miscellaneous articles - - -	14,355,380	15,719,871	—	1,364,491
(B.) Parcel post - - -	978,101	808,892	169,209	—
Total Value - - -	416,687,630	408,344,810	8,342,820	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURES.

	£	£	£	£
I. Animals, living - - -	790,812	666,332	124,480	—
II. Articles of food and drink - -	11,051,209	10,699,973	351,236	—
III. Raw materials - - -	18,334,292	19,810,587	—	1,476,295
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - -	101,423,997	96,025,467	5,398,530	—
(B.) Metals and articles manufactured therefrom (except machinery) - -	28,907,347	27,979,500	927,847	—
(C.) Machinery and millwork - -	15,215,110	14,205,215	1,009,895	—
(D.) Apparel and articles of personal use - -	9,319,839	8,736,702	583,237	—
(E.) Chemicals, and chemical and medicinal preparations - -	8,295,400	8,470,620	—	175,220
(F.) All other articles, either manufactured or partly manufactured - -	31,493,137	28,120,801	3,372,336	—
(G.) Parcel post - - -	1,337,931	1,109,136	228,795	—
Total Value - - -	226,169,174	215,824,333	10,344,841	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	59,970,763	57,961,534	2,009,229	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 7th December 1895 -	32,906 7	164,382 3	17,533 1
„ 14th „ -	29,010 7	152,223 3	19,506 7
„ 21st „ -	28,571 0	136,157 7	19,946 7
„ 28th „ -	24,377 0	103,556 4	16,168 5
December 1895 - - -	114,865 6	556,320 1	73,155 4
Corresponding month in 1894 -	244,832 7	715,164 1	99,431 7
„ „ 1893 -	241,449 2	588,124 7	74,636 5
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 7th December 1895 -	24 11	24 5	14 1
„ 14th „ -	24 9	23 11	13 11
„ 21st „ -	25 1	23 8	13 11
„ 28th „ -	25 0	23 11	13 10
December 1895 - - -	24 11	23 11	13 11
November 1895 - - -	25 9	25 1	14 2
October „ - - -	24 6	25 5	13 5
September „ - - -	22 10	24 0	13 7
August „ - - -	24 2	20 3	15 7
July „ - - -	24 9	19 8	16 1
June „ - - -	25 9	19 7	16 0
May „ - - -	22 5	20 5	15 2
April „ - - -	20 5	21 0	14 2
March „ - - -	19 11	21 3	13 10
February „ - - -	20 0	21 9	13 8
January „ - - -	20 7	21 6	13 11
December 1894 - - -	20 7	21 5	14 1
„ 1893 - - -	26 8	28 11	18 1
„ 1892 - - -	26 3	24 9	16 10
„ 1891 - - -	37 6	29 11	21 5

III.—EMIGRATION.—DECEMBER 1895.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out of EUROPE during the Month ended 31st December 1895, and the Twelve Months ended 31st December 1895, compared with the corresponding Periods of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.	Total for corresponding Month, 1894.
Month ended 31st December.							
English - -	2,065	177	570	1,223	789	4,824	5,049
Scotch - - -	323	18	35	153	55	534	574
Irish - - -	537	19	19	18	21	614	838
Total of British origin - - }	2,925	214	624	1,394	865	6,022	6,461
Foreigners - -	2,255	248	15	543	153	3,214	3,161
Nationalities not distinguished - }	1	—	5	—	232	238	246
Total - -	5,181	462	644	1,937	1,250	9,474	9,868
Total for corresponding Month, 1894 - }	6,614	346	751	1,113	1,047	9,868	—
Twelve Months ended 31st December.							
English - - -	61,237	14,176	9,299	17,882	10,059	112,653	99,590
Scotch - - -	13,231	1,363	617	2,075	941	18,227	14,432
Irish - - -	52,178	1,119	616	297	276	54,486	42,008
Total of British origin - - }	126,646	16,658	10,532	20,254	11,276	185,366	156,030
Foreigners - -	68,917	5,668	232	5,761	2,160	82,738	67,032
Nationalities not distinguished - }	113	65	24	3	3,545	3,750	3,765
Total - -	195,676	22,391	10,788	26,018	16,981	271,854	226,827
Total for corresponding Months, 1894 - }	159,431	23,633	11,151	16,760	15,852	226,827	—

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION.—DECEMBER 1895.

RETURN of the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM during the Month and Twelve Months ended 31st December 1895, compared with the corresponding Periods of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1895.	1894.	1895.	1894.	1895.	1894.	1895.	1894.	1895.	1894.	1895.	1894.
Month ended 31st December.												
Aliens not stated to be <i>en route</i> to America:*												
Arrived at London -	690	426	250	195	73	68	—	—	48	42	1,061	731
" Grimsby -	116	63	34	39	7	4	—	—	7	11	164	120
" Hull -	61	41	52	68	45	21	—	—	55	79	213	215
" Tyne Ports -	17	15	26	16	180	126	—	—	103	91	326	217
" Leith -	18	22	55	27	—	2	—	—	25	4	98	55
" Newhaven -	—	—	—	—	—	—	353	400	5	—	358	400
" Other ports -	45	85	109	49	78	64	—	—	128	123	360	291
Total -	947	655	526	363	383	298	353	400	371	353	1,580	1,059
Aliens <i>en route</i> to America:												
Arrived at Grimsby -	—	2	96	105	23	39	—	—	33	32	152	178
" Hartlepool, } " West -	151	89	—	—	—	—	—	—	—	—	151	89
" Hull -	135	39	44	27	271	165	—	—	274	368	724	599
" Leith -	221	213	10	55	—	—	—	—	10	2	241	270
" Other ports -	10	—	76	56	1	—	—	—	98	90	185	136
Total -	517	343	226	243	295	204	—	—	415	492	1,453	1,232
Total of aliens <i>en route</i> and of aliens not stated to be <i>en route</i> to America* -	1,464	998	752	606	678	492	353	400	786	845	4,033	3,341
Twelve Months ended 31st December.												
Aliens not stated to be <i>en route</i> to America:*												
Total No. -	14,029	10,818	5,358	6,329	5,932	5,971	7,063	6,763	8,033	8,622	14,415	13,803
Aliens <i>en route</i> to America: Total No. -	7,414	7,263	3,922	5,005	16,014	9,800	—	1	17,022	13,063	44,372	35,134
Total of aliens <i>en route</i> and of aliens not stated to be <i>en route</i> to America* -	21,443	18,081	9,280	11,334	21,946	15,771	7,063	6,764	25,055	21,687	184,787	173,637

* The distinction made in this Return between "Aliens not stated to be *en route* to America," and "Aliens *en route* to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be *en route* to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 213, Sess. 1895, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Goole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the aliens who arrived at ports in the United Kingdom not *en route* to America in the month of December 1895 was 629, and 562 in the same month of 1894; in the twelve months ended December of 1895 the number was 9,854, and in the same months of 1894 the number was 9,821.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER OF RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
Total gazetted - - -	No. 386	No. 329	No. 4,440	No. 4,778
Number gazetted in principal trades and occupations :—				
Farmers - - - - -	27	34	313	281
Publicans and hotel keepers, &c. - -	23	23	311	351
Grocers, &c. - - - - -	26	19	289	280
Builders - - - - -	17	21	229	284
Butchers - - - - -	6	7	144	119
Boot and shoe manufacturers and dealers	8	6	124	110
Tailors, &c. - - - - -	8	7	100	104
Bakers - - - - -	10	6	99	135
Drapers, haberdashers, &c. - - -	11	6	88	105
Greengrocers, fruiterers, &c. - -	6	3	75	57
Decorators, painters, plumbers, &c. -	7	9	74	99
Agents, commission and general	13	5	74	62
Coal and coke merchants and dealers -	8	3	69	66
Auctioneers - - - - -	1	1	57	53
Clerks, commercial and general - -	6	9	56	59
Fishmongers, poulterers, &c. - - -	9	2	48	52
Clothiers, outfitters, &c. - - -	2	3	42	39
Solicitors - - - - -	4	1	42	33
Travellers, commercial, &c. - - -	4	2	41	31
Provision merchants, &c. - - -	3	2	39	38
Engineers and founders, &c. - - -	3	2	39	44
Tobacconists, &c. - - - - -	3	2	37	37
Carpenters and joiners - - - - -	5	1	37	44
General dealers - - - - -	3	4	36	36
Jewellers, watchmakers, importers, silversmiths, &c. - - -	1	4	35	71
Carriers, carmen, lightermen, and hauliers - - - - -	5	1	35	21
Ironmongers - - - - -	2	5	33	38
Confectioners - - - - -	2	3	33	36
Merchants - - - - -	2	—	32	33
Cabinet-makers and upholsterers -	3	2	32	41
Saddlers and harness makers - - -	2	—	30	34
Printers and publishers - - - - -	—	1	28	25
Furniture dealers and makers - - -	4	—	28	35
Wine and spirit merchants, &c. - - -	—	—	27	20
Chemists, druggists, and chemical manufacturers - - - - -	—	5	27	26
Dairymen, cowkeepers, &c. - - -	4	5	27	36
Lodging-house keepers - - - - -	5	2	26	34
Stationers - - - - -	1	1	25	20
Corn, flour, seed, hay, and straw merchants and dealers - - -	2	4	25	47
Gardeners, florists, &c. - - - - -	4	—	25	16
Millers - - - - -	2	3	23	20
Wheelwrights - - - - -	1	2	22	15
Milliners, dressmakers, &c. - - -	2	4	22	29

	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.	No.	No.
Clerks in holy orders - - -	3	—	22	17
Contractors - - -	3	2	20	18
Hosiery, glovers, &c. - - -	—	1	19	12
Restaurant, coffee, and eating-house keepers - - -	—	1	19	26
Fishing net, smack owners, and masters - - -	—	—	19	20
Officers in Army - - -	3	2	18	30
Schoolmasters and schoolmistresses - - -	2	—	17	25
Hair-dressers - - -	3	2	17	18
Carriage, &c. builders - - -	—	4	16	28
China, glass, and earthenware, &c. dealers - - -	—	3	16	14
Blacksmiths, farriers, &c. - - -	1	1	15	24
Curriers, tanners, and leather merchants - - -	2	1	15	18
Timber merchants and wood dealers - - -	—	1	14	34
Architects and surveyors - - -	—	—	13	14
Brokers, stock and share - - -	4	1	13	21
Directors and promoters of public companies - - -	—	1	11	18
Photographers - - -	4	—	11	6
Stone, marble, and monument masons - - -	—	—	9	7
Booksellers and publishers - - -	1	—	8	9
Cab, coach, and omnibus proprietors - - -	—	2	7	11
Cattle and pig dealers - - -	—	—	6	20

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 31st December 1895, and during the Twelve Months of the Year 1895, compared with the corresponding Periods of the Year 1894.

Animals.	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
	No.	No.	No.	No.
Cattle - - - - -	48,218	69,183	781,331	827,251
Sheep - - - - -	15,749	35,867	649,934	957,969
Swine - - - - -	72,788	91,409	549,511	586,552
Goats - - - - -	14	20	4,343	6,620
Horses - - - - -	1,786	1,498	35,239	33,875
Mules or Jennets - - -	1	2	22	23
Asses - - - - -	8	7	1,292	693
Total - - - - -	138,564	197,986	2,021,672	2,412,778

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month and Twelve Months ended 31st December 1895, compared with the corresponding Periods of the Year 1894.

	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Brill	1,496	1,658	19,000	18,400
Soles	5,492	5,920	82,777	83,816
Turbot	5,875	6,507	77,931	84,826
Prime fish not separately distinguished	1,434	2,277	24,902	27,992
Total prime fish	14,297	16,362	204,610	211,034
Cod	39,561	38,429	495,923	436,537
Haddock	204,072	187,209	2,432,944	2,166,898
Hake	3,271	4,875	132,462	124,941
Halibut	2,387	2,509	113,378	102,382
Herrings	102,103	128,670	1,436,701	1,455,613
Ling	6,277	7,228	114,876	104,518
Mackerel	2,245	4,223	373,858	447,822
Pilchards	1,885	5,576	65,135	90,170
Plaice	58,031	53,363	788,923	855,213
Sprats	37,969	16,096	91,035	45,809
Fish not separately distinguished, except } shell fish	70,837	88,228	1,013,238	982,692
Total	542,935	553,368	7,263,083	7,623,529
Shell fish:—	No.	No.	No.	No.
Crabs	33,143	34,041	4,500,309	4,329,867
Lobsters	2,718	4,188	677,373	723,379
Oysters	2,669,000	2,503,000	25,244,000	28,117,000
Other shell fish	Cwts.	Cwts.	Cwts.	Cwts.
	50,996	36,100	576,686	498,092
VALUE.				
	£	£	£	£
Brill	3,970	4,111	48,637	47,289
Soles	37,683	39,490	564,253	540,847
Turbot	25,651	26,093	301,997	297,151
Prime fish not separately distinguished	5,006	5,361	82,224	70,208
Total prime fish	72,310	75,055	997,111	955,795
Cod	26,008	25,673	309,102	277,700
Haddock	120,568	97,791	1,187,398	1,112,969
Hake	3,440	4,682	107,202	94,978
Halibut	7,838	7,424	199,406	186,705
Herrings	34,224	35,987	417,159	431,593
Ling	4,903	5,239	67,025	67,774
Mackerel	2,280	4,003	304,354	353,108
Pilchards	545	1,157	17,144	16,700
Plaice	71,656	59,116	821,005	817,600
Sprats	6,842	4,158	14269	9,605
Fish not separately distinguished, except } shell fish	48,166	52,942	687,029	657,131
Total	398,780	373,127	5,128,204	4,981,958
Shell fish:—				
Crabs	436	156	55,046	52,327
Lobsters	155	230	29,894	31,236
Oysters	13,076	10,605	77,580	84,157
Other shell fish	10,931	9,079	144,478	141,357
Total	24,598	20,870	306,998	309,077
Total value of fish landed	423,378	393,497	5,435,202	5,291,035

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month and Twelve Months ended 31st December 1895, compared with the corresponding Periods of the Year 1894.

	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles)	1,043	1,173	19,351	17,885
Turbot	199	204	3,563	3,434
Cod	31,881	30,445	459,045	477,444
Eel	910	953	14,360	43,930
Flounder Plaice, Brill	6,920	6,605	79,782	81,262
Haddock	94,503	83,365	1,000,907	812,390
Halibut	142	134	28,951	31,463
Herrings	26,949	73,493	4,075,743	4,319,523
Ling	1,835	1,882	164,043	168,612
Mackerel	—	—	1,732	2,823
Saith (Coal Fish)	2,759	2,332	83,570	102,773
Skate	1,941	1,977	59,243	49,228
Sparling	27	10	346	389
Sprats	4,015	1,533	12,840	14,877
Torsk (Tusk)	18	18	10,404	8,759
Whiting	3,898	3,067	43,494	44,524
Fish not separately distinguished, except } shell fish	6,128	3,483	46,679	56,566
Total	183,168	210,345	6,104,053	6,172,092
Shell Fish:—	No.	No.	No.	No.
Crabs	125,761	140,900	2,546,883	2,674,405
Lobsters	34,541	40,641	604,526	714,745
Oysters	39,700	28,500	239,406	281,826
Clams	—	—	—	—
Mussels	3,387	3,692	19,405	25,583
Other shell fish	22,540	20,171	193,944	183,360
	Cwts.	Cwts.	Cwts.	Cwts.
	3,785	3,868	52,529	60,341
VALUE.				
	£	£	£	£
Soles (Lemon Soles)	2,525	1,879	37,653	30,593
Turbot	937	686	11,883	10,075
Cod	15,191	14,973	167,094	161,565
Eel	443	462	8,009	7,480
Flounder, Plaice, Brill	8,124	5,885	77,356	67,834
Haddock	44,253	33,724	409,572	372,162
Halibut	214	242	32,175	34,426
Herrings	4,483	14,043	918,674	769,933
Ling	819	826	42,613	50,916
Mackerel	—	—	1,103	1,425
Saith (Coal Fish)	467	460	9,494	12,389
Skate	651	560	13,467	11,722
Sparling	73	51	1,134	1,399
Sprats	479	110	2,756	1,173
Torsk (Tusk)	4	4	1,445	1,430
Whiting	1,652	1,268	18,639	20,108
Fish not separately distinguished, except } shell fish	1,443	940	10,361	13,853
Total	81,758	73,413	1,763,428	1,565,184
Shell Fish:—				
Crabs	451	544	13,074	12,990
Lobsters	1,547	1,811	25,402	29,200
Oysters	158	127	1,107	1,241
Clams	469	447	2,640	3,317
Mussels	1,304	1,101	12,127	10,498
Other shell fish	630	688	11,860	12,386
Total	4,559	4,718	66,210	69,632
Total value of fish landed	86,317	77,831	1,829,638	1,634,816

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month and Twelve Months ended 31st December 1895, compared with the corresponding Periods of the Year 1894.

	December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles - - - - -	60	81	1,852	4,035
Turbot - - - - -	56	53	981	1,545
Total prime fish - - -	116	134	2,833	5,580
Cod - - - - -	4,528	2,938	42,992	42,567
Haddock - - - - -	3,563	1,243	39,952	26,459
Hake - - - - -	564	1,030	18,723	47,154
Herrings - - - - -	4,430	4,081	126,600	94,176
Ling - - - - -	703	776	28,309	34,569
Mackerel - - - - -	2,531	2,331	314,498	515,191
Sprats - - - - -	—	—	2,673	4,522
Whiting - - - - -	1,042	1,142	11,884	14,024
Fish not separately distinguished, except } shell fish	2,646	2,374	49,281	65,916
Total - - - - -	19,923	15,954	628,745	850,158
Shell Fish:—	No.	No.	No.	No.
Crabs - - - - -	1,496	4,000	240,464	244,337
Lobsters - - - - -	4,016	4,112	275,069	229,235
Oysters - - - - -	14,500	86,900	547,050	1,560,592
Other shell fish - - - -	Cwts. 1,558	Cwts. 1,552	Cwts. 16,233	Cwts. 12,732
VALUE.				
	£	£	£	£
Soles - - - - -	273	372	6,968	10,926
Turbot - - - - -	215	173	3,171	5,523
Total prime fish - - -	488	545	10,139	16,449
Cod - - - - -	1,911	1,564	18,368	19,483
Haddock - - - - -	2,243	1,062	18,168	17,572
Hake - - - - -	189	343	7,502	15,335
Herrings - - - - -	1,094	1,037	34,597	23,229
Ling - - - - -	239	247	10,187	13,062
Mackerel - - - - -	1,402	670	126,677	123,615
Sprats - - - - -	—	—	834	860
Whiting - - - - -	548	531	6,319	6,121
Fish not separately distinguished, except } shell fish	1,290	1,074	23,742	21,688
Total - - - - -	9,464	7,973	256,033	260,464
Shell Fish:—				
Crabs - - - - -	10	25	794	1,252
Lobsters - - - - -	143	148	8,100	6,885
Oysters - - - - -	46	136	1,279	3,039
Other shell fish - - - -	279	258	2,809	2,399
Total - - - - -	478	567	12,982	13,575
Total value of fish landed -	9,942	8,540	269,015	274,039

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.—COTTON RETURNS.—DECEMBER 1895.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month and Twelve Months ended December 1895, compared with the corresponding Months of the Year 1894.

	Month of December		Twelve Months ended December	
	1895.	1894.	1895.	1894.
IMPORTS.				
	No.	No.	No.	No.
American - - -	336,847	583,612	2,958,179	2,981,669
Brazilian - - -	10,191	317	67,459	177,948
East Indian - - -	7,854	2,268	115,033	167,401
Egyptian - - -	92,648	59,359	407,137	371,056
Miscellaneous - - -	8,152	3,452	59,935	64,477
Total - - -	455,692	649,008	3,607,743	3,762,551
EXPORTS.				
	No.	No.	No.	No.
American - - -	16,743	27,658	236,311	259,149
Brazilian - - -	120	542	14,970	37,480
East Indian - - -	4,590	4,250	60,941	109,382
Egyptian - - -	9,875	9,229	85,964	69,804
Miscellaneous - - -	3,163	3,630	34,436	29,991
Total - - -	34,491	45,309	432,622	505,806
FORWARDED from PORTS to INLAND TOWNS.				
	No.	No.	No.	No.
American - - -	227,233	206,886	2,768,924	2,635,739
Brazilian - - -	9,742	1,329	50,668	166,268
East Indian - - -	2,231	1,472	34,318	63,190
Egyptian - - -	45,017	29,685	270,413	283,201
Miscellaneous - - -	13,792	8,809	105,418	91,386
Total - - -	298,015	248,181	3,229,741	3,239,784
FORWARDED from INLAND TOWNS to PORTS.				
	No.	No.	No.	No.
American - - -	441	—	4,469	2,511
Brazilian - - -	—	—	—	—
East Indian - - -	—	—	5	—
Egyptian - - -	—	—	10	—
Miscellaneous - - -	28	105	856	717
Total - - -	469	105	5,340	3,228

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note.—Rouble = 2*s.*; Franc = 9 $\frac{6}{10}$ *d.*; Milreis = 4*s.* 6*d.*; Lira = 9 $\frac{6}{10}$ *d.*; Gulden = 1*s.* 8*d.*; Drachma = 9 $\frac{6}{10}$ *d.*; Lew = 9 $\frac{6}{10}$ *d.*; L. Egyptian = 1*l.* 0*s.* 6 $\frac{1}{4}$ *d.*; Dollar = 4*s.* 2*d.*

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe -	Sept. -	Roubles 47,659,000	47,099,000	370,022,000	380,032,000
France -	Oct. -	Frs. - 301,080,000	282,924,000	2,980,340,000	3,205,327,000
Portugal -	June -	Milreis - 2,827,000	2,673,000	19,668,000	19,571,000
Italy -	Nov. -	Lire - 117,207,000	102,595,000	1,087,168,000	972,729,000
Austria-Hungary -	Nov. -	Gulden 65,519,000	64,177,000	670,602,000	635,860,000
Greece -	Oct. -	Drehms. 11,287,000	9,602,000	86,605,000	87,522,000
Bulgaria -	Sept. -	Lew - 7,469,000	9,740,000	49,553,000	72,766,000
Egypt -	Sept. -	L. Egypt 623,000	718,000	5,647,000	6,621,000
United States -	Oct. -	Dollars 75,067,000	60,020,000	676,076,000	563,610,000
Mexico* -	July -	" - 2,511,000	2,193,000	—	—
British India†	Sept. -	Rupees 5,03,19,330	5,62,91,153	32,05,36,983	33,06,72,908

II.—EXPORTS.

Russia in Europe -	Sept. -	Roubles 65,925,000	62,569,000	516,786,000	482,548,000
France -	Oct. -	Frs. 308,999,000	267,421,000	2,719,093,000	2,458,391,000
Portugal -	June -	Milreis - 2,196,000	1,800,000	12,717,000	11,284,000
Italy -	Nov. -	Lire - 96,085,000	88,306,000	913,099,000	933,791,000
Austria-Hungary -	Nov. -	Gulden - 71,443,000	76,676,000	682,435,000	728,558,000
Greece -	Oct. -	Drehms. 7,513,000	9,051,000	57,574,000	63,901,000
Bulgaria -	Sept. -	Lew - 10,358,000	5,790,000	49,550,000	52,637,000
Egypt -	Sept. -	L. Egypt 374,000	406,000	6,738,000	8,164,000
United States -	Oct. -	Dollars - 87,091,000	83,653,000	645,018,000	660,271,000
Mexico* -	July -	" 6,730,000	7,533,000	—	—
British India†	Sept. -	Rupees 7,78,63,643	7,23,51,786	52,24,05,206	53,59,48,833

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the "special" imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are "general." "Special" means, in the case of imports, imports for home consumption; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Six Months ended 30th September		Six Months ended 30th September	
	1895.	1894.	1895.	1894.
	R.	R.	R.	R.
I.—Animals—living - -	7,03,357	5,72,014	6,60,751	5,52,513
II.—Articles of food and drink -	3,90,53,901	3,33,35,072	15,33,74,381	13,50,46,701
III.—Metals and manufactures of—				
A.—Hardware and cutlery (including plated-ware)	66,03,545	62,89,532	84,735	51,096
B.—Metals - - -	2,94,61,818	2,13,89,943	4,09,023	3,67,408
C.—Machinery and mill-work	1,40,94,572	1,13,31,472	125	2,420
D.—Railway plant and rolling-stock (other than Government stores) -	81,47,344	1,05,44,818	—	—
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	97,01,730	79,95,232	5,78,79,276	5,33,50,715
V.—Oils - - - -	1,75,55,403	56,19,138	39,59,910	32,60,714
VI.—Raw materials and unmanufactured articles - -	2,24,07,062	2,05,46,492	18,38,76,860	22,36,12,739
VII.—Articles manufactured and partly manufactured—				
A.—Yarns and textile fabrics	13,61,31,214	13,15,57,116	6,67,56,484	6,32,68,013
B.—Apparel - - -	71,40,949	64,25,301	9,21,991	6,62,469
C.—Other articles manufactured - -	2,95,36,088	2,50,66,423	3,37,71,167	2,74,19,764
TOTAL - - -	32,05,36,983	33,06,72,908	50,16,94,713	51,30,94,549

* Exports of Indian Produce and Manufacture only.

XXIV.—LIST OF DIPLOMATIC AND CONSULAR REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1895, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.	No.	Place.	Price.	No.	Place.	Price.
1490	Berne -	1½ <i>d.</i>	1546	Cagliari -	1 <i>d.</i>	1602	Caracas -	1½ <i>d.</i>
1491	Copenhagen -	1 <i>d.</i>	1547	Pernambuco -	7½ <i>d.</i>	1603	Sofia -	2½ <i>d.</i>
1492	Stettin -	2½ <i>d.</i>	1548	Madrid -	1½ <i>d.</i>	1604	Belgrade -	2½ <i>d.</i>
1493	Rio Grande do Sul -	1½ <i>d.</i>	1549	Corunna -	5 <i>d.</i>	1605	Shanghai -	2½ <i>d.</i>
1494	Serajevo -	1 <i>d.</i>	1550	Leghorn -	2 <i>d.</i>	1606	Canton -	1½ <i>d.</i>
1495	Buenos Ayres -	9 <i>d.</i>	1551	New Orleans -	2½ <i>d.</i>	1607	Meshek -	1½ <i>d.</i>
1496	Florence -	2 <i>d.</i>	1552	Syra -	1½ <i>d.</i>	1608	Erzeroum -	1 <i>d.</i>
1497	Lisbon -	1½ <i>d.</i>	1553	Genoa -	9 <i>d.</i>	1609	Galatz -	2 <i>d.</i>
1498	Paris -	2 <i>d.</i>	1554	Berlin -	2½ <i>d.</i>	1610	Port Said -	1½ <i>d.</i>
1499	Lima -	1½ <i>d.</i>	1555	Tainan -	1 <i>d.</i>	1611	The Hague -	1½ <i>d.</i>
1500	Patras -	1½ <i>d.</i>	1556	Zanzibar -	4 <i>d.</i>	1612	Calais -	1 <i>d.</i>
1501	Bordeaux -	3 <i>d.</i>	1557	Dunkirk -	1 <i>d.</i>	1613	Newchwang -	1 <i>d.</i>
1502	Madrid -	2 <i>d.</i>	1558	Ningpo -	1 <i>d.</i>	1614	Copenhagen -	1 <i>d.</i>
1503	Amsterdam -	1 <i>d.</i>	1559	Pakhoi -	1 <i>d.</i>	1615	Odessa -	2 <i>d.</i>
1504	Snakim -	1 <i>d.</i>	1560	Chinkiang -	1 <i>d.</i>	1616	Gothenburg -	2 <i>d.</i>
1505	Angora -	1 <i>d.</i>	1561	New York -	2 <i>d.</i>	1617	Mannheim -	1½ <i>d.</i>
1506	Havre -	2½ <i>d.</i>	1562	Batoum -	11½ <i>d.</i>	1618	Old Calabar -	5 <i>d.</i>
1507	Algiers -	11 <i>d.</i>	1563	Naples -	2 <i>d.</i>	1619	Pekin -	2½ <i>d.</i>
1508	La Rochelle -	3 <i>d.</i>	1564	Resht -	4½ <i>d.</i>	1620	Taganrog -	2 <i>d.</i>
1509	Vera Cruz -	2 <i>d.</i>	1565	Copenhagen -	1½ <i>d.</i>	1621	Brindisi -	2½ <i>d.</i>
1510	Puerto Cortez -	1 <i>d.</i>	1566	Porto Rico -	1½ <i>d.</i>	1622	Jeddah -	1½ <i>d.</i>
1511	Taganrog -	1 <i>d.</i>	1567	Malaga -	3 <i>d.</i>	1623	Hamburg -	3 <i>d.</i>
1512	Baltimore -	1½ <i>d.</i>	1568	Galveston -	7½ <i>d.</i>	1624	Angora -	1½ <i>d.</i>
1513	Mexico -	1½ <i>d.</i>	1569	Tabreez -	½ <i>d.</i>	1625	Buda-Pesth -	1½ <i>d.</i>
1514	Zaila -	1 <i>d.</i>	1570	Fiji -	½ <i>d.</i>	1626	Beyrout -	1 <i>d.</i>
1515	Zomba -	3½ <i>d.</i>	1571	Athens -	2½ <i>d.</i>	1627	Bushire -	2 <i>d.</i>
1516	Valparaiso -	2½ <i>d.</i>	1572	Bilbao -	3½ <i>d.</i>	1628	Stettin -	2½ <i>d.</i>
1517	Charleston -	2½ <i>d.</i>	1573	Barcelona -	3 <i>d.</i>	1629	Porto Rico -	1 <i>d.</i>
1518	Serajevo -	1 <i>d.</i>	1574	Piome -	2 <i>d.</i>	1630	Rotterdam -	½ <i>d.</i>
1519	Saigon -	1 <i>d.</i>	1575	Tahiti -	1 <i>d.</i>	1631	Alexandria -	1½ <i>d.</i>
1520	Bangkok -	2 <i>d.</i>	1576	San Francisco -	6 <i>d.</i>	1632	Tokio -	2½ <i>d.</i>
1521	Tripoli -	1 <i>d.</i>	1577	Ichang -	1½ <i>d.</i>	1633	Tangier -	1½ <i>d.</i>
1522	Batavia -	1½ <i>d.</i>	1578	Amoy -	1½ <i>d.</i>	1634	Oporto -	1½ <i>d.</i>
1523	Dakar -	½ <i>d.</i>	1579	Wenchow -	1 <i>d.</i>	1635	St. Petersburg -	4 <i>d.</i>
1524	Havana -	2 <i>d.</i>	1580	Smyrna -	2½ <i>d.</i>	1636	Dantzic -	2 <i>d.</i>
1525	Riga -	2 <i>d.</i>	1581	Nice -	1½ <i>d.</i>	1637	Macao -	1 <i>d.</i>
1526	Trebizond -	1½ <i>d.</i>	1582	Soul -	1½ <i>d.</i>	1638	Hiogo and Osaka -	6 <i>d.</i>
1527	Pireus -	2½ <i>d.</i>	1583	Rio Grande do Sul -	4 <i>d.</i>	1639	Naples -	1½ <i>d.</i>
1528	Guayaquil -	1½ <i>d.</i>	1584	Nagasaki -	1 <i>d.</i>	1640	Kiungchow -	½ <i>d.</i>
1529	Marseilles -	1½ <i>d.</i>	1585	Hakodate -	1 <i>d.</i>	1641	Rome -	1½ <i>d.</i>
1530	Wuhu -	1½ <i>d.</i>	1586	Frankfort -	3 <i>d.</i>	1642	Beira -	½ <i>d.</i>
1531	Rio de Janeiro -	2½ <i>d.</i>	1587	Samoa -	1 <i>d.</i>	1643	St. Jago de Cuba -	4½ <i>d.</i>
1532	Trieste -	2 <i>d.</i>	1588	Cherbourg -	1½ <i>d.</i>	1644	Christiania -	6 <i>d.</i>
1533	Brest -	1½ <i>d.</i>	1589	Damascus -	1 <i>d.</i>	1645	Lisbon -	1½ <i>d.</i>
1534	Stockholm -	2 <i>d.</i>	1590	New York -	1 <i>d.</i>	1646	Brussels -	½ <i>d.</i>
1535	Warsaw -	1 <i>d.</i>	1591	Athens -	2 <i>d.</i>	1647	Vera Cruz -	½ <i>d.</i>
1536	Boston -	1½ <i>d.</i>	1592	Baghdad -	1 <i>d.</i>	1648	Tunis -	1 <i>d.</i>
1537	Mozambique -	2½ <i>d.</i>	1593	Vienna -	1½ <i>d.</i>	1649	Antwerp -	1 <i>d.</i>
1538	Callao -	1 <i>d.</i>	1594	Montevideo -	2½ <i>d.</i>	1650	Tokio -	1 <i>d.</i>
1539	Aleppo -	1½ <i>d.</i>	1595	Swatow -	1½ <i>d.</i>	1651	Hawaiian Islands -	½ <i>d.</i>
1540	Jaffa -	½ <i>d.</i>	1596	Poochow -	1 <i>d.</i>	1652	Stettin -	1½ <i>d.</i>
1541	Boston -	1 <i>d.</i>	1597	Tamsui -	1 <i>d.</i>	1653	Bangkok -	1 <i>d.</i>
1542	New Orleans -	1½ <i>d.</i>	1598	Chungking -	1 <i>d.</i>	1654	Batoum -	1½ <i>d.</i>
1543	Chicago -	3 <i>d.</i>	1599	Chefoo -	1 <i>d.</i>	1655	Mexico -	9½ <i>d.</i>
1544	Palermo -	2½ <i>d.</i>	1600	Tokio -	1½ <i>d.</i>	1656	Odessa -	1½ <i>d.</i>
1545	Bengazi -	1½ <i>d.</i>	1601	Bangkok -	1 <i>d.</i>			

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources :—

No.		Price.
350	<i>Servia</i> .—Report on the Mines of Servia -	2 <i>d.</i>
351	<i>Denmark</i> .—Further Report on the Free Port of Copenhagen -	11½ <i>d.</i>
352	<i>Italy</i> .—Report on the Volterra Alabaster Industry -	7½ <i>d.</i>
353	<i>Italy</i> .—Report for the Years 1892-93 on the Yield of Cocoons in Italy and on the condition of the Italian Silk Trade to May 1894 -	1 <i>d.</i>

LIST OF DIPLOMATIC AND CONSULAR REPORTS—*cont.*

No.		Price.
354	<i>Austria-Hungary</i> .—Report on the Mining Industry in Bosnia and Herzegovina - - - - -	1 <i>d.</i>
355	<i>Russia</i> .—Report on the Peasantry and Peasant Holdings in Poland - - - - -	2½ <i>d.</i>
356	<i>Russia</i> .—Report on the Agricultural Position of Russia - - - - -	1 <i>d.</i>
357	<i>Siam</i> .—Report on the Teak Trade in Siam - - - - -	4 <i>d.</i>
358	<i>Paraguay</i> .—Report on the New Australia Colony in Paraguay - - - - -	1½ <i>d.</i>
359	<i>Bavaria and Würtemberg</i> .—Abstract of the Reports for the Year 1894 of the Bavarian and Würtemberg Factory Inspectors - - - - -	½ <i>d.</i>
360	<i>United States</i> .—Report on the Agricultural Condition of the United States - - - - -	2 <i>d.</i>
361	<i>Germany</i> .—Report on the Agricultural Position of Germany - - - - -	4½ <i>d.</i>
362	<i>France</i> .—Report on the Tramway System of Paris and the Department of the Seine - - - - -	2 <i>d.</i>
363	<i>France</i> .—Report on the Use of the "Bouillie Bordelaise" in connection with Potato and Vine Diseases - - - - -	½ <i>d.</i>
364	<i>Italy</i> .—Report on the Italian Cotton Industry - - - - -	2½ <i>d.</i>
365	<i>Bavaria and Würtemberg</i> .—Report on the Co-operative Agricultural Credit Associations on the Raiffeisen System in Bavaria and Würtemberg - - - - -	1 <i>d.</i>
366	<i>France</i> .—Report for the Year 1894 on the Fluvial Traffic of Rouen and the Waterways of the Seine Basin - - - - -	11½ <i>d.</i>
367	<i>Brazil</i> .—Report on a Tour in the German and Italian Colonies in the Valley of the River Cohy - - - - -	3½ <i>d.</i>
368	<i>Greece</i> .—Report on the Emery Districts of Naxos - - - - -	10 <i>d.</i>
369	<i>Argentine Republic</i> .—Report on the Agricultural and Pastoral Condition and Prospects of the Argentine Republic - - - - -	1½ <i>d.</i>
370	<i>Colombia</i> .—Report on the Agricultural Products of Tolima - - - - -	1 <i>d.</i>
371	<i>Russia</i> .—Report on Measures Adopted for Improving the Commercial Prosperity of Archangel - - - - -	½ <i>d.</i>
372	<i>Russia</i> .—Report on the Association of Working Men known as Artels - - - - -	1½ <i>d.</i>
373	<i>Africa (Central)</i> .—Report on the Botanical Aspects of British Central Africa - - - - -	1½ <i>d.</i>
374	<i>Colombia</i> .—Report on the Commercial Importance of the Port of Barranquilla - - - - -	2 <i>d.</i>
375	<i>Greece</i> .—Report on the Deposits of Magnesite Ore and the Manufacture of Magnesite Fire-Bricks in Eubœa - - - - -	2 <i>d.</i>
376	<i>Denmark</i> .—Special Report on the Agricultural Condition of Denmark - - - - -	1 <i>d.</i>
377	<i>Denmark</i> .—Report on the Port of Frederikshavn - - - - -	4½ <i>d.</i>
378	<i>Italy</i> .—Report on the Pumice-stone Industry of the Lipari Islands - - - - -	½ <i>d.</i>
379	<i>France</i> .—Report on the Principal Fibres obtainable in the Society Islands - - - - -	½ <i>d.</i>
380	<i>Servia</i> .—Report on Apiculture in Servia - - - - -	1½ <i>d.</i>
381	<i>Belgium</i> .—Report on the Ivory Trade of Antwerp - - - - -	½ <i>d.</i>
382	<i>Germany</i> .—Report on the German Colonies in Africa and the South Pacific - - - - -	5 <i>d.</i>
383	<i>France</i> .—Report on the Bordeaux International Exhibition of 1895 - - - - -	1 <i>d.</i>
384	<i>Brazil</i> .—Report on the causes which have brought about a Diminution in the Receipts at the Custom-House at Rio de Janeiro - - - - -	½ <i>d.</i>
385	<i>Mexico</i> .—Report on the Culture of Cacao, Vanilla, India-Rubber, Indigo, and Bananas in Mexico - - - - -	2½ <i>d.</i>
386	<i>France</i> .—Report on the International Congress on Technical, Industrial and Commercial Education held at Bordeaux in September 1895 - - - - -	1 <i>d.</i>

XXV.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
27	Mauritius	1890	85	Hong Kong	1892
28	Gambia	"	86	St. Lucia	"
29	Hong Kong	"	87	Falkland Islands	"
30	Ceylon	1889	88	Gold Coast	"
31	Lagos	"	89	Basutoland	1892-93
32	Lagos	1890	90	Ceylon	1892
33	Ceylon	"	91	Zululand	"
34	Newfoundland	"	92	Mauritius	"
35	Jamaica	1889-91	93	Labuan	"
36	Victoria	1890	94	British Honduras	"
37	British New Guinea	1890-91	95	Lagos	"
38	St. Vincent	1891	96	Sierra Leone	"
39	Bermuda	"	97	Fiji	"
40	Seychelles and Rodrigues	1889 and 1890	98	Rodrigues	"
41	Gambia	1891	99	St. Helena	"
42	Barbados	"	100	British Bechuanaland	1892-93
43	Turks Island	"	101	British Guiana	"
44	Bahamas	"	102	Newfoundland	1892
45	Fiji	1890	103	Jamaica	1892-93
46	Seychelles	1891	104	Victoria	1892
47	British Bechuanaland	1890-92	105	Bermuda	1893
48	Malta	1890 and 1891	106	Gambia	"
49	Gibraltar	"	107	Hong Kong	"
50	Falkland Islands	"	108	St. Vincent	"
51	Leeward Islands	"	109	Gibraltar	"
52	Grenada	1890 and 1891	110	Bahamas	"
53	St. Lucia	"	111	Turks and Caicos Islands	"
54	Labuan	"	112	Leeward Islands	"
55	British Guiana	"	113	Barbados	"
56	Zululand	"	114	Malta	"
57	Mauritius	"	115	Zululand	"
58	Lagos	"	116	British Honduras	"
59	Straits Settlements	"	117	Trinidad and Tobago	"
60	Ceylon	"	118	Falkland Islands	"
61	Natal	1891-92	119	Rodrigues	"
62	Basutoland	"	120	Mauritius	"
63	St. Helena	1891	121	Straits Settlements	"
64	Sierra Leone	"	122	Labuan	"
65	Jamaica	1891-92	123	Basutoland	1893-94
66	Trinidad and Tobago	1891	124	St. Lucia	1893
67	Newfoundland	"	125	St. Helena	"
68	British New Guinea	1891-92	126	Ceylon	"
69	Victoria	1891	127	Fiji	"
70	Rodrigues	"	128	Grenada	"
71	Bermuda	1892	129	Sierra Leone	"
72	Fiji	1891	130	British Bechuanaland	1893-94
73	British Honduras	"	131	British New Guinea	1892-93 and 1893-94
74	Turks and Caicos Islands	1890	132	Lagos	1893
75	Gibraltar	1892	133	British Guiana	1893-94
76	Leeward Islands	"	134	Jamaica	"
77	Bahamas	"	135	Newfoundland	1893
78	Trinidad and Tobago	"	136	Gold Coast	"
79	Malta	"	137	Zululand	1894
80	Gambia	"	138	Bermuda	"
81	Straits Settlements	"	139	Bahamas	"
82	Grenada	"	140	Barbados	"
83	Barbados	"	141	Turks and Caicos Islands	"
84	St. Vincent	"	142	Malta	"
			143	Gambia	"

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast	Economic Agriculture.
2	Zululand	Forests.
3	Sierra Leone	Geology and Botany.
4	Canada	Advantages to Emigrants.

THE
Board of Trade Journal
OF
TARIFF AND TRADE NOTICES
AND
MISCELLANEOUS COMMERCIAL INFORMATION.

Vol. XXI.]

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[No. 115.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

The returns received show an improvement in the state of employment, both as compared with last month (for which, however, the returns were affected by the Christmas holidays), and also compared with January 1895.

In the 96 trade unions, with an aggregate membership of 401,410, making returns, 17,901 (or 4·5 per cent.) are reported as unemployed at the end of January, compared with 4·8 per cent. in December, and with 8·2 per cent. in the 76 unions, with a membership of 370,335, from which returns were received for January 1895.

Coal Mining.—Employment continues good in nearly all districts. At 971 pits from which returns have been received and employing 280,101 workpeople, the average number of days worked in January was 4·94 per week, as compared with 4·88 days in December and 4·70 days in January 1895. The proportion of unemployed miners in trade unions in Northumberland and Durham was 1·7 per cent. at the end of January, the same as in December, but less than in January 1895, when the percentage was 2·7.

Iron Mining.—Employment has improved and is now very good. The average number of days worked at 114 iron mines, employing 13,215 workpeople, was 5·70 per week in January, against 5·57 days in the previous month and 5·56 days in January 1895.

In the *Pig Iron* industry more furnaces are in blast and more men employed than in December or a year ago. At the end of January the ironmasters making returns had 328 furnaces in blast, being 4 more than at the end of December, and 20 more than at the corresponding date of last year. The number of workpeople employed at these furnaces was 21,071, an increase of 191 over the previous month, and of 1,057 over January 1895.

Employment in *Steel Works* has improved, the number employed in the 89 works for which returns have been received being 14 per cent. greater than in the corresponding period of 1895.

Returns from 71 *Puddling Furnaces* and *Rolling Mills* show a slight improvement compared with last year.

Employment in *Tinplate Works* is unsatisfactory, and many mills are idle. Returns from 37 tinplate manufacturers give the number of their mills in operation at the end of the month as 156, being 16 less than in the previous month, but 2 more than in January 1895.

In the *Engineering* and kindred trades the percentage of unemployed union members has fallen from 5.0 in December to 4.3 at the end of January, compared with 8.7 per cent. in January 1895.

In the *Shipbuilding* trades an improvement has also taken place, the percentage of unemployed at the close of the month being 9.1, compared with 10.5 in December. In January 1895, during the frost, the percentage reached 17.3.

The *Building* trades as a whole continue steadily employed. The percentage of unemployed in unions making returns for January is 3.7, compared with 3.8 last month, and 8.2 in January 1895, when the long frost was in progress.

The *Furnishing* and *Woodworking* trades remain steady. The percentage of unemployed in unions making returns is 4.2, compared with 4.4 in December and 6.7 in January 1895.

Some seasonal falling off is observable in employment in the *Printing* and *Bookbinding* trades, the percentage of unemployed union members having risen from 4.1 in December to 5.4 in January. The percentage for January 1895 was 5.3. Employment in the *Paper* trade remains steady, and the percentage of unemployed union members is 3.2, compared with 3.1 last month.

Employment in the ready-made branch of the *Tailoring* trade is better, but the bespoke branch is somewhat slack.

Employment in the *Boot* and *Shoe* trade continues to be only moderate in the principal centres.

Owing to seasonal causes, a slight falling off has taken place in employment in the *Leather* trades, and the percentage of unemployed in unions making returns is 5.4, compared with 4.1 in December and 8.8 in January 1895.

Employment for *Cotton* spinners has improved, but weavers in many cases have been irregularly employed. The *Woollen* and *Worsted* trades continue well employed in most districts. Employment in the *Hosiery* trade is, in general, good; in the *Silk* trade it is fair in most districts.

Information respecting 494 *Textile Mills*, employing about 86,000 women and girls, shows a decline in the state of employment, 87 per cent. being in mills giving full employment, compared with 91 per cent. among those for whom returns were received in December. The decline occurred in the weaving branch of the cotton trade.

Trade Disputes.—The number of disputes occurring in January was 50, as compared with 37 in December and 57 in January

1895. Eleven disputes took place in the textile trades, ten in mining and quarrying, seven in the clothing trades, six in metal trades, four in connection with engineering and shipbuilding, two each in the building and waterside trades, and eight in the miscellaneous group of industries. In 45 of the 50 disputes, 7,331 workpeople were involved—an average of 163 persons per dispute—against 115 in the previous month. Sixteen old disputes, involving 5,672 persons, were brought to a termination in January, and at the end of the month, 12 new and 20 old disputes, affecting together 4,562 persons, were known to be still unsettled.

Changes in Rates of Wages.—The changes reported during January included nearly 11,000 workpeople who received increases averaging about 1s. 9d. per week, and over 133,000 who sustained decreases averaging about 3½d. per week, resulting in a nett average decrease of less than 2d. per week for the total number (144,000) affected. About 6,000 engineers on the Clyde and over 1,000 at Belfast are estimated to have received advances in accordance with the recent settlement. In the coal-mining industry, 100,000 miners in South Wales and Monmouthshire, and 30,000 in Northumberland, sustained reductions in their rates of wages. Decreases, mainly of 10 per cent., were also reported as affecting 2,320 workpeople in the South Wales tinplate trade.

BOILER EXPLOSION AT GRANNY HILL QUARRIES, BRIGHOUSE.

Report of Formal Investigation.—A formal investigation was held at the Town Hall, Halifax, on the 6th and 7th instant, into the circumstances attending the explosion of a boiler at the Granny Quarries, Brighouse, on the 31st December last, whereby loss of life ensued.

The boiler, known as Booth's patent, was made in the year 1875, and it was attached to a Guy crane. It was used with others for lifting stone, and the boiler was examined from time to time by the engineer to the company, by whom the quarries were owned at the time of the explosion.

The fire was invariably banked at night and set away the following morning, the boiler being worked at a pressure of about 60 lbs. per square inch.

On the 30th December the boiler was pumped up about 4 p.m., and the fire was banked. On the morning of the 31st December the fire was set away, and about 7.20 a.m. the engineer was of opinion that the water-gauge showed half-glass. About 7.35 a.m. the boiler exploded, the crown of the furnace collapsed and ruptured the boiler, left its seat and fell into the quarry. A man standing close to the crane appears to have been so frightened that he ran away, and running in the wrong direction he fell into the quarry sustaining injuries from which he died.

The Court found that the explosion was due to the crown of the firebox, which had become wasted by corrosion, being over-

heated from shortness of water, and the engineer was deceived as to the water on the morning of the explosion, either from insufficiency of light from a match or from the top-cock of the gauge being partly closed. They found that some measure of blame attached to him for neglecting to ascertain beyond doubt that there was sufficient water in the boiler, but as this was a casual and not a serious act of negligence, they exonerated the owners from blame, and made no order as to costs.

The Court was of opinion that the fire had probably been set away, owing to high wind, during the night previous to the explosion, that steam had blown off, and that an unusual quantity of water had consequently been evaporated.

AWARDS FOR GALLANTRY.

The Board of Trade have awarded a binocular glass to John Turner, skipper, and bronze medals for gallantry in saving life at sea and a sum of 2*l.* each, to Robert Woodgate, 2nd hand, and Thomas Gardiner, boatswain, of the steam trawler "*Datura*," of Milford, in recognition of their services in rescuing the shipwrecked crew of the schooner "*John Williams*," of Carnarvon, which foundered at sea on 24th December last.

The Board of Trade have awarded a binocular glass and a silver medal for gallantry in saving life at sea to Henry W. Willis, skipper, and silver medals for gallantry in saving life at sea and a sum of 2*l.* each to Thomas Hewett, 3rd hand, and Ernest Saunderson, 4th hand, of the fishing smack "*Amy*," of Yarmouth, in recognition of their services in rescuing the crew of the fishing smack "*Edward and Sarah*," of Yarmouth, which was abandoned in the North Sea on the 8th December last.

The Board of Trade have awarded a binocular glass to Mr. J. F. Nielsen, master of the Norwegian barque "*Kalstad*," of Kragero, in recognition of his kindness and humanity to the shipwrecked crew of the schooner "*Pattie*," of Hull, which was abandoned at sea on the 18th December last.

The Board of Trade have awarded their bronze medal for gallantry in saving life at sea, to Mr. Andrew W. Simpson, 2nd officer of the S.S. "*Clan Mackenzie*," of Glasgow, in recognition of his services in connexion with the rescue of the crew of the S.S. "*Topaz*," of Glasgow, which was abandoned in the English Channel on the 5th December last.

GOVERNMENT CONTRACTS.

War Office.—The Secretary of State for War hereby gives notice that builders desirous of being applied to when tenders are invited for work in any military district should address the general officer commanding such district, giving trade and bank

references, and stating whether they wish to tender for (A) jobbing work, painting, and repairs; (B) new buildings or works up to 5,000*l.* estimated value; or (C) new buildings or works of any magnitude.

Applications unaccompanied by references will not be noticed.

Firms whose names are already on the War Office list need not apply.

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply wrought-iron bearing plates for rails.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that office by *two* o'clock p.m. on Tuesday the 18th February 1896, after which time no tender will be received.

SHIPS FOR SALE.

Tenders will be received until noon on Thursday, the 20th February, for the purchase of the following ships, as they lie:—

	Tonnage B.M.
"Talbot." 6th rate at Woolwich - -	500 .
Torpedo boat, No. 1, 1st class, at Portsmouth	—
Swimming vessel (late mortar boat) No. 18,	
at Devonport - - - -	179 $\frac{89}{94}$
	(approx.)
Hopper barge at Sheerness - -	110

Forms of tender, containing Conditions of Contract and all particulars, may be obtained on personal application at the Admiralty or by letter addressed "The Director of Navy Contracts, Admiralty, Whitehall, London, S.W."

(Signed) T. GWYN,
Director of Navy Contracts.

RUSSIAN PAPER MONEY BY POST.

A notice, dated 21st January last, has been received from the Postmaster-General warning the public against sending Russian paper money by post to Russia, where it is dutiable, so that its transmission by post to or from Russia contravenes the Postal Union regulations prohibiting the use of the post for the conveyance of dutiable articles. A fine of 25 per cent. of the value is chargeable in Russia on paper money sent contrary to this prohibition, and this fine, according to a recent notification of the Russian Post Office, is rigorously enforced.

INTERNATIONAL EXHIBITION AT INNSBRUCK OF APPLIANCES
FOR PHYSICAL TRAINING, &C.

The Secretary of State for Foreign Affairs has received from the Austro-Hungarian Chargé d'Affaires at the Court of Her Majesty, information that an International Exhibition of appliances for physical training, for preservation of health, for sport and for the branches of industry connected with these matters, will be held at Innsbruck between May and October of this year. The programme of the exhibition may be seen between the hours of 11 and 6 at the Commercial Department of the Foreign Office.

FORMALITIES AS TO BILLS OF LADING FOR BRAZIL.

With reference to the notification on page 6 of the "Board of Trade Journal" for July 1895, to the effect that the authorities of the State of Para had issued an Order that goods shipped on bills of lading made out to order should not be receivable by the holders of such bills until the absent shippers of the goods pay to the State authorities an annual tax of reis 300 \$000 as merchants trading without a settled domicile, a despatch, dated 18th January last, has been received at the Foreign Office from Mr. E. C. H. Phipps, Her Majesty's Minister at Rio de Janeiro, stating that, according to a despatch from Her Majesty's Consul at Para, the above Order has recently been modified, so as no longer to interfere with the legitimate business of properly established firms.

The Order would now appear to be enforced only so as to reach such of the persons originally intended to be included in its scope as have no fixed domicile at Para.

CERTIFICATE OF ALTERATION OF THE YORKSHIRE FISHERY
DISTRICT.

The following is a copy of a certificate issued by the Board of Trade on the 21st January 1896 for the alteration of the Yorkshire fishery district:—

Salmon and Freshwater Fisheries Acts, 1861 to 1892.

Whereas the fishery district of the Rivers Derwent, Wharfe, Nidd, Ure, Swale, and Ouse, commonly known as the Yorkshire fishery district, was formed and its limits were defined by a certificate under the hand of one of Her Majesty's Principal Secretaries of State, dated the 23rd day of April 1874:

And whereas the board of conservators of the said district have duly made application to the Board of Trade to grant a certificate enlarging and altering the limits of the said district:

Now, therefore, the Board of Trade, by virtue of the powers conferred upon them by the Salmon and Freshwater Fisheries Acts, 1861 to 1892, and otherwise, do by this certificate enlarge and alter the limits of the said fishery district, so that the limits of the said Rivers Derwent, Wharfe, Nidd, Ure, Swale, and Ouse, and of the fishery district of the said rivers shall include all such estuaries, rivers, streams, brooks, lakes, ponds, canals, dykes,

cuts, drains, channels, watercourses, and waters, and all such portions of the sea and sea coast as lie within the following limits; namely, a line commencing at the Red House, near Hayburn Wyke, in the North Riding of the county of York, and thence drawn straight to the nearest point of the natural watershed between the rivers and streams flowing into the sea north of the said house on the one hand and the rivers and streams flowing into the sea or into any estuary south of the said house on the other hand, thence continued along such watershed until it reaches the natural watershed of the rivers flowing into the Humber, thence along such last-named watershed until it reaches the parish church of Hatfield in the West Riding of the county of York, thence straight to Thorne Station on the Manchester, Sheffield, and Lincolnshire Railway, thence along such railway until it reaches the boundary between the counties of York and Lincoln, thence along such boundary to the point nearest to Green Bank End, thence straight to Whitgift Hall in the West Riding of the county of York, thence straight through Bosom Cross in the said riding until it reaches the boundary between the counties of York and Lincoln at or near Trent Fall, thence along such boundary in the River Humber until it intersects a line drawn true south-west from Spurn Head in the East Riding of the county of York, thence true east-south-east until it reaches the seaward boundary of the waters within which Her Majesty's subjects have by international law the exclusive right of fishing, thence in a northerly direction along such boundary until it reaches a point true east of the Red House aforesaid, and thence straight to such house where it began—the limits of the said rivers and fishery district being more particularly shown on a map marked S. 279—1896, and deposited herewith with the clerk of the county council of the West Riding of the county of York.

And the said rivers and fishery district as hereby enlarged and altered shall be for all purposes rivers and a fishery district within the meaning of the Salmon and Freshwater Fisheries Acts, 1861 to 1892, and may be described as the "Yorkshire Fishery District."

Signed and sealed by order of the Board of Trade, this twenty-first day of January 1896.

(Signed) A. D. BERRINGTON,
An Assistant Secretary to the
said Board.

INSPECTORS OF WEIGHTS AND MEASURES.

The following inspectors of weights and measures have, during the month of January 1896, passed the examination provided for in the 11th section of the Weights and Measures Act, 1889:—

Milner, John William	-	-	London County.
Patrick, William	-	-	Scarborough.

II.—THE FOREIGN TRADE OF THE UNITED KINGDOM.*

The total value of the imports into the United Kingdom from foreign countries and British possessions during the month of January 1896 was 38,473,856*l.*, as compared with 36,743,481*l.* in the corresponding month of 1895—an increase of 1,730,375*l.*, equal to 4·71 per cent., and the exports of British and Irish produce and manufactures were valued at 21,127,168*l.* as compared with 18,224,236*l.*, an increase of 2,902,932*l.*—or 15·93 per cent. The value of the exports of foreign and colonial merchandise amounted to 4,825,707*l.* against 3,887,258*l.* in January 1895, an increase of 938,449*l.* or 24·14 per cent.

The classes of goods imported which are higher in value last month than in January 1895, are animals, living; articles of food and drink (duty free); tobacco, metals, chemicals, dye-stuffs, and tanning substances, oils, raw materials for sundry industries, and manufactures, manufactured articles, and miscellaneous articles, and, in the aggregate, the increase in these classes is 2,418,325*l.* Those showing decreased values are articles of food and drink (dutiable), raw materials for textile manufactures and imports by parcel post. The total decrease in these is 697,950*l.*, and of this decrease, raw materials for textile manufactures alone account for 624,047*l.*, a fact which may be accounted for by the falling off in the imports of raw cotton, raw silk, and goat's wool. In raw cotton the quantity imported last month was 1,372,622 cwts. less, and the value 1,131,399*l.* less than in January 1895.

The principal increase is seen in articles of food and drink (duty free), of which the value of the imports last month was 640,670*l.* in excess of that for the same period of 1895; the chief articles contributing to the increase being wheat meal and flour, bacon, beef, salted and fresh, fresh mutton, fish, fresh and cured, lard, and refined and unrefined sugar. Raw materials for sundry industries and manufactures show an increase of 496,205*l.*, chiefly made up of sawn wood, caoutchouc, tallow, and stearine, manures (nitrate of soda), wet hides, and rosin. Manufactured articles show an increase of 403,777*l.*

As regards the exports of British and Irish produce and manufacture, improvement is to be found in every class: animals, living, articles of food and drink, raw materials, yarns and textile fabrics, metals and articles manufactured therefrom, machinery and mill-work, apparel and articles of personal use, chemicals and chemical and medicinal preparations, and all other manufactured articles, and parcel post articles, all showing increases, the aggregate

* See also Statistical Tables.

increased value being 2,902,932*l.* The increase as regards these exports is mainly attributable to the larger shipments of yarns and textile fabrics, metals, and articles manufactured therefrom, and machinery and mill-work. The value of coal, coke, and fuel shipped was 183,874*l.* in excess of the corresponding month of the previous year; of cotton piece goods, 112,943*l.* (in this article there was a decrease in the quantity of 27,644,300 yards); woollen and worsted yarn, 172,094*l.*; and woollen tissues, 164,869*l.* The value of iron, wrought and unwrought, exported increased by 364,986*l.*, and machinery by 214,998*l.* The increased value of the exports of apparel and articles of personal use was 208,007*l.*

Taking the whole of the year 1895, the increase in the imports over 1894 was most noticeable in the case of the imports from France, the value of which rose from 43,450,074*l.* to 47,452,860*l.*, an increase of 4,002,786*l.* There was also a large increase in the imports from the Argentine Republic.

As regards the exports of British and Irish produce, there was a large increase in the case of Germany—from 17,796,129*l.* to 20,646,918*l.*; and the exports to the United States rose in value from 18,799,485*l.* in 1894 to 27,998,063*l.* in 1895.

It may be useful to note that certain changes have been made in the arrangement of the accounts, the principal of these being as follows:—

As regards the imports, the changes are—the imports of oxen and bulls, cows, and calves, are shown now only from the Channel Islands, Canada, United States, Argentine Republic, and other countries; of sheep and lambs only from Canada, the United States, Argentine Republic, and other countries; of swine, the total imports only are now shown; butter imports from New South Wales, Victoria, and New Zealand, in lieu of “Australasia”; the imports of horses only from the United States, Canada, and other countries, without distinguishing stallions, mares, and geldings. There is a re-classification of the articles included under the heading of goats-wool. In the list of exports of British and Irish produce, the changes include the following:—the exports of horses are now shown only to Holland, Belgium, France, and other countries, without distinguishing stallions, mares, and geldings; and cattle, sheep, and lambs, swine, and animals of other kinds, which were formerly included with other animals, are now shown separately (number and value).

III—THE IMPORTATION OF FOREIGN PRISON-MADE GOODS.

A departmental committee consisting of the Right Hon. Lord Thring, K.C.B.; Mr. A. E. Bateman, C.M.G.; Mr. Batty Langley, M.P.; Col. the Hon. F. C. Bridgeman, M.P.; Mr. H. J. Roby, M.P.; Mr. E. J. Ruggles Brise; Mr. H. A. D. Seymour, and Col. Howard Vincent, C.B., M.P., with Mr. J. G. Willis as Secretary, was appointed in April 1895 by the President of the Board of Trade to inquire into the extent to which goods made in foreign prisons are imported into the country, and to report whether any, and if so, what steps can be taken effectually to restrict the importation of such goods.

It is stated in the report which has recently been issued, and which is signed by all the members with the exception of Colonel Howard Vincent, M.P., and Colonel Bridgeman, M.P. who withdrew, that the committee received evidence from eight witnesses representing the brush making industry, five of these being manufacturers, and three workmen delegated by their trade societies; from a dealer in brush-makers' materials; from a mat and matting manufacturer, president of an Employers' Association and speaking "for two-thirds of the whole trade"; and from two working mat-makers' delegates from their unions in London and Suffolk.* They further heard evidence relating to fancy leather articles of stationery, to cloth-covered buttons, and to rush goods. They also had the opportunity of examining the "special commissioner," whose account of his visit to certain German prisons formed the basis of an article in the "Hardwareman"; an English mat-maker who formerly held a "labour concession" in a Belgian State institution; and two gentlemen, now holding such concessions, who came over to London, one from Brauweiler in Germany, and one from Merxplas in Belgium, to give evidence. The committee state that in addition, they have been much assisted by the information which Mr. Bateman, a member of the committee, was able to give as the result of a visit paid at Easter last (at the direction of the Board of Trade) to the institutions in Belgium and Germany which had been most prominently mentioned by those who had drawn attention to the subject of the inquiry.

Much useful official information was laid before them by the solicitor to the Board of Customs, by the (acting) head of the statistical branch and by one of the surveyors-general of that department, and also by Sir Robert Giffen and Mr. Bateman of the Board of Trade.

* The mat-makers of Diss, another centre of this industry, were also invited to give evidence, but replied that they had none to offer.

Finally, they had the advantage of hearing the opinions on the economic questions involved in the matters under investigation, both of Sir Robert Giffen and of Lord Farrer, formerly permanent secretary to the Board of Trade.

After a preliminary review of the causes which led to the investigation, the report goes on to summarise the evidence laid before the committee, dealing especially with brushmaking, matmaking, and stationery. The following are, in brief, the answers which the committee make to the questions submitted to them:—

1. That no evidence has been brought before them to show that goods made in foreign prisons are imported into this country in such quantities as to injure British trade generally.

2. That two trades only, the brushmakers and the matmakers, have made serious complaints of injury, and their complaints were founded solely on the importation of Belgian and German goods showing that British industries in general do not feel any evil results from the competition of the various other trades carried on in foreign prisons throughout the world.

That in the case of the brush-making industry of this country as a whole, the allegations made of serious or lasting injury were not sustained, and in the case of the mat-making industry the injury was found to be but slight.

That it was clearly established that the cheapness of the goods was no proof of their being prison made, as owing to the cheapness of labour abroad, free labour could effectually compete with prison labour, and consequently if prison-made goods were prohibited the competition of free-labour goods would be quite as formidable.

3. That in the case of brushes it was further proved that brushes could be and were made in England by machinery as cheaply as, or cheaper than, they can be made by foreign prison labour or by any other description of labour. That in the case of mats, it was proved that foreign mats were not made more cheaply in prison than they were made outside. That the depression in the brush trade and mat trade was, so far as it existed at all, only connected with the production of the cheaper descriptions of goods, and that, in the case of the higher descriptions of brushes, it appeared that the production in this country was actually on the increase.

With respect to the instructions directing the committee to report whether any, and, if so, what steps can be taken effectually to restrict the importation of such goods;

4. The committee reply that no cause has been shown for the necessity to take any steps to restrict the importation of prison-made goods, and that if cause were shown no steps can be taken to restrict such importation which would not produce more harm than good. Any prohibitory legislation would involve administrative action of a kind which would be most injurious to trade, which would probably create international difficulties, and which would fail in effecting the objects intended.

The committee conclude with the following remark on the resignation of the two members, Colonel Howard Vincent and Colonel Bridgeman :—

“We regret that two of our original colleagues, Colonel Bridgeman and Colonel Howard Vincent, resigned their seats on the committee without having had the advantage of hearing some of the most important evidence submitted to us; in particular, that of Sir R. Giffen and Lord Farrer as to international trade relations; of M. Van Schelle and Mr. Loveday as to the conditions under which free and prison industries are carried on in Belgium; and of Messrs. Bradley and Page as to the part now played by machinery in the manufacture of brushes.”

Appended to the report is a statement which Colonel Howard Vincent, M.P., and Colonel Bridgeman, M.P., prepared on their withdrawal from the committee. They recapitulate the evidence taken up to the period when they vainly urged the committee to report instead of prolonging the inquiry, and conclude as follows :—

“Having now placed you in possession of the facts, we venture to express a hope that no further delay may ensue in carrying out the unanimous mandate of the House of Commons, pronounced over three months ago, supported as it is by the vote of the last Trades Union Congress representing over a million workers, and by several score of petitions to the Legislature signed by many thousands of the industrial community. The competition of British prison labour with free labour having been stopped, its replacement, in an aggravated form, by foreign prison labour is, in our opinion, the more indefensible.”

IV.—THE GERMAN IRON INDUSTRY IN 1895.

"The Engineering and Mining Journal" of New York states that the results of the iron and steel trade in Germany during the year 1895 can be designated as generally satisfactory. The amount of business increased, and the selling prices which, in the beginning of the year, were close to the cost of production, reached higher points later in the year. The production of pig iron in Germany, including Luxemburg, reached during the 10 months to October 31st 4,788,571 metric tons, showing an increase of 4.5 per cent. over the production during the corresponding period of 1894, in which 4,579,180 tons were made. The share of the Rhenish Westphalia district in this production was 2,277,793 tons. This district, which has heretofore been the principal producer of iron in Germany, has now to make an increasingly active fight against the growing competition resulting from the occurrence of "minette" ore in Lorraine, Luxemburg, and on the Saar; so the production of basic pig used in making Thomas steel in the last-named district showed an increase exceeding that in the Lower Rhine and Westphalia more than 25 per cent. A substantial advantage to the iron industry resulting from the use of minette ore as mentioned above, has been the improvement in processes resulting in the diminished consumption of coal. The direct conversion of molten pig iron into steel, that is, the direct transmission of iron from the blast furnace to the converter without a second melting, was carried on everywhere with the best results. New plants have been erected which produce from the ores billets and slabs without using any other fuel in the steel making than the gases generated in the blast furnaces.

The Rhenish Westphalian district is trying hard to get a share of the profits derived from the use of minette ores, though these ores have to be transported a distance of about 300 kilometres to the furnaces in that district, but with the Prussian State administration of the railroads, the attempt has not been a success. A scheme to open up water transportation by means of the canalization of the Moselle also failed. This district is therefore compelled for the purpose of keeping up its production to have increasing recourse to the importation of foreign ores. The first imports were of Spanish ores, but lately the importation of Swedish ores from Gellivara and elsewhere has increased until it reaches at present 500,000 or 600,000 tons a year.

In consequence of the rise in production, new plants have been erected. Hans Holsch's steel works at Dortmund have added two blast furnaces; the Deutscher-Kaiser works have also added two blast furnaces and one steel plant; the Dillinger iron works, at Dillinger-on-the-Saar, one Thomas steel plant. New blast furnaces are also in construction at Esch and at Burbach.

A deposit of iron ore containing phosphorus has been found in Middle Germany, and a new Thomas steel plant and also a blast furnace have been built to use those ores.

A strong market was caused by the extension for ten years of the Rhenish Westphalian Coal Syndicate, which includes almost all the coal mines.

The production of coal in Westphalia during the first three quarters of 1895 amounted to 30,129,963 metric tons, or about 0·28 per cent. above the previous year.

During 1895 nothing was heard of labour troubles or strikes; this is probably due in great part to the system of insurance of workmen against sickness, accidents, old age and disability. The assessments which fall on the iron industry from this system are considerable, but its results in operation have been upon the whole satisfactory.

Exports of iron products for 10 months to October 31st have been, in metric tons :—

-----						1894.	1895.
Pig iron	-	-	-	-	-	129,608	105,664
Beams, &c.	-	-	-	-	-	112,775	143,510
Rails	-	-	-	-	-	98,693	94,314
Bar iron	-	-	-	-	-	254,028	235,881
Steel ingots	-	-	-	-	-	32,345	51,162
Wire rods	-	-	-	-	-	102,115	95,036

This shows decreases in pig iron, rails, bar iron and wire rods, but increases in beams, &c., and in steel ingots.

V.—THE MINING INDUSTRY OF THE DUTCH EAST INDIES.

The minerals, the working of which have hitherto shown the best results in the Netherlands East Indies, would seem to be coal and petroleum, though gold is sometimes found, and occasionally diamonds.

With regard to petroleum, though it is met with often enough in certain parts of Java, the only company appearing to work with any amount of success is the "Dortsche Petroleum-Maatschappij," who exploit certain springs in the province of Soerabaya. The refinery is situated at Wonokromo, in the neighbourhood of which the most productive springs are found. It appears from a recent report of the Belgian Consul-General in the Dutch East Indies that, although many concessions for the working of the petroleum industry have been granted in various parts, it is more especially in Sumatra that rapid progress has been made.

The springs of Langkat in the eastern part of Sumatra are in full working order, and the report to the shareholders of the "Koninklijke Nederlandsche Maatschappij" in 1894 proves that, notwithstanding many preliminary difficulties, a satisfactory result has been obtained. Petroleum was sold in 1892, but the establishment was then by no means complete; now much new machinery has been bought, and several buildings for the refinery have been erected on the banks of the River Babalau near the springs. The sale for some time past has, however, been low, on account of the immense production of Russia and the United States and of the fall in the value of the dollar.

The production of raw petroleum in 1893 was 250,000 litres per diem, and the output of the establishment for the year amounted to 401,370 cases of refined oil.

The Langkat oil is sent by steamboats to Penang and Singapore, and, according to the statistics of the Straits Settlements, these two places imported respectively 70,000 litres and 52,384 cases in 1892 and 130,000 litres and 170,645 cases in 1893 of Langkat oil alone.

The 170,645 cases imported into Singapore represent about one-fourth of the total consumption of petroleum in that city. The oil also finds a market at Atjeh, in other provinces of Sumatra, at Saigon, in Siam, and even at Batavia.

Much profit has, however, not yet been derived from this industry on account of the large capital required for its development.

In Borneo, near the Manakkam river, in the south-eastern division of the island, is a concession which promises well, but which for some reason or other has not yet been fully developed.

With regard to coal in Sumatra, the Ombiliën mines belonging to the Crown already show promising results in spite of number-

less engineering and other difficulties. 47,833 tons were produced in 1893, an amount which was increased to 72,451 tons in 1894 in spite of a lack of labour which prevented the total from attaining to the estimated result of 100,000 tons. About 20,000 tons were loaded at Emmahaven, bound for Java, where it fetched a price of from 13 to 14 florins per ton.

The working of these mines has been frequently interrupted by fires, and the loss in 1893 was so considerable that it is said in the near future coal-mining will be to a large extent abandoned, its place being taken by coffee planting.

The mines are for the most part worked by convicts who numbered 1,227 in January 1894, as against 350 in January 1893. Failing this form of labour, coolies are resorted to, and these likewise are skilful workmen.

Gold was at one time extensively sought for in the western parts of Borneo, but the natives have abandoned the search for the precious metal in favour of the more remunerative pursuit of agriculture. The licenses issued to goldseekers were considerably fewer in 1894 than in previous years. The gold mines of the eastern portion of Borneo do not furnish much happier results, on account of the scarcity of water, but there are deposits which have lately come under notice on the northern coast of the island of Celebez, in the province of Menado, on land conceded to the "Nederlandsch-Indische Mijnbouw Maatschappij," which seem to promise better results. In 1894 some specimens of gold collected from native mines were considered satisfactory, and a gallery was excavated leading to a district said to be rich in auriferous products.

Diamonds have been found at Landak in the western half of Borneo. In 1891, 2,683 carats were obtained; in 1892, 1,400 carats; and in 1893, 2,167½ carats, of which 80 carats only represented stones of 2 carats each, and 209 carats stones of from 1 to 1½ carats, and these unsatisfactory results have led to a decrease in the number of licenses sold.

Ever since 1887 a certain number of requests, chiefly emanating from English subjects, have been addressed to the Dutch authorities for permission to seek for diamonds at Landak.

A report on the subject made by a mining engineer sent out by the Government of the Indies concludes as follows:

"The investment of European capital on the banks of the Landak and neighbouring streams is to be deprecated, the rich districts having been all, or nearly all, exploited, and seeming to be now exhausted. A good find here and there encourages a crowd of persons to the neighbourhood, but these seem to be entirely due to chance, and the majority of the explorers return home poorer than before."

VI.—FRENCH SUGAR FACTORIES AND THEIR METHODS OF PRODUCTION.

The following information respecting the French sugar industry is extracted from the “Bulletin de Statistique et de Legislation Comparée” :—

The law of the 29th July 1884 relating to the sugar industry was conceived with an idea of improving the condition of industry and agriculture in France. This law established under the name of *abonnement* a new method of *prise en charge*, i.e., legal yields, with the object of giving manufacturers a chance of creating excess yields by encouraging them in the progress of cultivation and manufacture. To this end the assessment of the tax has been re-arranged, and has now for its basis not the amount of sugar manufactured but the quantity of beetroots employed.

The *abonnement*, optional during the three sugar seasons 1884–85, 1885–86, 1886–87, was rendered obligatory from the 1st September 1887.

During the optional period, manufacturers not working under the *abonnement* benefited by an allowance for waste of 8 per cent. on their total output; manufacturers belonging to the *abonnement* were made chargeable with duty on 5 or 6 kilogs. of refined sugar per 100 kilogs. of beetroot, according as to whether they used continuous or hydraulic presses, or diffusion or other analagous process for the extraction of the juice.

The rate of the *prise en charge* had to be unified from September 1, 1887: by the law of the 4th July of that year, it was uniformly 7 kilogrammes of refined sugar for every 100 kilogs. of beetroots used for the season 1887–88; 7·25 kilogs. for the season 1888–89; 7·50 for the season 1889–90; and 7·75 for the season 1890–91; it was kept at this latter figure by the law of 29th June 1891, which, moreover, decreed that from the following 1st September, the excess yields obtained over and above 10·5 kilogs. of sugar, per 100 kilogs. of beetroot, should be divided into two equal parts; one part is added to the amounts taxed with the full duty of 60 francs per 100 kilogs.; the other half is only liable to the reduced duty of 30 francs per 100 kilogs.

This last law, which was passed under the influence of the exceptionally unfavourable circumstances of the season 1890–91, re-established the rule of an allowance for waste in favour of the manufacturers who, previous to the 1st November of each year, declare themselves willing to renounce the benefit of the reduced tax on excess yields; it allows them 15 per cent. on their total output.

It, in addition, raises to the same rate the allowance for waste of 12 per cent. which had been granted by the law of 4th July

1887 to manufacturing-distillers whose factories fitted for the use of the juice of the same beetroots for the simultaneous production of sugar and alcohol, were already installed at the time of the promulgation of the law of 29th July 1884.

Up to 1887, the quantity of sugar produced in excess of the legal yield or which represented allowances for waste in manufacture was freed from the tax. Since that time they have been subject to a special tax. This (according to the law of 27th May 1887 and 30th March 1888) was 10 francs per 100 kilogs. of refined sugar, which sum was raised to 20 francs by the law of 24th July 1889 and to 30 francs by the law of 5th August 1890, at which rate it now stands.

The consequences of this change in legislation with regard to sugar manufactories and means of production are as follows :—

The number of manufactories has fallen rapidly from 486 (season 1881–82) to 375 (1887–88); and since to 367. The closing of 111 establishments during the first of the periods above named is a result of the *abonnement* being obligatory from September 1, 1887. It was a question chiefly of old works badly fitted up, the alterations in which, under the new regime, would have entailed considerable expense out of all proportion to their ordinary stock of beetroot.

The 367 sugar factories at present are divided into 322 simple factories and 45 central works.

The central works produce 209,760,000 kilogs. of refined sugar out of a general total production of 704,454,108 kilogs.

The total number of hands employed (men, women, and children) has decreased from 65,293 in 1881–82 to 50,569 in 1894–95. The average daily wage has likewise decreased. This is, for the most part, in consequence of the closing of so many factories and the ever-increasing substitution of machinery for hand labour.

It is interesting to compare the results of the season 1894–95 with that of the close of the season 1887–88, during which *all* the works had been carried on for the first time under the *abonnement* system.

There had been no appreciable difference between the cost of the beetroot during the two seasons above mentioned; but in 1894–95 the amount of beetroot used was 7,138,000 tons, as compared with 3,615,000 tons in 1887–88.

With regard to production, the amount manufactured in 1894–95, expressed in refined sugar, was 704,454,000 kilogs., including the sugar represented by molasses. This is the largest amount produced in France since the commencement of the sugar industry, and is 368,878,000 kilogs. in excess of the production of the season 1881–82.

In 1881–82 the average production per manufactory was 690,485 kilogs.; in 1894–95 it reached 1,919,000 kilogs. To obtain such a result and reduce the amount of general expenses, manufacturers have had to perfect their means of production, and the total power of the generators and machines has been greatly

increased. Important improvements have been also effected in the material employed for the extraction and evaporation of the juice. In 1881-82 only 91 manufactories (of a total of 486) employed the "diffusion" process; in 1894-95 this number had increased to 364 out of a total of 367. In the treating of the syrups and the purification process many improvements have also been effected.

Two principal features are characteristic of the season 1894-95: one is the exceptional importance of the production, as seen above, and the other the marked fall in prices.

Sugars of 88° fell from 33·86 francs (average price in 1893-94), to 26·50 fr. in 1894-95; and No. 3 sugars from 35·65 fr. to 27·65 fr. This is a consequence of the general over-production to which France, for a long time the first of sugar-producing countries, has not much contributed.

Twenty years ago, in 1874-75, Germany produced 250,708 tons of raw sugar, Austria 106,312, and France 450,711. In 1894-95 the production of raw sugar had increased in Germany to 1,831,600 tons, in Austria to 1,044,500 tons, and in France to 745,100 tons only.

In connection with the foregoing, a statement is given by Sir J. A. Crowe, Commercial Attaché at Paris, in a publication recently issued by the Foreign Office (reports by Her Majesty's Representatives abroad on Bounties, other than those on Shipping and Navigation paid by the State in the countries in which they reside), showing the bounties given to sugar producers in France. The bounty in 1884-85 amounted to 19,822,268 francs; in 1885-86 to 38,976,838 francs; and in 1886-87 to 78,890,349 francs. In 1887-88 the bounty fell to 47,380,612 francs; and in 1888-89 to 43,469,129 francs; and rose in 1889-90 to 79,979,514 francs. In 1890-91 the bounty was 42,591,300 francs; and in 1892-93, 29,816,730 francs.

The amount of excise duty derived from sugar in France was 162,098,000 francs in 1892, and 150,797,000 francs in 1893. The amount of customs duty on foreign and colonial sugar was 41,875,000 francs in 1892, and 39,105,000 francs in 1893. The excise duty at present levied in France on sugar, raw and refined, is at the rate of 60 francs per 100 kilogs.

VII.—FRENCH MINING STATISTICS.

During the year 1894, 515 concessions for mines were exploited in France, producing about 31,787,000 tons of various products, representing a value of 340,646,000 francs. Combustibles form nine-tenths of the whole, viz., coal, 25,599,000 tons; anthracite, 1,365,000 tons; lignite, 453,000 tons, or a total of 27,417,000 tons, of a value of 307,760,000 frs.

According to the "Rapport de la Commission de Statistique de l'Industrie Minérale," other substances include, in the first place, iron ore, of which the total quantity obtained amounted to 3,772,000 tons, valued at 12,297,000 frs. Rock salt, 589,000 tons, worth about 9,500,000 frs., exclusive of the tax, comes next, followed by lead, zinc and silver ores, iron pyrites, schist and bituminous limestone, manganese and antimony ores, and a small quantity of alunite and marbles impregnated with sulphur. The total quantity of these last-named substances amounted to 663,000 tons, of a value of rather more than 13 millions of francs. Of peat (from private and commercial properties combined) 132,000 tons were produced, valued at 1,783,000 frs.

With regard to Algeria, 18 concessions furnished altogether in iron ore, argentiferous zinc and lead ores, mercury and antimony, and rock salt, a total production of 393,000 tons, of a value of 3,934,000 frs.

The production of coal is such an important item in the mineral statistics of France, that some supplementary details concerning it are given. The great coalfield of the departments of Nord and Pas-de-Calais, the working of which had been seriously interrupted in 1893 by a general strike of miners in the latter department, increased its production to 15,616,000 tons, or 57 per cent. of the total production of the country, an increase of 1,729,000 tons as compared with the preceding year. The production of mineral combustibles has also increased, though in a less degree in the departments of Saone-et-Loire, Gard, and Aveyron; and it has decreased, especially in the departments of Loire and Hérault, on account of a slackening in the orders of iron and steel workshops, the competition of neighbouring coal-fields, and the loss of certain openings for export in Switzerland and Italy.

Consumption of coal in France rose to 38,000,000 tons, a higher figure than had previously been reached.

Of this amount, 11,644,000 tons were of foreign origin. The importations as seen from the annual statistics drawn up by the Customs, consisted actually of 9,349,000 tons of coal and 1,530,000 tons of coke. Two-thirds of this coke came from Germany; nearly all the rest from Belgium. England and

Belgium are the principal exporters of coal to France. The total importation shows the slight increase of 243,000 tons over the figures for 1893. Exports of combustibles from France are of slight importance: in 1894 they amounted to only 801,000 tons, which went chiefly to Belgium and Switzerland, a total diminution of 68,000 tons in comparison with 1893.

With regard to the question of wages, the total amount of money paid in wages to coal miners was 158,974,000 frs. or 7 millions of francs more than in 1893; this increase is due to the fact that the number of day's wages paid in coal mines was 1,684,000 more than in 1893, owing to the absence of strikes. For the same reason, *i.e.*, on account of the fewer days of cessation from work—the average annual pay of the miner (men, women, and children included) has increased by 35 frs. to 1,181 frs., though the average daily pay remains the same as in 1893, *viz.*, 4.14 frs.

Although the average price of coal fell 27 centimes per ton, the cost of labour only decreased 13 centimes.

The total number of workmen employed in the mining industry in France was 146,900, of whom 134,550 worked in coal mines. This figure includes nearly 4,000 women, working only by day, and rather more than 9,200 children under 16 years of age, half of whom worked underground. The number shows an increase of 1,800 workmen, consequent on the development of the coal industry.

The result of the law of 2nd November 1892 passed to regulate the labour of children, girls, and women in industrial enterprises has been to reduce the number of women by 564, and that of young people between 16 and 18 years of age by 421. Industries in which so many people are employed entail a certain number of accidents, of which 289 proved fatal in 1894. That year was not, however, marked by any disaster: the majority of the accidents were individual, no single one causing more than five victims, killed and injured included.

In 1894, eight new concessions were granted:—one for anthracite, three for iron ore, one for lead, zinc, silver, &c., one for manganese, and one for rock salt.

The second part of the "*Statistique de l'Industrie Minérale*" is devoted to metallurgy. The production of the furnaces and iron and steelworks was 2,070,000 tons of pig iron, valued at 118,000,000 frs.; 786,000 tons of iron, 129,000,000 frs.; and 674,000 tons of wrought steel, 169,500,000 frs.; or a general total of 3,530,000 tons, valued at 416,500,000 frs. More than three-fourths of the cast iron is for refining purposes, *i.e.*, for the manufacture of iron and steel and the remainder for casting in first or second fusion. This production has increased by 67,000 tons, having again reached the point attained in 1883 previous to the great crisis in the mining and metallurgical industries.

The general total of 3,530,000 tons representing the production of French metallurgical works, shows an increase of 55,000 tons over the amount for the previous year. The corre-

sponding value has, nevertheless, dropped about 7,000,000 of frs., the consequence of a general fall in prices all round, with the exception of steel plates which improved in value. The market in the iron and steel industry exhibited the same tendencies and passed under the same influences as in the mining industry.

Statistical tables are also published with regard to the more important works in metals other than iron, which are found in France, or imported. Included among these is copper drawn from amalgam and silver extracted from native or imported lead. The products obtained in France and Algeria consisted of gold, silver, lead, zinc, copper, nickel, aluminium, regulus, and oxide of antimony and mercury representing a value of about 38 millions of francs, or $2\frac{1}{2}$ millions less than in 1893, on account of the depreciation of silver and metals in general. The distillation of bituminous schists and the treatment of asphaltic limestones furnished about 1,800,000 frs. worth of raw mineral oil, mastic and asphalt powder.

Statistics relating to the employment of steam and steam power are included in the same volume with those bearing on the mining and metallurgical industries.

These are divided into three parts: (1) in industrial establishments; (2) on railways; and (3) in the merchant marine.

In 1894, in France there were 93,918 boilers in work; 82,660 machines, of which 10,967 were locomotives. Steam-power was in use equal to 5,900,000 horse-power.

VIII.—THE ESPARTO-FIBRE INDUSTRY OF TUNIS.*

Esparto-fibre, known as *halfa* by the Arabs and *alfa* in French, is a kind of marine bulrush which grows abundantly on the North African coast, chiefly in Tripoli and Tunis.

The principal districts in Tunis whence this product is obtained are the territories of the following tribes:—Hemmema, Slass, Erghoumma, and in the neighbourhood of Cardato in Sfax and Susa.

Esparto-fibre is generally divided into two kinds:—The *rusia*, long and straight, and the *mahbula*, which takes its name from the twisted nature of the growth.

The *rusia* kind includes nine different qualities, all taking their names from the districts in which they are found.

The Hemmema kind has a long and rather thick stalk, and is used for cigar straws and for the manufacture of artistic objects in basketwork, &c. ; it is much employed in paper-making and is largely exported to England, where it is always in demand.

The Slass species is a very beautiful plant and preferred at Marseilles for the manufacture of *scourtins* (mats which are placed under the presses in oil manufactories); it is used also in rope and paper-making. The principal market in France is at St. Louis, on the Rhone, but though the consumption is on the increase, even there but little is asked for, paper being still largely made from wood and rags.

The Erghoumma kind all goes to England, being shipped at the port of Gabes; the Schebba kind is the most fine, very elastic, and is much used for textile purposes; and the other qualities are made up into paper, baskets, matting, carpets, &c.

The *mahbula* esparto is chiefly found along the coast from Mehdia to Tripoli, about 25 miles inland.

At Sfax, in the months of June, July, and August, the inhabitants of the Kerkene Islands flock to the coast of the mainland, gather it and make it into bundles, which are left to dry in the sun; in one day from 4,000 to 5,000 kilograms can be obtained; it is spread on the seashore to be dried, then steeped in the sea, and then stretched out along the sands to dry again, after which the fibre is twisted and rubbed in the hands till a fairly good cord is produced.

The inhabitants of these Kerkene Islands occupy themselves, in addition, with making other kinds of thicker rope, even cables, and these latter are much liked by sailors, being lighter than hemp, but cheaper and quite as durable. Sea-water, instead of deteriorating, seems to strengthen them, and were it not for a certain want

* From a report of Count Politi, Italian Consular Agent at Sfax, published in the "Bollettino di Notizie Commerciali."

of elasticity they would be more generally adopted by larger vessels. The Kerkenines also make baskets and mats for the backs of camels.

The export trade of *mahbula* by way of Marseilles is very great, a small amount only being sent to Italy, but the thicker rope is still sent to Trapani and Cagliari, where good use is made of it in the tunny fisheries.

As an article for the textile industry esparto-fibre is principally used in England, hardly at all in France, and very little in Italy.

The principal markets in Tunis for esparto are Sfax, Gabes and Skira, and the sale is effected by public auction the municipality exacting a land-tax; the stuff is done up in fresh bundles, which have to be well dried to avoid fermentation, and it is finally made up for exportation in bales (by means of hand-presses) of about 150 kilogs. each.

The Tunisian fibre pays an export duty of 1.27 francs per 100 kilogs., but, if worked up, is exempt. The Tripoli fibre, on the other hand, is not subject to any export duty at all.

IX.—THE FOREIGN TRADE OF ITALY.

Since the year 1870 the foreign trade of Italy has somewhat developed, and voluminous statistics have been annually issued showing the progress (or reverse) from all points of view.

From these official returns the following data are obtained, the original values being in each case converted into English currency.

The total value of the foreign trade of Italy in the year 1870, including silver bullion, is given as 65,987,000*l.* (imports, 35,775,000*l.*; exports, 30,212,000*l.*) In 1880 these figures had risen to 91,638,000*l.* (imports, 47,473,000*l.*; and exports, 44,165,000*l.*), but in 1890 the total trade was only 88,623,000*l.*, the imports showing the value of 52,786,000*l.*, while the exports had fallen to 35,838,000*l.* In 1891 a further fall in both imports and exports is noticeable, the former being valued at 45,063,000*l.*, and the latter at only 35,072,000*l.* The following three years show a decided improvement in both cases, the total figures for 1892 being 85,243,000*l.* (imports, 46,936,000*l.*; and exports, 38,327,000*l.*); 1893, 86,217,000*l.* (imports, 47,649,000*l.*; and exports, 38,568,000*l.*); 1894, 84,846,000*l.* (imports, 43,786,000*l.*; and exports, 41,060,000*l.*)

Comparing, therefore, the results of nearly a quarter of a century (1870–94), the progress made does not at first sight appear to have been very remarkable, the highest point having been attained in 1887, when the total trade reached a value of 104,324,000*l.*, which dropped to 82,661,000*l.* in 1888, but rose again in 1889 to 93,672,000*l.*, since which time it may be said to have been in a more or less slightly fluctuating state, showing neither decided improvement nor notable decline.

Attention, however, should be directed to the fall in prices which has taken place, and on this point the following remarks of Signor Busca, President of the Italian Customs Valuation Commission, may be quoted.

In his recent report to the Minister of Agriculture, Commerce, and Industry, he says:—

“ The general and progressive depreciation in the value of nearly all classes of goods which has been noticeable at various times, was more pronounced than ever in the year 1894.

“ In addition to the influence which monetary changes may have brought to bear, it is certain that the excess of production over consumption, and the fall in freights, have had much to do with the depreciation in the value of goods.

The Central Customs Valuation Commission on the bases of average prices taken from the various lists, and on information furnished by the most competent merchants and traders, has

effected notable reductions in many of the units of values established in the preceding session, reductions which considerably modify the figures representing the value of the exports and imports for 1894, during which period the new units of values were provisionally applied.

Comparing the values of the imports of 1894 according to the new units, with those of the preceding year, it is found that as a whole they diminished 6·42 per cent., the exports in the same way showing a decrease of 5·51 per cent. in value; and comparing the values of previous years with those of 1894 it is seen that in the year 1890, for example, the values were 13·26 per cent. higher than in 1894, in 1891 they were slightly diminished, but still 9·50 per cent. higher than in 1894; in 1892, 7·35 per cent.; and in 1893, as before stated, 6·42 per cent. higher. Similarly the values of the exports were calculated 12·05 per cent. higher in 1890 than in 1894; and in 1891 5·87 per cent.; in 1892, 5·74 per cent.; and in 1893, 5·51 per cent. higher."

It is perhaps useful to mark if fluctuations may be observed in any one category more than another, and for this purpose it will be sufficient in most cases to take the figures for the last few years, commencing with those for the year 1890. Turning first to the import trade the chief fluctuations in annual values are found to be in raw cotton, linen and hemp yarn, machinery and locomotives, wheat and rice.

Raw cotton was imported into Italy in 1890 to the value of 5,087,000*l.* which amount fell to 3,889,000*l.* in 1891, and to 3,704,000*l.* in 1892, increasing again to 4,343,000*l.* in 1893, and to 4,787,000*l.* in 1894.

The imports of linen and hemp yarn in 1890 were valued at 588,000*l.*, but at only 537,000*l.* in 1891, and after a slight increase in 1892, they fell to 484,000*l.* in 1893 and 386,000 in 1894.

The value of the imports of machinery and locomotives and parts thereof in 1890 was 1,577,000*l.*; in 1891, 1,114,000*l.*; in 1892, 1,064,000*l.*; in 1893, 1,056,000*l.*; and in 1894, 1,131,000*l.*

As regards rice, Italy at one time imported large quantities of this cereal. Going as far back as the year 1883, the value of rice imported was 925,000*l.*, which in 1884 rose to 1,058,000 but afterwards gradually declined in value year by year. In 1890, the figure was 90,000*l.*; in 1892, it had fallen to 5,000*l.*; in 1893, to 500*l.*; and in 1894, to less than 200*l.* The value of the exports of the same cereal which amounted to 1,112,000*l.* in 1883 and 1,001,000*l.* in 1884, gradually fell to 19,000*l.* in 1889, but improved to 442,000*l.* in 1891, 449,000*l.* in 1893 and 463,000*l.* in 1894.

In the year 1890, 644,986 tons of wheat were imported of the value of 5,160,000*l.*; (the figure had previously in the year 1887 risen as high as 8,127,000*l.*). In 1891 the amount was 464,367 tons of a value of 4,272,000*l.* but this rose in 1892 to 697,143 tons valued at 5,856,000*l.*, and in 1893 to 861,418 tons of a value of 5,858,000*l.*; only to fall again in 1894 to 486,846 tons valued at 2,629,000*l.* The export trade in this cereal, as in that of rice

has sunk almost to a vanishing point. As far back as the year 1881, 94,790 tons of a value of 1,024,000*l.* were exported. Five years later, in 1886, the amount had decreased to 7,702 tons valued at 71,000*l.*; in 1891 the amount was 696 tons of a value of 8,000*l.*; and in 1894 only 374 tons were exported barely reaching 3,000*l.* in value.

On the whole, however, the export trade of Italy has lately shown a tendency to be stationary, neither declining nor improving to any great extent, though the total for 1894, 41,027,000*l.* is higher than it has been for any single year since 1886, when it stood at 41,129,000*l.*

With regard to those articles which may be said to form the principal objects of cultivation and industry in Italy, the exports of dyeing and tanning stuffs, and hemp and flax, raw, have, since 1889, somewhat increased in value, but the trade in wine has fluctuated. In 1886 the value of wine exported was 3,356,000*l.*, which in 1887 rose to 4,296,000*l.* In 1888, however, a decided fall took place, the value of the wine exported that year being only 2,160,000*l.*, and this figure further decreased in 1889 to 1,916,000*l.*, and in 1890 to 1,372,000*l.* Since 1890 a slight improvement has taken place, the value of the last four years having been as follows:—1891, 1,480,000*l.*; 1892, 2,220,000*l.*; 1893, 2,140,000*l.*; and 1894 1,756,000*l.*

Silk of all kinds, marble and alabaster, and their manufactures, and sulphur, raw and refined, have remained about the same, while the most notable decrease is in straw plait, once an important article of Italian industry, but the value of which has much declined in the last few years; 18,665 cwt. of this article, of a value of 835,000*l.*, were exported in 1884, while the increased amount of 20,570 cwt. exported in 1894 was valued at only 201,000*l.*

X.—THE IRON INDUSTRY OF THE UNITED STATES.*

The history of the iron trade in the United States in 1895 shows a period of agitation and of sharp fluctuations, the changes being quite as marked as in 1893 and 1894, but in the reverse direction. In 1893 the first half of the year showed a large production, but a somewhat halting and uncertain market; the second half, after the beginning of the currency panic in June, showed a rapid and continuous fall in demand, in prices, and in output as furnaces and mills closed down, one after another, under the stress of the times. In 1894 the depth of the reaction in prices was reached, and about the middle of the year production was at its lowest level. In the latter half of the year a somewhat slow and hesitating recovery began, which gradually gained impetus, and the year closed with a marked improvement and the promise of continued gains in the future.

The year 1895 opened with less activity and more uncertainty as to the future than had been anticipated in the closing months of its predecessor. The tariff question had been settled, but the continued delay of Congress in acting upon the currency question and the uncertainty as to future values depressed business generally, and in the earlier months of the year there was a slight decline in production, while prices gained either very slowly or not at all. The revival of confidence which followed the closing of the syndicate contract for the government bonds at the close of February, when business men began to realise the fact that the government was at once ready and able to maintain the public credit, at first affected the iron trade slowly, and was manifested rather in the appreciation of prices and the diminution of stocks than in any immediate increase in production. February had shown an actual decrease in the rate of production of raw iron and steel, as the makers hesitated as to the future; while from March to June the production continued at a nearly even level, notwithstanding continued inquiries for material and other manifestations of increasing demand. The first half of the year had almost closed before the active list of blast furnaces much surpassed the point attained six months before.

In June, however, the improvement in general business had so far advanced, the demand for finished products had so increased, and the disposition to undertake new construction had become so general, that there was no longer any hesitation. It became evident that there would be a demand which would tax the resources of the producers of raw material and would speedily exhaust the existing stocks. The manufacturers of finished

* Based on a review in the "Engineering and Mining Journal," New York.

material in its various forms came into the market to secure supplies, and began to be urgent in their demands. As July opened prices were going up rapidly, and production was increasing at an unprecedented rate. As usual at such periods, the conditions of the market increased the anxiety of buyers and their disposition to supply themselves before a further rise should come; at the same time the element of speculation entered in, and operators for a rise are understood to have bought very considerable quantities of iron for future delivery. Under these circumstances the plants which had been able to keep at work during the depression enlarged their operations and put all their available capacity at work; others which had suspended started up, and as demand was still maintained, still others were brought in, until the output reached nearly the producing limit of the available plants.

This rise in prices, while it affected more or less all branches of the trade, was most marked and most active in steel billets. Steel has replaced wrought and cast iron to a large extent, and the result has been that steel billets have become almost the foundation of the trade in finished material, and to a great degree the barometer which indicates its condition. Accordingly we find that the first marked rise was shown chiefly in steel billets and Bessemer pig-iron, and while there was a very considerable increase in prices of other products, it was much less abrupt and later was much better maintained.

Rapid as was the gain in production, it did not seem to surpass that in demand, and throughout the third quarter of the year the prices were not only well maintained but continued to rise, and the readiness with which the output was taken up showed no sign of decreasing. By September the production of pig-iron had reached a rate equivalent to over 10,500,000 tons a year, which was the highest point on record, but there was no cessation in the growing activity and apparently none in the demand, since the output was absorbed as fast as it came on the market and stocks decreased somewhat rather than increased.

The speculative element in the market during the latter half of the year was confined almost entirely to steel billets and Bessemer pig. Those products were carried to the highest point and suffered from the succeeding reaction almost alone. The prices of foundry irons showed a steady rise from the beginning of the business improvement, and when the fall in Bessemer pig came in September they retained the advance and held it substantially until the close of the year. The same thing may be said of almost all forms of finished productions. Bar iron, structural iron and steel, merchant steel, and the other forms in which the market is supplied have advanced steadily in spite of a great increase in production.

Another evidence of the largely speculative nature of the billet and Bessemer market is the fact that the production of pig-iron has continued to increase steadily notwithstanding the much-talked-of "reaction," and that this increase has been taken up

without difficulty, the stocks reported on hand at the close of November being rather smaller than usual. Although, also, December is not generally a very active month, the production of pig-iron, which in November had reached the rate of 11,500,000 tons yearly, continued through that month at substantially the same rate.

One considerable element of demand has been largely absent from the market during 1895. Not many years ago the railroad purchases were almost the ruling element in the iron trade. The rapid extension of the use of iron and steel in building construction and for other purposes has reduced the railroad trade in relative rank and importance, although it remains large in absolute amount. The railroads felt the effects of the business depression very severely, and postponed all purchases and renewals as a rule, so that they have made very small purchases. In November and December a large number of orders for cars were placed, and some contracts for locomotives also; the rail orders, however, have been on a limited scale. At the opening of the year the price for standard sections steel rails was 22 dolls. (4*l*. 11*s*. 8*d*.) per ton at mill, and this quotation was continued up to June, orders, however, being very light, as the railroads had not yet recovered sufficiently from the depression to undertake renewals on any considerable scale, and the construction of new lines was very small. On June 30th a meeting of the rail-makers was held, and as the growing business improvement was then manifest, an increase to 24 dolls. (5*l*.) per ton at mill was ordered. To this, perhaps, no serious objection would have been offered, as prices of other descriptions of iron and steel had begun to rise, though the cost of fuel and raw materials to most of the rail-makers was practically unchanged. The combination was not satisfied, however, and three months later, in September, another meeting was held, at which the price was raised to 28 dolls. (5*l*. 16*s*. 8*d*.) per ton at mill. This action was taken in face of the fact that orders for rails continued to be much lighter than had been expected, and its direct effect has been to discourage purchases. The combination, in its eagerness for profit, probably overlooked two important facts: first, that the recovery in earnings had not yet passed the point which required strict economy in expenses; and second, that the railroad tracts are now very largely laid with steel and that renewals can be postponed for a certain length of time without seriously compromising the safety of operation. The result has been that the rail business has not shown the same growth as other branches of the trade; a few of the great companies—like the Pennsylvania—have placed their orders, though on a restricted scale, while many companies have held out altogether. With regard to the rate, rails are almost the simplest and least costly form in which finished steel can be put upon the market, and the profit upon their manufacture at the present price is out of all proportion to that obtained on other products under normal conditions.

As affecting the rail market, it is to be noted that the construction of new railroad lines fell during 1895 to a lower point than in any year of the past thirty. The total new mileage built during the year, as collected and estimated by the *Railroad Gazette*, was 1,300 miles, or considerably less than that constructed in 1894, when 1,760 miles were reported. It must be remembered that it takes some time to carry out a railroad project to completion, and that in the first year of depression there are generally a number of lines partially built, for the construction of which arrangements have been made and which must be carried out. The low mileage of 1895, therefore, really reflects the extreme depression of the previous year.

To some extent the rail demand from the steam railroads was made up by the increased construction of electric railroads. The additions to electric lines in 1895 were about 1,800 miles, against 1,500 miles in 1894. Not all of the mileage reckoned was new; probably one-half of it was roads which had previously used animal motive power, so that the demand for new rails was less than would appear at first sight. There was, however, an actual increase in new electric construction of about 400 miles.

The growth of electric railroads promises to furnish a continually growing demand upon the rail mills. Not only is the building of new lines going on rapidly, but it is found that renewals are required more frequently than on steam roads. The present construction, in which the heavy motors are carried on the axle, and all the shocks of rotation are communicated directly to the track, without the interposition of springs, is particularly trying, and the life of the heaviest steel rails yet used is found to be much shorter than even on the steam lines running the heaviest locomotives at high speeds.

The iron trade throughout the year has suffered little from labour troubles. The increase in business and the general rise in prices permitted a very general rise in wages also, and almost everywhere at least a part of the reductions made in 1893 and 1894 has been restored. There has been but little of the agitation and disturbance which have affected the bituminous coal trade to so great an extent.

Pig Iron.—The production of pig iron, by half years, for the last three years, is given in the following table. The quantities given are in gross tons.

Pig Iron Production in the United States.

	1893.	1894.	1895.
	Tons.	Tons.	Tons.
First half-year - -	4,562,918	2,717,983	4,087,558
Second half-year - -	2,561,584	3,939,405	5,259,078
Totals for the year	7,124,502	6,657,388	9,346,636

The production of the last six months of 1895 is estimated from the number of the furnaces in blast and their average weekly capacity. Although there were many changes during that period, it is believed to be close to the actual output.

The production of finished iron has shown a fair increase, less marked than that of pig-iron, because wrought iron is gradually ceasing to compete with steel in many forms of production. The cheapness with which Bessemer and open-hearth steels can be produced, and the improvements in quality made in recent years, have given them very great advantages in the competition, before which the older material is gradually giving way.

While the exact figures for the production of steel are not yet to hand, the increase was large and the output exceeded the maximum of 4,927,581 tons which was reached in 1892. It is to be noted that in the years of depression the output of steel did not decrease to as great a degree as might have been expected, nor in an equal ratio with that of pig-iron. The fact is that the growing use of steel for construction and other purposes to some extent offsets the general diminution in demand.

Iron Ore.—The supplies of iron ore from Lake Superior, through their abundance and quality, and the wide distribution given them by the cheap lake transportation, have gradually come to rule the market, and their output shows the condition of the trade better than any other figures which could be given. In 1894 the region suffered from the general rule of light demand and low prices. The production was greater than in 1893, but was 1,326,000 tons below that of 1892.

The total shipments of Lake Superior ore in 1895 amounted to 10,500,000 tons as against 7,748,932 tons in 1894.

The increase in shipments in 1895 over 1894 was therefore 2,751,068 tons, or 35.5 per cent. The tonnage of 1895 was the largest ever shipped from the Lake Superior mines. The actual shipments do not represent the whole of the consumption, since the stocks at Lake Erie ports were less by 418,000 tons at the close of the season in 1895 than in the preceding year. The total consumption may therefore be estimated at very nearly 11,000,000 tons.

The local iron ore mines throughout New Jersey and Pennsylvania showed a great increase in activity throughout the year. Similar reports come from the South. In Alabama the activity in iron mining has been unchecked. In Virginia a number of mines which had been closed down have been reopened to supply the demands of local furnaces. While it is impossible at this date to give figures, the output of iron ores has undoubtedly shown an increase of more than 30 per cent. over 1894.

The demands of the eastern coke and anthracite furnaces for Bessemer ore, however, could not be fully met from local sources, and imports of iron ore show a large increase. For the nine months ending 30th September the imports reported at all United States ports were 287,429 tons, and the arrivals at Philadelphia and Baltimore—where nearly all the ore imported is received—for the

last quarter, have brought the totals up to 468,000 tons, against a total of 167,307 tons for 1894. Nearly all of this ore was for furnaces in the Lehigh and Susquehanna valleys. The supply was drawn chiefly from Cuba and Spain, but in December several cargoes of Algerian ore were received. The revolution in Cuba has prevented the mines of the island from meeting the demand upon them, and several large contracts for Spanish ores have been made for 1896 delivery.

In conclusion, it may be asserted that the year 1896 opens with the prospect of large demand and production at a range of prices somewhat below the highest level of the past year, but still sufficient to permit well-managed plants a fair profit, in spite of increased cost of ores, fuel, and labour. The element of speculation which followed the rebound from the panic depression is gone, but there remains a demand which, should no contingencies now unforeseen arise, ought to give the iron trade a fairly prosperous season in all its branches.

XI.—THE JAPANESE STEAM NAVIGATION COMPANY.

A despatch, dated December 5 last, has been received at the Foreign Office from Sir E. Satow, Her Majesty's Minister at Tokio, enclosing a report on the operations of the principal Japanese Steam Navigation Company, and of its plans for the further development of its lines of steam communication with foreign countries.

The report states that the half-yearly general meeting of the "Nippon Yusen Kaisha" (Japan Mail Steamship Company) was held in Tokio on the 30th November last, and the accounts for the half-year submitted to the shareholders. Receipts from the ordinary service of the Company for the half-year amounted to over 4,000,000 dols., which admitted, after providing for depreciation of buildings, payment to reserve, and bonus to directors, of the declaration of a dividend at the rate of 10 per cent. per annum, and the carrying forward of nearly 460,000 dols. to next account.

There were, in addition, receipts from the "transport fund" (payments by the Government on account of the Company's vessels engaged in the war service) amounting to:—

	Dols.
Brought forward from previous half-year -	1,378,374
Receipts during past half-year -	2,363,921
Total - - - -	<u>3,742,295</u>

After providing for further payment to reserve fund, a bonus at the rate of 10 per cent. on each share, and liberal bonuses to the managers and employees of the Company, the latter amount admitted of the creation of a "special reserve fund" of considerably over 2½ million dols.

The shares of the Company, which are of a paid-up value of 50 dols. each, and which were quoted on the 1st November at

93 dols. each, rose during the month to 106 dols. each, but part of this rise must be attributed to the marked upward tendency recently shown throughout the whole share market, caused by the speculative mania now prevalent.

The proposed extension of the Company's lines to foreign countries was brought to the attention of the shareholders in the director's report in terms of which the following is a very brief précis :—"The opening of such lines has long been a cherished desire on the part of the Company, but available means have not been sufficient to warrant either the necessary expenses of construction or the annual loss that must at first be incurred in carrying out such a project. But the time is now highly favourable for it. The support of the State may be relied on, but in any case it is the duty of the Company, both as the greatest shipping company in Japan and also as a return for the great benefits it has already received from the Government, to use every effort to promote it. It has been therefore decided by the directors to use the special reserve fund and further profits accruing from the same source in at once initiating the necessary steps towards it. The three lines whose development is of primary importance in the interests of the State are those to England, America, and Australia. Of the three, that to America is the most urgent, and this should not be delayed for even a day longer than is necessary. Six large steamers of first-class speed, all capable of being converted into armed cruisers, should be provided as a beginning, and the other lines gradually proceeded with." Immediately after the meeting it was decided that the chief European manager of the Company should at once proceed to England; he was to leave, accompanied by one of the Japanese managers, by the San Francisco mail of the 7th December last. The object of his journey has not been publicly stated, but there can be no doubt that it is to make arrangements for the building of the six proposed steamers.

Owing to the large purchases of foreign steamers made during the late war and the absence of available statistics of their tonnage, it is difficult to give precise details as to the present extent of the Japanese mercantile marine. According to figures recently published by one of the Tokio economic journals, Japan possessed, on the 30th June last, 503 steamers of an aggregate tonnage of 314,623 tons. Of these, 255 vessels were under 200 tons capacity, 41 were between 2,000 and 3,000 tons, and only 14 above 3,000 tons. Nearly all in the last two classes were purchased during the war, and of the whole number only 15 are said to be provided with machinery that is up to date, and to be fitted by speed or economy for the purpose of opening new ocean lines even for freight traffic alone. If, therefore, the considerable extension of ocean traffic that is now proposed should be carried out, further orders for new steamers, beyond the six already suggested, may possibly be looked for by English builders.

XII.—GERMAN TRADE WITH JAPAN.

The official statistics of Japanese trade for 1894 permit of more than a passing glance at the development of Japan as a commercial nation during the last 10 years.

According to the “*Bollettino di Notizie Commerciali*,” which quotes from a German publication, the value of the sea-borne trade of Japan amounted in 1894 to 230,728,000 yen (yen (1894) = 2s. 1½d.), of which sum 117,482,000 yen represented imports into Japan and 113,246,000 yen exports from the country; from this should be deducted about 2,000,000 yen, as representing exported articles consumed on board Japanese ships.

In the year 1885, the combined value of imports and exports reached only 65,150,000-yen. The trade of Japan has thus almost quadrupled in the last 10 years, and the increase during that period has been continuous, that of the imports, however, being much greater than the exports; that is to say, in 10 years the import trade has almost quadrupled, while that of the exports has about trebled in value.

With regard to the proportion the various countries bear in the total trade of Japan, official statistics afford the following particulars; the trade with Europe in 1894 amounted in value to about 88,000,000 yen, with Asia 81,250,000 yen, and with America, 56,500,000 yen, the remainder belonging to Australia and the islands. A great part of the Asiatic trade, as so tabulated, should, however, be included with Europe, especially the Hong Kong trade which figures at 25,000,000 yen. This port is a port for transit trade alone; part of the products really destined for Europe which are transhipped in Hong Kong, appear under the heading of exports for Hong Kong, as likewise the goods said to come from there, but which are, in reality, of European origin.

The following table shows the distribution of the foreign trade of Japan between the principal foreign countries during the year 1894, the original values having been converted into sterling at the average value of the yen for the year given :—

Country.	Imports.	Exports.	Total.
	£	£	£
Great Britain - - -	4,614,517	626,019	5,240,536
Hong Kong - - -	946,844	1,704,319	2,651,163
British India - - -	1,155,048	388,021	1,543,069
Australia - - -	58,489	115,524	174,013
Canada - - -	4,966	232,687	237,653
Total British trade -	6,779,864	3,066,570	9,846,434
United States - - -	1,201,218	4,557,999	5,759,217
France - - -	475,570	2,051,435	2,527,005
Germany - - -	865,102	159,658	1,024,760
China - - -	1,842,355	927,304	2,769,659
Corea - - -	229,713	248,830	478,543

Other European countries occupy quite a secondary position. It is worthy of notice that the value of the trade with European Russia amounts to only about 36,000 yen, while with Asiatic Russia the value is over 2,000,000 yen.

As far as Germany is concerned the figures representing the value of German imports into Japan show an improvement on those of the previous year; the value of the export trade, as stated above, does not include the Hong Kong trade. The high figure of the French imports from Japan (nearly 19½ millions of yen) is explained by the large amount of Japanese silk, an article for which France is the principal customer. The value of the silk alone taken by France in 1894 was more than 18,000,000 yen.

Although German trade with Japan doubtless shows a considerable improvement, the commercial dealings of the two countries have by no means yet reached the limits of the capacity of the supply and demand of either country. If England can export merchandise to Japan to the value of over 42 millions of yen, the 8 millions of yen ascribed to Germany appears small enough, especially if there be taken into account the importance of the German firms established in Japan and the facility of transport afforded by the German line of steamers with East Asia.

A special feature of Japanese trade, however, merits the attention of those interested in it. It is a recognised fact that the Japanese are doing all in their power to limit the business of European firms and substitute native firms in their place. Remembering the commercial spirit and activity of the Japanese it was to be expected that noteworthy consequences would follow from such an endeavour, and in point of fact these efforts of the Japanese to replace European traders by native ones have surpassed all expectations.

In the year 1885 of a total trade of 65,000,000 yen, the share of the Japanese themselves was barely 7 millions, while that of Europeans and Americans amounted to over 58 millions; but in 1894, of a total movement of about 229 millions, the share of the Japanese had risen to over 55½ millions, while that of Europeans and Americans was 173,000,000 yen. That is to say, that the interest of Japanese firms had increased to eight times what it was in 1885; and European and American firms during the same period had only trebled the value of their trade. In short, the share of the Japanese in the total trade, which had only been a little over 11 per cent. in 1885, had attained to 25 per cent. in 1894.

In one category German imports into Japan show a vast superiority over all other countries, viz., drugs, chemicals, and dyes. Other industrial products in which German imports predominate are iron nails (1,000,000 yen in 1894): telegraph wire (85,000 yen, with a notable increase in the last three years): woollen yarn (489,000 yen): woollen articles of clothing, chiefly netted goods (367,000 yen: imports from England, 254,000 yen): flannels (German, 242,000 yen; English, 57,000 yen): satins,

half silk (German, 96,000 yen ; English, 14,200 yen) : zinc (German, 234,000 yen ; English, 143,000 yen).

Turning to other fields of industrial activity, it is seen that in iron goods Germany generally occupies the third position, after the United Kingdom and Belgium, and sometimes even the fourth. Iron bars are exported to Japan by the United Kingdom to the value of 659,000 yen ; Germany only 27,000 yen : wheels, &c. from United Kingdom for 572,000 yen ; from Germany for only 341 yen, and so on. With regard to machinery the values are : steam boilers, United Kingdom, 145,000 yen ; France, 8,500 yen ; Germany, 4,700 yen. Locomotives, United Kingdom, 990,000 yen ; United States, 553,000 yen ; Germany, 37,000 yen. Spinning machines, United Kingdom, 2,786,000 yen ; France, 57,600 yen ; Germany, 15,000 yen. Mining machinery, United Kingdom, 30,000 yen ; Germany, 8,000 yen. Electrical machinery, United States, 146,000 yen ; United Kingdom, 49,000 yen ; Germany, 18,000 yen.

Turning now to the textile industries, the following figures appear, always taking into account the trade for 1894. Cotton yarn, imports from United Kingdom into Japan, 7,250,000 yen ; from British India, 688,000 yen ; but from Germany only 1,000 yen. Cotton pocket handkerchiefs, United Kingdom, 149,000 yen ; Germany, 22,500 yen. Cotton satins, United Kingdom, 1,246,000 yen ; Germany, 32,000 yen. Cotton velvet, United Kingdom, 667,000 yen ; Germany, 33,500 yen. *Mousseline de laine*, France, 2,932,000 yen ; Germany (chiefly Alsace), 195,000 yen ; United Kingdom, 16,000 yen. Grey shirtings, United Kingdom, 9,232,000 yen ; Germany, 2,000 yen. White shirtings, United Kingdom, 337,000 yen ; Germany, *nil*. Italian cloths, United Kingdom, 1,700,000 yen ; Germany, 42,000 yen.

XIII.—FRUIT CULTURE IN THE HIMALAYAS.

In "Capital" (Bengal) an interesting account is given of the present condition and prospects of fruit culture in the Himalayas, taken from a recent issue of the "Agricultural Ledger." From this it appears that during the past 25 years or so Government has established and maintained numerous orchards and nurseries, carried out experiments, distributed free amongst cultivators, plants, seeds, seedlings and grafts of the choicer varieties of fruits suitable to the climate, in which and other directions considerable expenditure must have been incurred. The main object, it is stated, is not so much to produce high-class European fruit for those who can afford to pay the exorbitant prices which it commands, as to extend the cultivation of such fruit as can bear transport to the big hill-stations and to the plains, and thus to increase the production of an important food-supply. Looking to the vast quantities in which many kinds of fresh and dried fruits are annually imported into India from Kabul, it was considered that here was a trade well worth striving to secure for Kumaon and adjacent parts; and the favourable results that had already attended the founding of the gardens at Chaubattia (near Ranikhet)—which are alluded to as amongst the first of their kind—encouraged the belief that the scheme embodied the elements of success. In Kumaon, English apples thrive well from imported seed; the Ribston pippin, for example, which is rapidly becoming extinct at home, through a disease called "canker," comes to great perfection, this variety, as a whole, being described as splendid and highly coloured. English varieties of the peach also do well, and when the best results are obtained, are said to be in every way equal to the Californian production. The apricot tree flourishes and bears profusely; indeed, it is referred to as the most prolific and widely-spread of all fruit trees in Kumaon, and is equally at home at an altitude of 1,000 ft. or 8,000 ft., although it is at its best between 4,000 and 5,500 ft. The sweet chestnut has been introduced in more recent years, and great things are expected of it. Cherries do well in Kumaon and fetch good prices, but they are grown on a very limited scale. The climate has also been found suitable to the English plum, but somewhat, strange to say, this fruit has been neglected, although four varieties of the Cashmere plum, which are very acid even when quite ripe, are plentiful.

From various sources of information, however, it has been found that there are many obstacles in the way of extending the fruit-growing industry.

A long time necessarily intervenes between the outlay of capital on the plantation of orchards and realising returns from the crops, and this of itself has greatly deterred private enterprise. As regards the native, generally, it goes much against the grain to devote the necessary care and time to manuring, pruning, thinning, picking, packing, and to give or provide the necessary protection to the trees from predatory neighbours and robber-tribes of beasts and birds, while hail-storms, which create great havoc in some

situations, are beyond control. In fact, fruit culture—on the lines that have made it a success in gardens and nurseries belonging to Government, and to the few Europeans and the comparatively small number of the wealthier natives—is, it is to be feared, beyond the poorer class of cultivators. And even if it were within their reach, it is questionable how far their apathetic nature and conservative ways would permit them to avail themselves of the advantages offered. Hence, village orchards, it is said, are almost unknown. A section of substantial agriculturists, corresponding to the yeoman class in England, have, however, been found anxiously prepared to seize the opportunity offered by Government nurseries to establish orchards; and, although there have been numerous failures amongst them, others again have succeeded, and now export to Calcutta and elsewhere. Another great drawback to any material expansion of the export trade in fruit is, of course, its perishable nature and the consequent difficulty of safe transport. Even apples, which are least of all susceptible to injury, and which keep at least tolerably well, require to be packed with the utmost care, each fruit being wrapped separately in paper, and then nested in moss, but even then the loss from damage is considerable, which, if the packing is carelessly performed, may amount to as much as 75 per cent. On the other hand, we read of apples from Kulu finding their way as far as Quetta. Pears do not stand travel so well; and, moreover, are liable to instant decay when fully ripe. The peach, again, it is remarked, will not bear carriage to any distance, and this is also the case as regards cherries, raspberries, and other similar fruits, which must, therefore, depend chiefly on local demand. No mention is made of the apricot's character in this respect, but it is probably little less liable to injury than the peach.

The final conclusions appear to be that the cultivation of fruit in the North-West Himalaya is capable of being made a lucrative business with some outlay of capital, and good, scientific culture. The prices procurable for good dessert fruit in the large hill-stations and the cantonments on the plains are enormous; and, however cultivators may extend their operations, the demand must for many years exceed the supply. Even were it impossible to maintain the present very high rates, greatly reduced wholesale selling figures would still leave the cultivator a good margin of profit. It is recommended, therefore, that Government should continue to organise orchards in localities, and teach the zemindars how to ensure good crops; and, in short, to make the undertaking successful. It is also suggested that further efforts should be made to persuade villagers to establish similar enclosed gardens for themselves. It seems not improbable that were Government orchards, advanced to the stage of yielding crops, made available for purchase on fair and easy terms, Europeans possessed of small capital would be induced to invest and settle down on their property, thus promoting eventually it might be a partial permanent European colonisation of the North-West Himalaya.

XIV.—CUSTOMS TARIFF OF NEW ZEALAND.

(Continued from p. 53 of the January number.)

No.	Articles.	Rates of Duty.
CLASS VI.—CLOTHING AND TEXTILE GOODS— <i>cont.</i>		
96	Drapery n.o.e. - - - - -	20 % <i>ad val.</i> s. d.
97	Feathers, ornamental (including ostrich), and artificial flowers - - - - -	25 % <i>ad val.</i>
98	Forfar, dowlas, and flax sheeting, n.o.e. - - - - -	20 % <i>ad val.</i>
99	Furs - - - - -	25 % <i>ad val.</i>
100	Haberdashery n.o.e. - - - - -	20 % <i>ad val.</i>
101	Hats of all kinds, including straw hats, also caps - - - - -	25 % <i>ad val.</i>
102	Hosiery n.o.e. - - - - -	20 % <i>ad val.</i>
103	Lace and laces n.o.e. - - - - -	25 % <i>ad val.</i>
104	Millinery of all kinds, including trimmed hats, caps, and bonnets - - - - -	25 % <i>ad val.</i>
105	Ribbons and crape, all kinds - - - - -	25 % <i>ad val.</i>
106	Rugs, woollen, cotton, opossum, or other - - - - -	20 % <i>ad val.</i>
107	Shawls - - - - -	20 % <i>ad val.</i>
108	Silks, satins, velvets, plushes, n.o.e., composed of silk mixed with any other material, in the piece - - - - -	25 % <i>ad val.</i>
109	Textile piece-goods other than cotton or silk - - - - -	20 % <i>ad val.</i>
110	Umbrellas, parasols, and sunshades - - - - -	20 % <i>ad val.</i>
111	Yarns n.o.e. - - - - -	20 % <i>ad val.</i>
CLASS VII.—LEATHER AND MANUFACTURES OF LEATHER.		
112	Boots, shoes, and slippers, n.o.e.; goloshes, clogs, and pattens, vamps, uppers, and laces - - - - -	22½ % <i>ad val.</i>
113	Heel plates, and toe-stiffeners and plates - - - - -	22½ % <i>ad val.</i>
114	Leather:—	
	Leather belting and belt-leather, harness, bridle, legging, bag, kip (other than East India) - - - - -	Per lb. 0 4
	Buff and split, including satin hides and tweeds - - - - -	„ 0 3
	Cordovan, levantad leather, roans, sheepskins, morocco, n.o.e., basils - - - - -	„ 0 3
	Sole leather - - - - -	„ 0 2
	East India kip, Persians, lambskins, and goatskins (dressed other than Morocco), kangaroo and wallabi skins (dressed), tan and coloured calf - - - - -	„ 0 2
	Leather n.o.e. - - - - -	„ 0 1
115	Leather board or compo - - - - -	„ 0 4
116	Leather bags and leather-cloth bags n.o.e. - - - - -	20 % <i>ad val.</i>
117	Leather, chamois - - - - -	20 % <i>ad val.</i>
118	Leather cut into shapes - - - - -	22½ % <i>ad val.</i>
119	Leather leggings - - - - -	22½ % <i>ad val.</i>
120	Leather manufactures n.o.e. - - - - -	20 % <i>ad val.</i>
121	Portmanteaux, trunks, travelling-bags and brief-bags of leather or leather-cloth, 10 ins. in length and upwards, and carpet bags - - - - -	25 % <i>ad val.</i>
122	Saddlery and harness, whips and whip-thongs - - - - -	20 % <i>ad val.</i>

No.	Articles.	Rates of Duty.
CLASS VIII.—FURNITURE AND HOUSEHOLD FURNISHING.		
123	Basket and wicker-ware n.o.e., not being furniture - - - - -	20 % <i>ad val.</i> s. d.
124	Carpets and druggets, floorecloth, mats and matting - - - - -	20 % <i>ad val.</i>
125	Desks - - - - -	20 % <i>ad val.</i>
126	Furniture and cabinet-ware n.o.e., and other than iron - - - - -	25 % <i>ad val.</i>
127	Furniture-, knife-, and plate-powder and polish - - - - -	20 % <i>ad val.</i>
128	Mantelpieces, other than stone - - - - -	20 % <i>ad val.</i>
129	Upholstery n.o.e. - - - - -	25 % <i>ad val.</i>
CLASS IX.—CHINA, GLASS, AND EARTHEN GOODS.		
130	Bricks, known as firebricks - - - - -	20 % <i>ad val.</i>
131	China, porcelain, and parianware - - - - -	20 % <i>ad val.</i>
132	Drainage pipes and tiles - - - - -	20 % <i>ad val.</i>
133	Earthen flooring and garden tiles - - - - -	20 % <i>ad val.</i>
134	Earthenware, stoneware, and brownware - - - - -	20 % <i>ad val.</i>
135	Filters - - - - -	20 % <i>ad val.</i>
136	Fireclay, ground, and fireclay goods - - - - -	20 % <i>ad val.</i>
137	Glass, crown, sheet, and common window - - - - -	{ Per 100 superficial feet } 2 0
138	Glassware; also plate-glass, and glass polished, coloured, and other kinds n.o.e., globes and chimneys for lamps - - - - -	20 % <i>ad val.</i>
139	Lamps, lanterns, and lampwick - - - - -	20 % <i>ad val.</i>
140	Plate-glass, bevelled or silvered; mirrors and looking-glasses, framed or unframed - - - - -	25 % <i>ad val.</i>
CLASS X.—FANCY GOODS, MUSICAL INSTRUMENTS, &c.		
141	Artificial flies - - - - -	25 % <i>ad val.</i>
142	Cards, playing - - - - -	Per pack 0 6
143	Clocks - - - - -	20 % <i>ad val.</i>
144	Dressing-cases - - - - -	20 % <i>ad val.</i>
145	Fancy goods and toys - - - - -	20 % <i>ad val.</i>
146	Fishing tackle, including artificially baited hooks, other than flies - - - - -	20 % <i>ad val.</i>
147	Jewellery; plate, gold and silver; greenstone, cut or polished - - - - -	20 % <i>ad val.</i>
148	Mouldings in the piece, for picture frames, cornices, or ceilings - - - - -	15 % <i>ad val.</i>
149	Musical instruments of all kinds n.o.e. - - - - -	20 % <i>ad val.</i>
150	Oil, perfumed - - - - -	25 % <i>ad val.</i>
151	Papier-maché ware - - - - -	20 % <i>ad val.</i>
152	Perfumery n.o.e. - - - - -	25 % <i>ad val.</i>
153	Perfumed spirits and Cologne-water: before the 1st February 1896 - - - - -	Per liquid gallon 21 0
	Perfumed spirits and Cologne-water: after 1st February 1896 - - - - -	30 0
154	Photographic goods, n.o.e. - - - - -	20 % <i>ad val.</i>
155	Pictures, paintings, drawings, engravings, and photographs, framed or unframed; picture- or photograph-frames and mounts - - - - -	20 % <i>ad val.</i>
156	Plated ware - - - - -	20 % <i>ad val.</i>
157	Statues, statuettes, casts, and bronzes - - - - -	20 % <i>ad val.</i>
158	Tobacco pipes and cases, cigar- and cigarette-holders and cases, cigarette papers and cases - - - - -	25 % <i>ad val.</i>
159	Toilet preparations n.o.e. - - - - -	25 % <i>ad val.</i>
160	Watches - - - - -	20 % <i>ad val.</i>
161	Walking-sticks - - - - -	20 % <i>ad val.</i>

No.	Articles.	Rates of Duty.
CLASS XI.—PAPER MANUFACTURES AND STATIONERY.		
162	Calendars and show-cards, all kinds - - -	25 % <i>ad val.</i> s. d.
163	Cardboard boxes complete, or cardboard cut and shaped for boxes (including match-boxes) -	25 % <i>ad val.</i>
164	Directories of New Zealand, or of any part thereof ; also covers for directories - -	25 % <i>ad val.</i>
165	Handbills, programmes, and circulars, playbills and printed posters - - -	20 % <i>ad val.</i>
166	Ink, writing - - - - -	20 % <i>ad val.</i>
167	Paper bags, coarse (including sugar-bags) -	Per cwt. 7 6
168	Paper bags n.o.e. - - - - -	25 % <i>ad val.</i>
169	Paper-hangings - - - - -	15 % <i>ad val.</i>
170	Paper, wrapping, viz., blue candle, glazed cap, glazed casings, small hand, lumber hand, and tissue - - - - -	Per cwt. 5 0
171	Paper, wrapping, other kinds, including brown, cartridge, and sugar papers - - -	„ 5 0
172	Printed matter relating to patent or proprietary medicines ; trade catalogues, price-lists, and fashion-plates of the goods of firms or persons in the Colony - - - - -	25 % <i>ad val.</i>
173	Stationery and writing-paper n.o.e. - - -	20 % <i>ad val.</i>
174	Stationery, manufactured, viz., account books, manuscript books, billhead, invoice, and statement forms, printed or ruled paper, counter books, cheque and draft forms, tags, labels, blotting pads, sketch books, book covers, copying letter-books, manifold-writers, albums (other than for photographs), diaries, birthday books, plain or faint-lined ruled books, printed window-tickets, printed, lithographed, or embossed stationery, and Christmas, New-year, birthday, and Easter cards and booklets -	25 % <i>ad val.</i>
175	Stereotypes and matrices - - -	25 % <i>ad val.</i>
CLASS XII.—MANUFACTURES OF METAL.		
176	Bicycles, tricycles, and the like vehicles, also finished or partly finished or machined parts of same, n.o.e., including weldless steel tubing cut to short lengths - - - - -	20 % <i>ad val.</i>
177	Boilers, land and marine - - - - -	20 % <i>ad val.</i>
178	Brass cocks, valves, unions, lubricators, and whistles - - - - -	20 % <i>ad val.</i>
179	Brass manufactures n.o.e. - - - - -	20 % <i>ad val.</i>
180	Cartridges (shot), 10- to 24-bore - - -	Per 100 1 6
181	Cartridge cases - - - - -	„ 0 9
182	Cartridges n.o.e. - - - - -	20 % <i>ad val.</i>
183	Cash-registering machines - - - - -	10 % <i>ad val.</i>
184	Coffin-furniture - - - - -	20 % <i>ad val.</i>
185	Composition piping - - - - -	Per cwt. 3 6
186	Copper manufactures n.o.e. - - - - -	20 % <i>ad val.</i>
187	Copying-presses - - - - -	20 % <i>ad val.</i>
188	Crab-winchcs, cranes n.o.e., capstans, and windlasses - - - - -	20 % <i>ad val.</i>
189	Cutlery - - - - -	20 % <i>ad val.</i>
190	Fire-arms, all kinds - - - - -	20 % <i>ad val.</i>
191	Galvanised-iron manufactures n.o.e. - -	25 % <i>ad val.</i>
192	Gasometers and other apparatus for producing gas ; also gas-meters - - - - -	10 % <i>ad val.</i>
193	Gaspipes, iron - - - - -	5 % <i>ad val.</i>

No.	Articles.	Rates of Duty.
CLASS XII.—MANUFACTURES OF METAL— <i>cont.</i>		
194	Hardware, ironmongery, and hollowware -	20 % <i>ad val.</i> s. d.
195	Iron bridges and iron material n.o.e., for the construction of bridges, wharves, jetties, or patent slips - - -	20 % <i>ad val.</i>
196	Iron columns for building, and other structural ironwork - - -	20 % <i>ad val.</i>
197	Iron doors for safes and vaults - - -	20 % <i>ad val.</i>
198	Iron, galvanised corrugated sheets, screws, and nails - - -	Per cwt. 2 0
199	Iron galvanised tiles, ridging, guttering, and spouting - - -	20 % <i>ad val.</i>
200	Iron gates and gate-posts, staples, standards, straining posts, and apparatus - - -	20 % <i>ad val.</i>
201	Iron nails - - -	Per cwt. 2 0
202	Iron pipes, and fittings for same, including main-cocks - - -	5 % <i>ad val.</i>
203	Iron, plain galvanised sheet and hoop - - -	Per cwt. 1 6
204	Iron tanks, exceeding 200 gallons and not exceeding 400 gallons - - -	Each 10 0
205	Iron tanks of and under 200 gallons - - -	" 5 0
206	Iron work and wire work - - -	20 % <i>ad val.</i>
207	Japanned and lacquered metal ware - - -	25 % <i>ad val.</i>
208	Lawn-mowers - - -	20 % <i>ad val.</i>
209	Lead in sheets - - -	Per cwt. 1 6
210	Lead piping - - -	" 3 6
211	Machinery n.o.e. - - -	20 % <i>ad val.</i>
212	Machinery, electric, and appliances - - -	10 % <i>ad val.</i>
213	All machinery for agricultural purposes, including chaff-cutters, corn-crushers, corn-shellers, also articles used in manufacturing the same—namely, chaff-cutting knives, tilt-rakes, fittings for threshing-mills, forgings for ploughs; but excluding reapers and binders - - -	5 % <i>ad val.</i>
214	Machinery for dairying purposes (excluding separators and coolers) - - -	5 % <i>ad val.</i>
215	Machinery for flour-mills, woollen-mills, paper-mills, rope- and twine-making, dredging, saw-milling, planing, and wood-working (including lathes), oil-refining, boring, and also machinery for refrigerating or preserving meat, leather-splitting machines and band-knives for same - - -	5 % <i>ad val.</i>
216	Machinery for stamping and blocking tin - - -	5 % <i>ad val.</i>
217	Machinery of every description for mining purposes, including machine pumps, but excluding machinery for gold-saving purposes and processes - - -	5 % <i>ad val.</i>
218	Manufactures, n.o.e., of metal, or of metal in combination with any other material - - -	20 % <i>ad val.</i>
219	Nails n.o.e. - - -	Per cwt. 3 0
220	Portable engines on four or any greater number of wheels, with boilers of locomotive type; also traction-engines - - -	5 % <i>ad val.</i>
221	Printing machines and presses - - -	5 % <i>ad val.</i>
222	Pumps and other apparatus for raising water n.o.e. - - -	20 % <i>ad val.</i>
223	Railway and tramway plant and materials n.o.e. - - -	20 % <i>ad val.</i>
224	Sad-irons - - -	20 % <i>ad val.</i>
225	Shot - - -	Per cwt. 10 0
226	Soda-water machines; also machines for aerating liquids - - -	5 % <i>ad val.</i>
227	Steam-engines and parts of steam-engines n.o.e. - - -	20 % <i>ad val.</i>

No.	Articles.	Rates of Duty.	
CLASS XII.—MANUFACTURES OF METAL— <i>cont.</i>		<i>s. d.</i>	
228	Steam-engines and parts thereof (including the boiler or boilers thereof), imported specially for mining and dairying purposes - - -	5 %	<i>ad val.</i>
229	Tinware and tinsmiths' furniture n.o.e. - - -	25 %	<i>ad val.</i>
230	Waterworks pipes, iron - - -	5 %	<i>ad val.</i>
231	Weighbridges and weighing-machines - - -	20 %	<i>ad val.</i>
232	Wire mattresses and webbing - - -	20 %	<i>ad val.</i>
233	Zinc tiles, ridging, guttering, piping - - -	20 %	<i>ad val.</i>
234	Zinc manufactures n.o.e. - - -	25 %	<i>ad val.</i>
CLASS XIII.—TIMBER AND ARTICLES MADE FROM TIMBER.			
235	Bellows, other than forge - - -	20 %	<i>ad val.</i>
236	Blocks, wooden tackle - - -	20 %	<i>ad val.</i>
237	Buckets and tubs, of wood - - -	20 %	<i>ad val.</i>
238	Carriages, carts, drays, waggon, and perambulators, and the wheels for the same - - -	20 %	<i>ad val.</i>
239	Carriage shafts, spokes and felloes, dressed, bent carriage timber, n.o.e. - - -	20 %	<i>ad val.</i>
240	Doors, glazed, with ornamental glass - - -	Each	4 0
241	Doors, plain - - -	"	2 0
242	Sashes, glazed, with ornamental glass - - -	Per pair	4 0
243	Sashes, plain - - -	"	2 0
244	Timber, palings - - -	Per 100	2 0
245	Timber, posts - - -	"	8 0
246	Timber, rails - - -	"	4 0
247	Timber, sawn, dressed - - -	Per 100 feet superficial }	4 0
248	Timber, sawn, rough - - -	"	2 0
249	Timber, shingles and laths - - -	Per 1,000	2 0
250	Woodenware and turnery n.o.e., and veneers - - -	20 %	<i>ad val.</i>
CLASS XIV.—OILS, PAINTS, &C.			
251	Axle-grease and other solid lubricants - - -	20 %	<i>ad val.</i>
252	Harness oil and composition, and leather dressing - - -	20 %	<i>ad val.</i>
253	Naphtha - - -	Per gallon	0 6
254	Oil, kerosene - - -	"	0 6
255	Oil, linseed - - -	"	0 6
256	Oil, mineral, including shale-waste or unrefined mineral oil n.o.e. - - -	"	0 6
257	Oil n.o.e. - - -	"	0 6
258	Oil, olive - - -	"	0 6
259	Oil, vegetable, in bulk, n.o.e. - - -	"	0 6
260	Oil, vegetable or other, in bottle - - -	15 %	<i>ad val.</i>
261	Paints and colours, ground in oil or turpentine - - -	Per cwt.	2 6
262	Paints and colours, mixed ready for use - - -	"	5 0
263	Putty - - -	"	2 0
264	Stearine - - -	Per lb.	0 1½
265	Varnish, enamel paints, gold size - - -	Per gallon	2 0
266	Whiting and chalk - - -	Per cwt.	1 0
CLASS XV.—AGRICULTURAL AND FARM PRODUCTS, &C.			
267	Animals, food for, of all kinds, n.o.e. - - -	20 %	<i>ad val.</i>
268	Cattle (horned) - - -	Each	10 0
269	Chaff - - -	Per ton	20 0
270	Grain—namely, barley - - -	Per 100 lb.	2 0

No.	Articles.	Rates of Duty.	
CLASS XV.—AGRICULTURAL AND FARM PRODUCTS, &c.— <i>cont.</i>			
271	Grain and pulse of every kind n.o.e. - -	Per 100 lb.	s. d. 0 9
272	Grain and pulse of every kind, when ground or in any way manufactured, n.o.e. - -		
273	Horses - - - - -	Each	20 0
274	Linseed - - - - -	Per ton	20 0
275	Maize - - - - -	Per 100 lb.	0 9
276	Onions - - - - -	Per ton	20 0
277	Prepared calf-meal - - - - -	"	25 0
CLASS XVI.—MISCELLANEOUS.			
278	Bags, flour - - - - -	20 % <i>ad val.</i>	
279	Bags, calico, forfar, hessian, and linen - -	20 % <i>ad val.</i>	
280	Bagging and bags n.o.e. - - - - -	15 % <i>ad val.</i>	
281	Blacking and boot gloss - - - - -	20 % <i>ad val.</i>	
282	Blacklead - - - - -	20 % <i>ad val.</i>	
283	Blue - - - - -	Per lb.	0 2
284	Brooms, brushes, and brushware n.o.e. - -	25 % <i>ad val.</i>	
285	Brushes, hair, and combs; toilet, clothes, and hat brushes - - - - -		
286	Candies - - - - -	20 % <i>ad val.</i> { Per lb. or pack- age of that re- puted weight, and so in pro- portion for packages of greater or less reputed weight - }	0 2
287	Cement - - - - -	Per barrel	2 0
288	Cordage and rope n.o.e. - - - - -	20 % <i>ad val.</i>	
289	Cork, cut, including bungs - - - - -	20 % <i>ad val.</i>	
290	Fireworks, n.o.e. - - - - -	20 % <i>ad val.</i>	
291	Flock - - - - -	10 % <i>ad val.</i>	
292	Glue and size - - - - -	Per lb.	0 1½
293	Granite, sawn on not more than two sides, and not dressed or polished - - - - -	5 % <i>ad val.</i>	
294	Marble, granite, and other stone, dressed or polished, and articles made therefrom, including mantelpieces - - - - -	25 % <i>ad val.</i>	
295	Matches :— Wooden : In boxes containing not more than 60 matches - - - - - In boxes containing over 60 and not more than 100 matches - - - - - In boxes containing more than 100 matches, for every 100 matches or fraction thereof contained in one box - Wax : "Plaid vestas," in cardboard boxes containing under 100 matches - - - - - "Pocket vestas," in tin or other boxes containing under 100 matches - - - - - Sportsman's "ovals" and "No. 4 tins vestas," in boxes containing not more than 200 matches - - - - - Other kinds, for every 100 matches or fraction thereof contained in one box -	Per gross boxes " " " " " " " "	1 0 2 0 2 0 1 2 1 9 5 0 2 6
296	Nets and netting - - - - -	20 % <i>ad val.</i>	
297	Powder, sporting - - - - -	Per lb.	0 6
298	Rice, manufactured into starch, in bond - -	Per cwt.	2 0
299	Sacks, other than corn sacks and jute sacks -	15 % <i>ad val.</i>	

No.	Articles.	Rates of Duty.
CLASS XVI.—MISCELLANEOUS— <i>cont.</i>		
300	Sausage skins and casings (including brine or salt) - - - - -	Per lb. s. d. 0 3
301	Soap, common yellow and blue mottled - - -	Per cwt. 5 0
302	Soap n.o.e. - - - - -	25 % <i>ad val.</i>
303	Soap powder, extract of soap, dry soap, and soft soap - - - - -	20 % <i>ad val.</i>
304	Spirits, methylated - - - - -	Per liquid gallon 1 0
305	Spirits, cleared from warehouse, methylated, under prescribed conditions - - - - -	" 0 6
306	Starch - - - - -	Per lb. 0 2
307	Tarpaulins, tents, rick and waggon covers - -	20 % <i>ad val.</i>
308	Twine n.o.e. - - - - -	20 % <i>ad val.</i>
309	Washing powder - - - - -	20 % <i>ad val.</i>
310	Wax, paraffin, mineral, vegetable, and Japanese	Per lb. 0 1½
In addition to any duty chargeable by law on any goods imported into the colony, a further duty of 20 per cent. <i>ad valorem</i> shall be charged when the goods are prison-made.		

Table of Exemptions from Duties of Customs.

No.	Articles.
CLASS I.—FOODS, &C.	
311	Almonds, Barbary, Sicily, and French, used in confectioners' manufactures.
312	Anchovies, salted, in casks.
313	Arrowroot, sago, tapioca, macaroni, vermicelli, and prepared groats.
314	Rock salt.
CLASS IV.—NON-ALCOHOLIC BEVERAGES, &C.	
315	Cocoa-beans.
CLASS V.—DRUGS, &C.	
316	Acids, viz. : boracic, carbolic, in bulk; fluoric, muriatic, nitric, oxalic, oleic, pyrogallie, salicylic, sulphuric.
317	Concentrated extracts, or essences in liquid form or preserved in fat, for perfume-manufacturing purposes in manufacturing warehouses, in bottles of not less than 1 lb. in weight.
318	Disinfectants.
319	Drugs and chemicals, viz. : alum; sulphate of aluminium; sulphate of ammonia; anhydrous ammonia; aniline dyes; arsenic; bluestone, or sulphate of copper; borax; catechu; chloride of calcium; nitrate of silver; cochineal; creosote, crude or commercial; glycerine, crude; gum, arabic and tragacanth; gum benzoin; artificial gum arabic; gum damar; phosphorus; potash, caustic potash, and chloride of potash; pearlsh; cyanide of potassium; sal-ammoniac; saltpetre; acetate of soda, crude; soda-ash; caustic soda; nitrate of soda; silicate of soda; sulphate of soda; sulphide of sodium; strychnine; sulphur; chloride of zinc; iron sulphates; gall-nuts; turmeric; saffron; nitrous-oxide gas; tree washes; insecticides; maltine; chlorodyne.
320	Essential oils, except eucalyptus; cod-liver oil; oil of rhodium.
321	Horse-drenches.
322	Medicinal barks, leaves, herbs, flowers, roots, and gums.
323	Scrub-exterminator.
324	Sheep-dip; sheep-drenches; sheep-licks.
325	Surgical and dental instruments and appliances.
326	Scientific and assay balances, retorts, flasks, and other appliances for chemical analysis and assay work.
327	Water-hardening chemicals for brewers' use.

(To be continued.)

XV.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

RUSSIA.

Tariff Modifications.

A despatch, dated December 31 last, has been received at the Foreign Office from Mr. W. E. Goschen, Her Majesty's Secretary of Embassy at St. Petersburg, notifying the following classification of goods under the Russian Customs Tariff:—

Only such apparatus for parboiling cattle food is to be cleared under point 4, section 167 (duty 50 copecks, gold, per poud), the troughs or reservoirs of which shall not exceed 36 cubic feet (exterior measurement) in dimension; at the same time the distinguishing feature of the agricultural nature of such apparatus shall serve the requisite steam generator thereto.

Bootmakers' lasts of wood as also similar articles made of one or several materials to be cleared not under section 161, but according to material. The previous order respecting bootmakers' lasts is hereby repealed.

NETHERLANDS—EAST INDIES.

Export Duty on Sugar.

According to the "Bollettino di Notizie Commerciali," the Netherlands Parliament has approved the suspension for one year of the duty on sugar exported from the Dutch East Indies. The suspension of the duty will last provisionally until 1st June 1896, and after that date can be confirmed if the experiment meets with the success anticipated.

BELGIUM.

Alteration of Port Dues at Antwerp.

A despatch, dated January 31 last, has been received from the Foreign Office, enclosing extract from the "Moniteur Belge," containing copy of a Royal decree approving a decision adopted by the Antwerp Town Council to substitute a new tariff of port dues in that city, to come into effect from the 1st February last.

The articles of the above decree are as follows:—

1. Vessels entering the harbour will pay the town for each voyage, a due of 50 centimes per ton Moorsom, whatever be the number of voyages made in the course of the year.

2. On payment of the above due vessels can remain in harbour for two months, after which an additional 6 centimes per ton per month will be exacted, to commence on the 1st of each month.

3. Vessels loading or discharging at the roadstead, or at one of the river quays, as well as the vessels of regular lines admitted into the harbour, will pay :—For each of the first 10 voyages, 30 centimes per ton ; for each of the 10 following voyages, 20 centimes per ton ; for each additional voyage, 14 centimes per ton, inclusive.

For this reduction to be applicable the voyages must be made within 12 months of the date of the first arrival.

4. Vessels which remain in the roadstead and effect no operation of any kind will be exempt from all dues.

Vessels bound up stream and compelled to lighten are also exempt from dues provided that the lighters also accompany them.

Dock Dues at Ostend.

A despatch, dated 4th February last, has been received from the Foreign Office, enclosing extract from the “*Moniteur Belge*” containing copy of Royal Decree relating to the dock dues at Ostend, as follows :—

a. All trading vessels arriving from the sea or the canals and waterways of Belgium, making use of the docks and quays, in any part of Ostend, will pay on such voyage—

1. 30 centimes per ton for loading and discharging.

2. When calling only—with cargo or in ballast—10 centimes per ton Moorsom for more than 24 hours’ stay.

These dues once paid vessels may remain three months in dock, but a due of 5 centimes will be levied for each additional month.

b. Every trading vessel bound for the interior, and landing cargo, will pay 30 centimes per ton of cargo discharged.

c. Every trading vessel arriving from the interior, *via* canals and waterways, and shipping additional cargo, will pay 30 centimes per ton of cargo shipped at Ostend.

The dues mentioned in paragraphs *b* and *c* are not to be less than that of section 2 of paragraph *a* if the stay exceed 24 hours.

d. Trading vessels belonging to a regular line and carrying passengers or cargoes of manufactured goods will pay as quay dues :—

1. 30 centimes per ton for the first 25 voyages ;

2. 25 centimes per ton for the 15 following voyages ;

3. 20 centimes per ton for any additional voyages.

The reckoning is made each year on the 1st January for the whole of the voyages made by vessels flying the flag of the company, without distinction as to the names of the vessels.

e. Every lot of floating wood, remaining more than 15 days in the docks or their annexes, will pay 10 centimes per square metre, superficial.

f. An additional due of 3 per cent. on the amount of the above taxes will be levied on all sea-going vessels to cover the cost of lighting the quays and docks.

g. Every boat, other than a sea-going vessel, coming from the interior, and making use of the quays and docks to load or unload, or remain, will pay 7 centimes per cubic metre either by voyage or by period of three months' stay.

h. Every fishing boat leaving the docks will pay a fixed due of 2 frs. 50 c. Fishing boats may winter in the docks without being subjected to a supplementary tax.

i. Any boat or vessel which, remaining in the docks, serves as a dépôt for coal or other goods, will be taxed for its stay according to paragraph *g* of this decree.

Abolition of Light Dues.

A despatch, dated 21st January last, has been received at the Board of Trade from the Foreign Office, enclosing copy of a Belgian Royal Decree fixing February 1 as the date from which the general abolition of light dues in Belgium is to take place, in accordance with the law of July 12, 1895.

FRANCE.

Reduction of Tax on Excursionists at Dieppe.

A despatch, dated January 30 last, has been received from the Foreign Office enclosing extract from the "Journal Officiel," containing a Ministerial Decree which authorises that the tax of one franc per head levied by the law of 3rd September 1884 at the embarkation and landing of passengers be reduced to 50 centimes per head in favour of excursionists holding return tickets by the same special boat after a stay of one or two days at most, under condition of conforming to any regulations which may be drawn up as to the control of the tax.

Altered Rates of Pilotage at St. Malo and St. Servan.

A communication, dated February 10 last, has been received from the British Consulate at Cherbourg reporting that, by Ministerial decision, the rates of pilotage at the ports of St. Malo and St. Servan have been altered and fixed as follows:—

			Francs.
Steamers, inwards	-	-	0·215 per reg. ton.
" outwards	-	-	0·20 "
Sailing vessels, inwards	-	-	0·43 "
" outwards	-	-	0·40 "
Plus 3 per cent. for the pilot-major.			

Customs Decisions.

Pongees, corah, and tussah, of extra-European origin, made of raw silk are by the law of 11th January 1892 admitted into France free of duty. By a decision of the general direction of Customs of 20th December last, the above-named articles are

only admitted free of duty if they have undergone no operation of either dyeing, printing, or other form of preparation subsequent to leaving the loom.

According to the "*Moniteur Officiel du Commerce*" iron or steel tubes, closed at the extremities, for use in bakeries are classified under "tubes, iron or steel" and *not* under "manufactures of iron and steel, not denominated."

Tonnage Dues at Boulogne-sur-Mer.

A despatch, dated 10th January last, has been received at the Foreign Office from the Marquis of Dufferin and Ava, Her Majesty's Ambassador at Paris, transmitting extract from the "*Journal Officiel*," containing a decree reducing the tonnage dues at present levied on vessels touching at Boulogne-sur-Mer.

1. *By 60 per cent.* when the amount of cargo shipped or landed, expressed in freight tons, does not exceed the fourth part of the net official tonnage.

2. *By 40 per cent.* when the said amount shall exceed the fourth part, but not the half.

3. *By 20 per cent.* when it shall exceed the half, but amount to less than three-fourths.

Vessels having put in at any other French port and there paid a local tax shall continue in all cases to profit by the reduction established by the decree of 6th August 1890 when the application of the present order does not insure a more favoured treatment.

FRANCE—ALGERIA.

New Quay and other Dues at Algeria.

A despatch, dated January 29 last, has been received at the Foreign Office from Sir R. L. Playfair, K.C.M.G., Her Majesty's Consul-General at Algiers, with regard to important changes in the various dues levied on shipping at that port. Amongst other taxes there are quay duties of 50 centimes per ton, on the registered tonnage of all vessels discharging merchandise at Algiers. Thus, if a vessel of 1,000 tons burden discharged 2,000 tons of goods she has hitherto paid 50 centimes per ton on 1,000 tons or 500 francs.

No charge, however, was made on goods exported from Algeria.

A special régime was accorded to Algeria if a vessel of the same burden landed a part only of its cargo, say 100 tons, in which case the quay duties would be only charged on the amount of cargo so landed, not exceeding in any case the amount which would be due on her total registered tonnage.

Vessels receiving goods on board were altogether exempted from the payment of those duties. This enabled the various lines, such as vessels of the Holt, Moss, Papayanni, and other companies to bring a certain amount of cargo, and to carry goods to other places without having to pay very onerous duties.

Vessels coming from countries out of Europe, India for example, paid double the above amounts, but always on the quantity which they landed.

The Loi de Finance voted by the French Parliament on the 28th December 1895 completely changes this state of things by cancelling the Régime Spéciale. It renders the previous law applicable to Algeria as well as to France, and to a certain extent to French as well as to foreign vessels.

In order to ensure what is called *l'assimilation fiscale*, vessels arriving at Algiers will now have to pay quay duties on their entire registered tonnage, no matter how small the amount of cargo landed by them. They are chargeable, however, only on first arrival at an Algerian port; thus, if a vessel calls first at Oran and then at Algiers, it pays only at Oran. On the other hand, it has to pay full duties if it takes merchandise though landing none.

French vessels coming from ports in France direct, are exempt; they also are exempt if they call on their voyage at a foreign port to discharge goods, but if they take cargo on board at such ports, dues are leviable.

The geographical position of Algiers is quite exceptional, being on the route to and from the North of Europe, the Adriatic, the Black Sea, the Levant, Egypt, and the further east.

Vessels of various nations, belonging to no less than 20 companies, possessing 700 steamers, are in the habit of touching at Algiers without sensibly deviating from their routes. They only bring a small quantity of merchandise each, and carry on an equally small amount of the produce of the colony.

The new legislation affects not only vessels engaged in the transport of merchandise, but also those comprised in what are described as the much larger category of vessels called at Algiers *relâcheurs*.

The law provides that all merchandise arriving from or proceeding to foreign countries shall be required to pay in Algeria a *droit de statistique* of 10 centimes per package or per ton imported or exported.

This will have little effect on merchandise of a valuable nature, but it will press heavily on coal. If it be levied on all coal landed and shipped again, it will be equivalent to a tax of 20 centimes per ton.

With reference to the above, a despatch, dated February 7 last, has been received from the Foreign Office enclosing copy of an extract from the "*Dépêche Algérienne*" which states, that, as a first consequence of the new dues, only 92 *relâcheurs* put into the port in the month of January last as against 143 in December 1895; this, on an average of 150 tons of coal for each *relâcheur*, means 7,950 tons taken from the trade of Algiers elsewhere.

In addition, cargoes of vegetable fibre, cork, barks, &c., for London, Liverpool, Amsterdam, Antwerp, and Northern and Baltic ports in general, lumber the quays and cannot be shipped for want of vessels bound for those ports.

FRANCE—ST. PIERRE AND MIQUELON.

Tariff Changes.

According to the "Journal des Tarifs et Traités de Commerce," a decree of the 7th December last approves the following changes in the *Octroi de Mer* of St. Pierre and Miquelon, by which additional duties are imposed as under:—

	Fr.	Cent.
Liqueurs of all sorts, in casks, per litre - - -	0	10
Liqueurs of all sorts, in bottles (per case or hamper of 12 bottles) - - -	2	00
Beer, in casks or bottles, per litre - - -	0	05
Brandy, in bottles (per case or hamper of 12 bottles)	1	00

PORTUGAL—MADEIRA.

Import Duties on Sugar.

A despatch, dated December 31 last, has been received at the Foreign Office from Sir H. G. Macdonell, Her Majesty's Minister at Lisbon stating that:—

The duty on sugar and molasses made from the sugar-cane imported, for consumption into the district of Funchal (Madeira) subject to the provisions of this decree, is fixed as follows:—

Sugar of a superior type to that of No. 19 of the Dutch scale per kilog. 140 reis (7·46*d.*).

Molasses made from the sugar-cane per kilog. 30 reis. (1·60*d.*).

The above-mentioned duty will only remain in force so long as the makers of sugar and alcohol shall engage to buy any amount of sugar-cane offered to them at the minimum price of 400 reis to 450 reis (1*s.* 9*d.* to 2*s.*) per 30 kilogs.

The importation of molasses in accordance with this decree is in any case limited to the alcohol distilleries for the sole purpose of mixing the same with wine.

While the provisions of this decree are in force—that is to say for five years—molasses in the district of Funchal (Madeira) will be exempted from any local impost, and alcohol extracted from the same will not have to pay the tax leviable on its manufacture.

Moreover, the importation of sugar produced in the island of Maderia into Portugal, and into any of the Azores Islands for consumption will be exempted from the payment of import duties.

SPAIN.

Tariff Decisions.

A despatch, dated January 16 last, has been received at the Foreign Office from Sir G. F. Bonham, Secretary of Embassy at Madrid, enclosing copy and translation of a Royal order published in the Official Gazette concerning the duty to be paid on glass pendants and prisms.

The following decisions are therein laid down :—

1. That pendants and prisms of crystallised glass be dutiable according to No. 12 of the tariff.
2. That the duty be levied on net weight, the boxes of card-board or wood, or other interior receptacles in which contained, paying duty separately, *i.e.*, according to their respective numbers, and also according to kind.
3. That a general character be given to this decision, publishing it to this end, in the official journals, for the cognizance of Custom houses and of commerce.

ITALY.

Revision of Customs Tariff.

A despatch dated January 27 last, has been received at the Foreign Office from Sir Francis Clare Ford, Her Majesty's Ambassador at Rome, enclosing two copies of the Italian Customs Tariff, in the form approved by the Royal Decree of November 24, 1895. By a law, dated August 8, 1895, authority was given for a general re-arrangement and re-classification of the Italian Tariff and also permission to change and modify certain notes appended thereto, with the result that although the new tariff does not contain any increased duties beyond those which have been already notified, neither the numbers of the articles of the tariff nor the form in which they are sub-divided any longer coincide with those in the old tariff.

A copy of the complete tariff may be seen on application at the Commercial Department, Board of Trade, S.W., between the hours of 11 and 5.

Tariff Modifications.

According to the "Bollettino di Notizie Commerciali" the following modifications have been introduced in the Customs Tariff of Italy by decrees of the 30th November and 7th December last respectively :—

"Copper hammerings (*battiture di rame*) are assessed as waste copper and assimilated to 'copper in ingots, cake, filings, or scrap' (No. 211A of the Customs Tariff'.) "Artificial silk is assimilated to natural silk and classified as 'silk raw' (No. 145 of the Customs Tariff)."

UNITED STATES.

Customs Decisions.

The following decisions relating to the classification of articles in the Customs tariff and the application of the Customs laws of

the United States, have recently been given by the Customs authorities in that country :—

Ruffings or “frillings” are dutiable at 60 per cent. *ad valorem* under paragraph 373, Act of October 1, 1890.

Straw coverings for bottled merchandise are assessed as ordinary coverings, without the additional duty of 25 per cent. *ad valorem* under paragraph 352.

“Glazed fire-brick” and “adamantine clinkers” are assessed for duty as brick under paragraph 76.

Roving yarns are dutiable at 30 per cent. *ad valorem* under paragraph 280, as yarn.

Black hatters’ plush, if not used exclusively for making men’s hats, is to pay duty at the rate of 50 per cent. *ad valorem* under paragraph 299.

“Sirop de punch,” “citron extracts,” “pomeranza spirits,” and other preparations containing alcohol, not being in a condition fit for drinking, are dutiable as alcoholic compounds, not specially provided for, under paragraph 7.

“Inspis-ated ox gall,” is assessed for duty at 25 per cent. as a medicinal preparation.

Refined gum for textile printing is dutiable at 10 per cent. under paragraph 16½.

Sulphate of magnesia, or Epsom salts, is exempt from duty under paragraph 542.

Filtering stone, unmanufactured, is to pay duty at the rate of 7 cents per cubic foot, under paragraph 105½.

Manufactured filtering stone is dutiable at 30 per cent. *ad valorem* under paragraph 106.

BRAZIL.

Revision of Customs Tariff.

A despatch, dated the 5th January last, has been received at the Foreign Office from Mr. E. C. Phipps, Her Majesty’s Minister at Rio Janeiro, stating that the revision of the Brazilian tariff, which was carried through Congress during the last week of the session, was promulgated in the “Diario Oficial” on December 31, 1895.

It establishes certain specific duties on various articles enumerated; upon all other important articles the duties hitherto existing, including the surtaxes at 30 and 40 per cent., are doubled; the additional taxes at the rate of 50, and in some cases 60 per cent., imposed in 1891 being abolished. Thus, an article not subjected to specific duties which paid 100 reis original tax, 40 reis surtax, and 50 per cent. additional tax upon such 140 reis, *i.e.*, in all 210 reis, will now pay 280 reis.

Among the most noticeable specific duties may be mentioned :—Foreign beer, now paying 1 dol. 200 reis instead of 448 reis per

kilog.; champagne, which pays 3 dols. 600 reis instead of 2 dols. 900 reis; tinsplates (folha de Flandres), 30 reis instead of 60 reis; perfumery, 5 dols. instead of 2 dols. 340 reis; cotton thread for warp or weft, 300 reis instead of 512 reis; if coloured, 400 instead of 612 reis.

It is to be observed that certain raw materials necessary for manufacturing purposes in native industries (excluding raw cotton or cotton thread) enjoy a reduction of 30 per cent. Parts of ships and steamers for vessels under construction in Brazilian dock-yards are free of duty, and parts of machines and locomotives imported for the construction of railway stock pay 50 per cent., below the tariff duties, under certain restrictions.

All the revised duties in question are applicable to goods shipped for Brazilian ports previous to January 1, 1896.

In the revision of the tariff there does not appear to be any differential treatment of British imports; in the case of cotton (No. 450) and tinsplates (No. 772) there is a reduction of duty in order to meet the necessities of native industry.

The general opinion among importers is said to be that the new tariff will only operate temporarily, and that a fresh revision must ensue during the next session of Congress.

The following is the translation of the text of the decree in question:

1. Duty on imports for consumption, in accordance with the terms of Law No. 265 of December 24, 1894, and the legal regulations referred to therein, the duty-values of the merchandise taxed being modified, however, from the exchange of 24*d.* to that of 12*d.* per 1,000 reis; the additional duties of 50 per cent. and 60 per cent. being abolished, all other extra duties are to be included in the rate prescribed and consolidated in one rate only.

Exceptions.

Foreign beer.—Duty to be \$1.200 per kilog.

Liqueurs.—Sparkling wines of any quality like champagne, and in whatever way put up, duty to be \$3.600 per kilog.

Gin.—Duty to be \$1.500 per kilog.

Matches.—Wood matches to pay \$3.200 per kilog.; all other kinds \$4.500 per kilog.

Soaps.—Saplios and similar articles, unscented, to pay \$1.200 per kilog.

Ordinary enamel or vitrified cobalt for oilworks, \$2.500 per kilog.

Cyanide of potash, pure, will pay the same rate as the unrefined article.

Rough salt will pay 15 reis per kilog.

Rectified alcohol for pharmaceutical purposes will continue to pay the same rate.

Gum arabic, rough, will pay 600 reis per kilog.

Tin sheets will pay 30 reis per kilog.

To the Articles No. 119, Class 9, the following note is to be added :—

All oils to pay on the gross weight, including the casks in which they are contained.

Olive oil which is reported, after analysis in the National Laboratory, to contain foreign matter, or to be adulterated, will be thrown into the sea, and the importer will be fined by the inspector of Custom house \$200 to \$500.

To the Articles No. 127, Class 9, the following notes are to be added :—

Wines condemned by the National Laboratory will be thrown into the sea, and the importer fined \$200 to \$500.

Bottled wine will pay the same rate, and also the duty on bottles and casks.

Bottles, demijohns, jars, and flasks of all kinds, and wooden cases (in pieces or put together), when imported with an appearance imitating foreign liquids or beverages, labelled or not, will pay as if they contained the liquid indicated by the putting up or possible adulteration of said liquid.

Perfumery.—Articles No. 160, Class 10, will pay \$5 per kilog.

Playing cards will pay 1 \$ per pack ; on cardboard (unfinished) or in uncut sheets, painted or stamped \$5 per kilog.

Fish, not specified, mussels, oysters, and other shell-fish, and spawn. No. 60 :—

Preserved, in whatever way prepared, sardines, \$1 per kilog.

All other kinds, \$1·500 per kilog.

Sacks, plain, not specified, will pay \$1·500 per kilog.

Woollens.—Articles No. 546. Class 16. The limit of weight, for woollen cloths, and cloths of wool and cotton mixed, which will pay the higher duty, is increased to 500 grammes per square metre.

Jute cloth, &c.—No. 564. Class 17. The distinctions based upon the number of threads, also the distinction between plain and twilled cloth, are abolished. All kinds will pay 900 reis.

Capsules, comfits, jujubes, and all kinds of medicinal globules (Articles 209 and 297, class 11) will pay on the basis of 40 per cent. of an official value of \$73·200, the duty being thus \$29·280.

Elixirs, liqueurs, wines, syrups, and all kinds of medicinal liquids (Nos. 237, 240, and 341) will pay on the basis of 30 per cent. of an official value of \$20·750, the duty being thus \$6·225.

Fluid magnesia, of Murray or other preparers (No. 273), will pay the same rate as elixirs and medicinal liquids.

Medicinal pastils of all kinds (No. 293), basis 40 per cent. of the official value \$8·625.—Duty \$3·450.

Compressed medicinal pastils, on the basis of 45 per cent. of \$120 official value.—Duty, \$54.

Pills, boluses, and granulated medicines of all kinds, No. 301, on the basis of 40 per cent. of \$156·500, official value.—Duty, \$62·600.

Sarsaparilla will pay the same rate as elixirs and medicinal liqueurs.

Cotton, No. 450, Class 15. Cotton thread, simple, for weaving, unbleached or bleached, will pay 300 reis per kilog. Dyed thread will pay 400 reis per kilog.

2. Clearing of articles exempt from duty for consumption, in accordance with the laws in force (Law No. 126A of November 21, 1892, Article 424 of the *Consolidação das Leis das Alfandegas*).

Seeds intended for agriculture and grain wheat to be exempted.

3. Dock and harbour dues, ditto.

4. Warehouse rent, ditto.

With reference to the above information, a further despatch dated January 18 last, has been received from Mr. E. C. H. Phipps, Her Majesty's Minister at Rio de Janeiro, stating that a circular from the Minister of Finance has just been published, explaining some of the many obscure points in the alterations to the tariff voted in the Budget Law of December 30 last. The following are the points of interest to British trade :—

Goods shipped before December 31, and already received in the Custom-house warehouses or to be discharged in the harbour may be cleared up to February 29.

The same date is fixed for putting into force the exemptions from, or reductions of duties, and the increase of Custom-house fines (*multas de expediente*) provided for in the new law.

The duty on beer, in large or small bottles, or in any other vessel, earthenware or glass, is Rs. 1 \$200 per kilo, but no duty on the bottles.

The reduction in favour of tin (*folha de Flandres*) is only applicable to simple plates, which pay 40 reis per kilo.

Goods paying an *ad valorem* duty will continue to do so, but at the new official rate of exchange, the additional duties being already included as in the case of specific duties.

A clearing fine (*multa de expediente*) of double duty can only be imposed when the duty on the difference, whether of quantity or quality, amounts to Rs. 200 \$000. In all other cases the fine will be from 5 to 10 per cent., at the discretion of the Custom-house inspectors.

As regards implements for industries and other free articles, the 30 per cent. reduction is to be calculated on the clearing dues (*direitos de expediente*), the only impost which they pay.

With respect to the similar reduction provided for in favour of raw materials, such as dye-stuffs, chemicals, and other like articles for manufacturing purposes, the Minister says that such reduction cannot be put into force till congress shall have more clearly specified the articles.

The Minister of Finance also recognises the impossibility of executing the article of the law which provides that the duty on liqueurs and alcoholic beverages (Nos. 126 and 127 of the tariff), shall be paid by a stamp placed on the cork of each bottle.

ECUADOR.

Additional Export Dues.

A communication, dated 21st December last, has been received at the Foreign Office from Mr. G. Chambers, Her Majesty's Consul at Guayaquil, enclosing notice of a governmental decree imposing additional taxes on the exports of produce from Ecuador.

The decree, which was to come into effect on 13th December last, is to the effect that the Ecuadorian Government, with a view to meet increased expenditure, and to pay off the two loans contracted on July 15 and September 30 last, issued a decree on the 13th instant, creating extra dues upon certain articles of export.

The following is the tariff of such extra export dues:—

Sucre = 2s.		Sucres. Cts.	
Cotton, per 46 kilogs. (gross weight)	-	-	0 10
Coffee, per 46 kilogs. (gross weight)	-	-	1 50
Rubber, per 46 kilogs. (gross weight)	-	-	1 00
Hides, per 46 kilogs. (gross weight)	-	-	1 00
Paja Toquilla (hat straw) per 46 kilogs. (gross weight)	-	-	2 50
Split canes, per 100	-	-	1 00
Ordinary panama hats, per dozen	-	-	0 10
Fine panama hats, per dozen	-	-	0 40
Ivory nuts, per 92 kilogs.	-	-	0 05
Cocoa, per 46 kilogs. (gross weight)	-	-	0 50

From the moment this tariff is enforced, the recently imposed tax of 50 cents upon each quintal (100 lbs.) of cocoa and coffee exported ceases to have effect.

Four-fifths of the income derived from cocoa and coffee, under the present arrangement, is to be paid over each fortnight to the Bank of Ecuador, until the loan of sucres 200,000 (20,000*l.*) raised on July 15 last is liquidated, and the remaining fifth is to be handed to the Banco Commercial y Agrícola, in a like manner to pay off the loan contracted from that bank on September 30 last.

The foregoing additional Customs tariff will continue in force until July 30, 1896.

BOLIVIA.

Exemption of Wines of French origin.

According to "Le Journal des Tarifs et Traités de Commerce," a new municipal ordinance of La Paz exempts wine imported from France from all duty. Wines of French origin, but coming from countries other than France, will pay duty as usual. A consular invoice must accompany the wines to obtain the exemption required.

CHILE.

Bounties other than on Shipping and Navigation.

A despatch, dated 3rd December last, has been received at the Foreign Office from Mr. J. Hayes Sadler, Her Majesty's Chargé d'Affaires at Santiago, relative to bounties other than those on shipping and navigation, stating that, according to information received from the Chilean Government, agricultural industry in Chile enjoys entire freedom from duty on machinery and implements used in husbandry, and on live animals imported for breeding purposes, and that agricultural products are exempt from payment of all Customs impost on exportation.

With regard to the manufacture of sugar, machinery and implements which are imported for this purpose are also free from Customs duty, and by the Law of the 19th January 1893 the duties levied on impure sugars imported for the refineries established in the country have been reduced.

With regard to this reduction on raw sugar, the Customs authorities have made the following explanation:—

Sugar which up to the 31st January 1893 was customarily imported for the refineries of the country consisted as follows:—

		Valued per Metric Quintal.	
		Dols.	
Granulated sugar, 1st quality	- -	-	14.00
" " 2nd "	- -	-	10.50
Sugar Nos. 20, 19, 18, and 17 (Dutch)		-	16.20
" " 16, 15, 14, and 13	" -	-	13.50
" " 12, 11, 10, and 9	" -	-	10.50
" " 8 and other	- -	-	8.70

By the Law of the 6th January 1878, sugar paid an *ad valorem* duty of 35 per cent. This 35 per cent. was payable in the month of January 1893 in the following manner:—

75 per cent. in paper money, with a surcharge of 35 per cent.

25 per cent in gold at the rate of 6.31 dols. per 1l.

The Law of January 1893 converted the former *ad valorem* into specific duties, which came into force on the 1st February of that year.

As this Law converting the duties presented elements of uncertainty, it was found necessary to have recourse, in its application, to the superintendent of Customs at Valparaiso, whose interpretation was followed in the other Customs ports of the Republic. According to this interpretation of the Law of the 19th January 1893, sugar imported for the refineries of the country pays the following duties:—

1. Unrefined, and not white, sugar, that is raw sugar, pays 4.00 dols. duty per metric quintal. In this category are included all the classes mentioned above except the last.

2. Sugar, inferior to raw sugar, named "chaucaca" or "concrete," pays 3.20 dols. per metric quintal, and to this category belong the sugars mentioned in the last class in the list.

The above specific duties are paid in the following manner :—

- 50 per cent in currency, with a surcharge of 35 per cent. ;
- 25 per cent. in currency, with a surcharge at the rate on which the export duties on nitrate are paid ; and
- 25 per cent. in gold, at 6.31 dols. per 17.

URUGUAY.

New Duties and Customs Regulations.

A despatch, dated January 14 last, has been received at the Foreign Office from Mr. W. Baring, Her Majesty's Minister at Montevideo, enclosing an extract from the "Montevideo Times" on the subject of the new taxation, the bill for which has received the sanction of the President.

The substance of the new taxes is as follows :—

1. *Tobacco Duties.*—Tobacco, cigars, and cigarettes are subject to the following specific duties, including the packages or wrappers: Havana tobacco in leaf, 30 cents per kilog.; ditto, cut, 70 cents; black tobacco in twist, 30 cents; ditto, cut, 60 cents; tobacco from other parts, excepting Rio Grande and Paraguay, 30 cents; ditto, cut, 60 cents; tobacco from Paraguay and Rio Grande, 15 cents; Havana leaf cigars, \$3.00; cigars, not from Havana, Italian, Swiss, and others, \$1.00; all cigarettes \$2.50.

2. *New Import Duties.*—The following articles shall pay import duties according to the following tariff: Ploughs and parts thereof, 5 per cent.; wire for fencing and vines, up to No. 14, 5 per cent.; glass bottles, 15 per cent.; unmounted boxes, 15 per cent.; undressed hemp and jute, 5 per cent.; dynamite, 20 per cent.; empty flasks, 15 per cent.; binding thread, 5 per cent.; machinery for industrial establishments, and parts thereof, 5 per cent.; agricultural machinery and parts thereof, 5 per cent.; potash and potassic chlorate for industries, 5 per cent.; wicking, 10 per cent.; posts, half-posts, and standards for fencing, 10 per cent.; common and double soda, 5 per cent.; all vegetable oils imported shall pay a uniform and specific duty of 10 cents per kilog. of net weight.

4. *Additional General Duty.*—For a term of two years, all importers shall pay an additional extraordinary license (*patente*) of $2\frac{1}{2}$ per cent. on their importations. By importer shall be understood any one who introduces merchandise or any class of articles into the country. This tax shall be collected fortnightly by the Direction-General of Direct Taxes, or by the Departmental Revenue Officers, who shall be supplied by the Customs authorities with a statement of the articles imported and the respective importers.

5. *Internal Tobacco Tax.*—From a date to be fixed by the Executive, all tobacco, cigars, or cigarettes imported from abroad or manufactured in Uruguay from native or foreign tobacco, shall be subject to an internal tax on consumption as follows:—Cigarettes in packets, 1 cent for every 10 or fraction of 10; Havana cigars imported or made here of Havana leaf, 1 dol. per 100, and so in proportion for 50, 25, 10, 5, or 1; leaf cigars, not Havana, Italian, Swiss, and others, whether imported or made here, 40 cents per kilog., and so in proportion for fractions of 500, 250, 100, 50, and 10 grammes (including the wrapper); these fractions may be modified by the Executive if convenient; manufactured tobacco of all classes, intended for consumption, 40 cents per kilog.; leaf tobacco, cut in screws, &c., may only be sold for consumption in packets or boxes containing, 50, 100, 150, 500 grammes, 1, 5, and 10 kilogs.

Mr. Baring, in his despatch with reference to the above modifications, points out that the import duties on tobacco are generally lowered, but the difference is made up by the internal taxes which are to be levied. The result will be slightly to lower the duty on cigars and to increase that on tobacco. The clause of these regulations which will most affect importers of British goods is that which lays down that tobacco may only be sold in packets or boxes containing 50, 100, 150, and 500 grammes, or 1, 5, and 10 kilogs. Pipe tobacco from Great Britain and the United States is generally imported in tins or packets containing one pound or a fraction of a pound, and the sale of this article will consequently become difficult.

With regard to the new import duties on other articles, it is to be noted that sheep dip has escaped taxation, though some time ago it was reported that a duty would be imposed on it.

Wire for fencing, agricultural implements, and other articles, which, on account of their great utility, had hitherto been admitted duty free, are now taxed.

Besides these duties, an additional $2\frac{1}{2}$ per cent. is imposed on all imports in the form of a license (*patente*), and is to be charged on the existing tariff of valuations.

The additional tax is styled "license" and not Custom duty, and consequently 45 per cent. of it will not have to be set apart for the service of the foreign debt.

BRITISH INDIA.

Tariff Changes.

In exercise of the power conferred by section 23 of the Sea Customs Act (VIII. of 1878), the Governor-General in Council is pleased to exempt all spirit imported into British India, which has been rendered effectually and permanently unfit for human

consumption, from the whole of the Customs duty in excess of 5 per cent. *ad valorem*, leviable thereon on importation into British India.

ST. VINCENT.

Tariff Modifications.

With reference to the Customs tariff of St. Vincent, published on page 558 of the "Board of Trade Journal" for May 1895, a communication, dated 13th December 1895, has been received from the Administrator of the island of St. Vincent, enclosing copy of the "Customs Duties Ordinance, 1895 (ii.)," by which certain additional duties are imposed.

By this ordinance, the duty on flour, wheaten, will be 4*s.* per barrel, and on tobacco, unmanufactured, 9*d.* per lb., instead of 3*s.* per barrel and 6*d.* per lb. respectively.

In addition to the above modifications, Article 5 provides that all articles enumerated in the table (as given in the May No. of the "Journal"), and also the two above-named articles (*viz.*, flour, wheaten, and tobacco, unmanufactured) shall, from the 27th November 1895, the day on which this ordinance was to come into force, until the 31st December 1896, inclusive, pay a *further* duty of one-tenth of the duties and rates so set forth respectively.

GRENADA.

Tariff Modifications.

A communication has been received from the Colonial's Secretary Office, Grenada, dated 8th January 1896, enclosing copies of Ordinances modifying the Customs Tariff of Grenada.

Under Ordinance No. 16 of 1895 the following import tariff came into force from the 1st January last.

Import Tariff.

Articles.	Rate of Duty.
Bread - - - - -	Per 100 lbs. <i>s.</i> <i>d.</i> 2 0
Butter - - - - -	" pound 0 1
Butterine, oleomargarine, and similar compounds	" " 0 0½
Candles, except tallow	" " 0 2
Candles, tallow - - - - -	" 100 lbs. 5 0
Cocoa - - - - -	" " 10 0
Coffee - - - - -	" pound 0 2

Articles.						Rate of Duty.	
						<i>s.</i>	<i>d.</i>
Fish, dried or preserved	-	-	-	-	-	Per 100 lbs.	1 0
Salmon, pickled or preserved	-	-	-	-	-	" pound	0 1
Mackerel and other descriptions	-	-	-	-	-	" barrel	2 0
Flour :							
Wheaten	-	-	-	-	-	" "	4 0
Other kinds	-	-	-	-	-	" "	2 0
Grain :							
Corn, oats, maize, beans, and peas	-	-	-	-	-	" bushel	0 6
Barley	-	-	-	-	-	" 100 lbs.	0 6
Gunpowder	-	-	-	-	-	" pound	0 6
Lard	-	-	-	-	-	" "	0 1
Live stock :							
Asses	-	-	-	-	-	Each	6 0
Calves	-	-	-	-	-	"	6 0
Cows, bulls, and oxen	-	-	-	-	-	"	12 0
Dogs	-	-	-	-	-	"	10 0
Goats	-	-	-	-	-	"	1 0
Horses	-	-	-	-	-	"	25 0
Mules	-	-	-	-	-	"	12 0
Sheep	-	-	-	-	-	"	2 0
Swine and hogs	-	-	-	-	-	"	2 0
Malt, cider, and perry	-	-	-	-	-	Per gallon	0 6
Do., in bottles	-	-	-	-	-	" doz. quarts	1 0
Matches	-	-	-	-	-	" gross of large boxes	2 0
Do.	-	-	-	-	-		
						" gross of small boxes	1 0
Meal : Corn, oil, and oats	-	-	-	-	-	Per 100 lbs.	2 0
Meat, dried or preserved	-	-	-	-	-	" "	5 0
Molasses	-	-	-	-	-	" gallon	0 1
Oils :							
Olive, sperm, lard, and all other kinds used for cooking	-	-	-	-	-	" "	1 0
Kerosine and paraffine	-	-	-	-	-	" "	0 2½
All other kinds	-	-	-	-	-	" "	0 9
Opium	-	-	-	-	-	" pound	10 0
Rice	-	-	-	-	-	" 100 lbs.	2 0
Salt, coarse	-	-	-	-	-	" barrel	0 2
Soap	-	-	-	-	-	" 100 lbs.	2 0
Spirits :						Per proof gallon ac- cording to Sykes' hy- drometer, and so on in propor- tion for each degree of strength greater or less than the strength of proof	5 0
Gin or rum	-	-	-	-	-		
Brandy or whiskey or any other kind of imported spirits	-	-	-	-	-	Do., do.	6 0
or strong waters, except perfumed or methylated spirits	-	-	-	-	-	Per liquid gall.	5 0
Sweetened spirits, bitters, liqueurs, and cordials	-	-	-	-	-	Per 100 lbs.	5 0
Sugar, refined and crystals	-	-	-	-	-	" "	4 2
" Muscovado	-	-	-	-	-	" "	4 0
Tallow	-	-	-	-	-	" pound	0 6
Tea	-	-	-	-	-	" "	0 9
Tobacco	-	-	-	-	-	" "	1 0
Long cigars	-	-	-	-	-	" "	

Articles.	Rate of Duty.
Tobacco— <i>cont.</i>	
Other cigars - - - - -	Per pound s. d. 5 0
Cigarettes - - - - -	" 25 % <i>ad val.</i> 4 0
Wines, except medicated wine - - - - -	
Wood :	
Pitch pine, white, yellow spruce, and all other descriptions of lumber except spars - - - - -	{ Per 1,000 ft. } 10 0
Shingles - - - - -	{ superficial } 3 0

And after the above rates for any greater or less quantity of every such article.

Articles of any sort for which a specific rate of duty is not prescribed, and not contained in the list of exemptions from duty given in this Ordinance, shall pay a duty of 7*l.* 10*s.* per cent. *ad valorem*.

Table of Exemptions.

Coin, bullion, diamonds, fresh fruit and vegetables, manures straw, ice, fresh fish, coal, printed books, newspapers, and periodicals, plants of all kinds, works of art not imported for sale, tombstones and grave railings, old furniture not intended for sale, the personal baggage and apparel, professional apparatus, tools or implements of trade of passengers arriving, any articles for the use of Her Majesty's service, or for the use of the Government, or for the use of the Governor, or for the use of any place of worship, articles for the official use of any foreign consulate (provided that a similar privilege is accorded by such foreign country to the British consulate therein); fire-extinguishing apparatus, machinery and implements for waterworks, improved stills and other machinery or appliances considered by the Governor in Council to be useful for the development of local manufactures or products, packages in which goods are imported (except new trunks, canisters, and puncheons); racehorses, and horses and other animals and appliances imported by circus companies, if exported within six months; patterns and samples of no intrinsic value, surgical implements and appliances, poultry and eggs, raw cotton and cotton seed.

Table of Prohibitions.

Indecent or obscene prints, paintings, books, cards, lithographic or other engravings, or other indecent or obscene articles.

Under Ordinance No. 17 of 1895 there will be levied during the year 1896 an additional import duty of one-tenth of all the duties set forth above.

Export Duties.

Under Ordinance No. 19 of 1895 the levying of all export duties is suspended during the year 1896.

SIERRA LEONE.

Customs Amendments.

The following ordinance to amend the law relating to Customs duties upon spirits entering Sierra Leone was passed in the Legislative Council of that colony on the 27th December 1895:—

“Whereas it is desirable to amend the law relating to Customs duties upon spirits:

“Be it therefore enacted by the Governor of the colony of Sierra Leone, with the advice and consent of the Legislative Council thereof, as follows:—

“I. ‘The Customs Duties Amendment Ordinance, 1893’ is hereby repealed.

“II. In lieu of the duties hitherto payable upon spirits, there shall be raised, levied, and paid unto Her Majesty, Her heirs, and successors the following duties:—

“A. Spirits and strong waters of which the degree of strength can be ascertained by Sykes’s hydrometer:—

“(1.) For every Imperial gallon of less strength than the strength of proof by such hydrometer, and so in proportion for any greater or less quantity than an Imperial gallon of such strength - - - - Per Imperial gallon, 3s.

“(2.) For every gallon of proof strength by such hydrometer, and so in proportion for any greater strength of proof and for any greater or less quantity than a gallon

Per proof gallon, 3s.

“B. Spirits being sweetened or mixed so that the degree of strength cannot be ascertained, for every Imperial gallon of any greater or less quantity than an Imperial gallon

Per Imperial gallon, 3s.

“Spirits or strong waters such as naphtha in its crude state, methylated spirits, and perfumed waters, which are totally unfit for use as a potable spirit

An *ad valorem* duty of $7\frac{1}{2}$ per cent

“Spirits unenumerated - - - - Per Imperial gallon, 3s.

“III. It shall be lawful for the Governor, and he is hereby empowered to exempt, at his discretion, from the payment of Customs duties, official goods imported into the colony for the use of Consulates.

“IV. This ordinance shall be read and construed with ‘The Customs Ordinance,’ 1874, and the ordinances amending the same, and may be cited for all purposes as ‘The Customs Duties Amendment Ordinance, 1895,’ and shall not come into force until the first day of January 1896.”

NEW SOUTH WALES.

Modifications in Customs Tariff.

With reference to the notice of modifications in the Customs tariff of New South Wales, which appeared on page 64 of the "Board of Trade Journal" for January 1896, the date on which the Bill to amend the said Customs tariff was to come into force was January 1, 1896.

XVI.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

CLOSING OF NAVIGATION AT ODESSA.

A despatch has been received at the Foreign Office from Her Majesty's Consul-General at Odessa, reporting that the navigation at that port closed on the 27th January last.

THE SILK INDUSTRY OF THE CAUCASUS.

In a recent report to the Foreign Office, Mr. P. Stevens, Her Majesty's Consul at Batoum, states that the year 1895 was not a favourable one for those engaged in the silkworm industry, especially in the government of Kutais. About 40 per cent, and in some districts as much as 70 per cent. of the silkworms died in the fourth and fifth stages of their growth, so that to a great extent the labour of the silkworm breeders has been wasted. The principal cause of this unfortunate result is to be found in the fact that a very large proportion of the grains, or silkworms' eggs, sold to the peasantry, were imported chiefly from Turkey, and were of inferior quality, containing the germs of disease, which ultimately proved fatal to the worms. With a view to remedying this unsatisfactory state of affairs, the central silkworm establishment at Tiflis has taken steps to bring to the notice of all those engaged in silkworm raising, that it will henceforth supply at moderate prices what is known as cellular grain, *i.e.*, silkworms' eggs that have been microscopically examined for disease, and each separate grain of which is guaranteed to be free from disease.

The heavy mortality amongst the silkworms, referred to above, has resulted in a great scarcity of cocoons of good quality, and they have consequently risen considerably in value, as compared with prices ruling last year. Cocoons are now selling at 1*l.* 16*s.* to 1*l.* 18*s.* per pound of 36 lbs. English, whereas last year the price was 16*s.* to 18*s.* for the same quantity.

At the Caucasian silkworm establishment at Tiflis 114 persons were instructed in silk culture during the first seven months of 1895.

The practical advice and scientific instruction which has hitherto been afforded only at the sericultural establishment at Tiflis was this year extended to other districts, and specialists were despatched to the government of Kutais, to Kahetia, and the district of Zakatal to initiate the peasantry into the most approved systems of raising the silkworm. In the districts of Nuha, Aresh, Djevanshir, Shusha, Djebrail, and Geokchai, practical demonstrations were given of the methods of rearing and

feeding silkworms in accordance with scientific principles, and the cocoons so obtained were reserved for the production of eggs.

In addition to the increased sum provided in the estimates for next year for the maintenance of the sericultural establishments, a further sum of 15,000 r. is to be allotted for the organisation of travelling laboratories, for issuing books on the culture of the mulberry tree and the rearing of the silkworm, and for instructing the masters of village and lower agricultural schools in the art of rearing the silkworm, for which purpose they will be sent to Tiflis.—(*Foreign Office Annual Series*, No. 1654).

RESUMPTION OF TRAFFIC ON THE TRANS-CAUCASIAN RAILWAYS.

A report, dated January 22 last, has been received at the Foreign Office, from Mr. P. Stevens, Her Majesty's Consul at Batoum, stating that through communication between Baku and Batoum, both for passengers and goods, has been resumed on the Trans-Caucasian Railway.

A great deal of work, however, still remains to be executed before the line is placed in a thorough state of efficiency and is able to bear the amount of traffic which used to go over it previous to the occurrence of the breakdown.

The temporary wooden bridges that have been hurriedly constructed will have to be replaced by iron ones, and very extensive masonry work and earthworks still remain to be carried out between the railway stations of Molita and Adjamety; it is nevertheless worthy of note that quite a number of petroleum trains, which were on the line at the time of the interruption, have already arrived at Batoum.

Two pipe lines of small dimensions have also been laid between the stations of Molita and Bielogori; one by the Union of Baku petroleum refiners, and the other by the outsiders.

CONTRACTS FOR STEEL RAILS IN NORWAY.

A communication, dated 21st January last, has been received at the Foreign Office from Mr. A. S. MacGregor, Her Majesty's Acting Consul-General at Christiania, stating that according to a local Norwegian journal, the North-Eastern Steel Company, Ltd., Middlesbrough, through its representative at Christiania, has just undertaken (secured) the ordinary annual delivery of steel rails to the Norwegian State Railways; quantity about 2,800 tons.

EXPORTS OF NORWEGIAN FROZEN FISH TO GERMANY.

A despatch, dated January 29 last, has been received at the Foreign Office, from Mr. A. Stewart MacGregor, Her Majesty's Acting Consul-General for Christiania, containing a copy of the following notice which appeared in the "Aftenpost" newspaper for the 26th ultimo.

*Frozen Fish.**Norwegian Export for the German Army.*

During the past year considerable quantities of frozen fish, chiefly haddock, have been sent regularly from Hammerfest to Hamburg, where they have been sold at very cheap rates.

On account of this the German Minister of War has caused experiments to be made with a view to ascertaining the nutritive value of Norwegian fish as an article of food for the regiments of the Guard at Berlin.

The men have been fed once a week on haddock and potatoes, and while the soldiers themselves are reported to have liked this description of food, the doctors have watched the experiments and have reported favourably upon the results of their investigations.

In consequence of this, the Minister of War has therefore given orders that in all the garrisons in North Germany, during the winter season, Norwegian haddock shall be eaten for dinner two or three times a month.

GERMAN EXPORTS IN 1895.

A communication, dated February 5, 1896, has been received at the Foreign Office from Mr. C. J. Dundas, Her Majesty's Consul-General at Hamburg, reporting that, according to a paragraph which has recently appeared in the Hamburg newspapers on German exportation in 1895, the value of the whole exportation of German industries last year exceeded that of 1894 by about 18 millions of pounds.

This statement must, however, be accepted with some reserve, pending the appearance of the official statistics to confirm it, but there appears to be no reason to doubt that German exports will show a very considerable increase, even if it does not attain to the figures above stated.

The articles which are said to show the largest increase are, woollen goods, chemicals and paints, iron and ironware, silk goods, cotton goods, and leather and leather goods.

BILL FOR TAXING FOREIGN COMPANIES IN BELGIUM

A despatch, dated February 1 last, has been received at the Foreign Office, from Sir F. Plunkett, Her Majesty's Minister at Brussels, enclosing text of Bill for subjecting foreign companies doing business in Belgium to a patent tax.

The report of this Bill, which has been put aside for the moment, draws attention to the fact that Belgian limited liability companies and "Sociétés en Commandite" are subjected to a patent charge of 2 per cent. upon the amount of their profits.

Previously to 1888, the Belgian Government held that foreign companies doing business in this country were liable to the same

charge; but in that year an adverse decision of the Court of Cassation put an end to this claim.

Hence foreign companies in Belgium—except insurance companies, which are subject to a special law of March 24, 1873—are in a privileged position as compared with those formed in the country.

For instance, while the “Banque Nationale” pays an annual patent tax of 10,000*l.*, the “Société Générale” one of 7,000*l.*, and “the Société d’Anvers-Rotterdam” (the “Grand Central Belge” Railway Company) one of 4,280*l.*; the “Crédit Lyonnais” escapes with a payment of 45*l.* 11*s.* 8*d.*, and some other foreign companies with even smaller ones.

After alluding to the attempts respectively made by the Municipality of Liège and the Provincial Councils of Liège and Brabant to correct the disadvantage in which the Belgian companies are placed under the existing legislation, the report goes on to urge upon the Government the necessity for immediate action in the matter, so as to enable the tax to be levied upon foreign companies for their current financial year.

The bill places all “Sociétés Étrangères par Actions” which have a seat, branch, agency, establishment, or office of any kind in Belgium, upon the same footing, as regards patent dues, with Belgian companies, whether limited, or “en Commandite par Actions,” while only assessing the tax upon the amount of the profits derived from the business which they have transacted in Belgium.

The expression “Société Étrangères par Actions” is purposely used, in order to include certain companies, especially those constituted under British law, which might otherwise evade the tax, as neither falling under the head of “Sociétés Anonymes” nor under that of “Sociétés en Commandite par Actions.”

The Bill further requires every foreign company above referred to to appoint, subject to the approval of the Government, a responsible and solvent representative, residing in Belgium, who shall be liable for the payment of patent dues and of any fines which the company may have incurred, and responsible for due compliance with the provisions of Article 3 of the Law of January 22, 1849, and Article 12 of the Law of July 5, 1871.

NAVIGATION OF THE DANUBE.

A despatch, dated 13th December last, has been received at the Foreign Office from Lieutenant-Colonel Trotter, British Plenipotentiary to the European Commission of the Danube, reporting on the autumn session of the Commission.

Three separate proposals were before the Commission with respect to the increase of the tariff dues, but, after a somewhat lengthened discussion, it was decided that the rates should be maintained *in statu quo*. The practical result of the vote is that the tariff remains as it is for a further period of four years.

It is hoped that, with an average harvest, the returns for 1896 will be fully up to those of 1895.

The Roumanian Government has resolved to construct at its own expense a floating dock at Sulina. This will save the Commission the annual subvention of 20,000 francs which it had decided to contribute towards the construction of a dock.

THE FLORIDA SPONGE INDUSTRY.

A despatch, dated 31st December last, has been received at the Foreign Office from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, enclosing a report of the British Vice-Consul at Key West, Florida, on the Sponge Industry of the Florida Reef.

According to this report, the sponge industry on the Florida Reefs and surroundings has been, and is now, very successful.

The best quality of sponges is called "sheep's wool" or Rock Island. These sponges are caught in beds from Anclot Keys to St. Marks, Florida (off the coast of Apalachicola), with hooks on poles from 10 to 40 feet long. They are found in beds at the various places without any artificial protection.

Some six or seven years ago strangers to Florida endeavoured to catch sponges by diving, but it turned out a failure. Sponges are obtained by private firms or by individuals owning boats from 5 to 45 tons.

There are 80 to 100 boats engaged in this industry, with an average crew of 10 men. The value of the catch is about 200,000 dols. per annum.

There is another kind, called Key sponges, found on the Florida Reefs from Key West to Cape Florida, on the east coast, extending about 175 miles. These sponges are of an inferior quality, and are taken in small boats, at a depth of 4 to 12 feet. The value is estimated at about 36,000 dols. per annum.

There exists no law controlling the time at which sponges may be obtained, but there is a State law passed in 1892 which provides that—

"Whoever dredges or uses a dredge for the gathering of sponge in or upon the waters of the Gulf of Mexico within three marine leagues of the shore, or upon any of the grounds known as sponging ground along the coast-line of Florida from Pensacola to Cape Florida, or whoever gathers sponge less than 4 inches in diameter, shall be punished for each offence by fine not exceeding 500 dols., and by confiscation of the boat and machinery; and in default of the payment of said fine the offender shall be imprisoned not exceeding one year."

This law further enacts that—

"Whoever gathers or catches sponge in and upon any of the grounds known as sponging grounds, along the coast of Florida, by diving, either with or without a diving suit or armour, shall be punished by fine not exceeding 2,000 dols., and by confiscation of

all diving suits or armour and boats and vessels used in such unlawful gathering of sponge; and in default of payment of said fine, the offender shall be imprisoned not exceeding one year."

COMMERCE PASSING ST. MARY'S FALLS (U.S.A.)

A despatch, dated 29th January last, has been received from the Foreign Office, enclosing copy of a letter from the United States Secretary of War to the House of Representatives, giving information with regard to the commerce passing St. Mary's Falls during the season of 1895.

According to the above letter, the season of 1895 opened April 25 and closed December 11, a duration of 231 days, or three days less than in 1894. The Canadian Canal was opened September 9 and closed December 4, a period of 87 days, relieving the American lock of a very considerable portion of the work during the latter part of the season.

Excluding the traffic through the Canadian Canal, the season's business was the largest known in the history of the canal, and exceeded that of 1894 by 16 per cent. in number of vessels passed, 9 per cent. in lockages, 23 per cent. in registered tonnage, 10 per cent. in freight tonnage, and 13 per cent. in passengers. On July 17, 1895, the lock passed 119 vessels, which exceeds the record of any single day since the lock was opened to commerce.

THE FINANCES OF MEXICO.

A despatch, dated December 30 last, has been received at the Foreign Office from Mr. H. N. Dering, Her Majesty's Minister at Mexico, stating that Mr. Limantour, the Minister of Finance, submitted to the Chamber of Deputies on the 14th instant, a fiscal statement of the revenue and expenditure of the Federal Government during the fiscal year which terminated on the 30th of June last, showing the following result:—

		Dols.	£
Cash disbursements of all kinds	- -	43,967,318.40	4,763,523
Normal cash revenue	- - -	43,945,699.05	4,761,181
Deficit	- - - -	21,619.35	2,342

It should be stated, however, that even this small deficit is more apparent than real, for whilst every class of expenditure has been inserted in the account, including those to which the proceeds of loans were properly applicable, only the normal sources of revenue have been taken into consideration.

A further despatch, dated 31st December last, has been received at the Foreign Office, enclosing a memorandum on the Mexican budget estimates for the year 1896-97.

The memorandum states that, eliminating nominal appropriations, the estimates of revenue and expenditure for the year 1896-97, take the following form :—

		Dols.	£*
Estimated cash receipts for 1896-97	-	46,101,825.00	4,994,780
Estimated disbursements for 1896-97	-	46,915,162.52	4,985,391
Surplus	-	86,652.48	9,389

* Calculated at 26*d.* to dol.

Import duties are estimated to produce 18,500,000 dols. against 17,738,129 dols. collected last year. As, however, there has been an increase of nearly two millions in the revenue from these duties during the first five months of the current fiscal year, the estimate appears very moderate.

Stamp duties are estimated at 15,572,000 dols., or a little less than their actual yield last year.

Owing to the reduction by 40 per cent. of the tax on salaries, the revenue from that source is estimated at 700,000 dols. against a yield of 1,211,759 dols. last year.

Direct taxes. Portazgo dues, consumption dues and telegraphs are estimated at about the amounts realised last year. The post office is estimated to produce some 200,000 dols. less than last year, owing to a reduction in the rate of internal postage; whilst on the other hand an increased revenue of 150,000 dols. is expected from miscellaneous sources. It should be mentioned that if the projected abolition of the alcabalas (interstate duties) should be carried out as intended on the 1st of July next, some substitute will have to be provided for the portazgo and consumption dues.

The expenditure side of the account presents some features of interest of which perhaps the most important is the incorporation of the expenses of the Government of the federal district in the estimates for the Ministry of the Interior. This incorporation, however, depends to some extent on the acceptance by the several States of the scheme for the abolition of the alcabalas.

Increased appropriations have been made for several departments, and provision is made for a grant of 300,000 dols. to the municipality of Mexico for the improvement of the sanitary works. The completion of the works for the drainage of the Valley of Mexico relieves the budget of a charge of 1,000,000 dols., and enables the Finance Minister, while reducing the total expenditure of the Public Works Department, to make increased allowances for various works now in progress, and in particular for the harbour at Vera Cruz.

In calculating the amount required for the interest on the foreign debt, the Finance Minister estimates the exchange value of the Mexican dollar at only 24 pence. This is a very cautious estimate, for the dollar now stands at 26 pence, and 24 pence is the lowest rate that has ever been known.

Annexed is a statement of the various sources of revenue, with the amount they are estimated to produce :—

Taxes on Foreign Trade.

	Dols.
Import duties - - - -	18,500,000
Export duties - - - -	1,301,825
Two per cent. for port works - -	370,000
Special port duties - - -	47,000
Lighthouse, tonnage, and storage dues -	102,000
Shipping licenses - - -	1,000
Pilotage and dues to port masters -	18,000
Sanitary dues - - - -	60,000
Consumption dues in federal district -	188,000
Consular fees - - - -	181,000
Certificates issued by ministers or consuls under Article 24 of the Code of Com- merce - - - -	7,000
Storage dues at Vera Cruz - - -	28,000

Interior Taxes payable in the Federation.

Stamp taxes - - - -	15,572,000
Tax on salaries - - - -	700,000
Tax on gold and silver - - -	2,540,000
Trade marks and patents - - -	16,000
Direct taxes in federal districts and terri- tories - - - -	1,500,000
Portazgo dues in federal districts and territories - - - -	1,630,000
Taxes on successions and donations -	150,000
Other minor taxes - - - -	10,000

Public Services.

Post Office - - - -	1,140,000
Telegraphs - - - -	550,000
Proceeds of Tehuantepec Railway -	120,000
Lottery and miscellaneous - - -	1,320,000
	46,101,825

PRODUCTION OF VANILLA IN MEXICO.

In a despatch to the Foreign Office, dated November 5 last, Mr. H. N. Dering, Her Majesty's Minister at Mexico, encloses extracts from reports on various subjects issued by the Mexican Information Bureau.

With reference to the yield of the vanilla plant, a report states that 100 vines at three years of age yield, on an average, without any cultivation or artificial fertilisation, from 50 to 65 pods; at four years, 165 to 225 pods; at five years, 325 to 500 pods; and thenceforward the production diminishes annually up to the tenth year, when it is necessary to destroy the vines and replace them with new ones.

Some lands produce more, and some less, but on the average, taking one year with the other, it may be reckoned that from the third to the 10th year each vine will yield two pods a year. Between the fourth and sixth years the plantation is in its prime. There are vanilla vines which, in their prime, give as high as 85 pods and more, but these are isolated cases. In a good crop under thorough cultivation many of the vines are seen with several fruit-bearing branches, carrying as many as 14 or 16 pods.

Vanilla is generally bought and sold in Mexico by the 1,000 (millar). Green vanilla sells in Oaxaca, Michoacan, and Misantra from 5 to 8 dols. per 1,000. Dried vanilla sells in Michoacan from 40 to 50 dols. per 1,000, and in Misantra, Papantla, and Tuxpan from 50 to 75 dols. Statistics show that there is more vanilla exported from the port of Tuxpan than from any other port in the world, thus establishing the fact without a doubt that the Tuxpan Valley is the natural home of this valuable orchid. It is worth in New York city at present from 8 to 10 dols. gold per 1 lb., which, as will easily be seen, leaves quite a handsome margin of profit, even after all expenses of curing, packing, and shipping have been deducted. As much as 300 per cent. profit has been made on vanilla in good years. — (*Foreign Office Miscellaneous Series, No. 385.*)

CONTRACT FOR BUILDING A RAILWAY IN NICARAGUA.

A communication, dated 1st December last, has been received at the Foreign Office from Mr. Walter J. Chambers, Her Majesty's Acting Consul at Granada, in the Republic of Nicaragua, reporting that Mr. F. K. Morris, of German nationality, residing and doing a commercial business in the city of Managua, has signed a contract with the Nicaraguan Government to build a railway from the town of Masaya, through the villages of San Juan, Niquinohomo, Masatepe, San Marcos, Jinotepe, and Diriamba, a distance of about 26 miles.

Mr. Morris is to provide a good telephonic communication all along the route, and the necessary stations (built of wood with iron roofing and painted) at the different towns. The gauge of railway will be 2 ft. 6 ins. (English measure) maximum grade $2\frac{1}{2}$ per cent., and minimum for curves 190 ft. or about 30° . All bridges are to be made of iron, painted, and firmly placed on cement columns; they will also have to be strong enough to resist the weight of two locomotives each of about 20 tons.

The rails are to be of steel, weighing 30 lbs. to the yard, placed at a distance of 2 ft. 6 ins. on 6 ft. sleepers of hard durable wood, to which they will be spiked with 4 in. spikes on either side of rail.

A fence of barbed wire will be placed all along the route of line on both sides, consisting of four lines of barbed wire nailed to posts $2\frac{1}{2}$ yards distant and $2\frac{1}{2}$ yards high (English measure).

In the town of Diriamba, Mr. F. K. Morris will have to erect a machine shop with all necessities such as turning gear, circular saw, sledge hammers, blacksmith's forge, &c., &c., with 20 horse-power engine and boiler for motor, also an iron tank standing on iron pillars and sufficiently large to hold 6,000 gallons of water, and an artesian well with pump and machinery.

In Masaya, besides the station there will be built for storing firewood, a shed, 50 ft. by 21 ft. with iron roofing.

The work will be commenced within one month after signing of contract, and line finished and open to public within two years of same date, in payment whereof Mr. F. K. Morris will receive the sum of 620,000 silver dollars (equal to 6,200*l.*) which will be handed to him in instalments of 18,000 silver dollars at the end of every month, commencing from 31st October 1895.

THE TEA TRADE OF WENCHOW.

A despatch, dated January 21, has been received from the Foreign Office, enclosing copy of a report on the Wenchow tea trade by Mr. Fox, Acting Consul at Wenchow, as follows:—

“The export of tea from Wenchow, hitherto insignificant, has increased so rapidly during the last three years that a few notes on the conduct of the trade and its chances of development should be of interest to British merchants.

For many years after the opening of the port to foreign trade (in 1877) the tea grown in this district was sent, in an unprepared state, either overland to Foochow, or by junk and steamer to Shanghai and Hankow. At these places it was mixed with other teas, losing both its name and distinctive flavour. The trade was entirely in the hands of Chinese merchants, who, with limited capital, bought only small quantities at a time, being unwilling to pay out either the large sums for the cost of conveyance to Shanghai or the coast trade duty levied there. This latter charge was in the nature of an advance, the amount being refunded to the merchant on reshipment abroad. However, an arrangement was made in 1891 by which a bond was deposited by the owner of the steamer carrying the tea in lieu of the duty referred to above.

From this time forth the export of tea to Shanghai gradually increased. In 1893 a tea firing “hong” was established, and green teas (Gunpowder, Imperial, and Young Hyson) appear for the first time in the Customs' Export Returns. There are now (1895) five tea-firing establishments in Wenchow, and the export

of tea (black, green, and unfired) has exactly doubled since 1893, the figures being 2,668,933 lbs. for 1895 as against 1,381,600 lbs. for 1893, a sufficiently remarkable result when compared with the stagnant state of the trade in old established tea ports, like Foochow, Amoy, and Canton. The following table will show the increase under the different headings since 1891.

Year.	Quantity.			
	Black (Congou).	Green and other Kinds.	Unfired.	Total.
	Lbs.	Lbs.	Lbs.	Lbs.
1891	313,733	10,667	582,533	906,933
1892	401,200	4,133	620,133	1,025,466
1893	530,133	—	851,467	1,381,600
1894	412,800	35,467	1,201,867	1,650,134
1895	476,534	679,210	1,513,120	2,668,933

The prices in Wenchow this year were good, and some of the merchants have made large profit. Black tea averaged from 14 to 16 dols. (best Congou 25 dols.) per picul; green, from 13 to 15 dols. (best gunpowder 50 dols. per picul).

Tea can be prepared at less cost in Wenchow than many other places owing to the cheapness of labour and the abundance of charcoal. Freight to Shanghai, owing to the monopoly of a Chinese steamship company, is at present excessive, but the competition which must inevitably come should the trade of the port continue to increase, will effectually remedy this evil.

The time has certainly arrived for at least one British firm to establish a branch in Wenchow and buy teas locally. The expenses connected with coolie and cargo boat hire, godown, and packing charges are much lighter than in Shanghai and other places, the tea-growing districts are close at hand and easy of access. The Chinese dealers will be only too glad to sell their tea on the spot, instead of waiting for their profits till it is sold in foreign markets. The quality of the tea is good and likely to improve. The superior kinds have already been favourably commented on by connoisseurs. There is, therefore, a good opening at the present time for foreign enterprise in Wenchow, and it is to be hoped that British merchants will be the first to take advantage of the situation.

PROPOSED FOREIGN SETTLEMENT AT SOOCHOW.

A despatch, dated November 15 last, has been received at the Foreign Office from Mr. W. N. Beauclerk, Her Majesty's Secretary of Legation, at Peking, enclosing copy of a report from Mr. Jamieson, Her Majesty's Consul at Shanghai on a visit made by him to Soochow with the view to ascertain the position most

suitable for a settlement should it be deemed advisable to apply for any special site. Mr. Jamieson states that the Grand Canal which runs from Hangchow to Chinkiang skirts the city wall on two sides, the south and west, forming part of a continuous moat which surrounds the city on all sides. Access from Shanghai is obtained by two canals, the more northerly of which enters the moat near the east gate, while the other strikes the Grand Canal at a point some three or four miles south of the city wall.

This latter route has the deepest water and is the one usually followed by the steam launches and towboats which now ply regularly between Shanghai and Soochow.

The only part of the land outside the city wall which is at present built upon is a strip along the canal on the west side.

This portion of the waterway is usually crowded with boats and may be considered as the centre of the business quarter. All the rest of the ground adjoining the moat is clear of houses, being either waste or under cultivation. So far as concerns contiguity to the City and amenity of situation any part of this would be suitable for a settlement. The greatest depth of water, however, is to be found on the south and west sides, that is to say, along the line of the Grand canal, and for this reason it would seem that the best site for a settlement would be found in this locality.

A Commission of six officials, most of whom held the titular rank of Taotai, had been appointed to confer with Mr. Chinda, the Japanese Consul-General in regard to a settlement, but he complained of the delays occasioned by the necessity of referring every point as it arose to the viceroy at Nanking. He had been led to expect that he should find someone with full powers to settle with him on the spot. Instead of that, none of them seemed to have any discretionary power at all, and their only instructions appeared to be to keep the settlement as far off as possible. Some 10 days had already been spent and no progress whatever had been made.

Their first proposal was to offer a site on the Shanghai Canal near the point where it enters the Hang-chow canal, which is some four miles below the city wall, and quite in the open country. This was refused and a place asked for somewhere near the city moat. This appeared to be outside the scope of their instructions; nothing more could be done without reference to the viceroy and so the matter stood.

Mr. Hipplesley, Commissioner of Customs, who had then been there for some eight days, confirmed Mr. Chinda's statement, but added that he understood that the Viceroy had given way so far as to offer a place on the Grand Canal considerably nearer to the city than the first-mentioned spot.

Mr. Lo, a member of the Commission, stated that British merchants would be at liberty to claim the advantages of the Japanese treaty for purposes of trade if they desired it, and that it might be necessary to apply for a site for them to reside in.

The Shanghai merchants seem to be taking no interest in the doings of the Japanese Consul, or the prospective opening of the

new ports, and it is doubtful whether English subjects would think it worth while to establish themselves at either Soochow or Hangchow under present circumstances. It does not, therefore, seem at all pressing that immediate steps should be taken for the allotment of a site for a settlement. If Mr. Chinda succeeds, as he probably will, in getting a place on the south or west side of the city it would be easy enough to get a like grant of an adjoining piece, or probably, it could be arranged to have one site for a general foreign settlement.

The question of rules for the navigation of the inland waters and the fixing of the rate of duty to be levied on goods carried between Shanghai and these two new ports is one of greater importance even than the one of a site. Probably these matters will be settled by the Commercial Treaty now being discussed by the Japanese Minister at Peking. So far they have not come on for discussion at Shanghai.

THE EAST COAST ROUTE TO MASHONALAND

In a recent report to the Foreign Office, Mr. A. Carnegie Ross, Her Majesty's Consul at Beira, gives an account of the East Coast route to Mashonaland, starting from Beira in Portuguese territory, through Chimoio and Umtali to Salisbury.

Mr. Ross divides the journey into four sections, according to the means of conveyance. The first being by river steamer from Beira to Fontesvilla, a trip of from four to five hours, which will be rendered unnecessary when the railway from Fontesvilla is completed to Beira. The second section is by rail from Fontesvilla to Chimoio, a distance of 118 miles, generally taking nearly two days, the train stopping overnight at an 80 mile station, but running through each way once a week. From Chimoio to Umtali (75 miles) the journey is performed in a postcart drawn by oxen or mules driven by natives at the rate of about two miles an hour. From Umtali to Salisbury (150 miles) the mode of conveyance is also a postcart generally drawn by 10 mules.

Umtali (in Mashonaland) strikes one very favourably after the journey and compared with the camps at Fontesvilla and Chimoio. The houses (some 20 in number) are built of brick roofed with iron. They are scattered over the township, which is about one mile long by half a mile broad, situated in a broad valley on a gentle slope facing eastwards. A few of the residents still live in grass huts, but these are rapidly giving place to brick buildings. Judged by the experience of the residents, confirmed by the robust appearance of quite a number of young children, the place is a very healthy one. In fact, Umtali is said to be one of the healthiest townships in Rhodesia, and that in spite of the fact that very little has been done to improve upon nature by draining or planting of trees. Good water and fuel can be got quite near, and clay for bricks and lime are within reach. The township

itself is noticeable for its want of trees. It derives its importance from the proximity of important gold mines, such as the Penhalonga, the Streatham, and others. Scattered about in the neighbouring valleys and on the slopes of the lower mountains are a few farms, whence dairy produce, forage, and grain are supplied to the town.

The principal drawbacks to the East Coast route are—

1. The irregularity of the ocean steamer service to and from Beira, often entailing on travellers a stay of as much as 10 days on the unhealthy coast. This irregularity makes it impossible to establish a mail service of any importance with the interior.

This, of course, could be remedied. In 1895 a punctual fortnightly service was run by the Castle Company.

2. The want of telegraphic communication between Beira and the interior.

This want is being remedied.

3. The wretched steamer service between Beira and the interior, due to unsuitable plant and bad management, but which will soon be displaced by the railway.

4. The bad effect on men and animals of the country between Chimoio and Umtali: probably there is no remedy but a railway for this.

5. The uncertainty of transport along the whole of road traversed by ox waggons at the end of the winter season.

This has more force on the Cape route where the distance to be covered by waggon is so very much greater.

The inconveniences of passing through a foreign State apply to the Cape route as much as to this.

The roughness of accommodation and of food is not exceptional. It is the rule all through South Africa that anything is good enough for a post-cart traveller. If he can stand that method of conveyance, he will not be deterred from travel by the want of clean and comfortable food and sleeping accommodation, to provide which would only cost trouble.

In spite of all these drawbacks, it is now acknowledged that for Salisbury, Umtali, and the country to the north and east, Beira is the proper seaport. Against the drawbacks above enumerated have to be placed the following very important advantages of the East Coast route,

1. Its total cheapness.

2. The shortness of the land journey it entails.

3. The shortness of the post-cart journey.—(*No. 387, Foreign Office Miscellaneous Series.*)

XVII.—GENERAL TRADE NOTES.

SHIPPING THROUGH THE SUEZ CANAL.

According to the "Canal de Suez" bulletin for the 12th January last, the number of ships passing through the canal in October 1895 was 276, of a gross tonnage of 965,373 tons, 263 of which showed a tonnage of 913,162 tons.

Of these 276 vessels 177 were English (671,313 tons), 26 German (86,743 tons), 23 French (79,133 tons), 17 Dutch (43,099 tons), 7 Austrian (20,871 tons), 4 Spanish (16,945 tons), 6 Italian (13,091 tons), 4 Norwegian (10,835 tons), the remainder being composed of 5 Turkish, 4 Russian, 2 Portuguese, and 1 Chinese vessel.

RUSSIAN FLAX INDUSTRY.

The Russian flax season, according to the "Handels Museum," opened in the autumn of 1895 under conditions differing considerably from those existing in 1894. Stocks of old flax, which had affected the market so unfavourably in the previous year, were scarcely anywhere in existence at the beginning of the 1895 season. In general the crop was an average one. In foreign countries the new flax had suffered from the weather, but in Russia much better weather was experienced than in the previous year, so that the flax correspondingly improved and the supply of the first and second qualities was more abundant than that of the inferior varieties. Owing to these circumstances an extension of the Russian linen trade and increased exports may be expected, especially as the prevalence of fine weather in the autumn allowed the whole crop to be steeped and stacked in half the usual time. The increased demand for flax may help on a general improvement in the market for agricultural produce. Prices of cotton and wool, and their manufactures, have risen considerably. All varieties of yarns and fabrics were embraced in the export trade, which was especially supported by the increase in the demand from the United States, following on the revision of the Customs Tariff. The exports of linen yarn from England in October were thus 10.1 % above the average for the five previous years, and of linen goods 20.4 %. Similarly the United States imported 8,000,000 yards of linen goods, in place of 6,000,000 yards, an increase of nearly 35 per cent. About 20 per cent. more German fabrics were imported in September than in the corresponding month of the previous year, and in October about 22 per cent. more.

In spite of all these favourable circumstances the Russian linen export trade did not become any brisker; on the other hand

there was a general reluctance on the part of exporters and consequently to some extent a diminution of exports.

After referring to complaints on the part of foreign purchasers as to the price and quality of Russian flax, which it declares are unfounded, the "Handels Museum" goes on to discuss the statement that the present abnormal state of the Russian linen export trade is principally the result of speculation. In this connection it is observed that in July and August large portions of the future flax crop had already been sold. It is natural that traders should cease buying as soon as an increase in prices begins to make itself more or less felt, so as to produce an artificial fall in prices. In this state of affairs, however, the reluctance of foreign firms and Russian exporters is surprising.

The accumulation of large stocks on the market depressed export trade before exportation had actually commenced. Prices were so unsatisfactory that in some places farmers who had flax ready for export took it back to their store-houses. Such a state of things would long ago have affected the prices of flax if there had not been in operation special causes arising from the circumstances of internal trade in the present season, Russian manufactories having in fact increased their output as a result of larger orders.

In consequence of this the state of trade in the provinces which grow flax for export differs widely from that existing in districts in which manufactories are situated. In the former districts the reluctance of traders brought trade almost to a standstill, whilst in the latter the firmness of buyers made it brisker; as a result of this prices of flax varied almost everywhere.

THE OVERLAND TRADE BETWEEN CHINA AND RUSSIA BY WAY OF KIAKHTA.

According to a recent German consular report, published in the "Handels Museum," the total value of the overland trade between Russia and China viâ Kiakhta was 17,940,241 roubles in 1894, showing an increase of 672,907 roubles on the figures for 1893. The value of the exports from Kiakhta to China was 2,614,296 roubles, or 250,058 roubles more than in 1893. This increase in 1894 is due to the exportation of considerably larger quantities of silver and Russian paper. The value of silver exported increased from 1,115,781 roubles to 1,469,926 roubles, and the paper money exported from 117,455 to 250,968 roubles. The exports of candy-sugar rose from 2,864 pounds, value 29,228 roubles, in 1893 to 3,962 pounds, value 37,185 roubles, in 1894. The exports of materials for manufactories, &c., were doubled in 1894, the value increasing from 17,701 roubles in 1893 to 35,980 roubles in 1894. On the other hand the total value of Russian and foreign merchandise

exported shows a decline, as in 1893; this decrease is especially noticeable in the case of "mezeritski" cloth, the exports of which declined from 176,628 roubles to 121,348 roubles.

The value of the imports from China amounted to 15,325,945 roubles, showing an increase of about 422,850 roubles. This increase has arisen to a great extent from the increased value of the imports of tea by the Chinese, the figures being 1,564,086 roubles and 1,331,002 roubles for 1894 and 1893 respectively. Other imports of tea show a slight decline, the value imported in 1894 being 12,836,493 roubles or 142,464 roubles less than in 1893. Mongolian imports increased in value from 155,938 roubles to 193,477 roubles.

The variations in the value of imports, exports, and total trade viâ Kiakhta in the last decade are shown by the following figures. In 1884 the value of the exports was 2,639,480 roubles, of the imports 18,233,809 roubles, total, 20,873,289 roubles; in 1889, the exports amounted to 3,242,235 roubles, the imports to 15,630,256 roubles, and the total trade to 17,872,491 roubles; in 1893 the value of the exports amounted to 2,364,238 roubles, of the imports 14,903,095 roubles, total 17,267,333. For 1894 the amounts were 2,614,296 roubles, 15,325,945 roubles, and 17,940,241 roubles respectively. It will thus be seen that after the fall which took place in 1889 the value of the total trade has remained at about the same level.

THE NORWEGIAN HERRING FISHERY IN 1895.

According to a report of the French Consul-General at Christiania, published in the "Moniteur Officiel du Commerce," the herring fishery season off the Norwegian coasts for 1895 may be said to show very satisfactory results, the total quantity being estimated at 437,500 tons of fish.

If this figure be compared with some previous years when the amount of fish caught reached from 700,000 to 800,000 tons, it may not appear of much importance, but on account of the high price maintained throughout, owners reckon the year as a good one. The average price has been 10 kroner per barrel, with an upward tendency on account of the demand abroad. Another advantageous feature of the season has been that the fishery was not so localised as before, being spread over a greater stretch of coast.

The principal fish merchants of Trondhjem have recently amalgamated themselves into a society, with the object of studying how to increase their sales abroad; hitherto the means they have employed have only indirectly affected the foreign trade. One special resolution passed with a view to the facilitation of business was that for the future barrels of the same size and capacity are to be used for herrings, and it has been likewise

decided that in order to maintain the preference for their fish the fishermen are to be compelled to use greater care in its preparation.

The formation of the above society would seem to indicate that Norwegian trade considers it necessary to have recourse to combination for the development of its relations with other countries.

ORGANISATION OF DANISH STATISTICAL BUREAU.

A communication has been received from M. Marcus Rubin, Director of the State Statistical Bureau at Copenhagen, enclosing copy of a law, which came into force 1st January last, providing for the organisation of a State Statistical Bureau.

The work of the bureau includes the following :—

1. Statistics of population, including census, births, deaths, and marriages.
 2. Judicial and moral.—Including civil and criminal justice and moral questions (morality, abuse of alcohol, &c.).
 3. Social.—Embracing the conditions of life under various social aspects, such as food and consumption. A special feature will be a labour report and a report on workmen's insurance.
 4. Ways and means of livelihood.—Agriculture, industries, sheries, finance (including banking).
 5. Education and instruction.
 6. Public reports, such as State and communal finances and elections.
 7. International statistics.
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GERMANO-JAPANESE COMMERCIAL JOURNAL.

According to "Le Journal des Tarifs et Traités de Commerce," a German commercial journal is to be issued in the Japanese language, having for its object the making known of German merchandise. This paper will be edited and printed at Berlin under the direction of the Eastern Languages Institute, and will be distributed gratis among all classes of the Japanese population. Expenses will be covered by the advertisements, and by this means a direct communication will be established between sellers and buyers, the necessity for intermediary houses established in Japan being removed.

PROPOSED GERMAN COMMERCIAL MISSION TO CHINA AND JAPAN.

The "Handels Museum" of January 2nd, 1896, says that Germany is following the example of the Lyons Chamber of Commerce, which has already been imitated in England. The

Crefeld Chamber of Commerce made proposals to the Chambers at Elberfeld-Barmen, Gladsbach, and Aix-la-Chapelle to send a joint expedition to China and Japan to study the conditions of trade in those countries. The Chambers of Commerce at Elberfeld, Gladsbach, and Aix-la-Chapelle expressed their approbation of the projected mission, but provisionally declined to afford any financial support. The Crefeld Chamber of Commerce thereupon put itself in communication with the Minister of Commerce, and it is hoped that the project may be carried out with the co-operation of the State.

THE FRENCH COMMERCIAL MISSION TO CHINA.

With reference to the French Commercial Mission to China, organised by the Lyons Chamber of Commerce, and an account of which appeared in the "Board of Trade Journal" for October 1895, pp. 389-90, it is now announced in the "Journal des Tarifs and Traités de Commerce" that the Mission made a long stay in Tonquin, where they received the warmest welcome. They thus visited the most important centres of the Delta and the establishments of Along Bay (Hongay Kebao), and having ascended the Red River as far as Lao-Kai, entered China on the 25th November at Long-Po, near the Ho-Keou, one of the new Chinese markets recently opened to European trade by the Treaty of Shimonoseki, and where France is to establish a consulate. The Mission reached Mengtze on the 3rd December.

PRODUCTION OF IRON AND STEEL IN ITALY.

The Belgian "Bulletin du Musée Commercial" publishes some statistics of Italian iron production in 1894, taken from the Official Report of the Royal Office of Mines at Rome.

According to this, iron ore was produced in 1894 in four principal districts of Italy, viz., Florence, Milan, Turin, and Iglesias. It is chiefly in the Florentine district that the industry has made any advance, the output of the five establishments having been 174,229 tons of ore, of an average value of 11 lire (8s. 9½d.) per ton. Milan produced only 13,000 tons, valued at 13.25 lire (10s. 7d.) per ton.

In all Italy there are 39 iron mines at work, affording occupation to 2,178 miners. In the provinces of Carrara, Turin, and Iglesias 760 tons of manganese ore was produced of an average value of 30.92 lire (24s. 9d.) per ton, and in the province of Florence 5,810 tons of ferro-manganese at a value of 8 lire (6s. 5d.) per ton. Iron pyrites was produced in the districts of Carrara, Florence, and Turin to the extent of 22,638 tons of an average value of 13 lire (10s. 4d.) per ton.

Iron and steel were produced in eight Italian provinces; the principal centre of production is Carrara and the second in importance is in the neighbourhood of Perugia, near Rome.

In the year 1894 the output of iron from the province of Carrara was 39,000 tons and of steel 32,702; the province of Rome produced 2,600 tons of iron and 19,872 tons of steel.

The output of iron in Milan province was 34,505 tons, and in Florence 31,520 tons.

9,919 workmen were employed in the iron industry of Italy in the year 1894, and the value of the total production was estimated as follows: Iron 32,218,000 lire (1,289,000*l.*) and steel 16,769,000 lire (671,000*l.*).

ITALIAN WINE CROP OF 1895.

According to the "Journal Officiel" of the 13th January last the wine crop in Italy for the year 1895 amounted to 21,343,400 hectolitres as against 25,816,588 hectolitres in 1894. The falling off in quantity is attributed to the rains falling at a moment favourable to the incursion of cryptogamic diseases.

THE MARBLE QUARRIES OF MASSA-CARRARA.

The "Moniteur Officiel" publishes a report of M. Aréne, French Consul at Leghorn, on the marble quarries of Massa-Carrara, according to which the number of quarries in the district in 1894 amounted to 1,003, of which 409 were in working and 594 abandoned or temporarily suspended. The total amount of marble in the rough produced in 1894 was 206,000 tons, representing a value of 14,136,200 lire (565,400*l.*). The wages paid are as follows: foremen, 3 to 4 lire per diem; hewers and quarriers 2.50 to 3.20 lire: shifters, 2.20 to 2.75 lire: children, 90 centimes, and women 1 lira (lira = 9.6*d.*).

The amount of marble quarried in the Massa-Carrara district in 1880 was 102,712 tons, of which 96,905 tons were exported; in 1885 the figures were: production 159,169 tons, export 122,305 tons; in 1890 production 156,058 tons, export 161,548 tons; and in 1894 production 183,870 tons, export 161,750 tons.

INTERNATIONAL MARKET AND EXHIBITION OF AGRICULTURAL MACHINERY AT VIENNA IN 1896.

The "Journal Officiel" states that the Imperial and Royal Society of Agriculture of Vienna is organising a second international market with exhibition of agricultural machinery, to be held in the Austro-Hungarian capital from the 9th to the 14th May 1896. The first international market was held in the month of May last.

Applications must be sent, before 15th March 1896, to the Committee, 13 Herrengasse, Vienna. Applications concerning new apparatus, on which the inventor may wish to have the opinion of the committee, will be received by the latter up to March 1 next.

THE AUSTRIAN MINING AND METALLURGICAL INDUSTRIES IN 1895.

The following account of the course of trade in the mining and metallurgical industries of Austria in 1895 is extracted from the annual report of the Association of Austrian Mine-owners and Iron and Machine Manufacturers as published in the "Handels Museum."

Business in the coal and coke trades was very satisfactory during the year. Coal was continually in fair demand, whilst the demand for coke could only partially be met, and it was so considerable as to necessitate the erection of new coke depôts; under these circumstances prices showed an upward tendency. The great demand for coke was caused principally by the continued increase in 1895 of the production of pig iron. The prices of pig iron showed little alteration, but business was on the whole satisfactory. On the other hand, trade in "commercial" iron was unsatisfactory throughout the whole year. The consumption of "commercial" iron decreased in 1895 and, as at the same time the imports increased, prices could not be maintained at the figures prevailing at the end of 1894. Business in iron for buildings, &c., was always brisk, and pillars were especially in great demand. The consumption of fine and rough sheet-iron cannot be considered satisfactory, but prices were very low and have fallen considerably, particularly in the case of fine sheets. Trade in castings was brisk, especially in the first six months, but towards the end of the year the demand fell off considerably. Rail manufactories, though not fully employed, were occupied more in 1895 than in 1894.

Locomotive works were well supplied with work in the last six months, especially for the State railways; but carriage and truck factories were not fully employed. Throughout the whole year trade in agricultural machinery was dull; the purchasing power of the agricultural population was diminished owing to the poor-ness of the crops, whilst export trade was at a standstill, and consequently in many cases workmen had to be turned off. The metal market, in which very low prices prevailed at the beginning of 1895, took a remarkable upward movement about the middle of the year and since continued satisfactory, though with considerable fluctuations.

COAL MINING IN HUNGARY IN 1895.

The coal industry of Hungary is continually developing. The production is increasing and new beds are being opened. According to the "Handels Museum" the operations of the Salgó-Tarjan Coal Mining Company were developed with great success, the company having purchased the Petrozsény coal works during the year. In the chief mining district worked by the company two new shafts, at Eites and Pálfalva, were sunk and mining commenced. Work was carried on uninterruptedly in 1895 at those mines which had been re-organised in the previous season, and the total production in 1895 amounted to about 1,000,000 tons of 2,204 lbs., as compared with 950,000 tons in 1894. The incorporation of the Petrozsény works in the sphere of the company's operations took place in January 1895, and the success of this undertaking was shown by the fact that production in 1895 amounted to about 300,000 tons, or about 50,000 tons more than the production in 1894. A further increase in the production is projected, and it is hoped with some degree of certainty that the production in 1896 will be increased to 400,000 tons.

In consequence of a strike amongst its workmen in May, the production of coal by the Coal Mining Company of North Hungary did not attain the figure which had been expected, but in spite of the strike the production of coal amounted to about 360,000 tons, which was readily disposed of. In order to develop mining in the Nógrád district on scientific lines extensive improvements are being made, new shafts are being sunk and large tracts of land containing coal are being acquired, whilst in order to promote the well-being of workmen and officials dwelling-houses and other buildings are being erected or extended. Works are similarly being carried out in the Gran district on a large scale, and every preparation is being made to develop the coal beds there, which are likely to prove very rich.

The Hungarian General Coal Mining Company raised and sold 250,000 tons in 1895, or 70,000 more than in 1894. The Királd mines are being extended by new buildings, and at present a large shaft is being sunk with the most modern machinery. This shaft will shortly be ready for working and will yield about 200,000 tons annually. The mine at Sajo-Szentpéter has two shafts, and there are workmen's dwellings and a school in connection with it. At present about 200 tons of coal are raised daily. The Királd and Sajo-Szentpéter mines will produce 350,000 tons in 1896.

The Urikány-Zsithaler Mining Company increased its production considerably in 1895 and sold 16,377 tons of coal as compared with 9,144 tons in 1894, but the annual balance sheet of the company shows a loss of 40,671 florins. The Coal-mining and Brick-making Company raised in 1895 194,000 tons of coal as compared with 185,500 tons in 1894.

CENSUS OF HUNGARIAN GYPSIES.

The Hungarian Government has just published, under the title of *Czigányösszeírás Eredményei* (Buda Pest, 1895), a census of the gypsies in Hungary, which claims notice as the first attempt which has ever been made systematically and ethnodemographically to deal with the subject of these nomads in any part of the world. The wide distribution and large numbers of gypsies in the centre of Europe had long attracted the attention of Mr. Karl Hieronymi, and when, in the latter part of 1892, he was made Hungarian Home Secretary, one of his earliest acts was to order the collection of statistics in this direction. The census was completed in 1893, but it has taken two years to verify and arrange the results, and bring them out in the present form. The report deals not with the nomadic classes only, but with all persons of gypsy birth, and close attention has been given to the varieties of race, language, habits, and domicile.

The total number of gypsies in Hungary is given as 274,940, of whom 138,070 are males and 136,870 females. They preponderate most in the south-east, on the borders of Roumania, but they are numerous, especially the nomadic gypsies, on the west side of the Danube, the eastern counties from Buda Pesth down to the frontier of Servia being almost entirely free from them. They occupy the country between Debreczen and the Carpathians. In only one county of Hungary, Győr (or Raab), are they practically non-existent, in consequence of very strict local laws against vagabondage.

CESSATION OF REGULAR STEAMSHIP LINES ON THE DANUBE.

The French Consul at Galatz states that the boats of the Fraissinet Steamship Company have made their last journey for the season. The Austrian-Lloyd, on the 15th December, made its last trip between Galatz, Braila, and Constantinople, as has also the Ottoman "Courgi Company" and the Italian Florio-Rabattino. On the 18th December last the Russian company of navigation on the Black Sea and Danube made their final trip between Galatz and Odessa. The Austro-Hungarian Danubian Company will continue its service between Galatz and Turnu Severin until further notice, but will take very little cargo in addition to passengers. After the 25th December the Danube is seldom visited by steamers except in an unusually mild season.

JAPANESE STEAMSHIP LINE TO THE UNITED STATES.

The Japanese Consul at Tacoma, Washington, has, according to the "Handels Museum" of December 26, 1895, officially announced that he has been commissioned by his Government to select a port on the Pacific Coast, the best suited as a port of arrival for Japanese steamers, it being the intention of the Japanese Government to pay substantial subsidies to a steamship company which will make regular voyages between Japan and the Pacific coast. Regular steamship communication already exists between Japan and Australia.

GOLD MINING AT CAPE HORN.

What might be termed the revival of gold mining on the American continent has demonstrated, according to the New York "Engineering and Mining Journal," that not only can gold be profitably mined under the disadvantage of the short summer season and long winter of Alaska, but that in the inhospitable region of Cape Horn, the other extremity of the continent, where the snow is always present on the higher ground, the search for gold is successful.

To trace back the history of gold mining, or rather washing and sluicing gold from the magnetic iron sands of the mainland and islands adjoining the Straits of Magellan, sounds like a romance. The conditions also are so exceptional that the industry as carried on there to-day cannot fail to interest men who are striving to make their fortune in gold seeking elsewhere. Water has to be contended against, but not such water as is met with in sinking a shaft, which can be overcome by capital providing sufficient pumping power to control it. It is the water of the ocean, and impossible to control; and yet that very agent replenishes the supply of black sand from which the gold is won.

It may well be, from imperfect prospecting work, that no veins have been discovered which furnish the supply of gold, and that no placer ground of any great extent has been found much above tidewater level. At all events, the facts are plain that on the shores of the mainland and on those of the adjacent islands just above high-water mark, by removing a few feet of sand and gravel, a layer of black sand is found containing an unusually large amount of heavy gold. More frequently this thin bed of magnetic iron sand crops out between high and low water mark, and as it has been found impossible by any pumping to keep a shaft dry above high-water mark, and coffer dams have proved useless, the attempt to win the gold in that way has been abandoned and the sand is gathered up when the tide is out, the men consequently being compelled to work less than one shift in 24 hours, including washing out the gold. Under these conditions, however, they are reconciled to a certain extent to their enforced

idleness by the action of the ocean, as after each tide, especially if the weather is stormy, they find their supply of black sand the same as before.

SCARCITY OF WOOD IN ASIA MINOR.

The "Revue d'Orient" states that Marshal Chakir Pacha, inspector-general of the vilayets of Anatolia, has just proposed to the Sublime Porte the adoption of certain urgent measures in face of the want of fuel which is apparent in the vilayet of Erzeroum, where there are no forests except that of Erzindjian. Firewood brought from a point 30 hours away is sold at 10 paras the ocque, a price which might reach 30 paras. To obviate such an eventuality, the Marshal proposes the construction of a road across the pass of Courdji-Boghaz to the forests of Essir-Caza and a grant to the municipality of the right of exploiting the coal mines in the neighbourhood of that town. As far as the afforestation of the province is concerned, Chakir Pacha insists on the necessity of making it compulsory on the population to plant a certain number of trees each year.

The Marshal adds to his letter a proclamation to the same effect to be published in different parts of the province. These various plans have been under the consideration of the Ministry of Agriculture, Mines, and Forests, and made the subject of a report which has been sent to the Sublime Porte.

REFERENCES AS TO STABILITY OF MERCHANTS ESTABLISHED IN JAPAN.

In a report of the French Consul-General at Yokohama it is stated that in a country of extra-territorial jurisdiction like Japan, the interior of which is completely closed to strangers, the consuls find it difficult or impossible to reply to all questions as to the reputation, commercial morality, and extent of credit of merchants and firms established in their districts.

In Japan foreign merchants are compelled to reside within the limits of the European concessions, and not being permitted to own anything outside these limits the majority of them are lodgers in the houses they occupy. Their pecuniary situation can only be judged by appearances, often very deceptive; one who makes a grand show may be working with other people's money, owning nothing of his own and likely to become bankrupt in 24 hours; another without any outward show may be concerned in important affairs. If enquiries are made at the local banks, those institutions may plead professional secrets; even if they consent to give any information it will be rather to their own customers or to merchants who can bring them business than to consuls who may be suspected of opening official enquiries into any matter.

As to the commercial morality of persons or firms, a consul can of course give an opinion with regard to his compatriots, but nine times out of ten it is about foreigners that the information is required, and in such a case a vague formula is given as a reply, such as that nothing has been seen or heard which could impugn the commercial morality of the persons or firm in questions.

NEW RAILWAYS IN CHINA.

The "London and China Telegraph" states that it is rumoured that an Imperial Edict has been issued appointing officials to take charge of the construction of a railway from Tientsin to Lukow, about seventy-five miles, at an estimated cost of 2,400,000 haikwan taels; and the line is to be continued to Hankow, about 500 hundred miles further, as soon as a suitable man with ten million taels comes forward to undertake the work. The Government premises not to obstruct, but to do everything that is good for the success of the scheme. It is also reported that a syndicate of Hong Kong Chinese has obtained a concession for a railway between Canton and British Kowloon, and that negotiations have been completed for the opening of Wu-chow-fu as a treaty port; but both these rumours are very doubtful.

The native papers state that the Viceroy Chang Chih-tung has given the laying of the Shanghai Soochow Railway to three Belgian engineers, who have the sole control of the building of the said line.

INDIAN COTTON CROP.

The "Times of India" states that the Second General Memorandum on the Indian cotton crop of the season 1895 is as follows:—The area sown is below the area of the previous year, but the decrease is generally small, and is due chiefly to the long break in the rains during the sowing season. In Madras the decrease is only 2·3 per cent. and in Berar the deficiency is 3·6 per cent.; but in the Central Provinces the area of important cotton-growing tracts shows an increase which counterbalances the diminished areas in the other districts. In Bombay and Sind, though the area is not expected to equal that of the preceding year, it is generally in excess of the normal, except in the early districts of the Deccan, where the area is less by 9 per cent. than either the normal or last year's area. In the Punjab and the North-Western Provinces and Oudh the area is less than that of last year by 8·5 and 5 per cent. respectively.

Rainfall in the early part of the season was not favourable, and the crop appears to have suffered more or less from the long break in the rains in July and August. In Madras the rainfall was very favourable in some parts, while in others it was either excessive or in defect; but recent rains have improved the

prospects. In Bombay and Sind the rainfall was generally deficient, and retarded sowings; and though the season has subsequently proved beneficial, more rain is still needed. In the Punjab also rain was insufficient, but the crops have not yet suffered much in consequence, and prospects generally are satisfactory. The season in the North-Western Provinces and Oudh was favourable, and the weeding of the crop was satisfactorily carried out. In the Central Provinces also good weather prevailed, and prospects are decidedly good; but in Berar the long break in the rains did some harm. Recent rainfall has, however, greatly improved the condition of the crop, which is on the whole fair.

The probable outturn may be described as fair to good in Madras, and fair in Bombay and Berar. In the Punjab the outturn is expected to be equal to the average, and in the N.-W. P. and Oudh only very slightly below.

THE HARBOUR OF KURRACHEE.

The survey of Kurrachee harbour reported on by the Port Engineer was, according to the "Times of India," the thirty-second, and the net result of the year's work was an increase in water space of nearly six and three-quarter millions of cubic feet. The chief improvement was made at the entrance to the harbour, where there is now a through navigating channel of not less than twenty-four feet and a half at low-water springs. The width of the entrance between the twenty feet contours has been substantially increased, and is now two hundred and fifty-six fathoms as compared with fourteen hundred feet twelve months ago. Kurrachee harbour has now been brought up to the level of the Suez Canal so far as the draught of steamers is concerned. The official notification as regards the available depth of water in the entrance channel gives thirty-two feet and a half at high-water mean spring tides, and twenty-nine feet at high-water mean neap tides, and the maximum draught allowed has been fixed at twenty-six feet in the south-west monsoon and twenty-eight feet in the fair season. The dredging operations of the year comprehended the revival of nearly three-quarters of a million tons of material, or very considerably over fourteen million cubic feet. The blasting operations will be continued until vessels of ordinary draught can enter or leave the port at any state of the tide. The total improvement in water space effected in the entrance during the year was nearly two per cent., of which about half was due to "scour." In every direction the year was one of progress, and the facilities provided in the matter of landing and wharfage are already not only abreast but ahead of the times, as is evidenced by the empty berths at the wharves and moorings in the stream. Dependent as it is upon Sind and the Punjab for its exports, what Kurrachee now asks for is a direct line to Delhi and Cawnpore, and a map of the suggested route

viâ Shadipalli, Pachpadra, Jodhpur, Kuchaman, and Rehwari is appended to the Port Engineer's report. The total distance of this line would be seven hundred and forty-nine miles from Kurrachee to Delhi, thus giving the Sind port an advantage of a hundred and forty-one miles over Bombay. For more than half the distance this line is already in existence, but to link it up and extend it along the most direct course is estimated to cost nearly two crores rupees. It is the opinion of the Port Engineer that Kurrachee could thus be easily put in direct communication with Delhi by a route that could enable the local merchants to compete successfully with Bombay for the grain trade of Upper India, and that for less than half the money that the Government are about to spend on shortening a route which will still leave the mileage balance in favour of Bombay.

CANADIAN SHIPPING.

The tonnage of Canadian sailing vessels and steamers twenty years ago was a million tons. It is not so much to-day, the tonnage at the close of 1894 being placed by the Government Blue Book at 869,624 tons, represented by 7,245 vessels. Of this total number, which is by no means small, 1,640 are steam craft, with a tonnage of 240,906 tons. The reason for this decline, according to the Toronto "Monetary Times," is largely supplied by the fact that the wooden ships, which were so freely built in the maritime provinces and carried Canadian products to every foreign country, are being superseded by those of iron and steel construction, built on the Clyde and on the Tyne. That this is the case is shown, if proof were needed, by the official figures. The decline in the tonnage of Nova Scotia, New Brunswick, and Prince Edward Island was from 630,175 tons in 1892 to 527,339 tons in 1894, which is equal to 16.32 per cent., though at the same time the number of craft owned by these three provinces has risen from 3,873 to 3,904, which indicates probably that there are more coasters and fishing schooners, and fewer big ocean-going ships and barques. The difference in tonnage may be gleaned from the returns in this way: in 1892 the tons per vessel were 192 for New Brunswick and 156 for Nova Scotia, in 1894 the tons per vessel were 136 for New Brunswick and 137 for Nova Scotia. Quebec, which used to build such handsome wooden clippers, that made a goodly showing in the Navigation Returns, is now down to a total of 160,590 tons, though her decline since 1892 is trifling.

On the other hand, the province of Ontario shows an increase of tonnage equal to 6,775 tons between 1892 and 1894, which, however, consists of lake and river craft, while the western provinces of British Columbia and Manitoba are in 1894, respectively, 3,007 tons and 597 tons ahead of their 1892 record.

A comparison by Provinces will show the standing of each in this particular :—

Provinces.	Dec. 31, 1892.		Dec. 31, 1894.	
	No. Vessels.	Registered Tonnage.	No. Vessels.	Registered Tonnage.
New Brunswick - -	946	181,779	1,003	136,257
Nova Scotia - -	2,731	426,690	2,710	371,432
Quebec - -	1,408	162,638	1,427	160,590
Ontario - -	1,347	141,750	1,480	148,525
Prince Edward Island -	196	22,706	191	19,650
British Columbia - -	298	23,448	336	26,455
Manitoba - -	81	6,118	98	6,715
Total - -	7,007	964,129	7,245	869,624

The value of the vessels built and registered in the Dominion during last year, if taken at 35 dols. per ton, would reach 740,000 dols. There were 326 new craft built, of an aggregate tonnage of 21,243 tons, or only about 65 tons each, which must mean that a large share of them were fishing boats, or small craft for internal navigation.

UNDEVELOPED COAL FIELDS OF CANADA.

The "Canadian Gazette" states that it is believed in Alberta and British Columbia that this year or next will see the Canadian Pacific Railway Company build through the Crow's Nest. When a line is built, there will be opened up a coal field of great wealth.

The coal fields in the East Kootenay district, in what is known as the Crow's Nest Pass, lie in a south-easterly direction from Fort Steele, a distance to the nearest available coal of about sixty miles. These coal fields in the near future will have a world-wide reputation, both on account of the quality and quantity, and are without doubt the best coal fields in America, undeveloped. The western outcrop of the field is on the side of a mountain in the valley of Elk River, one of the largest tributaries of Kootenay River, on the eastern side, and the coal seems to have been traced a distance of 40 miles, with surface cross-cuts made at intervals showing the seams and walls.

The lowest known seam is some 1,500 feet above the drainage level of the valley, and is 30 feet in thickness. One hundred feet higher there is another seam 30 feet thick, then comes a seam 15 feet thick, then a small three-foot one, then a seven-foot one, and then another 30-foot seam, and above these there are five more workable seams from four to ten feet in thickness—11 seams in all, making a total of 148 feet in thickness of coal exposed. These seams dip with the mountain easterly at an angle of 30 to 35 degrees, the upper seams having the least dip.

There are three large creeks cutting this coal field, the seams are exposed on the banks of these creeks, and openings can be made to mine the coal without much preliminary expense. The cuts made by the creeks are more valuable than so many tunnels to prospect the coal, because there is room to operate the mines on both sides of the creek, and the mines are proved to be permanent without any cost. In addition to the coal on Elk River, further east on Martin's Creek and other tributaries of Mitchel's Creek, which is a large fork of Elk River, there is another large body of coal above the Elk River field, but of smaller area, a great amount of the field having been carried away by natural causes through the different ages since the coal was formed. The aggregate of the depth of the seams of coal in the upper basins is somewhat more than on Elk River, so that if a shaft be sunk through the whole field there would be found 300 feet of coal in workable seams. It would be hard to find another field of coal with so much coal to the acre, and so easy and cheap to work.

The eastern outcrop of the field is near the summit of the Rocky Mountains, the average distance from the western crop being 10 miles, showing a coal field 40 miles north and south by 10 miles east and west, and an area exceeding 250,000 acres. The coals in this field differ, owing, no doubt, to the different ages of the coal, there being three different qualities. The lower seams are anthracite in their nature, while the upper seams are the bituminous coals. In between, both above and below the bituminous coals, are a number of seams of coal different from anything heretofore known. It is somewhat similar to cannel coal, but superior to any cannel known.

These coals have been analysed and tested by different parties, amongst them Professor Hoffman, Government Assayer at Ottawa for the Geological Department, and the results, as shown in the Department reports, prove that they would lose nothing by comparison with the best coals of the same variety in Pennsylvania. Owing to their position these coals can be mined at a small cost, and can be loaded on cars ready for shipment at 1 dol. 25 cents per ton, and with ordinary freight rates can be placed on all the western markets to compete with any and all of the coals now used.

DEVELOPMENT OF BRITISH COLUMBIA.

The "Monetary Times" states that in British Columbia new mining discoveries and further developments of working operations, some of the latter involving transfers of important properties, are reported from various quarters. One of the most significant of such movements probably points to an early large development of gold and copper mining in the upper country by means of ample American capital, and may thus lead in that section to a general expansion of activity as remarkable as that of other parts.

Reports of valuable gold finds about Harrison Lake, and in Sumas Mountain, at points mainly accessible by means of Fraser River navigation, are rousing better hopes in New Westminster city, which has suffered somewhat severely from stress of Vancouver competition. There seems to be actuality behind certain of these reports, but nothing very definite is yet ascertainable. Meanwhile money payments—a sign, of course, of local improvement—are being better made in New Westminster, where a larger proportion than expected of prompt payments of city taxes for the year has just been made.

THE CROPS OF MANITOBA FOR 1895.

The "Canadian Gazette" states that the Manitoba Government has issued its last crop bulletin, which details completely the marvellous harvest of 1895. The total wheat acreage is shown to have been 1,140,276, from which 31,775,038 bushels were raised, an average of nearly 28 bushels per acre. Of oats there were 482,658 acres with a total product of 22,555,733 bushels, or an average of nearly 47 bushels per acre. The barley acreage was 153,839; total yield 5,645,036; average per acre, 36.69. Of flax, 1,281,354 bushels were raised; of rye, 81,082 bushels; and peas, 28,229 bushels. The total grain crop is thus shown to be 61,566,472 bushels, an amount nearly 4,000,000 bushels in excess of the Government's estimate made just before the threshers got to work. Of potatoes there were 16,716 acres under crop, with a total production of 4,042,562 bushels; the average yield per acre being nearly 244 bushels. The total crop of roots and potatoes was 6,327,845 bushels. Crop correspondents, in the course of their remarks, say that the immense wheat crop has enabled farmers to sell as much as was sold at this date last year, so that they can pay off pressing liabilities while still holding the greater part of their crop for higher prices. The bulletin shows that the stock exports were nearly doubled during last year, and the figures show that stock raising for export is growing to such proportions that it will soon become as important an industry as wheat raising. A great advance has also been made in dairy products. The output of butter and cheese was valued at nearly 200,000 dols. The bulletin shows that 5,000 harvest hands from Ontario assisted in garnering the crop, and were paid 400,000 dols. by the farmers in wages.

TRADE OF THE CAPE OF GOOD HOPE.

A communication, dated January 6 last, has been received from the Agent-General for the Cape of Good Hope enclosing extracts from the Cape of Good Hope Government Gazette of December 13 last, from which it appears that the value of the

imports into the colony for the eleven months ended 30th November 1895, including specie, amounted to 17,364,000*l.*, as compared with 10,548,000*l.* for the corresponding period of 1894. With regard to the exports, the total value for the eleven months ended November 30, 1895, including specie, was 15,410,000*l.*, as compared with 12,753,000*l.* for a like period in 1894.

REVENUE RETURNS OF CAPE COLONY.

A communication, dated January 6 last, has been received from the Agent-General for the Cape of Good Hope, enclosing copies of comparative statements from which it appears that the revenue received in the month of November last was 550,873*l.*, showing a net increase of 138,560*l.* on the corresponding amount for 1894. For the five months July–November 1895 it amounted to 2,618,825*l.*, showing a net increase of 493,994*l.* on the amount for the corresponding period of 1894.

RAILWAY TRAFFIC EARNINGS OF CAPE COLONY.

The “Cape of Good Hope Government Gazette” for the 24th December last publishes a statement from which it appears that the traffic earnings on Cape Government railways for the month of October last were 318,208*l.* as compared with 247,602*l.* in October 1894. For the ten months ended October the total traffic earnings were 2,690,983*l.* as compared with 2,240,643*l.* in 1894.

RAILWAY TRAFFIC IN NEW SOUTH WALES.

From communications received from the Agent-General for New South Wales it appears that the earnings of the New South Wales railways in October last amounted to 308,423*l.* as compared with 326,138*l.* in October 1894. In the ten months ended 31st October 1895 the aggregate earnings were 2,280,659*l.* as compared with 2,218,121*l.* for the same period of 1894.

As regards the tramways the receipts for October last amounted to 23,983*l.* as against 23,911*l.* in 1894.

The earnings of the Government railways for November last amounted to 311,734*l.*, as compared with 338,575*l.* in November 1894. In the 11 months ended 30th November 1895 the aggregate earnings were 2,592,393*l.*, as compared with 2,556,696*l.* for the same period of 1894.

As regards the tramways, the receipts for November last amounted to 23,537*l.*, as against 24,024*l.* in 1894.

The length of line now open is—railway, 2,531 $\frac{1}{4}$ miles; tramway, 61 miles.

MERINO SHEEP.

According to a statement in the "Adelaide Observer" the total number of the sheep in the world is estimated to amount to 555,000,000. Of this number more than one-third, nearer one-half, are Merinos. Though a change has set in recently in favour of "long wools," the 16,000,000 sheep which Australia has added to her flocks during the last five years have been chiefly "fine wools," and of the total 122,000,000 sheep in Australia, it is stated that 110,000,000 are Merinos. In South America the increase of the Merino has been phenomenal during recent years. Of the 75,000,000 sheep in the Argentine not fewer than 45,000,000 are Merinos; of the 28,500,000 sheep in Mexico, Chili, Peru, Brazil, &c., 16,000,000 are Merinos, and of the 46,500,000 sheep in the United States and Canada probably more than 5,000,000 are Merinos, making in all 65,000,000 Merinos in America. In Europe there is said to be very nearly the same number. Spain has more than 12,000,000 Merinos; and France, Germany, and Russia must each now have almost as many fine-woolled sheep as Spain; while the Merino either predominates or is bred extensively throughout every other European country outside the British Isles. Asia and Africa, with 78,000,000 sheep, have at least 15,000,000 Merinos. There are thus some 150,000,000 pure-blooded Merino sheep in the world at the present time, not to mention the various other breeds which have one or more crosses of the Merino in them.

TRADE OF NEW ZEALAND FOR 1894-95.

The "New Zealand Trade Review" in a recent number compares the result of the aggregate trade of New Zealand for the past year with previous years, according to the record presented in the official returns of the Customs department. For this purpose the totals of exports and imports for the last six years and the balances, or excess of exports over imports, are stated. The following are the figures:—

Year.	Exports.	Imports.	Excess of Exports.
	£	£	£
1894-95 - -	8,678,569	5,866,332	2,812,237
1893-94 - -	8,851,066	6,183,860	2,667,206
1892-93 - -	9,120,203	6,491,364	2,628,839
1891-92 - -	9,285,884	6,764,574	2,521,310
1890-91 - -	9,778,952	6,298,371	3,480,581
1889-90 - -	9,784,530	5,828,816	3,955,714

The above figures are exclusive of specie, but for the last few years the banks have, in preparation for possible emergencies, been strengthening their position by importing large amounts in specie, a considerable proportion of which is still in evidence in the increased holdings of coin shown in the bank returns.

XVIII.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Belfast.—The annual meeting of the Belfast Chamber of Commerce was held on February 4 last, Mr. R. L. Patterson, the outgoing president, in the chair. The annual report was adopted, and Mr. Patterson delivered a long address on subjects of interest during the past 12 months, including the Board of Commissioners on Irish lights; Holyhead mails; Ulster Conciliation Board, recent gold discoveries, and their probable effect on the value of money; linen trade of Belfast; Jurors' Act; the question of a training-ship for Belfast Lough; and the centenary of the Chamber.

The Marquis of Londonderry, K.G., was elected president for the year.

Blackburn.—The annual meeting of the Blackburn Chamber of Commerce was held on 5th February last, Mr. H. Harrison, J.P., president, in the chair.

In his annual address to the members the president said, with reference to the Indian cotton duties, that they might look with some slight satisfaction at the Bill which had just passed the Indian Government, but nothing less than the entire abrogation of the duties would satisfy that Chamber. The Bill was more in favour of the Indian millowners than of the millowners in this country, especially the spinning industry, and they could not help but see it if they went carefully into the matter. In Bombay there were 32,000 looms, and he thought he was fair in saying that, considering the export of the productions of these looms, a very small number would be subject to any duty at all.

The proposal of the Blackburn Chamber to send a commercial mission to China had been exceedingly well received generally by the Chambers of the country. So many trades would like to join the mission as to make it difficult to know what to do. It had been thought desirable to unite the cotton trade of Lancashire with the woollen trade of Yorkshire. Indeed, the Chamber had been most anxious for Yorkshire to join them, and had made every effort to induce them, but it was very unlikely they would. Under those circumstances they would have to rely upon themselves. Oldham thoroughly approved of the mission, and it was hoped to obtain the support of Manchester. Their next duty would be to have an interview with Mr. Curzon and permanent officers at the Foreign Office, and ask for their assistance. The probable cost of the mission would be 3,000*l.* a year, and he thought the Chamber might make a grant from its funds. It was to be noted also that

the Leeds Chamber of Commerce had taken up the question of opening the south-western markets of China.

The president further made reference to railways in India, local trade, and, touching on the subject of the Paris Exhibition of 1900, he said, whilst he should be very glad if some townsmen exhibited, he could not advise them to exhibit samples of their best manufactures except an increase in our trade with France followed.

Bristol.—The usual monthly meeting of the council of this Chamber was held on January 23 last, Mr. George H. Perrin in the chair.

The official programme of resolutions for the annual meeting of the Associated Chambers of Commerce was presented.

A circular-letter was read from the London Chamber of Commerce, announcing that the Congress of Chambers of Commerce of the empire would be held in London in June next. Amongst the subjects down for consideration are:—Commercial relations between the mother country and her colonies and dependencies; boards of labour conciliation and arbitration; codification of the commercial law of the empire; bills of lading reform; commercial education; the decimal system of weights and measures and currency; imperial penny postage; light dues; railways—light railways; railway communication with India and the East; inter-colonial trade relations; arbitration for international disputes; imperial trade marks registration and patent law; cable communication—construction, rates, codes; steamship communication—rates, subsidies, war risks, insurance; closer connection between Governments and chambers of commerce by establishment of commercial advisory or consultative councils, home and colonial; bills of exchange—uniform procedure; supply of Government publications to chambers of commerce and mutual interchange of documents; Parliamentary commercial parties, home and colonial; representation of the United Kingdom in colonies, and of colonies in the United Kingdom, to make up for want of consular officers, who are only appointed to foreign countries; appointment of an Imperial council to consider questions of Imperial interest; and copyright.

Other business transacted was of local interest only.

Glasgow.—The annual meeting of the Glasgow Chamber of Commerce was held on 20th January last, Mr. Walter Duncan in the chair.

The president, in moving the adoption of the annual report, referred in a speech of some length to the chief events in the commercial world of the past year, with especial reference to progress in Japan, colonial expansion, bi-metallism, and strikes and industrial conflicts.

Leeds.—The monthly meeting of the Council of the Leeds Chamber of Commerce was held on February 5 last, Mr. E. Slater, president, in the chair.

The Secretary read a letter from the Postmaster-General, in reply to a communication, forwarding a list of additional trunk wires which, in the opinion of the Chambers of Commerce of the West Riding, ought to be erected at an early date. The Postmaster-General had caused a minute and careful inquiry to be instituted in the West Riding, with a view of ascertaining what further facilities were necessary.

A letter was read from the secretary of the Worsted Committee with reference to the robberies from bales in the Hull docks. It appeared that, notwithstanding extra police superintendence, the pilfering still continued, and it was recommended that until some surer means of protection were adopted, it would be better to forward the goods by some other route. It was said that the Hull Chamber of Commerce had taken the matter up, and were advising the Corporation to enclose the docks. The question had been before the committee for some months. There was no doubt that a great deal of pilfering went on in a systematic way, which had for a long time eluded the vigilance of the police. The Hull Chamber of Commerce and the Corporation were seriously interesting themselves in the matter. It was quite evident that there was only one thing to be done, viz., the enclosing of the docks; and the only question was, who was to do it. There was a divided responsibility—partly the Dock Company's, partly the Railway Company's, and partly the Corporation's.

It was agreed to accept the invitation of the Wakefield Chamber to a conference on the subject.

Liverpool.—The monthly meeting of the council of the Liverpool Chamber of Commerce was held on January 10th last, Mr. Charles M'Arthur in the chair. With reference to an official vocabulary for code telegrams, the general trade committee now recommended that a further communication be made to the Postmaster General, urging (1) that the Government be requested to instruct their delegates to the conference to move that the compulsory use of the vocabulary for European telegrams be postponed for reasons already given by chambers of commerce of the United Kingdom, supported by Continental chambers; and (2) that in view of the proposed revision and enlargement of the vocabulary, care be taken to protect the rights of those who, on the understanding that the vocabulary was a safe one, that it had been approved internationally, and that it would be made compulsory, have already used it in compiling new private codes for European telegraph service.

The proceedings of the general trade committee stated that the Government had consulted the chamber on the subject of their taking part in the arrangement for the next Paris International Exhibition, which it is proposed should take place in 1900. A large number of manufacturers, members of the chamber, had been communicated with, and an expression of their views had been transmitted to the Marquis of Salisbury.

Mr. Bindloss stated that a large majority of those whose opinion had been taken had replied that they thought it inadvisable to take part in the exhibition in question. They stated that English products could not be sold in France owing to the high protective tariffs, and further, that exhibitions in the past had not been in the interests of the manufacturing industries of this country.

Reference was also made to the continuous sittings of High Court in Lancashire, the charges of the port committee, cable-grams by telephone, and the signal station at the Fastnet.

A meeting of the committee of the African trade section of the Liverpool Chamber of Commerce was held on the 17th January last, Mr. Alex. Sinclair in the chair.

Further action as to the Lagos railways scheme was postponed till the receipt of a report from the Manchester committee.

A letter was read from the Colonial Office, stating (1) that the duty chargeable on spirits which were in bond at the date of the passing of the Lagos Ordinance No. 5, 1895, is 1s. per gallon, and (2) that by section 2 of the same enactment all drawbacks on spirits exported from the colony since the passing of the Ordinance are abolished, and that there is no exemption in favour of spirits ordered, or at sea, with or without a through bill of lading, or already landed in Lagos.

Mr. J. H. Jackson gave notice that at the next meeting of the committee he would draw the attention of the committee to the subject of the adulteration of produce, especially palm oil and kernels coming from the Oil Rivers, and would move a resolution in favour of the issue of Governmental regulations by the Protectorate authorities dealing with the matter.

There was no other business of importance.

A further meeting of the African trade section was held on the 31st January last, when it was resolved that a letter should be written to the Foreign Office, drawing attention to the condition of affairs in Benin, and asking the Government to interfere.

Nottingham.—A meeting of the council of the Nottingham Chamber of Commerce was held on the 27th January last, Mr. J. Billyeald in the chair.

The following letter was read from the Foreign Office, dated January 13th:—

“I am directed by the Marquis of Salisbury to inform you that, in compliance with the request made in your letter of the 7th inst., his lordship has instructed Her Majesty’s Consuls in China to do what they can to procure information respecting textile goods supplied and required in different parts of China. Any samples which the Consuls may send home will be forwarded to the Chamber of Commerce at Nottingham.”

The first meeting of the new council was held on February 3 last, but beyond the election of officers and a discussion on a motion (which was lost) to suggest the desirability of arbitration in national disputes, no business of importance was done.

Sheffield.—A meeting of the council of the Sheffield Chamber of Commerce was held on the 16th January last, Sir William Leng in the chair.

Letters were read from Messrs. Jonas and Beardshaw, who at the last meeting undertook to report on the adoption of the metric system in Germany and Finland respectively. No difficulty appears to have arisen in introducing the system into these countries, and even in Finland, where the peasantry are perhaps the most conservative part of a very conservative country, six months sufficed to overcome the difficulties in connection with the matter.

On the motion of Alderman F. Brittain, seconded by Mr. Belk, it was resolved that the secretary be instructed to transmit the reports of Messrs. Jonas and Beardshaw to the Right Hon. A. J. Balfour, M.P., with an explanatory letter to be drawn up by the president and secretary.

A letter was read from the India Office, in reply to one from the Sheffield Chamber, requesting that the Government of India would adopt the same procedure as has been initiated by Mr. Chamberlain at the Colonial Office, in order to furnish a comparative return of British and foreign trade with India. The Secretary of State informed the Chamber that he was fully alive to the importance of the matter, and that the Government of India had been directed to give the proposal of the Sheffield Chamber careful, and, if possible, favourable consideration.

There was no other business of public interest.

The annual meeting of the Chamber was held on January 30 last, Sir William Leng, the retiring president, in the chair.

The report was adopted, and speeches were made on the metric system, Companies Act Amendment Bill, prison-made goods and trade with the Colonies, and other topics, by the Rt. Hon. A. J. Mundella, M.P.; Sir Howard Vincent, M.P.; Sir Frederick Mappin, M.P., and others.

Mr. S. E. Howell was elected president for the year.

XIX.—RECENT TRADE BLUE BOOKS.

1. *Statistical Abstract for the Several Colonies and other Possessions of the United Kingdom in each Year from 1880 to 1894. Thirty-second Number.* (C.—7904.) Price 1s.

This is one of the three statistical abstracts prepared and issued annually by the Board of Trade, the other two relating respectively to the United Kingdom and to foreign countries. The present issue of the Colonial Abstract gives information relating to the colonial possessions of the United Kingdom down to the end of 1894. An abstract of the rates of Customs duties on the imports of principal articles on which such duties are levied in each colonial possession is also appended.

2. *Index to Report of Her Majesty's Diplomatic and Consular Representatives abroad on Trade and Subjects of General Interest (and Appendix), 1886-95.* Commercial No. 9 (1895). (C.—7899.) Price 2s. 2d.

In an introductory note to this publication it is stated that the series thus established are divided into two categories, "Annual" and "Miscellaneous." In the former are comprised reports on Trade, Finance, Agriculture, &c., which are annually required from Her Majesty's missions and consulates abroad. In the latter are included reports on subjects of general economic interest, which are either specially called for by the Secretary of State or voluntarily submitted for publication by their authors. Each of these series have their consecutive numbers, which run on from year to year. From the summer of 1886 down to September 5, 1895, when Parliament adjourned, 1,631 reports of the Annual, and 378 of the Miscellaneous series, have been issued in the separate form, and it is to the salient features of the 2,009 reports that the body of the present work is designed to facilitate reference. The system adopted is as follows: The black numeral preceding the letter A. or M. refers to the number of the paper; the letters themselves refer to the series, "Annual" or "Miscellaneous," as the case may be; and the smaller figures following the letter denote the page on which the subject is referred to. From what has been said above, it will be seen that, with certain exceptions, the majority of the reports received from abroad, and which it is deemed expedient to make public, are now issued separately in one of the two series referred to. But it not unfrequently happens, when information from different countries on a specific subject is required, that it is considered preferable to publish all replies collectively, and thus consolidate the information in one book. Examples of such cases as this may be found

in the volume of Reports on Co-operative Societies abroad (No. 20, Coml., 1886), Laws relating to Employers' Liability Abroad, (No. 21, Coml. 1886), and numerous other instances. Such collective information is still published in Blue Books of the commercial series as command papers, and numbered according to the year of issue. The symbols of reference to such papers being somewhat different, all reports so published, together with those on subjects of general commercial interest which appeared prior to the arrangement made in 1886, and already referred to, have been dealt with in Part II., which embraces a period of 14 years, viz., from 1882 to 1895 inclusive.

3. *Boiler Explosions. Report to the Secretary of the Board of Trade upon the working of the Boiler Explosions Acts, 1882 and 1890, with Appendices.* (C.—7856.) Price 3d.

This is an annual return prepared by the Marine Department of the Board of Trade. In the report it is stated that during the 12 months ended 30th June 1895, 87 preliminary inquiries and 27 formal investigations were held under the provisions of the Boiler Explosions Acts, 1882 and 1890. During the 12 months 43 persons were killed and 85 persons injured by boiler explosions. The total number of cases inquired into shows a small increase upon that of the preceding year; and the number of persons killed, as also the number of persons injured, is larger than in any year since the Boiler Explosions Act of 1882 came into force. During last year, however, several exceptionally serious cases were dealt with, more than half the total number of deaths having resulted from four explosions.

In one case, at the Redcar Iron Works, 12 boilers, each about 66 ft. in length and 4 ft. 6 ins. in diameter, were destroyed and thrown broadcast about the works, killing 12 men and injuring eight. Moreover, the year 1894–95 was remarkable for a period of exceptionally severe frost, and one result of this was an unusual number of explosions of boilers used for heating public buildings and places of business. There were 27 cases of this kind dealt with, and with three exceptions they were probably caused by the freezing of water in some part of the apparatus. Leaving out of question the exceptional cases and the considerable number of cases in which the explosions were of a slight character, it will be seen that the figures for the year do not show that there is any increase as regards laxity of management or inspection on the part of steam-users or boiler insurance companies, or that there is any general increase in the number of serious boiler explosions.

Appendices are attached giving particulars of the cases dealt with, classifications of the causes of explosions, and of the types of boilers which exploded, together with statistics of the numbers of persons killed and injured since 1882, and a report from the Board's solicitor on the formal investigations held.

4. *Rule of the Road at Sea. Report of the Committee appointed by the President of the Board of Trade on the Screening of Ships'*

Side Lights, with Minutes of Evidence and Appendices. II. Evidence, Appendices, and Index. (C.—7908—1.) Price 8s. 4d.

This is the second part of the report of the committee appointed on 18th March last to consider and report:—

1. Whether the rule laid down in the Order in Council of January 30, 1893, is in accord with, and calculated to secure compliance with, the requirements of Articles 3 and 15 of the International Regulations for Preventing Collisions at Sea, as to the fixing and screening of the side lights of vessels.

2. Whether it is desirable in the interests of safety, and in order to secure due compliance with the above-named articles, that any, and what, alterations should be made in the instructions issued by the Board of Trade to their surveyors in the matter.

The report itself was referred to on page 105 of the January number of the "Board of Trade Journal." The present publication contains a list of the witnesses who were examined before the Committee, together with the minutes of evidence, an appendix, and index.

5. *Tramway and Gas and Water Orders. Session 1895. Report by the Board of Trade of their Proceedings under the Tramways Act, 1870, and the Gas and Water Works Facilities Act, 1870, during the Session of 1895. (C.—7909.) Price 1d.*

The present return states that the number of applications to the Board of Trade for Provisional Orders under the Tramways Act, 1870, made in December 1894, was eleven, viz.:—

Aldershot and Farnborough (Extension).

Carlisle and District.

Gloucester, city of.

Great Yarmouth.

Hartlepool Electric.

Hartlepoons.

London, Deptford, and Greenwich.

London United.

Newcastle-upon-Tyne Corporation.

Somerton, Keinton-Mandeville, and Castle Cary.

Wigan and District.

All these applications, except those relating to Hartlepoons and Newcastle, included powers for the construction of tramways, the aggregate length being 3 miles 43 chains of double line, and 14 miles 35 chains of single line, and the estimated cost 73,215*l*.

The Hartlepoons application was to sanction a transfer of the undertaking, and to amend former Orders; while the Newcastle application was for authority to work certain existing tramways by cable traction, and to enable the Corporation to borrow 130,000*l*.

The Aldershot, Great Yarmouth, and London, Deptford, and Greenwich applications were not proceeded with by the promoters. The Carlisle application was granted, and a Provisional Order

made ; but, the promoters failing to comply with the provisions of the Tramways Act, 1870, at a subsequent stage, the Order was not submitted to Parliament for confirmation.

Provisional Orders were made in the remaining cases, and laid before Parliament in Confirmation Bills.

With regard to the gas and water Provisional Orders, it is stated that, in December 1894, 11 applications for Provisional Orders under the Gas and Water Works Facilities Act, 1870; were made to the Board of Trade, six relating to gas, and four to water undertakings, and one to a combined gas and water undertaking.

Power was sought in these Orders to raise 127,500*l.* by shares and 32,375*l.* by loan, of which 82,000*l.* share and 21,000*l.* loan capital related to gas undertakings, and 45,500*l.* share and 11,375*l.* loan capital to water undertakings.

The following is a list of the applications :—

<i>Gas.</i>	<i>Water.</i>
Barnstaple.	Holyhead.
Bognor.	Mid Kent.
Felixstowe.	Newton Nottage.
Kildwick Parish.	South Hayling.
Newark.	
Rothwell.	<i>Gas and Water.</i>
	Llanberis.

Five applications for Gas Orders were in respect of undertakings already authorised by Parliament, viz., Barnstaple, Bognor, Kildwick Parish, Newark, and Rothwell. The Barnstaple and Kildwick Parish Gas Companies applied for power to raise additional capital; the Bognor Gas Light and Coke Company (Limited), for power to construct additional works for the manufacture and storage of gas; the Newark Gas Company, for power to raise additional capital, to extend their limits of supply, and to construct and maintain additional works; and the Rothwell Gas Light Company, for power to purchase additional lands, and to extend their limits of supply.

The application for the Felixstowe Gas Order related to an undertaking already existing without parliamentary authority, and sought for such authority to maintain and continue the existing works, and to continue the manufacture and supply of gas.

Objections were lodged against some of the applications, but, with certain modifications and amendments, the Board of Trade granted an order in each case.

6. *Weights and Measures. Report by the Board of Trade on their Proceedings and Business under the Weights and Measures Acts, 1878 and 1889. (432.) Price 5d.*

The report states that in pursuance of section 33 of the Weights and Measures Act, 1878, 41 & 42 Vict. c. 49, the Board of Trade make the following report to Parliament :—

Since the issue by the Board of Trade of their last report (2nd August 1894), the several standards and instruments

mentioned in Appendix No. 1 of the present report have been verified at the Standards Department for official use, or for the use of Governments and authorities abroad.

Copies of the following Board of Trade standards of length, weight, and capacity, deposited at the Royal Irish Constabulary Depôt, Phoenix Park, Dublin, have been duly re-verified this year by comparison with the Imperial standards. These copies are used annually in the verification of the standards provided by the local authorities in Ireland:—

Avoirdupois, standards, from 56 lbs. to $\frac{1}{2}$ dram.

Grain weights, from 4,000 grains to 0·01 grain.

Measures of capacity, from a bushel to a quarter gill.

A yard measure, sub-divided into feet and inches, the inch being divided into decimal, duodecimal, and binary equal parts.

At the annual verification in September last the local standards of Belfast, Cork (West Riding), Drogheda, Galway (county of the town), Leitrim County, Louth (district of Dundalk), Mayo County, Roscommon County, and Westmeath, were duly re-verified at the depôt by the Standards Department.

An Appendix contains a list of districts of local authorities, for which local standards have been verified or re-verified by the Board of Trade, since 2nd August 1894, in accordance with the provisions of sections 37, 40, and 41 of the Act of 1878.

Treaty Series No. 2, 1896. Supplementary Convention between Great Britain and Japan, respecting the Duties to be charged on British Goods Imported into Japan. Price 1½d.

This is the text of the supplementary convention between Great Britain and Japan, respecting the duties to be charged on British goods imported into Japan, to which a tariff is annexed. The convention was signed at Tokio on July 16, and ratifications exchanged on November 21, 1895.

XX.—QUARANTINE NOTICES.

MADAGASCAR.—All vessels arriving at Madagascar and bringing with them bills of health must have such documents viséd by the French Consular authorities at their last port of call.

PORTUGAL.—Centa is reported infected with cholera from January 1. Hong Kong and all ports of the province of Canton are declared infected with bubonic plague.

ITALY.—The following is a translation of a decree of the Italian Ministry of the Interior, relating to maritime sanitary precautions.

No. 9.

Considering the convenience, generally speaking, of exempting ships on arrival at Italian ports, under ordinary circumstances, from presenting their bill of health (*patente di sanità*) when the sanitary information is marked on the ship's papers (*carte di bordo*); and in virtue of the power conferred by the marine sanitary regulations of determining (in concert with the Minister of Marine) what measures are to be taken in such matters; the Minister of the Interior hereby decrees—

That every vessel arriving at Italian ports from the other European ports, with the exception of Turkish ports, are exempted from the presentation of the bill of health (*patente di sanità*); and this exemption applies equally to ships from the Atlantic ports of North America and Canada.

But, in any case, ships arriving from ports which have been either declared infected by the local authorities, or have been proclaimed as such by the Italian Ministry of the Interior, are not included, and will have to be furnished with bills of health.

And every ship included in the terms of the present decree, shall have marked on its list of the crew, or on its certificate, information respecting the sanitary conditions of the place of departure, the hygienic conditions of the ship itself and of its cargo, and the state of health of the crew and of the passengers, both at the moment of starting and at each port touched at. Dated, Rome, November 23, 1895.

TURKEY.—A telegram from Her Majesty's representative at Constantinople reports five days' quarantine with disinfection on all arrivals from coast of Egypt, to be performed at Beyrout, Smyrna, or Tripoli in Africa.

GIBRALTAR.—Fourteen days' quarantine is imposed on arrivals from Mazagan, and seven days' against ports within 165 kilometres.

MALTA.—A telegram has been received from the Governor of Malta reporting the removal of quarantine against Morocco, and the enforcement of 11 days' quarantine on arrivals from Alexandria.

The Board of Trade have received, through the Secretary of State for the Colonies, the following copy of a notice issued by the Governor of Malta, respecting quarantine :

Government Notice.

His Excellency the Governor having heard the opinion of the Board of Health, has been pleased to modify Government Notice, No. 230, of 17th December 1895, and to direct that the following regulations be observed, viz. :—

1. Vessels to stop outside the harbour with permission for coaling under such restrictions as may in each case be directed by the Collector of Customs.

The following shall not be permitted to enter the harbour, but may be allowed to coal and take provisions in strict quarantine, with the least delay, and subject to the orders of the Collector of Customs; and shall in all cases be ordered to quit with all despatch after coaling :—

(a.) Vessels that have on board, or have had within seven days, cases of cholera, or cases of a disease with symptoms resembling those of cholera.

(b.) Vessels with pilgrims from the East.

(c.) Vessels from North or South America when cases of yellow fever have occurred on board during the voyage, unless such vessels have been properly disinfected at an intermediate port in Europe.

(d.) Vessels arriving from Arabian ports in the Red Sea which have not been admitted to free pratique at Suez and Port Said.

2. Quarantine for seven days to be reckoned from the commencement of the voyage, with permission for handling cargo in quarantine.

The following shall be subject to a period of seven days' quarantine, but shall be permitted under the directions of the Collector of Customs, to discharge goods (which are not susceptible of communicating contagion) by means of the vessel's crew on lighters, or to employ for handling cargo local labourers subject to seven days' quarantine, viz. :—

Vessels arriving from any port without a clean bill of health.

Other Precautionary Measures to be adopted when necessary.

Vessels arriving at this port having on board cases of scarlet fever, measles, or small-pox will be subject to such precautionary measures as the head of the Government may deem necessary.

Medical Inspection.

All vessels arriving at Malta shall undergo strict medical inspection.

Passengers.

(a.) Passengers arriving by any vessel subject to quarantine, are required to undergo the same quarantine restrictions as the vessels on which they arrive.

(b.) Passengers arriving on vessels having a duly qualified medical officer on board, are to be allowed to land without medical inspection, provided that the medical officer in charge on board shall declare on oath that during the voyage there has not been on board a case of cholera, or of any disease with symptoms resembling those of cholera either among the passengers or the crew. The declaration above referred to shall be countersigned by the master of the vessel.

Goods.

(a.) The importation of cotton seed from any port subject to quarantine is forbidden.

(b.) The importation of rags is prohibited.

(c.) The importation before disinfection of soiled wearing apparel, or bedding material that is not new, is prohibited.

(d.) The importation of vines, vine shoots, fruit packed in vine leaves, or roots from any port of the Mediterranean is prohibited.

(e.) The importation of grapes, poles, or supports used in the cultivation of vines and plants, vegetable earth, vegetable or mixed manure, is prohibited unless the importation is accompanied by a sworn declaration, made by the shipper before the British Consular authority of the place of its origin, to the effect that the shipment does not contain vine plants, or cuttings, or that its contents come from nurseries, hothouses, fields, or gardens, which are at least 100 yards distant from the nearest vines or vineyards, and that moreover the *Phylloxera* is not known to exist at the place of origin.

(f.) Provided, however, that the Collector of Customs may grant permission for the importation of plants and vegetable produce under proper restrictions when application has been made and the precautions necessary are established before the shipment leaves the place of origin.

CYPRUS.—Five days' quarantine on arrivals from Egypt direct.

XXI.—POST OFFICE NOTICES.

MAILS FOR THE WEST AND SOUTH-WEST COASTS OF AFRICA.

The following table shows the places for which mails will be despatched by the mail packets leaving Liverpool for the west and south-west coasts of Africa during the latter part of the month of February 1896.

Wednesday, 19th February.—Sierra Leone, Sherbro, Accra, Lagos, Fernando Po, Rio del Rey, Cameroons, Batanga, Eloby, Gaboon, Mayumba, Loango, Landana, N'Gove, Nyang, Cape Lopez, Fernan Vaz, Sette Cama, Black Point, Cabenda, Banana, Boma (Congo), Muculla, Ambrizette, Mussera, Kinsembo, Ambriz Loanda, Victoria, and Bibundi.

Saturday, 22nd February.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Benin, Sapelli, Warree, Brass, and Akassa.

Wednesday, 26th February.—Madeira, Teneriffe, Goree, Dakar, Rufisque, Bathurst, Isles do Los, Conakry, *Sierra Leone, *Sherbro, Monrovia, Sinoe, Grand Bassa, Kroo Coast, Half Jack, Lahou, Cape Lahou, Grand Lahou, Drewin, Grand Bassam, Assinie, Axim, *Cape Coast Castle, Elmina, Salt Pond, *Accra, Pram Pram, Addah, Quitta, Jellah Coffee, Lome, Little Popo, Grand Popo, New Calabar, Abonema, Bakana, and Degama.

Saturday, 29th February.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Bonny, Old Calabar, Opobo, Benin, Warree, New Calabar, Bakana, and Brass.

In London the correspondence should be posted on the previous day in each case.

Correspondence for Senegal (Goree, Dakar, Rufisque), the Congo (Banana, Boma) and Portuguese possessions on the west and south-west coasts of Africa is not sent viâ Liverpool unless specially so superscribed.

These vessels carry parcel mails for Bathurst, Sierra Leone, Monrovia, Axim, Cape Coast Castle, Salt Pond, Winnebah, Accra, Addah, Quitta, Lagos, Benin, Bonny, Brass, New Calabar, Old Calabar, Opobo, and Warree.

The parcel mails are made up at Liverpool on the afternoon of the day before the departure of each vessel.

* Correspondence for Sierra Leone, Sherbro, Cape Coast Castle, and Accra will not be sent by the steamer of the 26th of February unless specially so superscribed, as the direct Packet leaving on the following Saturday is due at those ports some days earlier.

XXII.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month ended 31st January 1896, compared with the corresponding Month of the year 1895.

MONTH ENDED 31ST JANUARY.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 31st January		Increase.	Decrease.
	1896.	1895.		
I. Animals, living (for food) - -	£ 770,337	£ 474,727	£ 296,110	—
II. (A.) Articles of food and drink, duty free - - - -	12,512,868	11,872,138	640,670	—
(B.) Articles of food and drink, dutiable - - - -	1,892,900	1,950,708	—	57,808
Tobacco, dutiable - - - -	316,862	227,524	89,338	—
III. Metals - - - -	1,681,443	1,600,662	80,781	—
IV. Chemicals, dyestuffs, and tanning substances - - - -	816,472	600,533	215,889	—
V. Oils - - - -	823,163	676,876	146,287	—
VI. Raw materials for textile manufactures	9,001,136	9,623,233	—	624,047
VII. Raw materials for sundry industries and manufactures - - - -	3,189,712	2,693,507	496,205	—
VIII. Manufactured articles - - - -	6,087,692	5,683,915	403,777	—
IX. (A.) Miscellaneous articles - - - -	1,258,115	1,203,847	49,268	—
(B.) Parcel post - - - -	122,666	123,764	—	6,098
Total Value - - - -	38,473,856	36,743,481	1,730,375	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - - -	54,685	39,114	15,571	—
II. Articles of food and drink - - - -	876,805	731,396	145,409	—
III. Raw materials - - - -	1,531,702	1,308,503	223,199	—
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - -	10,085,943	9,240,205	845,738	—
(B.) Metals and articles manufactured therefrom (except machinery) -	2,558,834	2,033,598	525,236	—
(C.) Machinery and millwork - -	1,336,415	1,121,417	214,998	—
(D.) Apparel and articles of personal use	990,839	782,332	208,007	—
(E.) Chemicals and chemical and medicinal preparations - - - -	761,859	662,667	99,192	—
(F.) All other articles, either manufactured or partly manufactured -	2,813,085	2,220,621	592,464	—
(G.) Parcel post - - - -	117,001	83,333	33,118	—
Total Value - - - -	21,127,163	18,224,236	2,902,932	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - - -	4,825,707	3,887,258	938,449	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 4th January 1896 -	10,848 6	56,290 6	8,809 2
„ 11th „ „ -	23,524 2	99,021 0	18,088 0
„ 18th „ „ -	31,809 2	124,130 2	23,601 2
„ 25th „ „ -	34,186 1	110,151 5	23,898 5
January 1896 - - -	100,368 3	389,593 5	74,397 1
Corresponding month in 1895 -	177,550 7	427,829 5	70,710 6
„ „ 1894 -	198,909 3	321,159 7	63,205 0
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 4th January 1896 -	25 2	24 7	13 10
„ 11th „ „ -	25 4	23 11	13 9
„ 18th „ „ -	25 10	23 6	13 11
„ 25th „ „ -	26 1	23 7	13 10
January 1896 - - -	25 7	23 10	13 10
December 1895 - - -	24 11	23 11	13 11
November „ - - -	25 9	25 1	14 2
October „ - - -	24 6	25 5	13 5
September „ - - -	22 10	24 0	13 7
August „ - - -	24 2	20 3	15 7
July „ - - -	24 9	19 8	16 4
June „ - - -	25 9	19 7	16 0
May „ - - -	22 5	20 5	15 2
April „ - - -	20 5	21 0	14 2
March „ - - -	19 11	21 3	13 10
February „ - - -	20 0	21 9	13 8
January 1895 - - -	20 7	21 6	13 11
„ 1894 - - -	26 3	25 11	17 11
„ 1893 - - -	26 3	25 4	16 11
„ 1892 - - -	35 4	29 2	20 6

III.—EMIGRATION.—JANUARY 1896.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out of EUROPE during the Month ended 31st January 1896, compared with the corresponding Month of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.
Month ended 31st January 1896.						
English - - - -	2,089	313	491	1,436	1,228	5,557
Scotch - - - -	331	31	30	125	114	631
Irish - - - -	637	31	18	16	23	730
Total of British origin -	3,107	375	539	1,577	1,370	6,968
Foreigners - - -	2,053	214	1	528	134	2,960
Nationalities not distinguished	1	—	—	—	261	262
Total - - - -	5,191	589	540	2,105	1,765	10,190
Month ended 31st January 1895.						
English - - - -	2,863	390	742	822	863	5,675
Scotch - - - -	247	36	60	64	75	432
Irish - - - -	744	21	33	14	30	842
Total of British origin -	3,854	437	835	900	973	6,999
Foreigners - - -	2,189	99	18	291	106	2,703
Nationalities not distinguished	7	6	—	3	230	246
Total - - - -	6,050	542	853	1,194	1,309	9,948

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION—JANUARY 1896.

STATEMENT showing the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM, during the Month of January 1896, compared with the corresponding Period of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.
Month ended 31st January.												
Aliens not stated to be en route to America: *												
Arrived at London -	616	444	123	167	82	112	—	—	127	36	948	1,729
„ Grimsby -	77	39	35	11	3	2	—	—	24	19	139	71
„ Hull -	129	29	34	87	27	52	—	—	121	87	302	255
„ Tyne Ports -	16	11	22	18	111	128	—	—	127	109	276	266
„ Leith -	32	25	21	20	7	—	—	—	15	4	75	49
„ Newhaven -	—	—	—	—	—	—	412	405	—	—	412	406
„ Other ports -	50	84	85	16	63	129	—	—	144	99	342	323
Total -	911	692	320	319	293	423	412	406	558	354	12,494	12,104
Aliens en route to America :												
Arrived at Grimsby -	2	37	29	64	137	80	—	—	15	25	183	206
„ Hartlepool, West -	139	65	—	—	—	—	—	—	—	—	139	65
„ Hull -	105	27	6	4	183	198	—	—	442	596	736	825
„ Leith -	73	122	4	51	2	—	—	—	11	5	90	178
„ Other Ports -	4	—	—	3	—	—	—	—	79	59	83	62
Total -	323	251	39	122	322	278	—	—	547	685	1,231	1,336
Total of Aliens en route and of aliens not stated to be en route to America * -	1,234	853	359	441	615	701	412	406	1,105	1,039	3,725	3,440

* The distinction made in this Return between "Aliens not stated to be en route to America," and "Aliens en route to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be en route to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 213, Sess. 1895, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Goole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers, and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the Aliens who arrived at ports in the United Kingdom not en route to America in the month of January 1896 was 645 and 630 in the same month of 1895.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER of RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	January	
	1896.	1895.
Total gazetted	No. 384	No. 383
Number gazetted in principal trades and occupations:—		
Grocers, &c.	37	29
Publicans and hotel keepers, &c.	33	31
Builders	24	26
Farmers	23	23
Greengrocers, fruiterers, &c.	13	7
Butchers	10	7
Decorators, painters, plumbers, &c.	9	9
Bakers	8	13
Confectioners	8	2
Solicitors	7	2
Tailors, &c.	6	8
Boot and shoe manufacturers and dealers	6	7
Clerks, commercial and general	6	5
Carpenters and joiners	6	2
Travellers, commercial, &c.	5	6
Auctioneers	5	5
Clothiers, outfitters, &c.	5	3
Agents, commission and general	4	7
Drapers, haberdashers, &c.	4	1
Merchants	4	1
Restaurant, coffee, and eating-house keepers	4	1
Schoolmasters and schoolmistresses	4	1
Printers and publishers	4	1
Millers	4	1
Fishmongers, poulterers, &c.	3	6
Engineers and founders, &c.	3	5
Carriers, carmen, lightermen, and hauliers	3	5
Provision merchants, &c.	3	4
Ironmongers	3	4
Lodging-house keepers	3	4
Wine and spirit merchants	3	3
Gardeners, florists, &c.	3	3
Architects and surveyors	3	1
Coal and coke merchants and dealers	2	10
Saddlers and harness makers	2	4
Jewellers, watchmakers, importers, silversmiths, &c.	2	3
Cattle and pig dealers	2	3
Tobacconists, &c.	2	2
Furniture dealers and makers	2	2
Carriage, &c. builders	2	2
Hair-dressers	2	2
Contractors	2	2
Milliners, dressmakers, &c.	2	1
Dairymen, cowkeepers, &c.	2	—
Clerks in holy orders	1	4

	January	
	1896.	1895.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.
Wheelwrights - - - - -	1	4
China, glass, and earthenware, &c. dealers - -	1	4
Fishing net, smack owners and masters - -	1	3
Corn, flour, seed, hay, and straw merchants and dealers - - - - -	1	2
Cabinet-makers and upholsterers - - - - -	1	2
Brokers, stock and share - - - - -	1	2
Officers in Army - - - - -	1	1
Chemists, druggists, and chemical manufacturers	1	1
Blacksmiths, farriers, &c. - - - - -	1	1
Timber merchants and wood dealers - - - - -	1	—
Directors and promoters of public companies -	—	3
Stone, marble, and monumental masons - - -	—	3
General dealers - - - - -	—	2
Hosiers, glovers, &c. - - - - -	—	2
Photographers - - - - -	—	2
Stationers - - - - -	—	1
Curriers, tanners, and leather merchants -	—	1

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 31st January 1896, compared with the corresponding Month of the Year 1895.

	January	
	1896.	1895.
Cattle - - - - -	39,101	39,230
Sheep - - - - -	11,016	16,100
Swine - - - - -	68,292	75,929
Goats - - - - -	9	3
Horses - - - - -	1,658	945
Mules or Jennets - - - - -	3	—
Asses - - - - -	4	3
Total - - - - -	120,083	132,210

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month of January 1896, and the corresponding Month of the previous Year.

	January 1896.				Corresponding Month of the previous Year.
	East Coast.	South Coast.	West Coast.	Total.	
QUANTITY.					
Brill - - - - -	Cwts. 1,132	Cwts. 419	Cwts. 178	Cwts. 1,729	Cwts. 1,660
Soles - - - - -	6,382	1,242	483	8,107	5,749
Turbot - - - - -	5,038	823	775	6,636	5,354
Prime fish, not separately distinguished - - - - -	1,649	672	—	2,321	1,766
Total prime fish - - -	14,201	3,156	1,436	18,793	14,529
Cod - - - - -	37,800	1,879	5,059	44,738	36,873
Haddock - - - - -	209,059	—	3,639	212,698	169,499
Hake - - - - -	331	212	1,194	1,737	2,473
Halibut - - - - -	1,598	—	—	1,598	1,531
Herrings - - - - -	173	20,764	294	21,231	4,500
Ling - - - - -	4,795	994	2,546	8,335	7,267
Mackerel - - - - -	—	1,063	—	1,063	167
Pilchards - - - - -	—	4,295	—	4,295	279
Plaice - - - - -	43,562	6,324	4,082	53,968	44,794
Sprats - - - - -	29,384	12,700	31	42,115	14,434
Fish not separately distinguished, except shell fish - - - - -	59,244	26,728	10,300	96,272	81,364
Total - - - - -	400,147	78,115	28,581	506,843	377,510
Shell fish:—	No.	No.	No.	No.	No.
Crabs - - - - -	71,727	16,098	928	88,753	31,441
Lobsters - - - - -	323	11,421	344	12,088	10,360
Oysters - - - - -	1,148,000	548,000	117,000	1,813,000	999,000
Other shell fish - - - - -	Cwts. 33,132	Cwts. 5,308	Cwts. 8,867	Cwts. 50,307	Cwts. 29,456
VALUE.					
Brill - - - - -	£ 3,954	£ 937	£ 422	£ 5,313	£ 5,300
Soles - - - - -	40,708	7,251	2,745	50,704	46,876
Turbot - - - - -	22,876	3,023	2,698	28,597	23,647
Prime fish, not separately distinguished - - - - -	5,203	2,626	—	7,829	6,607
Total prime fish - - -	72,741	13,837	5,865	92,443	84,430
Cod - - - - -	27,557	1,304	2,884	31,745	31,663
Haddock - - - - -	124,901	—	2,099	127,000	102,126
Hake - - - - -	431	245	1,193	1,869	2,481
Halibut - - - - -	4,449	—	—	4,449	4,875
Herrings - - - - -	51	3,795	142	3,988	1,709
Ling - - - - -	4,080	665	1,585	6,330	5,504
Mackerel - - - - -	—	1,145	—	1,145	362
Pilchards - - - - -	—	1,303	—	1,303	65
Plaice - - - - -	61,002	7,251	4,316	72,572	60,649
Sprats - - - - -	2,486	3,865	14	6,365	1,889
Fish not separately distinguished, except shell fish - - - - -	40,631	14,359	7,233	62,223	55,679
Total - - - - -	338,329	47,774	25,331	411,434	352,892
Shell fish:—					
Crabs - - - - -	460	482	7	889	442
Lobsters - - - - -	18	505	16	539	497
Oysters - - - - -	4,903	1,331	293	6,527	3,469
Other shell fish - - - - -	7,751	1,618	2,398	11,767	6,311
Total - - - - -	16,072	3,936	2,714	19,722	10,719
Total value of fish landed	351,401	51,710	28,045	431,156	353,111

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month of January 1896, and the corresponding Month of the previous Year.

	January 1896.				Corresponding Month of the previous Year.
	East Coast.	Orkney and Shetland.	West Coast.	Total.	
QUANTITY.					
	Cwts.	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles) - - -	1,083	—	6	1,089	965
Turbot - - - - -	211	—	23	234	165
Cod - - - - -	46,751	1,372	2,270	44,393	39,214
Eel - - - - -	308	3	857	1,168	1,576
Flounder, Plaice, Brill - - -	10,567	—	1,282	11,789	5,868
Haddock - - - - -	124,726	5,315	4,758	134,799	83,190
Halibut - - - - -	308	11	35	354	405
Herrings - - - - -	16,833	21	14,969	31,823	39,448
Ling - - - - -	2,938	150	1,198	4,286	4,836
Mackerel - - - - -	—	—	2	2	2
Saith (Coal Fish) - - - - -	1,846	45	2,564	4,455	5,593
Skate - - - - -	3,366	18	822	4,206	2,455
Sparling - - - - -	39	—	1	40	2
Sprats - - - - -	29,598	—	—	29,598	301
Torsk (Tusk) - - - - -	57	—	10	67	51
Whiting - - - - -	3,435	—	1,129	4,564	2,185
Fish not separately distinguished, except shell fish - - -	3,179	144	409	3,732	3,916
Total - - - - -	239,185	7,079	30,335	276,599	150,177
Shell fish:—	No.	No.	No.	No.	No.
Crabs - - - - -	193,775	—	4,180	197,955	84,308
Lobsters - - - - -	2,411	700	49,441	52,552	25,219
Oysters - - - - -	1,800	—	37,300	39,100	27,900
	Cwts.	Cwts.	Cwts.	Cwts.	Cwts.
Clams - - - - -	2,753	—	10	2,763	2,505
Mussels - - - - -	16,740	770	11,390	28,900	23,351
Other shell fish - - - - -	1,742	270	3,010	5,022	2,957
VALUE.					
	£	£	£	£	£
Soles (Lemon Soles) - - -	2,612	—	7	2,619	2,628
Turbot - - - - -	793	—	26	819	751
Cod - - - - -	16,400	313	1,068	17,811	19,478
Eel - - - - -	168	1	381	550	787
Flounder, Plaice, Brill - - -	12,188	—	878	13,066	7,615
Haddock - - - - -	45,401	1,278	1,535	48,274	42,879
Halibut - - - - -	573	11	23	607	844
Herrings - - - - -	4,167	7	4,693	8,867	11,095
Ling - - - - -	1,246	33	380	1,659	2,020
Mackerel - - - - -	—	—	2	2	2
Saith (Coal Fish) - - - - -	323	6	303	632	856
Skate - - - - -	1,222	3	127	1,352	799
Sparling - - - - -	93	—	5	98	11
Sprats - - - - -	1,739	—	—	1,739	31
Torsk (Tusk) - - - - -	12	—	2	14	14
Whiting - - - - -	1,139	—	640	1,779	1,833
Fish not separately distinguished, except shell fish - - -	1,038	22	78	1,138	999
Total - - - - -	89,114	1,704	10,208	101,026	92,042
Shell fish:—					
Crabs - - - - -	679	—	17	696	344
Lobsters - - - - -	131	35	2,093	2,259	1,182
Oysters - - - - -	9	—	149	158	129
Clams - - - - -	389	—	3	383	351
Mussels - - - - -	1,298	40	888	2,226	1,249
Other shell fish - - - - -	312	28	700	1,040	607
Total - - - - -	2,809	103	3,850	6,762	3,865
Total value of fish landed	91,923	1,807	14,058	107,788	95,907

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month of January 1896, and the corresponding Month of the previous Year.

		January 1896.					Corresponding Month of the previous Year.
		North Coast.	East Coast.	South Coast.	West Coast.	Total.	
QUANTITY.							
		Cwts.	Cwts.	Cwts.	Cwts.	Cwts.	Cwts.
Soles	- - - - -	—	36	22	10	63	112
Turbot	- - - - -	—	33	12	15	65	67
Total prime fish		—	74	34	25	133	179
Cod	- - - - -	1,449	2,202	409	1,332	5,392	4,109
Haddock	- - - - -	529	3,221	55	909	4,714	2,507
Hake	- - - - -	—	84	4	75	163	336
Herrings	- - - - -	622	—	3	238	863	2,985
Ling	- - - - -	127	380	346	839	1,692	1,822
Mackerel	- - - - -	—	—	11	319	330	7,713
Sprats	- - - - -	—	—	—	—	—	—
Whiting	- - - - -	30	543	33	954	1,560	980
Fish not separately distinguished, except shell fish	- - - - -	333	1,223	439	440	2,435	1,888
Total		3,090	7,727	1,334	5,131	17,282	22,569
Shell fish:—		No.	No.	No.	No.	No.	No.
Crabs	- - - - -	2,200	57	—	—	2,257	2,100
Lobsters	- - - - -	2,652	812	—	654	4,118	2,157
Oysters	- - - - -	—	35,900	5,500	—	41,400	65,050
Other shell fish		Cwts.	Cwts.	Cwts.	Cwts.	Cwts.	Cwts.
	- - - - -	—	44	338	1,200	1,582	927
VALUE.							
		£	£	£	£	£	£
Soles	- - - - -	—	136	75	76	287	496
Turbot	- - - - -	—	140	47	86	273	234
Total prime fish		—	276	122	162	560	730
Cod	- - - - -	378	1,181	221	564	2,344	2,004
Haddock	- - - - -	358	1,890	51	368	2,657	1,634
Hake	- - - - -	—	55	4	32	91	178
Herrings	- - - - -	88	—	1	50	139	594
Ling	- - - - -	51	141	199	241	632	537
Mackerel	- - - - -	—	—	3	138	141	3,450
Sprats	- - - - -	—	—	—	—	—	—
Whiting	- - - - -	11	401	16	347	775	498
Fish not separately distinguished, except shell fish	- - - - -	184	564	388	272	1,408	903
Total		1,070	4,498	1,005	2,174	8,747	10,228
Shell fish:—							
Crabs	- - - - -	14	1	—	—	15	13
Lobsters	- - - - -	103	43	—	19	165	75
Oysters	- - - - -	—	67	17	—	84	86
Other shell fish	- - - - -	—	26	65	192	283	159
Total		117	137	82	211	547	333
Total value of fish landed		1,187	4,635	1,087	2,385	9,294	10,561

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.—COTTON RETURNS.—JANUARY 1896.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month ended January 1896, compared with the corresponding Month of the Year 1895.

					Month of January	
					1896.	1895.
					IMPORTS.	
					No.	No.
American	-	-	-	-	293,801	675,573
Brazilian	-	-	-	-	10,382	—
East Indian	-	-	-	-	9,260	1,302
Egyptian	-	-	-	-	66,683	52,576
Miscellaneous	-	-	-	-	1,514	6,253
Total	-	-	-	-	381,640	733,704
					EXPORTS.	
American	-	-	-	-	17,383	26,955
Brazilian	-	-	-	-	764	253
East Indian	-	-	-	-	3,951	2,325
Egyptian	-	-	-	-	17,316	13,112
Miscellaneous	-	-	-	-	3,164	2,586
Total	-	-	-	-	42,578	45,231
					FORWARDED from PORTS to INLAND TOWNS.	
American	-	-	-	-	243,457	262,056
Brazilian	-	-	-	-	7,006	1,172
East Indian	-	-	-	-	2,771	2,451
Egyptian	-	-	-	-	38,929	33,304
Miscellaneous	-	-	-	-	10,499	12,720
Total	-	-	-	-	302,662	311,703
					FORWARDED from INLAND TOWNS to PORTS.	
American	-	-	-	-	99	—
Brazilian	-	-	-	-	7	—
East Indian	-	-	-	-	3	—
Egyptian	-	-	-	-	—	—
Miscellaneous	-	-	-	-	94	11
Total	-	-	-	-	203	11

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note.—Rouble = 2s.; Franc = 9 $\frac{6}{10}$ d.; Milreis = 4s. 6d.; Lira = 9 $\frac{6}{10}$ d.; Gulden = 1s. 8d.; Drachma = 9 $\frac{6}{10}$ d.; Lew = 9 $\frac{6}{10}$ d.; L. Egyptian = 1l. 0s. 6 $\frac{1}{4}$ d.; Dollar = 4s. 2d.

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe -	Sept. -	Roubles 47,659,000	47,099,000	370,022,000	380,032,000
France -	Dec. -	Frs. - 364,891,000	329,565,000	3,698,742,000	3,850,445,000
Portugal -	June -	Milreis - 2,827,000	2,673,000	19,668,000	19,571,000
Italy -	Nov. -	Lire - 117,207,000	102,595,000	1,087,168,000	972,729,000
Austria-Hungary -	Dec. -	Gulden - 63,317,000	64,219,000	727,437,000	700,079,000
Greece -	Oct. -	Drchms. 11,287,000	9,502,000	86,605,000	87,522,000
Bulgaria -	Oct. -	Lew - 7,235,000	8,369,000	56,788,000	81,135,000
Egypt -	Sept. -	L. Egypt. 623,000	718,000	5,647,000	6,621,000
United States -	Nov. -	Dollars - 63,345,000	50,567,000	739,417,000	614,178,000
Mexico* -	Aug. -	„ 2,995,000	2,539,000	5,506,000	4,732,000
British India† -	Nov. -	Rupees 6,28,14,756	6,60,29,140	45,01,61,913	45,29,33,018

II.—EXPORTS.

Russia in Europe -	Sept. -	Roubles 65,925,000	62,569,000	516,786,000	482,548,000
France -	Dec. -	Frs. - 328,499,000	342,889,000	3,387,851,000	3,078,145,000
Portugal -	June -	Milreis - 2,196,000	1,800,000	12,717,000	11,234,000
Italy -	Nov. -	Lire - 96,085,000	88,306,000	913,099,000	933,791,000
Austria-Hungary -	Dec. -	Gulden 60,046,000	66,322,000	712,481,000	794,910,000
Greece -	Oct. -	Drchms. 7,513,000	9,051,000	57,574,000	63,901,000
Bulgaria -	Oct. -	Lew - 11,569,000	6,541,000	61,118,000	59,178,000
Egypt -	Sept. -	L. Egypt. - 374,000	406,000	6,738,000	8,164,000
United States -	Nov. -	Dollars - 87,247,000	79,954,000	732,265,000	740,225,000
Mexico* -	Aug. -	„ 7,651,000	6,415,000	14,381,000	13,948,000
British India† -	Nov. -	Rupees 8,93,97,996	7,62,51,305	70,44,41,315	69,73,56,458

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the “special” imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are “general.” “Special” means, in the case of imports, imports for home consumption; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Eight Months ended 30th November		Eight Months ended 30th November	
	1895.	1894.	1895.	1894.
	R.	R.	R.	R.
I.—Animals—living - - -	25,62,886	23,23,677	9,19,606	7,14,252
II.—Articles of food and drink -	5,46,68,770	5,16,97,183	19,62,37,432	17,82,25,859
III.—Metals and Manufactures of—				
A.—Hardware and Cutlery (including plated-ware)	92,35,932	84,35,702	1,10,529	68,817
B.—Metals - - -	3,99,61,883	2,98,26,343	5,73,181	4,92,704
C.—Machinery and mill-work	1,92,81,525	1,52,37,466	400	2,435
D.—Railway plant and rolling-stock (other than Government stores) -	1,12,36,407	1,16,10,213	—	—
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	1,35,96,377	1,17,77,331	8,05,84,707	8,18,77,738
V.—Oils - - -	2,20,95,779	1,10,34,708	50,70,545	49,50,405
VI.—Raw materials and unmanufactured articles - -	3,28,93,791	2,97,19,768	25,65,72,676	27,77,68,565
VII.—Articles manufactured and partly manufactured—				
A.—Yarns and textile fabrics	19,24,27,339	24,64,19,179	8,90,85,671	8,18,56,744
B.—Apparel - - -	1,07,72,247	96,38,564	13,56,300	9,79,622
C.—Other articles manufactured - - -	4,14,28,957	3,51,90,887	4,50,64,554	3,76,31,332
TOTAL -	45,01,61,943	46,29,33,018	67,55,75,601	66,45,68,573

* Exports of Indian Produce and Manufacture only.

XXIII.—LIST OF DIPLOMATIC AND CONSULAR
REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1895, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.	No.	Place.	Price.	No.	Place.	Price.
1493	Rio Grande do Sul	1½d.	1549	Corunna -	5d.	1605	Shanghai -	2½d.
1494	Serajevo -	1d.	1550	Leghorne -	2d.	1606	Canton -	1½d.
1495	Reunes Ayres -	9d.	1551	New Orleans -	2½d.	1607	Mesned -	1½d.
1496	Florence -	2d.	1552	Syra -	1½d.	1608	Erzeroum -	1d.
1497	Lisbon -	1½d.	1553	Genoa -	9d.	1609	Galatz -	2d.
1498	Paris -	2d.	1554	Berlin -	2½d.	1610	Port Said -	1½d.
1499	Lima -	1½d.	1555	Tainan -	1d.	1611	The Hague -	1½d.
1500	Patras -	1½d.	1556	Zanzibar -	4d.	1612	Calais -	1d.
1501	Bordeaux -	3d.	1557	Dunkirk -	1d.	1613	Newchwang -	1d.
1502	Madrid -	2d.	1558	Ningpo -	1d.	1614	Copenhagen -	1d.
1503	Amsterdam -	1d.	1559	Pukhoi -	1d.	1615	Odessa -	2d.
1504	Snakim -	1d.	1560	Chinkiang -	1d.	1616	Gothenburg -	2d.
1505	Angora -	1d.	1561	New York -	2d.	1617	Mannheim -	1½d.
1506	Havre -	2½d.	1562	Batoum -	11½d.	1618	Old Calabar -	5d.
1507	Algiers -	11d.	1563	Naples -	2d.	1618	Pekin -	2½d.
1508	La Rochelle -	3d.	1564	Resht -	4½d.	1620	Taganrog -	2d.
1509	Vera Cruz -	2d.	1565	Copenhagen -	1½d.	1621	Brindisi -	2½d.
1510	Puerto Cortez -	1d.	1566	Porto Rico -	1½d.	1622	Jeddah -	1½d.
1511	Taganrog -	1d.	1567	Malaga -	3d.	1623	Hamburg -	3d.
1512	Baltimore -	1½d.	1568	Galveston -	7½d.	1624	Angora -	1½d.
1513	Mexico -	1½d.	1569	Tabreez -	½d.	1625	Buda-Pesth -	1½d.
1514	Zaila -	1d.	1570	Fiji -	1d.	1626	Beyrout -	1d.
1515	Zomba -	3½d.	1571	Athens -	2½d.	1627	Bushire -	2d.
1516	Valparaiso -	2½d.	1572	Bilbao -	3½d.	1628	Stettin -	2½d.
1517	Charleston -	2½d.	1573	Barcelona -	3d.	1629	Porto Rico -	1d.
1518	Serajevo -	1d.	1574	Piume -	2d.	1630	Rotterdam -	¾d.
1519	Saigon -	1d.	1575	Tahiti -	1d.	1631	Alexandria -	1½d.
1520	Bangkok -	2d.	1576	San Francisco -	6d.	1632	Tokio -	2½d.
1521	Tripoli -	1d.	1577	Ichang -	1½d.	1633	Tangier -	1½d.
1522	Batavia -	1½d.	1578	Amoy -	1½d.	1634	Oporto -	1½d.
1523	Dakar -	½d.	1579	Wenebow -	1d.	1635	St. Petersburg -	4d.
1524	Havana -	2d.	1580	Smyrna -	2½d.	1636	Dantzic -	2d.
1525	Riga -	2d.	1581	Nice -	1½d.	1637	Macao -	1d.
1526	Trebizond -	1½d.	1582	Sul -	1½d.	1638	Hugo and Osaka -	6d.
1527	Piræus -	2½d.	1583	Rio Grande do Sul -	4d.	1639	Naples -	1½d.
1528	Guayaquil -	1½d.	1584	Nagasaki -	1d.	1640	Kiungchow -	½d.
1529	Marseilles -	1½d.	1585	Hakodate -	1d.	1641	Rome -	1½d.
1530	Wuhu -	1½d.	1586	Frankfort -	3d.	1642	Beira -	½d.
1531	Rio de Janeiro -	2½d.	1587	Samoa -	1d.	1643	St. Jago de Cuba -	4½d.
1532	Trieste -	2d.	1588	Cherbourg -	1½d.	1644	Christiania -	6d.
1533	Brest -	1½d.	1589	Damascus -	1d.	1645	Lisbon -	1½d.
1534	Stockholm -	2d.	1590	New York -	1d.	1646	Brussels -	½d.
1535	Warsaw -	1d.	1591	Athens -	2d.	1647	Vera Cruz -	½d.
1536	Boston -	1½d.	1592	Baghdad -	1d.	1648	Tunis -	1d.
1537	Mozambique -	2½d.	1593	Vienna -	1½d.	1649	Antwerp -	1d.
1538	Callao -	1d.	1594	Montevideo -	2½d.	1650	Tokio -	1d.
1539	Aleppo -	1½d.	1595	Swatow -	1½d.	1651	Hawaiian Islands -	1½d.
1540	Jaffa -	1d.	1596	Poochow -	1d.	1652	Stettin -	1½d.
1541	Boston -	½d.	1597	Tamsui -	1d.	1653	Bangkok -	1d.
1542	New Orleans -	13d.	1598	Chungking -	1d.	1654	Batoum -	1½d.
1543	Chicago -	½d.	1599	Chefoo -	1d.	1655	Mexico -	9½d.
1544	Palermo -	2½d.	1600	Tokio -	1½d.	1656	Odessa -	1½d.
1545	Bengazi -	1½d.	1601	Bangkok -	1d.	1657	Réunion -	1d.
1546	Cagliari -	1d.	1602	Caracas -	1½d.	1658	Tokio -	1½d.
1547	Pernambuco -	7½d.	1603	Sofia -	2½d.	1659	Maranham -	1d.
1548	Madrid -	1½d.	1604	Belgrade -	2½d.	1660	Copenhagen -	1d.

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources:—

No.		Price.
350	<i>Servia</i> .—Report on the Mines of Servia - - -	2d.
351	<i>Denmark</i> .—Further Report on the Free Port of Copenhagen -	11½d.
352	<i>Italy</i> .—Report on the Volterra Alabaster Industry - - -	7½d.
353	<i>Italy</i> .—Report for the Years 1892–93 on the Yield of Cocoons in Italy and on the condition of the Italian Silk Trade to May 1894 - - -	1d.

LIST OF DIPLOMATIC AND CONSULAR REPORTS—*cont.*

No.		Price.
354	<i>Austria-Hungary</i> .—Report on the Mining Industry in Bosnia and Herzegovina - - -	1 <i>d.</i>
355	<i>Russia</i> .—Report on the Peasantry and Peasant Holdings in Poland - - -	2½ <i>d.</i>
356	<i>Russia</i> .—Report on the Agricultural Position of Russia - - -	1 <i>d.</i>
357	<i>Siam</i> .—Report on the Teak Trade in Siam - - -	4 <i>d.</i>
358	<i>Paraguay</i> .—Report on the New Australia Colony in Paraguay - - -	1½ <i>d.</i>
359	<i>Bavaria and Wurtemberg</i> .—Abstract of the Reports for the Year 1894 of the Bavarian and Wurtemberg Factory Inspectors - - -	½ <i>d.</i>
360	<i>United States</i> .—Report on the Agricultural Condition of the United States - - -	2 <i>d.</i>
361	<i>Germany</i> .—Report on the Agricultural Position of Germany - - -	4½ <i>d.</i>
362	<i>France</i> .—Report on the Tramway System of Paris and the Department of the Seine - - -	2 <i>d.</i>
363	<i>France</i> .—Report on the Use of the "Bouillie Bordelaise" in connection with Potato and Vine Diseases - - -	⅓ <i>d.</i>
364	<i>Italy</i> .—Report on the Italian Cotton Industry - - -	2½ <i>d.</i>
365	<i>Bavaria and Würtemberg</i> .—Report on the Co-operative Agricultural Credit Associations on the Raiffeisen System in Bavaria and Würtemberg - - -	1 <i>d.</i>
366	<i>France</i> .—Report for the Year 1894 on the Fluvial Traffic of Rouen and the Waterways of the Seine Basin - - -	11½ <i>d.</i>
367	<i>Brazil</i> .—Report on a Tour in the German and Italian Colonies in the Valley of the River Cohy - - -	3½ <i>d.</i>
368	<i>Greece</i> .—Report on the Emery Districts of Naxos - - -	10 <i>d.</i>
369	<i>Argentine Republic</i> .—Report on the Agricultural and Pastoral Condition and Prospects of the Argentine Republic - - -	1½ <i>d.</i>
370	<i>Colombia</i> .—Report on the Agricultural Products of Tolima - - -	1 <i>d.</i>
371	<i>Russia</i> .—Report on Measures Adopted for Improving the Commercial Prosperity of Archangel - - -	½ <i>d.</i>
372	<i>Russia</i> .—Report on the Association of Working men known as Artels - - -	1½ <i>d.</i>
373	<i>Africa (Central)</i> .—Report on the Botanical Aspects of British Central Africa - - -	1½ <i>d.</i>
374	<i>Colombia</i> .—Report on the Commercial Importance of the Port of Barranquilla - - -	2 <i>d.</i>
375	<i>Greece</i> .—Report on the Deposits of Magnesite Ore and the Manufacture of Magnesite Fire-Bricks in Eubœa - - -	2 <i>d.</i>
376	<i>Denmark</i> .—Special Report on the Agricultural Condition of Denmark - - -	1 <i>d.</i>
377	<i>Denmark</i> .—Report on the Port of Frederikshavn - - -	4½ <i>d.</i>
378	<i>Italy</i> .—Report on the Pumice-stone Industry of the Lipari Islands - - -	½ <i>d.</i>
379	<i>France</i> .—Report on the Principal Fibres obtainable in the Society Islands - - -	⅓ <i>d.</i>
380	<i>Servia</i> .—Report on Apiculture in Servia - - -	⅓ <i>d.</i>
381	<i>Belgium</i> .—Report on the Ivory Trade of Antwerp - - -	½ <i>d.</i>
382	<i>Germany</i> .—Report on the German Colonies in Africa and the South Pacific - - -	5 <i>d.</i>
383	<i>France</i> .—Report on the Bordeaux International Exhibition of 1895 - - -	1 <i>d.</i>
384	<i>Brazil</i> .—Report on the causes which have brought about a Diminution in the Receipts at the Custom-House at Rio de Janeiro - - -	½ <i>d.</i>
385	<i>Mexico</i> .—Report on the Culture of Cacao, Vanilla, India-Rubber, Indigo, and Bananas in Mexico - - -	2½ <i>d.</i>
386	<i>France</i> .—Report on the International Congress on Technical, Industrial and Commercial Education held at Bordeaux in September 1895 - - -	1 <i>d.</i>
387	<i>Portugal</i> .—Report on the East Coast Route to Mashonaland - - -	1 <i>d.</i>
388	<i>Austria-Hungary</i> .—Report on the Iron Industry in Styria and the Betler Ironworks in Hungary - - -	5 <i>d.</i>

XXIV.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
27	Mauritius - - - -	1890	85	Hong Kong - - - -	1892
28	Gambia - - - -	"	86	St. Lucia - - - -	"
29	Hong Kong - - - -	"	87	Falkland Islands - - - -	"
30	Ceylon - - - -	1889	88	Gold Coast - - - -	"
31	Lagos - - - -	"	89	Basutoland - - - -	1892-93
32	Lagos - - - -	1890	90	Ceylon - - - -	1892
33	Ceylon - - - -	"	91	Zululand - - - -	"
34	Newfoundland - - - -	"	92	Mauritius - - - -	"
35	Jamaica - - - -	1889-91	93	Labuan - - - -	"
36	Victoria - - - -	1890	94	British Honduras - - - -	"
37	British New Guinea - - - -	1890-91	95	Lagos - - - -	"
38	St. Vincent - - - -	1891	96	Sierra Leone - - - -	"
39	Bermuda - - - -	"	97	Fiji - - - -	"
40	Seychelles and Rodriguez - - - -	1889 and 1890	98	Rodrigues - - - -	"
41	Gambia - - - -	1891	99	St. Helena - - - -	"
42	Barbados - - - -	"	100	British Bechuanaland - - - -	1892-93
43	Turks Island - - - -	"	101	British Guiana - - - -	"
44	Bahamas - - - -	"	102	Newfoundland - - - -	1892
45	Fiji - - - -	1890	103	Jamaica - - - -	1892-93
46	Seychelles - - - -	1891	104	Victoria - - - -	1892
47	British Bechuanaland - - - -	1890-92	105	Bermuda - - - -	1893
48	Malta - - - -	1890 and 1891	106	Gambia - - - -	"
49	Gibraltar - - - -	"	107	Hong Kong - - - -	"
50	Falkland Islands - - - -	"	108	St. Vincent - - - -	"
51	Leeward Islands - - - -	"	109	Gibraltar - - - -	"
52	Grenada - - - -	1890 and 1891	110	Bahamas - - - -	"
53	St. Lucia - - - -	"	111	Turks and Caicos Islands - - - -	"
54	Labuan - - - -	"	112	Leeward Islands - - - -	"
55	British Guiana - - - -	"	113	Barbados - - - -	"
56	Zululand - - - -	"	114	Malta - - - -	"
57	Mauritius - - - -	"	115	Zululand - - - -	"
58	Lagos - - - -	"	116	British Honduras - - - -	"
59	Straits Settlements - - - -	"	117	Trinidad and Tobago - - - -	"
60	Ceylon - - - -	"	118	Falkland Islands - - - -	"
61	Natal - - - -	1891-92	119	Rodrigues - - - -	"
62	Basutoland - - - -	"	120	Mauritius - - - -	"
63	St. Helena - - - -	1891	121	Straits Settlements - - - -	"
64	Sierra Leone - - - -	"	122	Labuan - - - -	"
65	Jamaica - - - -	1891-92	123	Basutoland - - - -	1893-94
66	Trinidad and Tobago - - - -	1891	124	St. Lucia - - - -	1893
67	Newfoundland - - - -	"	125	St. Helena - - - -	"
68	British New Guinea - - - -	1891-92	126	Ceylon - - - -	"
69	Victoria - - - -	1891	127	Fiji - - - -	"
70	Rodrigues - - - -	"	128	Grenada - - - -	"
71	Bermuda - - - -	1892	129	Sierra Leone - - - -	"
72	Fiji - - - -	1891	130	British Bechuanaland - - - -	1893-94
73	British Honduras - - - -	"	131	British New Guinea - - - -	1892-93 and 1893-94
74	Turks and Caicos Islands - - - -	1890	132	Lagos - - - -	1893
75	Gibraltar - - - -	1892	133	British Guiana - - - -	1893-94
76	Leeward Islands - - - -	"	134	Jamaica - - - -	"
77	Bahamas - - - -	"	135	Newfoundland - - - -	1893
78	Trinidad and Tobago - - - -	"	136	Gold Coast - - - -	"
79	Malta - - - -	"	137	Zululand - - - -	1894
80	Gambia - - - -	"	138	Bermuda - - - -	"
81	Straits Settlements - - - -	"	139	Bahamas - - - -	"
82	Grenada - - - -	"	140	Barbados - - - -	"
83	Barbados - - - -	"	141	Turks and Caicos Islands - - - -	"
84	St. Vincent - - - -	"	142	Malta - - - -	"
			143	Gambia - - - -	"

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast - - - -	Economic Agriculture.
2	Zululand - - - -	Forests.
3	Sierra Leone - - - -	Geology and Botany.
4	Canada - - - -	Advantages to Emigrants.

THE
Board of Trade Journal
OF
TARIFF AND TRADE NOTICES
AND
MISCELLANEOUS COMMERCIAL INFORMATION.

Vol. XX.]

March 1896.

[No. 116.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

Most important industries continue to show an improvement in the state of employment, both compared with last month and with February 1895, at which period, however, outdoor work was much affected by the long frost.

In the 106 trade unions, with an aggregate membership of 409,102 making returns, 16,315 (or 4·0 per cent.) are reported as unemployed at the end of February, compared with 4·5 per cent. in January, and with 7·9 per cent. in the 84 unions, with a membership of 385,594, from which returns were received for February 1895.

Coal Mining.—Employment continues good in nearly all districts. It shows hardly any change compared with the previous month, but is not quite up to the level of February 1895. The average number of days worked by 1,064 pits at which 299,437 workpeople were employed, was 4·91 per week in February as compared with 4·94 in January, and 5·00 in February 1895. The proportion of unemployed miners in trade unions in Northumberland and Durham was 2·0 per cent. of the membership at the end of the month, the corresponding percentages at the end of the previous month and of February 1895 being respectively 1·7 and 2·6.

Iron Mining.—Employment in this industry again shows improvement. Very nearly full time was worked by most of the miners, the general average for February last at the 116 mines included in the returns being 5·85 days per week, as compared with 5·70 in February 1895 and January 1896. The total number employed at these mines was 13,577, or 700 more than a year ago.

In the *Pig Iron Industry* the number of furnaces in blast and the number of men employed have increased both as compared

with January and with a year ago. At the end of February the ironmasters making returns had 336 furnaces in blast compared with 328 at the end of January, and 314 at the corresponding date of last year. The number of workpeople employed at these furnaces was 21,424, or 331 more than in January and 1,508 more than a year ago.

Employment in *Steel Works* is considerably better than a year ago, the number of workpeople employed in the 95 works from which returns have been received being now 30,396 or nearly 10 per cent. more than in February 1895. Returns from 76 *Puddling Furnaces* and *Rolling Mills* give the number employed as 15,835 in the last week of the month, an increase of nearly 3 per cent. compared with the corresponding period of last year.

In *Tinplate Works*, employment is, on the whole, somewhat better than in January, but not so good as a year ago. The number of mills working in the 88 works included in the returns was 319 at the end of February, compared with 305 in January.

The improvement in employment in the *Engineering* and kindred trades has been well maintained, and the percentage of unemployed union members has fallen from 4·3 in January to 3·4 at the end of February, compared with 8·3 per cent. in February 1895.

The *Shipbuilding* trades are also better employed, the percentage of unemployed union members at the close of the month being 7·8, compared with 9·1 in January, and 16·3 in February of last year (when work was affected by the severe weather).

Employment is good in the *Building* trades, the percentage of unemployed in unions making returns being only 2·5 compared with 3·7 in January. In February 1895, during the frost, the corresponding percentage was 10·1.

Employment in the *Furnishing* trades has improved, the percentage of unemployed in unions making returns for February being 2·6, compared with 5·4 in January, and 8·7 in February 1895.

Employment in the *Printing* and *Bookbinding* trades is slightly better than in January, but about the same as at this time last year. The percentage of unemployed union members is returned as 4·9, compared with 5·4 last month, and 4·8 in February 1895. Employment in the *Paper* trade has also slightly improved, and the percentage of unemployed union members has fallen from 3·2 to 2·8.

Employment in the ready-made *Clothing* trade continues to improve. In the bespoke branch it is still only moderate, but with a tendency to improve.

In the *Boot* and *Shoe* trade employment at the principal centres is rather slack for the time of year, but in some districts an improvement is reported.

In the *Leather* trades employment is rather quiet, as is usual at this season. The percentage of unemployed in unions making

returns is 6·2, compared with 5·4 in January, and 6·3 in February 1895.

Cotton Spinners are somewhat slack in the Oldham district, but continue fully employed at Bolton. Other districts show little change. Employment for *Cotton Weavers* shows some improvement. *Woollen* and *Worsted* operatives are well employed in the principal centres, but there has been some slight falling-off in employment in the worsted trade. In the *Hosiery* trade employment is good, except where affected by disputes; and in the *Silk* trade shows very little change.

As regards the employment of *women* in the *Textile* trades, information respecting 475 mills, employing about 88,000 women and girls, shows little change, 88 per cent. being in mills giving full employment, compared with 87 per cent. among those for whom returns were received for January. Employment in cotton mills (chiefly weaving) showed some improvement; while the woollen and worsted mills are not quite so fully employed as a month ago.

Trade Disputes.—The number of disputes occurring in February was 78, as compared with 50 in January, and 55 in February 1895. Most of the disputes were unimportant. In 71 of them 16,391 workpeople were involved, an average of 231 persons per dispute, as against 160 in the previous month. Twenty disputes took place in the mining industry, 16 in the building trades, 15 in the textile trades, 6 each in the clothing and metal trades, 5 in the engineering and shipbuilding trades, and 10 in a miscellaneous group of industries. Ten old disputes, involving 891 workpeople, were settled during February, and at the end of the month it was known that 24 new and 26 old disputes, involving about 9,000 workpeople, were still unsettled.

Changes in Rates of Wages.—Of the 26,000 workpeople affected by changes reported during February 21,000 received increases amounting on an average to about 1s. 4d. per week, and 5,000 sustained decreases averaging about 2s. 8d. per week, the nett result of all the changes being an average increase of about 6d. per week for the total number of workpeople affected. The increases were mainly in the engineering and shipbuilding trades, over 12,000 workpeople on the Clyde, Mersey, and Tyne, and at Belfast, Barrow, Edinburgh and Leith, and Dundee, having received increases varying from 1s. to 2s. 3d. per week. In the building trades 2,837 workpeople received increases, including an advance of $\frac{1}{2}$ d. per hour to over 2,500 joiners in the Glasgow district. The decreases reported affected 2,500 miners in Scotland and 1,685 workpeople in the tinplate trade.

OFFICIAL ENQUIRY UNDER THE BOILER EXPLOSIONS ACT.

A formal investigation was held at Soham on the 8th instant into the circumstances attending the explosion of a boiler at a millwright's on the 1st January last.

The boiler, one of the Cornish type, was purchased second-hand about 30 years ago, and from that time to 1893 it was not examined. It then required cleaning out, and it was examined by the millwright and his father, who at that time was the owner of the boiler. Pitting and corrosion were discovered, but they considered the boiler safe to work.

The father died in 1894 and the son then took over the management of the business, the boiler being used occasionally when the machinery could not be driven by the windmill. In 1895 the windmill was blown down and the boiler thereafter supplied steam to an engine which drove the machinery. In the same year the millwright's mother died, and he continued to carry on the business in the interests of himself, his brother, and sisters.

The boiler was worked at a pressure of about 30 lbs. per square inch; one safety-valve was loaded to blow off at a pressure of 57 lbs. and the other at 207 lbs.

On the 1st January, whilst the engine was standing, the boiler exploded and wrecked the premises in which it stood, but fortunately no one was injured.

The Court found that the explosion was due to the flue-tube having become so reduced by corrosion as to be unable to resist the pressure to which it was subjected at the time of the explosion. It collapsed and ruptured; it was found to be wasted on the fireside and on the waterside; it was pitted and grooved, and the remaining thickness in parts was reduced to $\frac{1}{32}$ nd and $\frac{1}{16}$ th of an inch. The Court were of opinion that the boiler had not been examined by any competent person since it was first set to work in or about the year 1865, and that neither the millwright nor his father were competent to examine it. No measures were taken to ascertain the safe working-pressure of the boiler, or to adjust the safety-valves to blow off accordingly.

The Court found the millwright to blame for the explosion, but as he was carrying on the business in the interests of his brother and sisters, rather than on his own account, they ordered him to pay the sum of 10*l.* towards the costs and expenses of the investigation.

TENDERS FOR WORKS IN EGYPT.

The Secretary of State for Foreign Affairs has received, from Her Majesty's Agent and Consul-General at Cairo, information to the effect that tenders for the construction of the Arabic Museum and Khedival Library at Cairo will be received up to 12 noon on May 2, and for the construction of the new museum

for Egyptian antiquities in the same capital up to 12 noon on April 18 next. Further particulars may be ascertained at the Commercial Department of the Foreign Office between the hours of 11 and 6.

TREATMENT OF TRANSIT AND TRANSHIPMENT GOODS UNDER THE MERCHANDISE MARKS ACTS.

A general order issued by the Commissioners of Her Majesty's Customs, and dated 17th February last, is as follows :—

“ With reference to general order ⁹⁹₁₈₈₇, paragraph 27, and general order ⁹⁴₁₈₉₃, paragraph 11, the Board direct that *in the absence of information*, transhipment and transit goods shall not be detained under the Merchandise Marks Acts unless they infringe the provisions of those Acts in one or other of the following ways :—

“ (a) By reason of their bearing marks which raise a clear and direct claim to British origin ; or

“ (b) By reason of their bearing the name or trade mark of a manufacturer, dealer, or trader in the United Kingdom, without qualification, such name or trade mark being that of a known firm having its principal place of business in the United Kingdom ; or

“ (c) By reason of their bearing a trade mark specially registered at the Customs.

“ Collectors and (in London) inspectors are further authorised to apply this order to goods entered for home consumption, but intended to be forthwith re-shipped for exportation, on production to them of satisfactory evidence (such as bills of lading, invoices, &c.), of the final destination abroad of the goods.

BOARD OF TRADE AWARDS FOR GALLANTRY.

The Board of Trade have awarded a binocular glass to Gabriel G. A. Gordon, master ; a silver medal for gallantry in saving life at sea to William Johnston, mate ; and bronze medals for gallantry in saving life at sea, and a sum of 1*l.* each to Martin H. Jockem, Andreas T. Lund, Thomas Manson, and John Zieboldt, seamen, of the S.S. “ Tortona,” of Leith, in recognition of their services in rescuing the crew of the fishing smack “ Invincible,” of Hull, which was abandoned in the North Sea on December 8, last.

The Board of Trade have awarded a piece of plate to José Grau y Mas de Xaxar, master of the Spanish steamer “ Antonio Lopez,” of Barcelona, in recognition of his kindness and humanity to the shipwrecked crew of the S.S. “ Madura,” of West Hartlepool, which was abandoned at sea on the 28th November last.

IMPERIAL RAILWAYS OF NORTH CHINA.

Tenders for Railway Plant.

A despatch, dated 6th January last, has been received from Mr. W. Beauclerk, Her Majesty's Secretary of Legation at Peking, enclosing extract from the "Peking and Tientsin Times," containing an announcement of the Imperial Railways of North China, inviting tenders for steel girders, wheels, axles, and various railway material.

The particulars of the tenders invited are as follows:—

Bridge Contract.

The directors invite tenders for the supply of steel girders as under:—

Nine spans, 100 ft. in clear—consisting of two main girders and double track, through type.

Twelve spans, 60 ft. in clear—consisting of two main girders and single track, half-deck type.

One hundred spans, 30 ft. in clear—consisting of two main girders and single track, deck type.

Specifications and all necessary details may be obtained from Sir Benjamin Baker, Queen Anne's Mansions, London, and tenders sent in to him will be opened on *30th March 1896*, and girders must be delivered on Tong-ku Wharf not later than October 1896.

The work, both during manufacture and on acceptance, will be rigidly inspected before shipment, and on arrival in China any parts injured in transport or delivery will be replaced at cost of contractor.

Payments will be made as under two-thirds in London on completion of shipment, and one-third in Tientsin 10 days after completed delivery.

Passenger Coach Wheel and Axle Contract.

The directors invite tenders for the supply of passenger coach wheels and axles as under:—

400 wheels with wrought iron solid pressed centres or of cast steel. Tyres to be 42 ins. diameter on tread, and of Siemens-Martin acid steel of best quality.

200 axles for the above, to be of best hammered iron, equal in quality to that known as "Yorkshire."

Specifications and details can be procured at the office of Government Consulting Engineer, C. P. Sandberg, Esq., C.E., Great George Street, London, S.W., during the month of March, and tenders must be sent to him not later than 27th April 1896.

The wheels, &c. must be delivered on Tong-ku Wharf not later than November 1896, and unless a certificate is produced showing them to be fully up to specification, after due inspection by Mr. Sandberg before shipment, they cannot be accepted. Wheels and axles injured during transport to China will be rejected on arrival.

Payment will be made as under: two-thirds on shipment, and one third 10 days after acceptance in China.

No material will be accepted which is not in strict accordance with specification as detailed above.

Quotations are to include freight, insurance, and all other charges (except import duty) laid down at the Tong-ku Wharf.

A firm can quote for either or both of the above-mentioned contracts.

The directors do not undertake to accept the lowest or any tender, and are not responsible for any expenses incurred by intending contractors in obtaining the quotations, &c.

GOVERNMENT CONTRACTS.

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply (1) wheels and axles for railway carriages; (2) india-rubber springs; (3) steel laminated springs; (4) steel volute springs; (5) pine sleepers.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that Office by 2 o'clock p.m. on Tuesday, 17th March 1896, after which time no tender will be received.

The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply locomotives.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that office by *two* o'clock p.m. on Tuesday, the 31st March 1896, after which time no tender will be received.

INSPECTORS OF WEIGHTS AND MEASURES.

The following inspectors of weights and measures have during the month of February 1896 passed the examination provided for in the 11th section of the Weights and Measures Act, 1889 :—

Boardman, Henry	-	-	-	Oldham.
Casburn, John Robert	-	-	-	Do.
Dickson, Robert Angus	-	-	-	Peebles-shire.
Macdonald, Charles Findlater	-	-	-	Glasgow.
Presslie, George	-	-	-	Aberdeen (City).
Thompson, John	-	-	-	Northumberland.
Watson, William	-	-	-	Arbroath.

II.—THE FOREIGN TRADE OF THE UNITED KINGDOM.*

The total value of the imports into the United Kingdom from foreign countries and British possessions during the month of February 1896 was 35,476,736*l.*, as compared with 28,134,489*l.* for the corresponding month of 1895, an increase of 7,342,247*l.*, equal to 26·10 per cent.; and the exports of British and Irish produce and manufactures were valued at 19,683,456*l.*, as compared with 15,973,095*l.*, an increase of 3,710,361*l.*, or 23·23 per cent. The value of the exports of foreign and colonial merchandise amounted to 5,638,232*l.*, as against 4,407,824*l.* in February 1895, an increase of 1,230,408*l.*, or 27·91 per cent. All classes of goods imported are higher in value last month than in February 1895, the chief increases being in articles of food and drink, both dutiable and duty free, which show an improvement of 2,856,229*l.*; manufactured articles, 1,155,902*l.*; raw materials for sundry industries and manufactures, 871,410*l.*; raw materials for textile manufactures, 566,622*l.*; oils, 372,998*l.*; metals, 367,104*l.*; miscellaneous articles, 356,714*l.*; and animals, living (for food), 353,711*l.*; of these animals, nearly all came from the United States and Argentina.

In the class of articles of food and drink the chief increase is found in unrefined sugar (604,460*l.*), wheat meal and flour (352,428*l.*), Indian corn or maize (351,408*l.*), sugar, refined, and candy (306,217*l.*), barley (185,978*l.*), wine (185,008*l.*), butter (142,779*l.*), wheat (135,743*l.*), oats (114,969*l.*), and fish, cured or salted (111,795*l.*). In manufactured articles the improvement is chiefly seen in silk broad stuffs (increase 223,777*l.*), silk or satin ribbons, iron manufactures, and gloves.

In the class of raw materials for textile manufactures there is a decrease in value of 523,780*l.* in cotton, raw, which is, however, more than compensated for by the increase in other items of the same class, notably one of 755,909*l.* for sheep and lambs' wool. The total increase in this class, as mentioned above, is 566,622*l.*

Other articles showing notable increases in value are:—tobacco, iron ore, petroleum, flax, hemp, caoutchouc, tallow and stearine, and flax-seed and linseed.

With the exception of cotton, raw, noted above, lard (decrease, 122,760*l.*), coffee (124,397*l.*), and jute (51,346*l.*), there is no article imported which shows a decrease in the period referred to of more than 33,000*l.*

As regards the exports of British and Irish produce and manufacture, improvement is to be found in every class: animals, living, articles of food and drink, raw materials, yarns and textile fabrics, metals and articles manufactured therefrom, machinery

* See also statistical tables, pp. 370 and 371.

and millwork, apparel and articles of personal use, chemicals and chemical and medicinal preparations, and all other manufactured articles, and parcel post articles, all showing increases, the aggregate increased value being 3,710,361*l*. The increase as regards these exports may be found chiefly in yarns and textile fabrics, metals, machinery, apparel and articles of personal use, and raw materials. The value of cotton piece-goods was 776,502*l*. in excess of the corresponding month of the previous year; of other cotton manufactures, 116,820*l*.; and of woollen tissues, 165,531*l*. The increase in the value of metals was 777,999*l*., of which iron, wrought and unwrought, represented 433,331*l*.; of machinery, 251,059*l*.; of apparel and articles of personal use, 209,132*l*.; of articles of food and drink, 131,810*l*.; and of coal, coke, and fuel, 103,654*l*. The most important decrease in value in the items of the various classes was in skins and furs, 13,406*l*., and alkali, 8,974*l*.; and in no other single article did it amount to 7,000*l*.

III.—THE IRON INDUSTRY OF RUSSIA.*

The Urals, up to 20 years ago, were the centre of the Russian cast-iron manufacture, as the most favourable conditions existed there for the production, in extensive beds of ore, an abundance of fuel, and extremely cheap labour. The Ural foundries, however, could not keep pace with the development of Russia, and were unable to supply the immense quantities of cast iron required for the extensive system of railways. The causes of this fact are, first, that the organisation of the industry is based upon antiquated principles; and, second, that the Ural producers were unwilling to reduce the high prices to which they were accustomed.

The chief difficulties in the way of a rapid development of the Ural industry are caused by the expenses of transport and management, which surpass anything known in Western Europe. These difficulties are produced by the geographical position of the district where the industry is situated, its great distance from the centre of consumption, the undeveloped state of railway communication, and the peculiar method of production, several factories being concerned in the manufacture of one and the same article, so that it must often be conveyed forwards and backwards in a half-finished state. Accordingly, attempts have long been made to give a great impetus to the iron trade of the south, west, and north of the Empire, in order to be independent, as far as possible, of foreign countries.

* Based on a Report of Mr. Theodore M. Stephan, United States Consul at Annaberg.

Efforts were especially made in this direction in the government of Yekaterinoslav, where all the conditions required for successful development of mining exist in a degree rarely found elsewhere. Favoured with a warm climate and a fruitful soil this tract of country contains beds of ore yielding, in some parts, 70 per cent. of pure iron. Extensive beds of coal, and large quantities of dolomite and fireclay, increase the natural wealth of this government. Notwithstanding these favourable circumstances, there scarcely existed a mining industry in Southern Russia previous to 1887.

The attempts to produce iron in large quantities in Southern Russia date from the year 1797, but it was not until 90 years later that these assumed any very great importance. The impulse was given in 1887 by the manufacturing company of Brjansk, which opened the splendidly situated factory of Alexandrov, on the Dnieper. This was followed in 1889 by the South Dnieper Company, in 1892 by the Krivoi-Rog Company (the Granzev factory), and in 1894 by the Donetz Company with the factory of Druschkowski. The production of these six establishments rose from 322,439 tons in 1893 to about 416,157 tons in 1894, and will probably reach 644,839 tons at the end of 1896, so that in this short time the yearly output will have increased by 100 per cent.

The iron-beds of Southern Russia are very extensive, and, almost without exception, supply ore containing a very large proportion of iron. Those of Krivoi-Rog, for instance, are 33 miles in extent, and the veins are, as a rule, from 218 to 656 yards in length and 21 to 131 yards in thickness, and contain up to 3,224,285 tons of ore, with a percentage of iron reaching 70 per cent.

In the year 1894 twelve concerns were working in the district of Krivoi-Rog, all of which, with two exceptions, are conducted by mining engineers. The total output would, perhaps, amount at present to about 806,071 tons.

It may be stated in favour of the Krivoi-Rog ore that it is comparatively free from foreign admixtures; on the other hand its fusibility is not great, about 45 per cent. of flux being required to smelt it.

For the successful development of every industry concerned in the production of cast iron, the presence of cheap and good coal is indispensable, for more than 55 lbs. of coal are required for the manufacture of each poud (36 pounds) of cast iron. But in this respect, also, the future of the Southern Russia mining industry appears completely assured, as the immediate neighbourhood of the iron-fields—the whole Donetz basin—may be looked upon as a huge coal-bed. The output of coal in the year 1893-94 alone amounted to more than 5,352,288 tons. From a scientific investigation of these coal-beds, it appears that their exhaustion within a measureable period is an impossibility. Limestone, also, which is not less important for the production of iron, is to be found in vast quantities on the spot.

Under these circumstances, it is beyond doubt that the near future will see an immense development of the mining industry in Russia. At present, however, the total product falls short of the requirements, which are constantly increasing. The larger factories in the Urals, in the south and north of Russia, in the Moscow and Warsaw districts, are continually overwhelmed with orders for railways (rails, iron plates, &c.), for shipbuilding, and for private industry. Moreover, in consequence of the introduction of the State monopoly of brandy in 25 governments, which was to commence in January 1896, an enormous number of iron cisterns and reservoirs is required, and applications for tenders have already been made.

The total production in 1894 of cast iron in Russia was quite inadequate to the demand. This deficiency of cast iron has been, until very lately, a constant impediment to the development of the metallic trades, which can only be redressed by the importation of foreign goods.

In the production of assorted iron and bar iron, also, the mining industry has not met the requirements, and in the case of these articles there is a great demand, with a rise of prices. The demand for bar-iron, which is used in large quantities for shipbuilding in the Volga district, is very great, and can be met only with difficulty by the home industry.

On account of this state of affairs, the overcrowding with orders of the western and southern mines, and the comparatively small results of the Ural industry, the home manufacturers are exposed to many difficulties in procuring a sufficient supply of the necessary raw material. Accordingly, at a meeting of the leading representatives of the Russian machine makers, which was held in 1895 at the Department for Trade and Manufacture, special emphasis was laid upon the facts that the Russian iron founders will only sell their raw material at very high prices, that they only accept orders for cash in advance, and that the material supplied by them rarely meets the requirements of machine manufacturers, either in quality or in kind, especially in the case of manufacturers of agricultural machines. The Russian producer who is favoured by a protective duty of 75 copecks (about 1s. 0½*d.*) per pound (36 lbs.), on finished goods, has to pay twice as much for raw material as the foreign manufacturer, which makes it impossible for him to compete with foreign producers.

The importation of foreign iron, especially from Prussian Silesia, has frequently had a regulating influence, since the commercial treaty between Russia and Germany, upon the constant tendency of prices to rise on the Russian iron market on account of the small inland competition. This influence of German iron made itself felt first on the markets of the south, west, and south-west, but extended itself afterwards, in consequence of the extraordinary cheapness of the goods, into the very heart of the Empire. At the same time the German manufacturers, by carefully observing Russian customs and adapting their assortments to the Russian requirements, have

been enabled to introduce their goods into the widest circles. Favourable conditions for the importation of foreign iron and iron goods continue to exist. The German foundries are fully employed, and, in consequence of the active Russian demand (especially for iron plates of all kinds), are unable to execute all their orders, and therefore constantly require longer periods of time to carry them out. This would be a very favourable opportunity for iron founders to extend their sales in Russia and to supply at least a part of the demand of that country, which still shows a tendency to rise and will probably continue to do so for a considerable time. By sending representatives as early as possible, by observing most carefully Russian usages, and especially by charging low prices, foreign manufacturers will be able to increase considerably their business connections with Russia, and bring about permanent relations, and it might be also desirable—of course, with due caution—to effect credit sales on the most favourable terms possible. This is of special importance in the case of agricultural machines. By this means the foreign product will be placed in as favourable a position as the Russian, which, as is well known, can be pledged at the State bank.

It may be mentioned here that the zemstvos (local boards), when purchasing foreign scythes and ploughs have adopted a system which seems calculated to abolish (where the supply of Austrian scythes is concerned) the customary and often undesirable Russian commission trade. For this purpose, the zemstvos have established their own stores in Vialka, Perm, Ufa, Kostroma, Samara, and Nijni-Novgorod in order to provide the peasants with cheap agricultural implements. Formerly, the peasants bought scythes from the Russian agents at 1.25 roubles (2s. 6d.); and ploughs at 33 roubles (3l. 6s.); now the zemstvos supply the scythes at 55 copecks (1s. 1d.), and the ploughs at 23 roubles (2l. 6s.) including carriage. It is a well-known fact that these cheaper goods are imported by a German firm. Considering that in this way foreign products must become more popular and be more widely diffused, it is advisable to pay greater attention to this matter, and foreign manufacturers will find it worth their while to occupy themselves with the question, and to enter into similar relations with the zemstvos.

Notwithstanding the undeniable increase in the production of cast iron, the Russian manufacturers are still forced, on account of the high prices in their own country, to obtain a great part of their raw material from abroad. According to details lately published on the subject of the import of German machines into Russia in the first half of 1895, it appears that this import shows a very satisfactory advance for the first half of 1895, and has more than doubled since 1893. On comparing the amount of the eight most important articles imported in the first six months of 1895 with those of a like period in 1893 the following result is seen.

Article.	First Six Months of 1895.	First Six Months of 1893.
<i>Iron and Ironware.</i>		
	Cwts.	Cwts.
Corner and angle iron - - - -	275,000	121,000
Rod iron - - - -	863,000	390,000
Plates and sheets - - - -	387,000	166,000
Coarse iron goods - - - -	132,000	93,000
Total - - - -	1,657,000	770,000
<i>Machines.</i>		
Locomotive engines and railway locomotives -	10,000	3,000
Machines :		
Cast iron - - - -	222,000	99,000
Wrought iron - - - -	26,000	19,000
Sewing - - - -	4,000	6,000
Total - - - -	272,000	127,000

This result is the more remarkable, as, in the first half of 1893, prohibitive duties on German commodities did not exist in Russia. These figures, therefore, show very plainly the considerable advance which has been made in the import of German iron since the time preceding the tariff war.

The subjoined figures show the percentage of German goods supplied to Russia as compared with the total export of Germany:--

	Per cent.
Corner and angle iron - - - -	17·7
Rod iron - - - -	30·6
Plates and sheets - - - -	35·0
Coarse iron goods - - - -	12·6
Locomotive engines and railway locomotives	17·6
Machines :	
Cast iron - - - -	25·4
Wrought iron - - - -	18·0
Sewing - - - -	15·7

These details have been given in order to show that in this branch of trade there is an opportunity of exporting in large quantities to Russia if the right measures are taken.

IV.—GERMAN EMIGRANTS AS STATE AGENTS.

According to a recent report by the American Consul at Chemnitz, a new law looking to the correction of abuses and to furthering Germany's interests in future emigration movements is making many friends inside and outside official circles. An effort is to be made to make conscienceless agencies impossible by erecting agencies under State control, these to give emigrants or persons intending to emigrate all the necessary information regarding the land in which they intend to settle. To aid these State-established agencies, others are to be opened in foreign lands. These latter are to furnish the home agencies with facts and figures and information as to the best ways and means to be employed to aid intending immigrants before leaving and after arriving. In fact, they are to actively aid them. They are to be placed under the control of German consuls in foreign parts. They are to find out what parts of the foreign land offers the best prospects, and they are to report such information to the home offices; they are to protect immigrants from the impositions and frauds so common in countries receiving large numbers of immigrants. Of course, the system presupposes a central office with branches all over the Empire; these are to be connected with the local imperial council boards. The hope is expressed that such a system may succeed in leading very large numbers in directions best suited to secure favourable and permanent connection for and with the Fatherland.

Of course it will cost a great deal to construct and carry on such agencies, but their builders look for large returns. It is hoped that they will aid the German element to unite its parts more easily, and thus play an important part in assisting German trade. Such an element so aided would, out of gratitude, turn to the Fatherland for all its needed supplies. Agents of home houses would find their paths pleasanter and their tasks lighter in lands where Germans, through union and co-operation, could aid them. Such immigrants will be little less useful to the Fatherland than colonists. In many ways they will be worth more, for, coming or going into well-settled countries, they will be able to point out to the people at home how best to supply the wants of those among whom they toil. It is not only necessary to know what is wanted, but what forms are best suited to local tastes. In this way, henceforth, German emigration is to aid directly and indirectly the home trade.

The moral and political importance of such a system goes beyond all calculation; it certainly enters into the plan as a very important feature. Nor are its projectors unconscious of the fact that thus millions of Germans and their descendants may be made to aid in extending the Empire's influence and in opening up new avenues for its rapidly-growing foreign commerce.

V.—COMMERCIAL RELATIONS BETWEEN FRANCE AND HER COLONIES.

The following information with regard to the commercial relations between France and her colonies has been received by the Foreign Office from the Marquis of Dufferin, Her Majesty's Ambassador at Paris, in reply to a circular of the Earl of Kimberley, dated the 25th June last, and forms part of the series of replies published in "Reports from Her Majesty's Representatives abroad on the fiscal advantages accorded by certain foreign countries to goods imported from their colonial possessions, and, conversely, by the said colonial possessions to goods from their mother country." (Commercial No. 5, 1895.)

The commercial relations between France and her colonies and foreign possessions are now governed by Articles 3, 4, 5, 6, 7, and 8 of the Customs Law of the 11th January 1892, which run as follows :—

"Article 3. The duties and immunities applicable to products imported into the mother country from *Colonies*, from French possessions, and from countries of the Indo-Chinese protectorate are established in Schedule (E), annexed to the law.

"The French territories of the Western Coast of Africa (with the exception of Gaboon), Tahiti and dependencies, the French establishments in India, Obock, Diego-Suarez, Nossi-Bé, and Sainte-Marie of Madagascar, are exempt from the stipulations of Schedule (E). French long-cloth proceeding from the French establishments in India shall, however, be free of duty. Exemptions or reductions may, in addition, be granted for other natural products, or for those originally manufactured in the above-mentioned establishments; such reductions and exemptions shall be established by means of Decrees issued by the Council of State. Natural products, or those originally manufactured in the aforesaid establishments, for which no exemption or reduction has been granted, shall pay, when imported into France, the duties stipulated in the minimum tariff.

"Foreign products *imported into Colonies*, French possessions, and countries of the Indo-Chinese protectorate, with exception of the territories enumerated in the preceding paragraph, *shall be subject to duty as if imported into France.*

"Decrees in the form of public administrative regulations, issued on the proposal of the Minister of Commerce, Industry, and Colonies, with the advice of the General Councils or the Administrative Colonial Councils, shall designate the products

which, in exception to the preceding provision, shall be subject to special duties.

"Paragraphs 1 and 3 of the present Article shall not be executory for any colony until the regulations mentioned in paragraph 4 have been issued; the effect of this provision is limited to one year. The Government may, however, immediately grant all or part of the privileges stipulated in Schedule (E) to colonies which at the present time levy on foreign products the whole of the tariff duties of the mother country, or who subject colonial goods coming from abroad to the duties stipulated in the said tariff.

"Art. 4. The General Councils and the Administrative Colonial Councils may also suggest that modifications be made in the tariff of the mother country. The suggestions shall be submitted to the Council of State, whose decisions thereon shall be issued in the same manner as the public administrative regulations mentioned in the preceding article.

"Art. 5. Native products of a French colony which are imported into another French colony shall be exempt from Customs duty.

"Foreign products imported from a French colony into another French colony shall be subject in the latter to the payment of the difference between the duties of the local tariff and those in the tariff of the exporting colony.

"Art. 6. The method of assessment, the regulations for collection and for the repartition of the sea octroi ('octroi de mer'), shall be established by the General or the Administrative Councils, and ratified by Decrees issued in the form of Public Administrative Regulations.

"The sea octroi tariffs shall be voted by the General or Administrative Colonial Councils. They shall be rendered executory by Decrees issued on the proposal of the Minister of Commerce, Industry, and Colonies.

"They may, however, be temporarily put in force in virtue of a Governor's Decree. The expenditure of the Customs service, for salaries, &c., shall be entered as obligatory expenses in the local Colonial Budgets.

"Art. 7. The provisions of Article 10 of the Law of the 29th December 1884, relating to Algeria, are maintained in force.

"Art. 8. The Government is authorised to levy surtaxes on, or to prohibit the importation of, all or part of goods the product of countries who levy or may levy surtaxes on, or prohibit the importation of, French goods.

"These measures must be submitted immediately to the Chambers for ratification, should they be sitting; should the Chambers not be sitting they must be submitted at the opening of the following session."

The tariff for products imported into the mother country from French colonies and possessions, and from countries of the Indo-Chinese Protectorate, are set forth in Schedule (E) referred to in Article 3, section 1, as follows :—

SCHEDULE (E).

Articles.	Duties (additional Taxes included).
Products of colonial origin* :	
Sugar, molasses, not for distilling purposes -	{ Duties stipulated in the tariff of the mother country.
Syrups and bonbons, sweet biscuits - -	Ditto.
Jams and fruits of all kinds preserved with sugar or honey - - - -	Ditto.
Cocoa - - - - -	{ One-half of the duty stipu- lated in the tariff of the mother country.
Cocoa, ground - - - - -	Ditto.
Chocolate - - - - -	Ditto.
Coffee, in the bean, or roasted, or ground -	Ditto.
Tea - - - - -	Ditto.
Pepper, pimento, cloves, cinnamon, cassia lignea, amomums and cardamoms, nutmegs, mace, and vanilla - - - -	Ditto.
All products not above mentioned, the origin of Colonies or possessions - - - -	Free.
Products of foreign origin :	
Imported from Algeria :	
After having been nationalised there through the payment of the duties specified in the tariff of the mother-country - - -	{ Ditto.
After payment there of special duties -	{ Pay the difference between the duties of the Algerian tariff and of that of the mother country.
Having paid no duty in Algeria, or arriving after having been warehoused or trans- shipped there - - - -	{ Duties stipulated in the tariff of the mother country.
Imported from other French colonies or possessions - - - - -	Ditto.
The prohibitions or restrictions established by the Customs tariff, whether in the interest of public order or in consequence of the existence of a monopoly, are applicable to goods imported from French colonies and possessions, whether such articles be colonial or the produce of foreign countries.	

By implication, all French goods imported into the colonies, &c., are admitted free of duty.

The application of this law, is not, however, as yet, universal, as by Article 3, section 2, certain colonies are exempted from its provisions; the practical result being that, as regards tariff legislation, the French colonies may be divided into three groups :—

* The products of French colonies and possessions shall only enjoy this favour on condition that the importation be direct, and on production of the proper certificates of origin.

1. *Colonies where all French Imports are free, but Foreign Imports pay the Duties of the French Tariff.**

These colonies are as follows:—

Algeria.
Comoro Islands.
Gaboon.
Guadeloupe and dependencies.
Guiana.
Indo-China (French).
Martinique.
Mayotte and dependencies.
New Caledonia and dependencies.
Réunion.
St. Pierre and Miquelon.

The Customs Law (Article 3, paragraph 2) authorises, however, the Government to alter by Decree the French tariff, as applied in the colonies. This faculty has been freely used, and it is only by carefully comparing these Decrees with the French tariff that an exact knowledge of the French Colonial rates can be arrived at.

In all the colonies of this group French products enjoy a privileged treatment, as compared with those of foreign countries.

2. *Colonies where French Produce as a Rule pays the same Import Duties as Foreign Produce.*

These colonies are:—

Benin (Gulf of).
Congo, French (Conventional Basin).
Guinea, French.
Ivory Coast.
Nossi-Bé.
Oceania, French establishments of.
Senegal.
Soudan, French.
Tahiti and dependencies.

In all these colonies, with the exception of Senegal, Nossi-Bé, and Oceania, French goods pay the same rates as those of foreign origin. In Senegal, however, there are certain additional *ad valorem* duties imposed on foreign produce, from which French produce is exempted.

In Nossi-Bé and Oceania the Customs tariffs apply only to foreign goods.

3. *Colonies where no Custom Duties exist.*

In Obock and the French possessions of India, at Sainte-Marie de Madagascar, and Diego-Suarez, there are practically no Customs duties.

* In the case of countries having commercial treaties with France, this will be the minimum tariff.

Colonial Produce imported into France.

As regards duties on colonial produce imported into France, all the colonies in Group 1 are exempt from any duty except those duties specified in the table given above.

As regards colonies in Group 2, their produce pays the rates of the French minimum tariff, but Article III. of the Law of the 11th January 1892, provides that special exemptions or reductions can be made by Decrees of the Conseil d'Etat; and there is scarcely one of the colonies which has not obtained in this manner considerable privileges for their produce.

Algeria.

Algeria, as shown above, is classed among the colonies in Group 1, where all French imports are free, but foreign imports pay the duties of the French tariff. There are, however, certain special rates for salted meats, coffee, sugars, tobacco, cigars, &c. Matches, which are a government monopoly in France, and are altogether prohibited, may be imported into Algeria, where there is no monopoly.

Goods, the produce of Tunis and Morocco, are admitted free when imported by land, the only exception being for bark from Tunis, which is prohibited.

All Algerian goods imported into France are free of duty.

Tunis.

There is an *ad valorem* duty of 8 per cent on foreign goods, generally, imported into Tunis from France and other foreign countries.

Up till 1890 all Tunisian produce imported into France was subject to the duties of the general tariff. By the Law of the 19th July 1890, however, certain produce, such as cereals, olive oils, animals, &c., are admitted free up to a certain quantity, which is fixed annually by Decree. All other produce pays the rates of the minimum tariff.

Export duties are levied on dates, sponges, halfa, olive oil, fresh olives, wool, bones, skins, and a few other articles.

It may be useful to add that the commercial relations between Great Britain and Tunis are fixed by the Convention of the 19th July 1875, Article VII of which provides that in no case shall the rate of duty exceed 8 per cent. *ad valorem*, while Article XI stipulates that the Convention shall remain in force until its revision shall have been accomplished by common consent after the expiration of seven years from the date of its conclusion.

Colonial Sugars.

By Article 2 of the Law of the 13th July 1886, sugars from the French colonies imported into France are entitled to an allowance called "*déchet de fabrication*," equivalent to the

average of the excess of yields obtained by native sugars during the previous season's working. The amount of this allowance is fixed every year by Decree. In the years 1892-93, 1893-94, 1894-95, and 1895-96, it has been 23 fr. 80 c., 19 fr. 47 c., 21 fr. 19 c., and 21 fr. 73 c. per cent. respectively. Though not a bounty in the proper sense of the word, its effect is to protect colonial sugars.

"Octroi de Mer."

Goods imported into the French colonies are subject to a tax called "*octroi de mer*," which is levied upon all produce, whether of French or foreign origin.

These taxes are fixed by the colonial administrations, and vary considerably in the different colonies. They are local taxes, and devoted to local purposes.

"Taxes de Consommation."

In many, if not all the French colonies, special taxes ("*taxes de consommation*") are levied on spirits, wines, beers, tobacco, &c., of foreign and local origin.

The above information only gives the general principles on which the commercial relations of France with her colonies are established. Their intention has been to protect, as far as possible, the produce of each colony in competition with foreign countries, and to bring the colonies, as far as circumstances will permit, within the French customs frontier.

In the case of the colonies in Group 2, however, geographical conditions or diplomatic obligations have hitherto prevented a complete fulfilment of the object aimed at; but every opportunity will no doubt be taken of putting the law of the 11th January 1892 into full execution, and of thus placing French produce imported into the colonies on a privileged footing as regards the produce of foreign countries.

VI.—THE TRADE OF BUSHIRE AND MOHAMMERAH.

The commercial movement of the district of Bushire, which includes also the ports of Bunder-Abbas and Lingah, showed considerable improvement in 1894, both in the exports and imports. At Bushire the value of the exports was increased in the shipments of corn, wool, carpets, and by a rise in opium.

A correspondent of the "*Economiste Français*," in a recent issue of that paper, says that the imports attained a total value of 25,487,650 francs in 1894, as against 24,150,700 frs. in 1893, of which cotton tissues represented a sum of 14,824,050 frs. (or considerably more than one-half the total amount) in addition to 344,625 frs. for cotton yarn; tea, 1,866,700 frs.; indigo, 1,321,975 frs.; loaf sugar, 958,250 frs., and powdered sugar, 945,350 frs.; arms and ammunition, 941,950 frs.; copper, 641,975 frs.; woollen goods, 587,700 frs.; spices, 268,275 frs.; porcelain, 268,025 frs.; and other articles, about 250,000 frs.

With regard to the exports from Bushire, opium holds the first place with an amount valued at 6,336,525 frs. out of a total trade of 14,045,875 frs. In 1893 the total trade was valued at 13,342,175 frs.

Next in importance to opium ranks carpets, 1,969,550 frs.; arms and ammunition, 750,100 frs.; corn, 532,475 frs.; skins, 435,100 frs.; wool, 408,650 frs.; gum, 271,400 frs.; almonds, 262,825 frs.; and rose water 109,475 frs.; this last item is the product of the cultivation of immense fields of roses situated in the districts watered by the Persian Gulf.

The United Kingdom was the best customer of Bushire in 1894, taking merchandise to the value of 2,436,625 frs., and exporting in return to the value of 13,352,725 frs.

India and British colonies came next with 2,606,475 frs. and 9,709,725 frs. respectively; China with a total trade of 6,034,800 frs.; Turkey, 1,817,500 frs.; and Egypt, 629,575 frs.; the value of the trade with Continental Europe amounted to 1,352,925 frs. only. Asiatic Russia imported from Bushire, by way of Batoum, to the value of 66,650 frs.; and Zanzibar's trade amounted altogether to 38,300 frs.

At the port of Bunder-Abbas the value of the exports in 1894 amounted to 6,705,900 frs. as against 6,003,100 frs. in 1893, the principal articles in the order of importance being fruit and vegetables, wool, opium, carpets, and drugs.

The imports in value were reckoned at 13,637,150 frs., or 3,564,475 frs. more than in 1893. The chief articles of the import trade were tea (5,366,775 frs.), cotton tissues (5,443,000 frs.), and indigo, powdered and loaf sugar, and metals wrought and unwrought.

The foreign trade of Bunder-Abbas is carried on chiefly with India and other British colonies, China, Turkey, and United Kingdom. The value of the imports into Bunder-Abbas from Western Europe amounted in 1894 to only 131,250 frs. 233 vessels visited the port during the year, of which 103 were British, 62 Persian, 40 Mascatine, 25 Arab, and 2 Turkish.

The total trade of Lingah in 1894 was valued at 30,538,125 frs. (exports 14,174,900 frs. and imports 16,365,225 frs.), the principal articles being pearls, cotton goods, cereals, coffee, dates, woollen goods, and spices. Cotton goods and cereals are likewise the most important articles of the export trade. The countries doing the most trade with Lingah are the same as those which trade with Bunder-Abbas and Bushire.

Finally, at Mohammerah the value of the exports in 1894 was 2,207,300 frs., as against 2,225,975 in 1893; and of the imports 3,299,225 frs., as against 2,802,425 frs. in 1893, the principal articles of importation being always piece goods.

It is noteworthy that in the trade of these four ports of the Persian Gulf the imports always exceed the exports in value, a fact which is perhaps worth the attention of European exporters.

This region is the centre of the pearl trade, the total value of which in 1894 amounted to 22,798,100 frs., as against 22,343,200 frs. in 1893. These values, as declared by the Customs, are more likely to be under than above the real values, if account be taken of the smuggling carried on, and of the trade of the Turkish port Massorah, which is not included in the figures above given.

VII.—COMPETITION WITH ENGLISH TRADE IN PERSIAN MARKETS.

Her Majesty's Consul at Ispahan, in a recent report to the Foreign Office upon the trade and commerce of Ispahan for the year 1894-95, observes that that market has been very largely over-stocked from the south and also from the Baghdad side. Importations have greatly increased. It would seem that the increase over the year 1892-93 is about 30 per cent., taking all classes of goods, and over 1893-94 about 20 per cent. At the present time all stores, both European and native, are full of goods, and business is extremely dull. Although there have lately been such strong advances in the Lancashire cotton market, yet importers in Ispahan are unable to get any advance on old prices, and owing to the stocks in hand, it is doubtful when they will be able to avail themselves of this rise—rather the contrary, as at present the market seems to be a falling one. The native merchants lately seem to have sprung into life again, and now import from their agents at home or in Bombay. It was expected that the imposition of the 5 per cent. duty in Bombay on Manchester goods would have improved the prospects of that market; but as the Bombay Customs have allowed a rebate on all goods re-exported within a given period, this expectation has not been realised. The chief source of supply would appear to be Baghdad, whence the goods are brought *via* Kermanshah and Hamadan; these chiefly go to Tehran and to the towns on that line, but it closely affects the trade of Ispahan, which previously almost entirely supplied those markets with Manchester goods.

This change in the direction of the trade is due partly to the disturbance of the transport arrangements already referred to, and although acting detrimentally on the sales of local European importers, does not in any way diminish the total of home exports.

It simply means that the Persian merchant in this case has been somewhat more wide awake than the European trader, has got a remedy for the evil, and has acted upon it. Meanwhile the European sits in his office and complains, utterly oblivious of the fact that he can, if he so wills it, find a way to overcome all the obstacles of carriage, and help to open up a very much shorter and less expensive route. Meanwhile he has to lament over an unsatisfactory year to himself and to his clients. The latter probably, as they must be supplied, will turn to the merchants of their own country, and the European will in the future find his trade and profits gradually diminishing. When too late he will perhaps seek the means which are now available.

There has not been any increase in the arrivals of Russian goods in Ispahan; only some 300 bales or so have been brought in, and these only by small traders who have created business in the north, especially Khorasan, and buy these goods as a vehicle to recover

their money here. Indirectly, however, the advent of Russian goods has had an effect on the market of Ispahan, inasmuch as they have lately been largely imported into Tehran and Khorasan, which were, especially the former, mainly supplied from Ispahan, and this is one of the causes of the flatness and over-stocking of the market.

The Turkey-reds of the Russians are very much liked, and find a ready sale. They have lately taken to imitate some of the Manchester prints which have a vogue in the Persian markets, but so far they have not proved very successful. But a very decided attack has been opened on British trade from another and unsuspected quarter, to wit, Holland. A quantity of goods manufactured in that country have made their appearance in Ispahan during the past year. They are very fair imitations of goods hitherto made in Manchester, especially Turkey-red twills, white shirtings, and white cambrics. The trade marks are the same as on those hitherto imported from Manchester. The prices obtained are the same as were obtained for the similar Manchester goods. It would thus seem that they must leave a margin for profit.

As regards the import trade of Yezd the Consul says :

"The figures given in the list of imports of all cotton goods show a great improvement on those for 1891-92, due to the greater facilities which merchants now have of ordering goods in Bombay, and to the more extensive operations of the British firms in the place.

"In this class British goods are all one can readily find ; the small amount from Russia, as already shown, dribbles into the place a few bales at a time.

"Russian prints are only bought by the wealthier Persians, principally for carpet coverings (*ruifارشى*). They have no hold whatever on the people.

"It is quite possible that their importation may increase a little, as many merchants are extending their business northwards, and will thus come in touch with Russian trade, the mutual transactions of themselves and their agents leading to an exchange of various commodities.

"There can be no doubt that British traders would find their position firmer if they imported in a small quantity a better class of article than those at present offered by them, suitable for a rougher service than that of clothing. It is true that the Russian article is, so far, not by any means in competition with the Manchester fabrics, but merchants would do well to look ahead.

"A great discouragement to British traders consists in the large number of petty traders who have sprung up within the last few years. These men acquire a certain amount of credit, and British firms are tempted to advance them goods, then, without warning, the men stop payment, and by various frauds get off without any loss of prestige in the eyes of Persians and with half or the whole of their creditors' money in their pockets. A year or two later sees the same man started with renewed energy in some other town."—(*No. 1662, Foreign Office Annual Series.*)

VIII.—EUROPEAN SEWING THREAD IN CHINA.

The following extract from a recent report of the Austro-Hungarian Consul-General at Shanghai has been published by the Department of State at Washington, U.S.A.:—

Sewing thread on spools—cotton thread, reel of cotton, or bobbin; Chinese, *yang hsien*, i.e., foreign thread—which has become in the course of years a necessary article of use for the Chinese, has acquired a great importance at present in the foreign trade of China, and promises for the future still greater extension.

In the trade statistics published by the Chinese Customs authorities, sewing thread has only appeared since the year 1887. The quantities and values imported into the whole of China were:—

Year.	Quantity.	Value.
	Lbs.	£
1887	114,661	7,200
1888	198,750	11,400
1889	155,998	9,600
1890	254,692	14,500
1891	237,188	13,200
1892	275,082	15,500
1893	210,144	11,300
1894	279,748	16,400

The decrease in the imports which took place in 1893 was to some extent occasioned by the decline of silver prices and the subsequently unfavourable exchange rates. The importers were forced to demand higher prices, which naturally tended to check the sale. As chief sources of supply England and Belgium must be named; Germany, however, has lately joined them.

Although it is not yet ten years since Belgium first appeared with its manufactures on the Chinese market, it has already good results to record, especially in the south of China. Almost the half of the southern requirements is now furnished by Belgium; and as also in the north the importation of Belgian goods is being constantly increased, this country is becoming every year a more dangerous rival for the English manufactures. Though the Belgian goods are far inferior to the English in quality, yet the former have a point in their favour which plays a very important part with the Chinese, and will probably turn the scale for the Belgian goods, viz., cheapness. It is only this which has enabled them to obtain a footing in China, and to supplant the better, but more expensive, English thread.

The English article was introduced into China many years ago. The English goods of Coates and those of Brooke and Clarke are

well known and approved, and as such obtain higher prices than new unknown goods, even if the quality of the latter is quite equal to that of the English article. The Chinaman is thoroughly conservative, and holds with incredible tenacity to the commodity which has for years been found good and useful. To introduce a new article with success among the Chinese, it is not only necessary to work for some years without any profit, but it is also extremely advisable to distribute numerous samples gratis. The importer in Shanghai even engages Chinese for this purpose, whom he sends into the interior to distribute the samples to the consumers directly, whereupon the agent tries to obtain orders. In this way, the attention of a large number of Chinese shopkeepers is called to the new article; the small consumer will naturally be glad to buy goods made accessible to him in this way, and it is an acknowledged fact that to such a proceeding English thread owes its place in the great Chinese market.

Austria and Germany have never made energetic efforts to obtain a share in the Chinese trade, and could hardly compete with Belgium in prices, although German goods, for instance, are cheaper than English.

The thread that comes into the market is of six and three cords, and it is chiefly in the latter that Belgium competes with the English article. Six-cord thread is sold only in 200 yards, three-cord thread in 100 and 200 yards.

The leading colours are white, black, and light and dark blue. White amounts to about 60 per cent. of the consumption. Next to white comes black; a small portion is also imported in blue, while the other colours do not amount to much. The kinds mostly asked for are:—

Two hundred yards, six cords.—No. 8, 4 gross; No. 10, 1 gross; No. 12, 1 gross; No. 16, 4 gross; No. 20, 1 gross; No. 24, 1 gross; No. 30, 4 gross; No. 36, 3 gross; No. 40, 4 gross; No. 50, one-half gross; No. 60, one-half gross; No. 70, one-half gross; No. 80, one-half gross—total, 20 gross per case.

Two hundred yards, three cords.—No. 20, 1 gross; No. 24, 1 gross; No. 30, 1 gross; No. 36, 3 gross; No. 40, 7 gross; No. 50, 5 gross; No. 60, 5 gross; No. 70, 1 gross; No. 80, 1 gross—total, 25 gross per case.

One hundred yards, three cords.—No. 8, 2 gross; No. 10, 2 gross; No. 12, 3 gross; No. 16, 3 gross; No. 20, 3 gross; No. 24, 2 gross; No. 30, 2 gross; No. 36, 2 gross; No. 40, 3 gross; No. 50, 2 gross; No. 60, 1 gross—total, 25 gross per case.

The market prices for Belgian manufactures are, approximately, 3·60 to 3·75 taels per gross for 200 yards, six cords; 2·45 to 2·60 taels per gross for 200 yards, three cords; 1·50 to 1·60 taels per gross for 100 yards, three cords. For the English article of Coates, 4·30 to 4·50 taels per gross for 200 yards, six cords.

The most usual packing is in white cardboard boxes of one-twelfth of a gross, 12 of such boxes being made up in blue paper into a package; each case contains 25 gross. On the lid of each

box there must be a large label printed in black or red, showing, besides a stately factory, the way in which the article in question is placed upon the reels.

White thread is required on black spools; black and blue on white spools. The English make-up is regarded as standard. New reels varying from the English form are looked upon by the dealers with mistrust, and would greatly hinder the introduction of new manufactures, if not entirely prevent it.

Besides the above-mentioned black, white, and blue threads, the so-called glazed thread in various colours, with 50 yards on a reel, forms a very considerable article of import. In this case, also, it is England and Belgium which command the Chinese market, though Germany has had some success as a competitor. The glazed thread is packed in cardboard boxes, covered with coloured glazed paper, blue or red, and provided with a glass lid. The spools are of unpolished wood and have a label above and below, one of which shows the trade mark (*chop*) and the number of the thread, as well as the Chinese name of the firm (*hong*), while the other contains a statement of the length in yards and the name of the manufacturer or importer, and sometimes the words, "made in Germany," or "made in Austria." A wooden chest with tin casing contains 100 boxes.

It is generally difficult to give definite statements as to colour, because of the various uses in the different provinces. In glazed thread, blue of all shades, especially dark indigo blue, always plays an important part, as the Chinese prefer this colour for their costumes.

The sale of new articles in China should only be placed in the hands of a single firm, well acquainted with the import trade, for the Chinese will very soon take advantage of the offer of one and the same article by several firms, to beat down the price, to the ultimate disadvantage of the manufacturer. It must further be taken into consideration that a newly-introduced article can in no case count upon obtaining the market prices of Coates, even if the new commodity is equal or superior to that of Coates. The Belgian and German articles already introduced are actually quoted at 5 to 10 per cent. cheaper, according to quality, than those of Coates.

IX.—THE CAMPHOR TRADE.

The following particulars concerning the camphor trade are based on the "Official Returns of Trade" for 1894, compiled by the Chinese Imperial Maritime Customs, on reports by (1) Sir Ernest Satow, Her Majesty's Minister at Tokio in 1895; (2) Her Majesty's Consul at Hiogo in 1892 and 1895; (3) M. Klobukowski, Consul-General for the French Republic at Yokohama; and (4) on a special report on the island of Formosa, furnished by Mr. N. Perkins, Assistant in Her Majesty's Consular service.

In times gone by camphor was produced in Sumatra and Borneo and in other parts of the East Indies, as well as in China; now, however, the crude camphor of commerce is a product of Japan proper and the island of Formosa exclusively.

Great difficulties remain to be surmounted before the Formosa trade can be depended on, for the trees are only found inland, where the inhabitants are aboriginal barbarians, who make matters most unpleasant for explorers.

The Formosa drug is inferior to the Japanese; the latter, by reason of its greater purity, pinkish colour, and bold grain commanding in foreign countries higher prices, by some 2 per cent. or 3 per cent., in spite of its comparative abundance.

The districts in Japan famed for camphor trees are Kiushu, Shikoku, Iga, Suruga, Isé, and Kishiu. The forests owned by the people are almost denuded of timber, and soon very little will be left. However, the Government forests are still very rich in camphor trees, and it has been estimated that this alone will maintain annually, during the next 25 years, the full average supply of crude camphor.

Formerly very little care was bestowed upon the preservation and cultivation of this valuable timber. More recently, however, not only the Government but also the people have been giving to this most important question the attention it long ago deserved.

The roots contain a much larger proportion of camphor than the trees, 10 lbs. of crude camphor out of 200 lbs. of wood-chips being thought satisfactory. The Suruga timber yields a much smaller percentage.

It appears that the only process of extracting camphor from the wood among the mountains in Japan, and the materials used, are of the roughest and most unscientific description. The wood-chips are boiled, the vapour being conducted into a receptacle containing several partitions surrounded by cold water. In the sides of these partitions are apertures, which cause the vapour to fill the divisions by a circuitous route, thus improving the grain of the camphor.

The crude article is brought to market, as a rule, in very rudely constructed wooden tubs.

To make it fit for shipment requires much work, diligence, and experience. Each tub is carefully sampled, vertically and diagonally, and the samples are tested by fire and sometimes by alcohol. If no solid adulterant is discovered, the condition of the

drug is next inquired into, the difficulty of this step being best explained by at once stating that this crude camphor contains a quantity of water, or oil and water, varying anywhere between 5 per cent. and 20 per cent.

This trouble overcome, the rest is comparatively easy, and consists in weighing, cutting, mixing, and packing for shipment, the packing being in tubs prepared on the premises, partly out of the original packages.

Of course the camphor cannot be packed in its crude condition, much of it has to be drained and dried, and frequently a special parcel of "dry" must be bought, at a very high figure, for mixing with it.

Exports of camphor from Japan appear to have increased in value, in proportion to the quantity, during the last 10 years, as the following table will show. In this case the value of the yen has been converted, at the average rate for the years given, which varies in each case.

Export of Camphor from Japan.

Years.	Quantity.	Value.
	Lbs.	£
1886	7,267,000	151,000
1887	8,637,000	179,000
1888	6,074,000	153,000
1889	6,629,000	213,000
1890	5,952,000	314,000
1891	5,905,000	272,000
1892	1,419,000	186,000
1893	3,317,000	167,000
1894	2,762,000	109,000

An intelligent and well-directed exploitation of the island of Formosa, where the camphor tree is so abundant, would probably have the effect of lowering prices, by increasing the amount of camphor placed on the market.

The principal countries dealing in camphor are Hong Kong and the United States. The exports from Japan in 1894 were as follows :—

Countries.	Quantity.	Value.
	Lbs.	£
Australia - - - - -	10,600	918
India - - - - -	78,300	6,096
China - - - - -	6,600	268
France - - - - -	4,100	302
Germany - - - - -	13,100	7,459
United Kingdom - - - - -	108,600	5,928
Hong Kong - - - - -	1,654,900	87,524
United States - - - - -	772,500	37,464
Other countries - - - - -	2,900	162

Hong Kong is only a transit port where the camphor is refined previous to exportation to Europe.

A special feature of the year 1894 was a material reduction in business in camphor from Japan (as distinguished from camphor from Formosa, now also a part of Japan) in comparison with former years, attributable perhaps to the ever increasing production of Formosa and the gradual improvement of the quality of the island product.

During the year there was a fraudulent attempt at mixing inferior Formosa camphor with Japanese produce, and passing it off as Japan camphor. Through the energetic and effective measures which were at once taken by leading camphor exporters, supported by the Japanese Chamber of Commerce, the attempt was discovered at once and no such mixtures left the ports.

Another feature of remark was the formation of a guild amongst the native sellers, with the declared object of establishing a much-needed reform in quality by introducing certain new and uniform delivery terms, and these have no doubt done away with a great deal of the trouble and disputes previously existing.

Mr. N. Perkins, Assistant in Her Majesty's Consular Service, in his report on the island of Formosa, dated January 31, 1895, states that in the north of the island the trade in camphor is rapidly growing in importance; it is, however, attended by considerable risks, as heavy advances have to be made, and sometimes, whether due to the raids of savages or other causes, without any return, either in cash or camphor.

New districts are from time to time opened up; the chief at present being Hoonim or Yünlin, Kagee or Chia-I, Chip Chip, Polihisia, and Ao-doâ-pö.

According to the returns of trade for the year 1894, compiled by the Chinese Imperial Maritime Customs, the exports of camphor from Tamsui in 1894 (almost entirely to Hong Kong) represented 91 per cent. of the total value of the trade, the total shipments having been 3,708,000 lbs., of the value of 569,365 H. taels (about 91,000*l.*).

From the port of Tainan during 1894 the amount of camphor exported was 1,565,000 lbs., of a value of 263,878 H. taels (about 42,200*l.*).

Tainan and Tamsui are the only two ports of Formosa where there is a camphor trade of any importance.

There are no available figures as to the amount of camphor exported from Formosa for any period subsequent to 1894, but according to a recent report of Sir Ernest Satow, Her Majesty's Minister at Tokio, the amount exported from Japan during the first six months of 1895 was 1,530,000 lbs., of a value of 649,457 yen (about 69,000*l.*).

X.—TEA AND COFFEE CULTIVATION IN INDIA.

From official statistics published by the Department of Revenue and Agriculture of the Indian Government it appears that the area under tea in India at the end of 1894 extended over 422,551 acres, a little less than two-thirds of this area (nearly 64 per cent.) being in the valleys of the Brahmaputra and Surma which contain as much as 268,796 acres, 154,284 in Assam (the Brahmaputra Valley), and 114,512 in Cachar and Sylhet (the Surma Valley). In extent of cultivation Bengal comes next, though the acreage is much smaller than in either of the divisions of Assam, the area under tea being 121,121 acres or about 29 per cent. of the whole. In the North-Western Provinces the area under tea in 1894 was 7,652 acres, in the Panjab, 8,921 acres; in Madras, 6,102 acres; and in Travancore and Cochin, 9,079 acres. There is, besides, a smaller area of 880 acres in Burma.

The area under tea has expanded from year to year without a pause during the last 10 years. In 1885 the area was 283,925 acres; in 1894 it had increased to 422,551 acres, the increase being in the ratio of 48·8 per cent.

The average of the acreage added in the last five years was very much larger (17,770 acres) than the average increase (12,444 acres) in the four preceding years. The largest increase of all, it may be observed in passing, was in the two years 1893 and 1894, the former of these being the year in which the mints were closed, an event which was regarded by some as the herald of disaster to the tea-planting industry.

The quantity of tea produced has increased in 10 years in much greater ratio than the area under cultivation, for, while the area has increased by less than 49 per cent., the increase in production has been over 88 per cent.

In 1892 the production in Assam and Cachar declined, the decline in Cachar and Sylhet especially being so considerable as to affect the general results, though there was a large increase in Bengal and elsewhere. In 1894 there was again a decline, but a smaller one, in Cachar and Sylhet, and in Bengal there was but a small increase.

The number of persons employed in the tea industry in 1894 is returned at 383,505 (permanently) and 156,120 (temporarily), or altogether a little over half a million people (539,625 persons), which would work out to about 1·28 persons to the acre. The accuracy of the figures is, however, open to question.

The tea produced in India is exported mainly to the United Kingdom, to the extent of about 96 per cent. of the average production. The subjoined figures give approximately the quantity of tea consumed in India, the figures representing the average of the five years ending 1894-95.

Indian tea :

			Lbs.
Produced	-	-	124,971,905
Exported	-	-	120,439,095
<i>Left in India</i>	-	-	4,532,810

Foreign tea :

Imported	-	-	6,789,337
Re-exported	-	-	4,341,044
<i>Left in India</i>	-	-	2,448,293

Thus the average consumption of all tea in India seems to be about seven million pounds, of which four and a half million are Indian and two and a half million are foreign. It may be added that Ceylon provides India with nearly a million pounds of the foreign tea imported.

This consumption comes to only 0.024 lb. per head of the population according to the census of 1891, or say one-fortieth of a pound, a quantity which contrasts strongly with a consumption in the United Kingdom of from $5\frac{1}{4}$ to $5\frac{1}{2}$ lbs. per head. Broadly, it may be said that the India consumption of tea per head is exceeded by the United Kingdom more than 200 times. Indeed, such as it is, the consumption of tea in India is to a substantial extent due to the European population, for more than one million pounds are taken by the Commissariat for the British army, and at least an equal quantity must be consumed by the European civil population. However, the consumption is increasing amongst the population of the larger towns, especially the Mohammadans, and there is room for great expansion in this direction.

The prices of tea in Calcutta have fluctuated greatly since 1873. Taking the price in March 1873 to be represented by 100, it appears that until 1884 the level was well above that point, varying from 110 to 148. Coincidentally with the great fall in exchange and in general prices in 1885 the level fell to 90, and though in 1886 it rose to 96, a low level was maintained in the following years until 1894. In 1895 the level rose to 97. These figures all represent the course of prices of fine Pekoe in January of each year as given by the Bengal Chamber of Commerce.

In the Statistical Bureau the average prices of the various descriptions of tea sold at the public sales held in Calcutta during the tea season have been computed for some years past, and the results are regularly published in the review of the trade of India.

Turning to the coffee industry, the same statistics show that there were, at the end of 1894, 289,080 acres of land under coffee in India, all of it, with the exception of 10,746 acres in Burma, being in Southern India. The cultivation of coffee is in fact restricted for the most part to a limited zone in Mysore, Coorg, and the Madras districts of Malabar and the Nilgiris. In Mysore there are 136,052 acres, in Coorg 71,181 acres, and in the Nilgiris and Malabar 45,652. If to these are added 6,587 acres in Travancore and Cochin it is seen that about 90 per cent. of

the coffee-bearing area of India is concentrated in the hilly region above the south-western coast, where the rainfall is heavy and the climate generally approximates to that of the coffee-bearing area of Ceylon.

In the Madras Presidency coffee is not grown to any extent except in the two districts already mentioned, and in Salem and Madura. The only other province in which coffee is grown is Burma, mostly in Toungoo, and the industry there is of recent origin.

In the last 10 years the area in the Madras districts has fluctuated, remaining in 1894 at but little above the level of 1885. In Coorg there was a sudden and large increase in 1894, in Mysore there has been a steady and considerable increase, while in Travancore and Cochin there has been no advance; in Travancore, indeed, many coffee-growers have abandoned that industry for the less precarious cultivation of tea.

The yield has fluctuated greatly; in 1894 it was about $35\frac{1}{4}$ million pounds, which was but little larger than in 1885, notwithstanding the increased acreage.

According to the returns, there were, in 1894, 37,903 persons permanently and 118,014 temporarily employed on the coffee estates, making a total of 155,917 persons, which is equal to about one person (1.07) to two acres, while in tea estates the average is over $2\frac{1}{2}$ persons to two acres. The difference is explained by the much greater labour required in the repeated plucking of tea, and in the subsequent processes of preparing the leaf for the market.

The following figures show the average of the production and exports for the five years ending 1894-5:—

Indian coffee—				Lbs.
Production	-	-	-	34,444,087
Exports	-	-	-	31,595,514
<i>Left in India</i>	-	-	-	2,848,573
Foreign coffee—				
Imports	-	-	-	1,820,426
Re-exports	-	-	-	585,245
<i>Left in India</i>	-	-	-	1,235,181

It appears, therefore, that nearly 92 per cent. of the production is exported, and that of the coffee consumed in India foreign coffee represents less than half the quantity of Indian coffee. The rate of consumption is little more than half that of tea, amounting to only 0.014 lbs. per head of the population. It is said to be rather freely drunk by the native population in southern India, but that is certainly not so in northern India.

There is no trustworthy or complete record of the prices in India of Indian coffee; and it appears that there are no materials for the preparation of a record of prices, and that the prices in fact depend upon and follow the fluctuations of prices in London of Ceylon plantation coffee, the price of Indian coffee being about 5s. per cwt. less than the price of Ceylon coffee. This being so,

the prices in London, in February of each year of the last 10 years, of Ceylon coffee (plantation) are here subjoined :—

Per Cwt.				Per Cwt.			
		s.	d.			s.	d.
1886	-	-	56 0	1891	-	-	104 6
1887	-	-	77 0	1892	-	-	105 0
1888	-	-	72 0	1893	-	-	108 0
1889	-	-	90 0	1894	-	-	100 0
1890	-	-	99 0	1895	-	-	104 0

Ten years, from 1879 to 1888, of depressed prices, combined with the havoc wrought by the borer and the leaf disease, greatly discouraged coffee-planting in India and Ceylon, and the prospects of the industry seemed so forlorn that both in Ceylon and India much coffee land was placed under tea. In 1889, however, there was a sharp rise in prices, and the level has ranged high since that year under the operation of speculative corners, political troubles in Brazil (whence the main supplies of the world are derived), and other circumstances. The maintenance of prices at their present comparatively high level has given to the Indian coffee planter a stimulus and an encouragement which were greatly needed.

XI.—THE OPENING UP OF SOUTH AFRICA.

(FROM A FRENCH POINT OF VIEW.)*

Johannesburg, the chief town of the Rand, which was only connected with the sea by rail in 1892, can no longer complain of a deficient means of communication, as the lines of rail starting from it terminate at no less than five different points of the south and south-eastern coast of Africa, viz., Cape Town, a distance of 1,007 miles; Port Elizabeth, 714 miles; East London, 665 miles; Durban (Natal), 437 miles; and Lourenço-Marquez (Delagoa Bay), 396 miles. All these railways have a common trunk about 11 miles in length from Johannesburg to Elandsfontein junction, and it is over this, the only double line of rails in South Africa, that passes the enormous goods traffic of the capital of the gold fields in addition to the coal mined at Boksburg for the use of the central and western Rand. The trains run on this portion at short intervals, though not so frequently as they might, and many complaints are made as to the disorder consequent on goods delayed and stations encumbered.

At Elandsfontein, the Delagoa Bay line branches off to the north passes by Pretoria and then bears again towards the east; the Natal line takes a south-easterly direction, and that of the three Cape Colony ports a south-westerly direction, not dividing into its three branches, however, till after it has traversed the whole of the Orange Free State.

Although the land journey is much longer, the Cape Colony lines have hitherto monopolised the greater part (80 per cent.) of the Rand traffic, having made great reductions in the cost of transport of machinery and certain food products. Cape Town takes most passengers, and Port Elizabeth the greater part of the merchandise. This latter town, the centre of the really English part of the colony, has developed greatly since the discovery of the gold mines. Its population (25,000 in 1891) has largely increased, and the importance of its import trade surpasses that of Cape Town.

East London, though rather nearer to Johannesburg, cannot compete with Port Elizabeth, for the reason that the harbour is a poor one and entrance impossible to large vessels on account of a sand-bar which reforms as soon as removed; and although Port Elizabeth is also badly off from this point of view, and the largest vessels cannot unload at the quays, yet, being 311 miles nearer to the Rand, it is a less costly route than *viâ* Cape Town.

Durban (Natal) has also a harbour guarded by a shifting sand-bar, which, however, has been rendered accessible, except in bad

* The opinions of M. Pierre Leroy-Beaulieu 'as expressed by him in an article published in the "Economiste Français" of the 22nd February last.

weather, to vessels drawing from 13 to 16 feet of water. This town had the monopoly of the Transvaal trade previous to 1892, until the completion of the railway from the Rand to Cape Town, but since then it has lost it all. The Durban-Johannesburg line was opened on the 15th December 1895, and by means of this route Natal may possibly recover an appreciable part of the trade which she had lost; indeed it seems likely that Durban will be the port of entry for the Heidelberg district, to the prosperity of which Natal has greatly contributed.

But the port, above all, which should serve as the starting-point for all goods for the Rand, and indeed for the entire Transvaal, is Lourenço-Marquez on Delagoa Bay. While the entrance to the harbours of Port Elizabeth, East London, and Durban are obstructed by shifting bars, and the roadsteads of these places are in no way protected from the south and south-easterly winds, and while the entrance even to the harbour at Cape Town is often difficult in bad weather, the vast and deep waters of Delagoa Bay, sheltered from the tempests of the open sea by a long tongue of land, present the best anchorage for vessels to be found anywhere in Africa south of the equator. The largest ships can enter freely and approach right up to the shore. Unhappily, this magnificent port has not yet attained that importance which must one day belong to it. Goods cannot yet be landed on the quay, and once they are on shore must be left to the sun and rains of a tropical climate, for want of warehouses and sheds in which to await the loading on the cars. A mole is being built 260 yards long, the end of which will be in deep water, where four vessels can unload at a time.

Lourenço-Marquez is extremely unhealthy, the town being built in the middle of swamps, and the mortality amongst Europeans being exceedingly high. Drainage works are, however, in contemplation, and it is proposed to establish a settlement close to the town at a height where it is much less unhealthy. The population is between 3,000 and 4,000 of which 1,000 are Europeans (among whom 150 English and 700 Portuguese), 500 Hindoos, and the remainder negroes.

A branch of the Pretoria line is talked of to Barberton, centre of the Dekaap district, which is only 143 miles from Delagoa Bay, and other lines will connect it with Lydenburg and the mining camps of the North-East. Notwithstanding the bad arrangements at the port, the Delagoa Bay railway, open hardly a year, takes nearly 20 per cent. of the goods entering and leaving the Rand, and when certain improvements have been made at Lourenço-Marquez it ought to absorb at least three-fourths of the whole.

All the South African railways are of the narrow gauge of 3 feet 6 inches, instead of the normal gauge of European lines. The narrower gauge favours a better adaptation of the line to tunnels and inclines, a most important fact in a country where lines attain an altitude of 5,723 feet as at Johannesburg.

For example, the Hex River Pass, by which the Johannesburg Cape Town line mounts the Karrou plateau, winding in and

about the slopes of a deep ravine with but a few short tunnels, would have necessitated much more extensive ones had the wider gauge been employed.

The English seem to be able to distinguish (and the French do not) between a railway in a new thinly-populated country, and one which passes through the richest parts of Europe; and they do not build at frequent intervals stations of brick or stone, where numerous employés are kept in idleness, and there is no traffic to take up. In the Karrou district, where the settlements are few and far between, the distance separating two consecutive stations is sometimes as much as 31 or even 39 miles; several of these stations are mere sheds where there are no employés, and there are, in addition, halting places where there is no building whatever, the name being simply indicated on a board nailed to two poles. When a traveller from a neighbouring farm wishes to take the train he waves a flag by day, or a lantern by night, and the driver stops to pick him up.

There is one train a day from the ports to Johannesburg and Kimberley, and, on the districts of the coast, and especially in the neighbourhood of Cape Town, there are sometimes more. But beyond Kimberley, on the road to Mafeking, the actual terminus of the railway, there are only three trains a week. This is the proper way to construct and exploit colonial lines, and is very different from the method followed by France in Algeria.

The Cape railway system, including the lines of the Orange Free State, worked by the railway administration of the Colony, and including also the extension of the Kimberley-Mafeking line in British Bechuanaland, amounts to 2,174 miles in length. In addition there are 404 miles of railway in Natal, and 497 in the Transvaal, including the short section of the Delagoa Bay line, which traverses the Portuguese possessions. There are thus upwards of 3,100 miles of railway connected one with the other in South Africa.

This railway system will be much increased some few years hence when it is extended into the territories of the Chartered Company. Gold is found in those parts in many localities, and amongst the numerous mines worked some no doubt will pay and others will not, but what the proportion will be cannot at present be estimated, the information to hand as to their richness being confused and often contradictory. The extreme difficulty, added to the great cost of transport of material, first by railway to Kimberley and later on to Mafeking, and then on by bullock waggon for 497 miles to Buluwayo, or 810 miles to Salisbury, has certainly much retarded the mineral working in these parts. It is pointed out that the Rand produced gold to the value of 2,400,000*l.* in 1891, and 4,000,000*l.* in 1892, although the railway was not opened before the end of the latter year; the difficulties of transport and setting up were, however, far less in the Transvaal than in the Chartered territory, owing to the white population of Boers with their numerous bullocks.

Rhodesia already can boast of important towns. Buluwayo and Salisbury have each 2,000 to 3,000 white inhabitants, with numerous brick buildings, churches, hotels, and clubs, &c., and it should be remembered it is but a short time since Lobengula's kraal occupied the site of the former of these places. A third town, Umtali, in Manicaland, near the Portuguese frontier, counts some hundreds of whites in its population. In three or four years at the latest the railway, which the Chartered Company is rapidly constructing, will have been extended from Mafeking to Buluwayo. Already a line runs across the Portuguese territory connecting Fontesvilla, on the lower Pongoné, with Chimoio, 118 miles off, on the frontier of the Chartered territory, and 217 miles from Salisbury. In this way is passed the belt of the *tsetse* fly, whose bite is harmless to man but mortal for horses and bullocks. Within a year this line will be extended, on the one hand to the good port of Beira, on the other to Umtali, which is only 149 miles from Salisbury, and in the neighbourhood of which are many important gold mines. The railway will doubtless reach Salisbury about the same time as Buluwayo, and it is proposed then to connect these two towns and carry the line on to the Zambesi. If gold be found in Rhodesia in any great quantity the country will soon have 100,000 inhabitants.

In the whole of British and Boer South Africa there are about six millions of inhabitants, of whom from 750,000 to 800,000 are white. It seems probable that the proportion of the three races, English, Dutch, and coloured, will change very little, for the coloured people flourish under European rule; and if, on the one hand, a stream of Anglo-Saxon emigrants, for the most part only temporary, flows into South Africa, on the other hand the Boers are also prolific and form the stable agricultural population, who will not leave the country when the gold mines are exhausted. European capital will doubtless find plenty of openings in South Africa, a country whose varied resources are as yet so little developed. Traders, manufacturers, and farmers with capital, and artisans, will all find remunerative employment for their energy. But it is by the labour of the blacks, directed by Europeans, that South Africa will be finally exploited, and perhaps the whites will do well never to completely sever the ties which bind them to their own countries.

XII.—THE CUSTOMS TARIFF OF GUATEMALA

ALTERATIONS AND MODIFICATIONS.

A despatch dated January 22 last, has been received from Mr. J. Frederick Roberts, Her Majesty's Chargé d'Affaires at Guatemala, enclosing translation of a Decree authorising additions and reforms in the present Customs tariff (of 1894).

In the following translation of the Decree in question, the articles against which no numbers are placed, are additions to the various sections of the tariff, and italicised portions of any number betoken the alterations made in the wording of that number as compared with the original tariff.

No.	Article.
SECTION 2.	
<i>Articles free from Import Dues.</i>	
A.	
26	Printed advertisements lithographed or without frames, <i>provided they have no determined use.</i>
B.	
35	Salt codfish <i>and other dry fish</i> , salt or smoked, in wooden cases.
<i>Articles.</i>	
48	Carbons <i>for galvanic fountains.</i>
78	Filters <i>of all classes</i> not specified <i>for domestic use.</i>
96	Scientific instruments not specified.
	Organic liquor of the brown Sequar formula for hypodermic injections, &c.
2775	Galvanic fountains and their loose pieces (without salts or acids for working) of any system or manufacture.
	Wooden spades for agriculture.
	Attenuated virus, such as the antidyphtheric serum of Doctor Rouex, the anti-rabic of Doctor Pasteur and other products framed by bacteological attenuations.

No.	Article.	Duty.
SECTION 3.		
<i>Cotton Articles.</i>		Dols. cts.
214	Dress pieces of cloth of cotton and silk, as cambric, muslin, lawn, or any other similar material adorned or embroidered, with weight of package - - -	Kilos. 5 00
219	Bed ticking of cotton, gross weight - - -	„ 0 50
220	Cretonnes of cotton, coloured, for mattresses or other analogous uses, gross weight - - -	„ 0 50

No.	Articles.	Duty.	
		Dols.	Cts.
220	Ribbons of cotton velvet, with weight of package -	Kilo.	3 0
	Canvas of hemp or cotton, for embroidering, with weight of package -	"	1 0
	Cotton cloth, for clothing, white or coloured, gross weight -	"	1 0
231	Cotton drill, imitation of kerseymere, gross weight -	"	0 50
	Cotton laces with silk embroideries, with weight of package -	"	8 0
254	Cotton gauze, worked or embroidered in the loom, with weight of package -	"	2 0
267	Chintzes or prints, <i>cotton Batistes</i> , gross weight -	"	0 70
	Cotton goods, embossed, marbled, listed, with work or designs for making rebozas, gross weight -	"	0 70
269	Baby's dresses, skirts, and apparel of cotton material, adorned or embroidered with the same material, with weight of package -	"	7 0
270	Baby's dresses, skirts, and apparel of cotton material for children, ornamented or embroidered with linen, wool, or silk, with weight of package -	"	8 0
273	"Lamas" of cotton, <i>with weight of package</i> -	"	3 0
280	Lustring of cotton, with spangle or tinsel of ordinary metal, with weight of package -	"	5 0
	Wicks of cotton, loose or twisted, for candles, even if prepared with boric acid or other substances, with weight of package -	"	0 25
291	Manufactures of <i>cloth or stocking-net</i> of cotton, not specified, with weight of package -	"	1 30
	White cotton Batiste, gross weight -	"	1 0
300	Moleskins, of cotton, white or coloured, gross weight -	"	0 50
304	Handkerchiefs of <i>cambric</i> , lawn, or muslin, of cotton, plain, worked, or embroidered, gross weight -	"	1 0
305	Handkerchiefs of cotton, madapolam, jean, bandanna, serged imitation silk, <i>always provided that they are not of cambric, muslin, or lawn, "paleacates," &c.,</i> gross weight -	"	0 70
329	Serviettes of damask linen or cotton damask, with weight of package -	"	1 0
	Sacks of hemp, gross weight -	"	0 30
	Cloth textures which contain silk, always provided that this material does not form part of the warp or the woof, and that it is only to be found in small quantities in narrow separate slips or in small knots, with weight of package -	"	2 0
365	Morning gowns of any kinds of linen, pure or mixed, plain, ornamented, or embroidered, in the same material	Each	6 0
366	Morning gowns of any kind of linen, pure or mixed, with ornaments of wool or silk -	"	8 0
	Little bags of hemp or jute or <i>pita</i> , for holding coffee, gross weight -	Kilo.	0 10
402	Head-dresses of lawn or linen, not pure, or mixed, plain, ornamented, or embroidered -	Doz.	9 0
403	Head-dresses of linen, pure or mixed, with ornaments of wool or silk -	"	9 0
414	Pieces of linen, pure or mixed, ornamented or embroidered with wool or silk, for women's dresses, with weight of package -	Kilo.	5 0
429	Embroideries of cloth of linen, pure or mixed, with weight of package -	"	3 50
447	Hammocks of hemp, netting, or jute, with weight of package -	"	0 75
452	Baby's dresses, skirts, and apparel of linen, pure and mixed, plain, ornamented or embroidered in the same material, with weight of package -	"	7 0

No.	Articles.	Duty.
		Dols. ets.
453	Baby's dresses, skirts, and apparel of linen, pure or mixed, ornamented or embroidered with wool or silk, with weight of package - - -	Kilo. 8 0
469	Muslins of pure or mixed linen, white or coloured, with weight of package - - -	„ 1 50
496	Parasols of linen, pure or mixed, with ornaments or embroideries of cotton or wool - - -	Each 1 0
497	Parasols of linen, pure or mixed, with ornaments or embroideries of silk - - -	„ 1 50
499	Fancy cloths of linen, pure or mixed, white or coloured, with weight of package - - -	Kilo. 1 50
502	Strips of linen cloth, pure or mixed, ornamented or embroidered with wool or silk, with weight of package -	„ 8 0
504	Dresses or gowns of linen, pure or mixed, not specified, and their parts, when sewn, of all classes and stuffs, although they have ornaments of lace or slips embroidered with cotton or linen strips, ribbons of silk, or ordinary metal for adults and children over eight years of age, with weight of package - - -	„ 8 0
505	Dresses or gowns of linen, pure or mixed, not specified, and their separate parts when sewn and ornamented with silk or part silk stuff for adults and children over eight years, with weight of package - - -	„ 9 0
506	Dresses or gowns of linen, pure or mixed, not specified, and separate parts, when sewn, which have petticoats or over petticoats of lace or cotton or linen, for adults and children over eight years old, with weight of package - - -	„ 9 0
509	Dresses of linen, pure or mixed, of all kinds and textures for children under eight years old, with silk ornaments, with weight of package - - -	„ 4 0
523	Morning gowns of wool, pure or mixed, with cotton or any other vegetable fibre, plain, ornamented, or embroidered with the same material - - -	Each 6 0
531bis	Tassels of wool, pure or mixed, though they have interior of other materials, <i>with silk ornaments, provided this material does not predominate in the surface</i> , with weight of package - - -	Kilo. 6 0
563	Jackets or coats of any kind of wool, pure or mixed, with ornaments or embroideries of the same stuff, for women -	Each 5 0
568	Ribbons or threads of velvet, pure wool or mixed, with cotton or any other vegetable fibre, with weight of package - - -	Kilo. 4 0
572	Head dresses of any kind of wool, pure or mixed, plain, ornamented, or embroidered with the same stuff -	Dozen 9 0
578	Corsets of wool, pure or mixed, with or without ornaments, with weight of package - - -	Kilo. 4 0
587	Collars of cloth or wool, pure or mixed, plain, ornamented, or embroidered, with weight of package -	„ 10 0
599	Skirts of wool, pure or mixed, plain, ornamented, or embroidered for children, with the weight of package -	„ 7 0
600	Skirts of wool, pure or mixed, ornamented or embroidered in silk for children, with weight of package - - -	„ 8 0
605	Fringes of wool, pure or mixed with ornaments of silk, provided the latter do not predominate in the surface, with weight of package - - -	„ 8 0
613bis	Embroidery lace (galloon) of pure wool with silk ornaments, always provided that the latter material does not predominate in the surface, with weight of package -	„ 8 0
614	Caps of any material or stocking net of pure or mixed wool, ornamented or embroidered in the same stuff, with or without peaks - - -	Dozen 6 0

No.	Articles.	Duty.
		Dols. cts.
620	Woollen thread for weaving or embroidering, gross weight	Kilo. 0 50
648	Muslins of wool, pure or mixed, containing silk, with weight of package - - -	„ 3 0
688	Dresses or gowns of wool, pure or mixed, of cotton or any other vegetable fibre not specified, and their loose parts, sewn, of all kinds and textures, though they may have ornaments of lace or slips embroidered with wool or silk, ribbons of silk or common metal, for adults and children over eight years old, with weight of package -	„ 8 0
690	Dresses or gowns of wool, pure or mixed with cotton or other vegetable fibre not specified, and their separate parts, when sewn with petticoats or over petticoats of lace or cotton netting or linen, for adults and children over eight years age, <i>with weight of package</i> - - -	„ 9 0
707	Blouses of any kind of silk, pure or mixed, plain, ornamented, or embroidered, <i>with weight of package</i> - - -	„ 20 0
712	Brocade of silk, pure or mixed, with weight of package -	„ 9 0
713	Veil stuff of silk or mixed, crimped, plain, or ornamented, with weight of package - - -	„ 6 0
714	Veil stuff of pure silk, plain or ornamented, with weight of package - - -	„ 12 0
715	Silk socks, pure or mixed, with weight of package - - -	„ 15 0
723	Shawls or <i>rebozos</i> , of silk, pure or mixed, worked, ornamented, or embroidered, with weight of package -	„ 9 0
735	Cord of silk, pure or mixed, although having an interior of other material, with weight of package - - -	„ 3 75
741	Pieces of pure silk, such as Gros, surah, moiré, satin, tafita, ottoman, and other kinds of pure silk, plain, ornamented, or embroidered with silk, pure or mixed, for women's dresses, with weight of package - -	„ 13 50
744	Crape, of silk, pure or mixed, with weight of package -	„ 15 0
746	Damask, of silk, pure or mixed, <i>for curtains or furniture</i> , with weight of package - - -	„ 6 0
	Damask, of silk, pure or mixed, bound or embroidered with thread of metal, real or false, for curtains and furniture, with weight of package - - -	„ 4 50
757	Plush, of silk, pure or mixed, <i>for curtains or furniture</i> , with weight of package - - -	„ 6 0
	Stuff known as " <i>jenéro</i> " of raw silk, plain and mixed with cotton or other vegetable fibre, with weight of package -	„ 4 50
	Stuff known as " <i>jénero</i> " of raw silk, plain and without mixture of vegetable fibre, with weight of package -	„ 9 0
	Ribbons of velvet, of silk, raw or mixed, with weight of package - - -	„ 7 50
793	Handkerchiefs of lace, <i>gauze</i> , or <i>muslin</i> , of silk, pure or mixed, with weight of package - - -	„ 15 0

SECTION 7.

Articles of Iron.

849	Hoops and bands of iron for barrels, pipes, or other industrial uses, gross weight - - -	„ 0 3
884	Chains of iron, of all classes, <i>excepting chains for key rings</i> , gross weight - - -	„ 0 25
914	Nails, of iron, forged or of wire, <i>of upwards of 24 millimetres</i> , gross weight - - -	„ 0 10
916	Iron nails with heads of brass, glass, or porcelain, <i>of upwards of 24 millimetres</i> , with weight of package - -	„ 0 25
917	Nails and screws of galvanised iron, for roofs and pulleys of galvanised iron, or of lead for the same purpose, gross weight - - -	„ 0 05

No.	Articles.	Duty.
935	Sets of carving knife and fork, common, with or without sharpeners, without including case - - -	Dols. cts. Each 0 05
936	Sets of carving knife and fork, medium and fine quality, with or without sharpeners, and wooden spoon, without including case - - -	" 3 0
938	Spoons, and large spoons of iron, tinned, enamelled, or varnished, of all sizes, with weight of package -	Kilo. 0 30
1A 962	Statues, busts, and half reliefs of iron, when <i>nett</i> weight does not exceed 10 kilos. each, gross weight - -	" 0 30
963	Statues, busts, and half reliefs of iron, when <i>nett</i> weight does not exceed 10 kilos. each, gross weight -	" 0 20
964	Statues, busts, or half reliefs, when <i>nett</i> weight does not exceed 50 kilos. each, gross weight - - -	" 0 15
966	Stoves or iron chimneys, <i>nett</i> weight of which does not exceed 10 kilos. each, gross weight - - -	" 0 20
967	Stoves or iron chimneys, <i>nett</i> weight of which does exceed 10 kilos. each, gross weight - - -	" 0 10
2A 968	Knives and side-arms ("machetes") smaller than a sword and larger than a dagger, with or without guard, such as the "machetes" called "Cock's eye" or other similar, with or without scabbard, with weight of package -	" 0 75
970	Iron figures, <i>nett</i> weight not exceeding 10 kilos. each, gross weight - - -	" 0 40
971	Iron figures, <i>nett</i> weight exceeding 10 kilos. each, gross weight - - -	" 0 30
972	Iron figures, <i>nett</i> weight exceeding 50 kilos. each, gross weight - - -	" 0 25
975	Iron fountains, <i>nett</i> weight not exceeding 50 kilos. each, gross weight - - -	" 0 25
976	Iron fountains, <i>nett</i> weight exceeding 50 kilos. each, gross weight - - -	" 0 10
984	Hinges of iron, of all kinds, <i>with weight of package</i> -	" 0 30
	Fine tools for watchmakers and jewellers, with weight of package - - -	" 1 0
999	Stoves or grain roasters, with weight of package -	" 0 30
1003	Sets of iron or tin for washers, in all kinds, complete or in parts, <i>with weight of package</i> - - -	" 0 30
1006	Lamps, of iron, put together or in part, without gilding, silvering, or nickeling, with weight of package -	" 0 30
1007	Lamps, of iron, put together or in parts, gilt, silvered, or nickeled, with weight of package - - -	" 0 50
	Iron knockers of all classes, for street doors, with weight of package - - -	" 0 30
1011	Tires of iron for wheels, <i>and iron for tires, such being understood to be the iron forged in bars, each measuring at least 5 centimetres wide</i> , gross weight - -	" 0 02
1016	English wrenches for nuts and pipes, with weight of package -	" 0 30
1019	"Machetes," fine, steeled, for agriculture, with or without sheaths, with weight of package - - -	" 0 30
1020	Iron handles for doors or windows, with or without bolt, of all classes, although with glass or porcelain, with weight of package - - -	" 0 30
1044	Basins of iron or tin, whether tinned or not, varnished or enamelled, <i>with weight of package</i> - - -	" 0 30
1047	Jars of iron, for domestic purposes, with weight of package - - -	" 0 30
	Cribs of iron, gross weight - - -	" 0 10
1054	Kettles of iron, small, for domestic uses, whether tinned or not, with weight of package - - -	" 0 25
1056	Latch keys of iron, of all kinds, for doors, with or without handles, with weight of package - - -	" 0 30
1066	Pruning knives of iron or steel, <i>with handles of wood for agriculture</i> , gross weight - - -	" 0 12

No.	Articles.	Duty.	
		Dols.	cts.
1076	Lamps of iron, put together or in pieces, without gilding, silvering, or nickeling, with weight of package -	Kilo.	0 30
1076 <i>bis</i>	Lamps of iron, put together or in pieces, silvered, gilt, or nickeled, with weight of package -	"	0 50
1077	Rat traps of iron or which contain iron, with weight of package -	"	0 30
1079	Clocks of iron or steel, for wall or table, of all kinds, with or without ornaments of other materials, with weight of packages -	"	1 0
1083	Revolvers or pistols of all kinds, with or without covers or cases, with weight of package -	"	8 0
1084	Wheels of iron for furniture, with weight of package -	"	0 30
1090	Frying pans of iron, of all classes, <i>with weight of package</i> -	"	0 30
1091	Saws for workmen, with or without handles, <i>with weight of package</i> -	"	0 30
1094	Tacks of iron, up to 24 millimetres long, gross weight -	"	0 15
1104	Pruning scissors of iron or steel, <i>for gardeners</i> , with weight of package -	"	0 30
1105	Scissors of iron or steel, of all shapes, sizes, and qualities not specified, <i>including those specially for cutting the hair</i> , without cases, with or without weight of package; Scissors, as above, with cases of all kinds, with weight of package -	"	1 0
1111	Screws of iron, small, not specified, with or without nuts, <i>with weight of package</i> -	"	2 0
1118	Curtain rods of iron or steel, <i>for carriages</i> , covered with cloth or leather, with weight of package -	"	0 30
SECTION 8.			
<i>Articles of Copper and its Alloys, Lead, and Tin.</i>			
1144	Pins of copper or its alloys, for the hair, hat, or cravat, with weight of package -	"	2 0
1179	Chains and "leontinas" of copper and its alloys gilt, silvered, or nickeled, of all kinds, for watches or key rings, with weight of package -	"	2 0
	Boxes of tin, lead, or zinc, small, for the dressing table or other analogous uses, with weight of package -	"	1 50
	Boxes of compasses of copper or its alloys, for drawing, with weight of package -	"	0 50
	Cliches of copper or brass, with weight of package -	"	1 0
	Cliches of type metal, with weight of package -	"	0 50
1213	Crosses of copper or its alloys, of all kinds, with weight of package -	"	2 0
1220	Netting for fences of copper, iron, or its alloys, with weight of package -	"	0 50
1232	Statues, busts, and half reliefs of brass, bronze, or zinc, whose <i>nett</i> weight does not exceed 10 kilos. each, gross weight -	"	1 0
1233	The same when <i>nett</i> weight exceeds 10 kilos. each, gross weight -	"	0 50
1234	The same when <i>nett</i> exceeds 50 kilos. each, gross weight -	"	0 25
1236	Figures of brass, bronze, or zinc, <i>nett</i> weight not over 10 kilos. each, gross weight -	"	1 0
1237	The same <i>nett</i> weight over 10 kilos. each, gross weight -	"	0 50
1238	The same <i>nett</i> weight over 50 kilos. each, gross weight -	"	0 25
1245	Copper fountains or alloys of same <i>nett</i> weight over 50 kilos. each, gross weight -	"	0 20
1253	Buckles of copper or its alloys <i>for vests, trousers, or straps</i> , with weight of package -	"	0 80
1255	Thread of copper or brass gilt or silvered in boxes or on reels with weight of package -	"	1 60

(To be continued.)

XIII.—CUSTOMS TARIFF OF NEW ZEALAND.

(Continued from page 174.)

No.	Articles Free of Duty.
CLASS VI.—CLOTHING AND TEXTILES.	
328	Accoutrements for military purposes, excepting uniform clothing.
329	Brace-elastic and brace-mountings.
330	Bunting, in the piece.
331	Butter- and cheese-cloth.
332	Buttons, tapes, wadding, pins, needles.
333	Calico, white and grey, also cotton sheetings, in the piece.
334	Corduroy, moleskin, and plain beaverskin, of cotton, in the piece.
335	Coloured cotton shirtings; flannelette shirtings.*
336	Forfar, dowlas, and flax sheeting, when cut up under supervision in sizes not exceeding 47 in. × 36 in. for making flour-bags, and not exceeding 54 in. for lining wool mats.
337	Fur-skins, green or sun-dried.
338	Gold or silver lace or braid for military clothing.
339	Hatmakers' materials, viz.: silk plush; felt hoods; shellac; galloons; calicoes; spale-boards for hat-boxes; leather- and linings; blocks; moulds; frames; ventilators; and tassels.
340	Hessians, plain or striped, and serim.
341	Leather-cloth.
342	Minor articles (required in the making-up of apparel, boots, shoes, hats, caps, saddlery, umbrellas, parasols, and sunshades), enumerated in any order of the Commissioner, and published in the "Gazette."
343	Sailcloth, canvas, and unbleached double-warped duck, in the piece.
344	Sewing cottons, silks, and threads; crochet, darning, and knitting cottons; angola mendings not exceeding 45 yards on cards.
345	Silk for flour-dressing.
346	Silk-twist (shoemakers' and saddlers').
347	Staymakers' binding, eyelets, corset-fasteners, jean, ticks, lasting, sateen, and cotell.
348	Tailors' trimmings, viz.: plain coloured imitation hair-cloth, canvas; plain Verona and plain diagonal, and such patterns of checked Italian cloths as may be approved of by the Commissioner of Customs; Italian cloth of cotton or wool; buckram; wadding and padding; silk, worsted, and cotton bindings and braids; stay bindings; Russia braids; shoulder pads; buckles; silesias; drab, slate, and brown jeans; pocketings; slate, black, and dyed unions and linens.
349	Umbrella-makers' materials, viz.: reversible and levantine silk mixtures, gloria, and satin de chène of not less than 44 in. in width; alpaca cloth, with border; zanella cloth, with border; also other piece-goods on such conditions as the Commissioner may approve; sticks, runners, notches, caps, ferrules, cups, ribs, stretchers, tips, and rings.
350	Union shirtings, the invoice value of which does not exceed 6 <i>d.</i> the yard.*
351	Waterproof material in the piece.
CLASS VII.—LEATHER, AND ARTICLES USED IN LEATHER MANUFACTURES.	
352	Boot-elastic.
353	Bootmakers' linings, canvas, plain or coloured, bag and portmanteau linings of such materials, qualities, and patterns as may be approved by the Commissioner.

* Whenever any dispute arises as to the application of the exemption in favour of coloured cotton, flannelette, or union shirtings, in the case of fabrics alleged to be such shirtings, the Commissioner has power to decide such dispute; and in case of doubt on his part he may require the fabric in dispute to be cut up for shirt-making under such conditions as he prescribes.

No.	Articles Free of Duty.
CLASS VII.—LEATHER, AND ARTICLES USED IN LEATHER MANUFACTURES— <i>cont.</i>	
354	Boots, shoes, and slippers, viz., children's Nos. 0 to 3.
355	Cork soles, and sock soles.
356	East India kip, crust or rough-tanned, but undressed.
357	Goatskins, crust or rough-tanned, but undressed.
358	Grindery, except heel and toe plates.
359	Hogskins.
360	Kangaroo- and wallabi-skins, undressed.
361	Leather, japanned or enamelled; goatskins, dressed as morocco, coloured (other than black).
362	Saddle-trees.
363	Saddlers' ironmongery (except bits and stirrup irons), hames, and mounts for harness; straining, surcingle, brace, girth, and roller webs; collar-check, and the same article plain, of such quality as may be approved by the Commissioner; legging-buckles.
364	Tanning materials, crude.
CLASS VIII.—FURNITURE, &c.	
365	Blind-webbing and tape.
366	Upholsterers' webbing, hair-seating, imitation hair-seating, curled hair, gimp and cord of wool, cotton, or silk, tufts and studs.
CLASS IX.—CHINA, GLASS, &c.	
367	Bottles, empty, plain glass, not being cut or ground; also, jars up to 3 in. in diameter at the mouth.
368	Glass plates (engraved) for photo-lithographic work.
369	Jars or other dutiable vessels, containing free goods or goods subject to a fixed rate of duty, and being ordinary trade packages for the goods contained in them.
CLASS X.—FANCY GOODS, &c.	
370	Action-work and keys, in frames or otherwise, for manufacture of organs, harmoniums, and pianos, organ pipes, and stop-knobs.
371	Artists' canvas, colours, brushes, and pallet-knives.
372	Magic-lanterns, lenses, and slides.
373	Microscopes and astronomical telescopes, and lenses for same.
374	Musical instruments specially imported for volunteer bands.
375	Paintings, statuary, and works of art, presented to or imported by any public institution or art association registered as a body corporate, for display in the buildings of such institution or association, and not to be sold or otherwise disposed of.
376	Photographic cameras and lenses.
377	Photographs of personal friends in letters or packets.
378	Precious stones, cut or uncut and unmounted.
379	Sensitised surfaces for photographic purposes.
CLASS XI.—PAPER, &c.	
380	Bookbinders' materials, viz.: cloth, leather, thread, headbands, webbing, end-papers, tacketing-gut, marbling-colours, marble-paper, blue paste for ruling-ink, staple-presses, wire staples, staple-sticks.
381	Butter-paper, known as parchment paper or waxed paper.
382	Cardboard and pasteboard, of sizes not less than that known as "royal."
383	Cardboard boxes, material for, viz.: gold and silver paper, plain and embossed, gelatine and coloured papers, known as "box papers."
384	Cartridge-paper for drawing-books.
385	Cloth-lined boards not less than "royal."
386	Cloth-lined papers, enamelled paper, ivorite and gelatine, metallic paper, not less than "demy."

No.	Articles Free of Duty.
CLASS XI.—PAPER, &c.—<i>cont.</i>	
387	Copy-books and drawing-books.
388	Copying paper, medium and double-foolscap, in original mill wrappers and labels.
389	Hand-made cheque-paper.
390	Ink, printing.
391	Masticated para.
392	Millboard and bookbinders' leather-board.
393	Paper, hand-made or machine-made book or writing, of sizes not less than the size known as "demy," when in original wrappers.
394	Printing-paper.
395	Printed books, papers, and music n.o.e.
396	School slates, and educational apparatus.
CLASS XII.—METALS.	
397	Anchor.
398	Artificers' tools.
399	Axes and hatchets; spades, shovels, and forks; picks; mattocks; quartz and knapping hammers; scythes, sheep-shears, reaping-hooks; soldering-irons; paperhangers' scissors; butchers' saws and cleavers.
400	Axles, axle-arms, and boxes.
401	Band-saws and folding saws, including frames.
402	Bellows-nails.
403	Bicycles and tricycles, fittings for, viz.:—rubber tires, pneumatic tires, outside covers, and inner tubes; rubber and cork handles, and pedal rubbers; also drop-forgings and stampings. ball-bearings, weldless steel tube in full lengths, rims, forks, and spokes in the rough.
404	Blacksmiths' anvils, forges, and fans.
405	Bolts, 5 in. by $\frac{1}{2}$ in. in diameter and under, and nuts for same.
406	Brass and copper, in pigs, bars, tubes, or sheets.
407	Brass tubing and stamped work, in the rough, for gasaliers and brackets.
408	Caps, percussion.
409	Card-clothing for woollen-mills.
410	Chains, trace and plough chains; or metal articles required to repair or complete riding or driving harness or saddlery to be repaired or made in the colony.
411	Copper and composition rod, bolts, sheathing, and nails.
412	Couch-roll jackets, machine-wires, beater-bars, and strainer-plates for paper-mills.
413	Crucibles.
414	Emery-grinding machines and emery-wheels.
415	Empty iron drums, not exceeding 10 gallons capacity.
416	Engineers' machine tools.
417	Eyelets.
418	Fire-engines, including Merryweather's chemical fire-engines.
419	Fish-hooks.
420	Galvanising bath, welded.
421	Gas-engines and hammers, and oil-engines.
422	Glass makers' moulds.
423	Hydraulic cranes.
424	Iron- and brass-wove wire and wire-gauze; also wire-netting.
425	Iron boiler-plates and unflanged end-plates for boilers; boiler-tubes not exceeding 6 in. in diameter, and unflanged; Bowling's expansion rings; furnace-flues.
426	Iron, plain black sheet, rod, bolt, bar, plate, hoop, and pig.
427	Iron rolled girders.
428	Iron plates, screws, and castings for ships.
429	Iron wire n.o.e., including fencing-wire, plain and barbed.
430	Lead, in pigs and bars.
431	Locomotives.
432	Machine saws.
433	Machinery for gold-saving purposes and processes.

No.	Articles Free of Duty.
CLASS XII.—METALS—<i>cont.</i>	
434	Metal fittings for trunks, portmanteaux, travelling bags, leggings, bags, and satchels.
435	Metal sheaves for blocks.
436	Metallic capsules.
437	Perambulators and the like vehicles, fittings for, n.o.e.
438	Perforated or cellular sheet zinc or iron.
439	Printing type and materials n.o.e.
440	Rails for railways and tramways.
441	Reapers and binders, reaping and mowing machines, and extra parts for same; materials for manufacturing agricultural machinery, namely: reaper-knife sections, fingers, brass and steel springs, malleable castings, discs for harrows, mould-boards and plough-shares, mould-board plates and steel share plates cut to pattern, skeith-plates, ploughs and harrows, combined threshers.
442	Riddles and sieves.
443	Rivets and washers.
444	Separators and coolers for dairying purposes.
445	Set screws, engineers' studs, and split-pins.
446	Sewing-, knitting-, and kilting-machines.
447	Spiral springs (except sofa and mattress springs).
448	Steam and hydraulic pressure and vacuum gauges.
449	Surveyors' steel bands and measuring-tapes.
450	Swords.
451	Tacks of all kinds.
452	Tea-packing lead.
453	Tin, in pigs, bars, or sheets.
454	Tinsmiths' fittings, including stamped or blocked tin, planished or unplanished.
455	Tins, tops of, ornamented.
456	Wire, of brass, copper, or lead.
457	Zinc, plain sheet.
458	Zinc plates and copper plates for photolithographic work.
CLASS XIII.—TIMBER, &c.	
459	Ash, hickory, and lancewood timber, unwrought.
460	Blacksmiths' bellows.
461	Brush woodware.
462	Carriage and cart shafts, spokes and felloes in the rough; hubs, all kinds poles, if unbent and unplanned, all kinds; bent wheel-rims.
463	Carriage- and cart makers' materials, viz.: springs, mountings, trimmings, brass hinges, tire-bolts, shackle-holders, step-treads, and other iron fittings (except steps, lamp-irons, dash-irons, seat-rails, and fifth wheels), rubber-cloth.
464	Churns.
465	Lignum-vitæ.
466	Sieves, hair.
467	Wooden handles for tools.
CLASS XIV.—OILS, &c.	
468	Benzine, in bulk.
469	Oils, viz.: candlenut, fish, whale, seal, and penguin, and palm.
470	Paints and colours n.o.e.
471	Shale oil, once run, suitable for gas-making.
472	Spirits of tar.
473	Turpentine, driers, and terebene.
CLASS XV.—MISCELLANEOUS.	
474	Apparatus and appliances solely for teaching purposes, as may be approved by the Commissioner.

No.	Articles Free of Duty.
<i>CLASS XV.--MISCELLANEOUS—cont.</i>	
475	Belting for machinery other than leather.
476	Binder-twine.
477	Bricks, other than fire-bricks.
478	Building materials n.o.e.
479	Brushes for cream-separators and combine-screens.
480	Candlenuts and candlenut-kernels.
481	Candle-wick.
482	Canvas aprons and elevators for reapers and binders.
483	Carpenters' baskets.
484	Charts and maps.
485	Cottonwaste.
486	Dyestuffs and dyeing materials, crude.
487	Felt sheathing.
488	Food preservative n.o.e.
489	Gum boots.
490	Hawsers of 12 in. and over.
491	Honey and brown Windsor soap composition.
492	Iron and steel cordage.
493	Jute bagging, bags, and sacks.
494	Manures.
495	Marble and other stone, hewn or rough sawn, not dressed or polished.
496	Net-makers' cotton twine.
497	Official supplies for consular officers of countries where a similar exemption exists in favour of British Consuls.
498	Paper-makers' felts.
499	Passengers' baggage and effects, including only wearing apparel and other personal effects that have been worn or are in use by persons arriving in the colony; also implements, instruments, and tools of trade, occupation, or employment of such persons; and household or other effects not exceeding 100 <i>l.</i> in value, which have been in use for 12 months prior to embarkation by the persons or families bringing them to the colony, and not intended for any other person or persons or for sale; also cabin furnishings belonging to such persons.
500	Plaster of Paris.
501	Powder, blasting and meal.
502	Ship chandlery n.o.e.
503	Ships' rockets, blue lights, and danger signals.
504	Spirits for manufacturing perfumed spirit, flavouring essences, and culinary essences in manufacturing warehouses. This exemption to cease on the 1st day of February 1896.
505	Stones, mill, grind, oil, and whet.
506	Tobacco for sheepwash or for insecticide, after being rendered unfit for human consumption to the satisfaction of the Commissioner.
507	Treacle or molasses, mixed with bone-black in proportions to the satisfaction of the Commissioner.
508	Tubular woven cotton cloth in the piece, for meat-wraps.
509	Type-writers.
510	Wax, bottling.
511	Woolpacks and wool-pockets.
512	Yarn, viz., coir, flax, hemp.
513	Articles and materials (as may from time to time be specified by the Commissioner) which are suited only for, and are to be used solely in the fabrication of goods within the colony. All decisions of the Commissioner in reference to articles so admitted free to be published from time to time in the "Gazette."
514	And all articles not otherwise enumerated.

Table of Excise Duties.

No.	Articles.	Rates of Duty.	
			<i>s. d.</i>
515	Tobacco - - - - -	Per lb.	1 0
516	Cigars, cigarettes, and snuff - - - - -	"	1 6
517	Beer - - - - -	Per gallon	0 3
518	Articles in which spirit is a necessary ingredient, manufactured in a warehouse appointed under section 26 of "The Customs Laws Consolidation Act, 1882, namely,—		
	Pharmacopœia tinctures, essences, extracts, and medicinal spirits containing more than 50 per cent. of proof spirit - - -	Per lb.	0 9
	Pharmacopœia tinctures, essences, extracts, and medicinal spirits containing less than 50 per cent. of proof spirit - - -	"	0 3
	Culinary and flavouring essences, from 1st February 1896 - - -	Per liquid gall.	12 0
	Perfumed spirit, from 1st February 1896 - - -	"	20 0
	Toilet preparations which are subject to 16s. the liquid gallon on importation - - -	"	12 0
	Toilet preparations which are subject to 25 per cent. duty on importation - - -	"	6 0

Duties Imposed by His Excellency the Governor under Section 17 of "The Customs and Excise Duties Act, 1888."

No.	Articles.	Rates of Duty.	
			<i>s. d.</i>
519	Olive stones, ground (<i>see</i> "New Zealand Gazette," 15th May 1890) - - - - -	Per lb.	0 4
520	Brewers' caramel (<i>see</i> "New Zealand Gazette," 21st August 1890) - - - - -	"	0 3
521	Liquid hops (<i>see</i> "New Zealand Gazette," 21st December 1893) - - - - -	"	0 6

XIV.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

SWEDEN.

Harbour Dues on Partial Cargoes in Swedish Ports.

A despatch, dated February 12 last, has been received at the Foreign Office from Mr. George E. Welby, Her Majesty's Chargé d'Affaires at Stockholm, enclosing copy of a memorandum received from the Swedish Ministry for Foreign Affairs, in reply to the representations made with regard to the amount of the harbour dues levied on partial cargoes at ports along the east coast of Sweden.

These representations have been under careful consideration of the Swedish Government, and in accordance with the Royal Decree issued on October 11 last, renewing for a period of three years the concessions granted to the exporters of wood, it has been now decided that dues are only to be levied on half instead of on the whole tonnage of vessels in the case where the amount of the cargo shipped or discharged does not exceed the half of the total carrying capacity of the vessel.

This regulation came into force on January 1 of the present year, and the necessary instructions with regard to it have been sent to the governors of the respective provinces.

GERMANY.

Importation of Tinned Meat.

A despatch, dated February 19 last, has been received at the Foreign Office from Sir Frank Lascelles, Her Majesty's Ambassador at Berlin, enclosing translation of an extract from the "Central-Blatt für das Deutsche Reich" of December 13 last, notifying the following decision of the Federal Council relative to the Customs treatment of tinned meat:—

"Meat (of cattle) imported in hermetically closed vessels, whether fresh or simply prepared (boiled, roast, smoked, or salted), shall be subject to the tariff duty of 20 marks per 100 kilogrammes under No. 25 g 1 of the Tariff, even if it comes from countries having commercial treaties with Germany, or enjoying the advantages of the Most Favoured Nation clause."

FRANCE.

New Entry into the Port of St. Nazaire.

A despatch, dated February 25 last, has been received from the Foreign Office with copy of an extract from the "Journal Officiel," containing text of a law of the 20th February last, authorising the construction of a new entry into the port of St. Nazaire at a cost of 12,000,000 frs. (480,000*l.*), and detailing the means by which such a sum is to be raised.

Regulations affecting the Port of St. Nazaire.

The "Journal Officiel" of February 9 last contains a Presidential Decree, by virtue of which the following regulations are made respecting the port of St. Nazaire.

Art. 1. The port of St. Nazaire (Loire Inférieure) is added to those designated for the free admission of products of the island of Corsica.

Art. 2. The Custom-house of St. Nazaire is opened to the importation of mineral oils and their essences, cotton yarns, wool, alpaca, lama, vicuna, camels hair, rattans (not worked up) of 9 millimetres or less in diameter.

Art. 3. This office is also authorised to receive the primary declarations, and to pass the papers in connection with the shipments of sugar exports after discharge of temporary admission bonds.

Art. 4. The same office is added to others previously designated for the importation under the rules for temporary admission of the following articles:—

1. Chlorate of potash for conversion into chlorate of soda, or chlorate of baryta.

2. "Crêpe de chine," plain, in shawls or lengths, for the purpose of being embroidered, dyed, or printed.

3. Foulards, unbleached, and tissues of silk waste in lengths, for the purpose of being printed, dyed, and prepared.

4. Tissues of silk mixed with cotton or other materials, for the purpose of being printed, dyed, and prepared.

The re-exportation of the above-named products after being worked up can also be effected at this office.

Alteration of Pilotage Dues at St. Malo and St. Servan.

A communication, dated February 13 last, has been received at the Foreign Office from Mr. M. C. Gurney, Her Majesty's Consul at Cherbourg, stating that the rate of pilotage at the ports of St. Malo and St. Servan have been altered and fixed as follows:—

Steamers,	inwards,	0.215 fr. per register ton.
"	outwards,	0.200 fr. "
Sailing vessels,	inwards,	0.430 fr. "
"	outwards,	0.400 fr. "
plus 3 per cent. for the pilot-major.		

Customs Regulations.

Copies of Custom-house circulars have been received at the Foreign Office from the Marquis of Dufferin and Ava, Her Majesty's Ambassador at Paris, by which is decreed—

"The prohibition until further orders of the export to Cuba from France, Algeria, French colonies, and countries under the French protectorate, of all war arms or finished portions thereof and ammunition."

FRANCE.—INDO-CHINA.

Increased Export Duty on Paddy.

A despatch, dated February 11, last has been received from the Foreign Office, enclosing extract from the “*Journal Officiel*” of February 2 last, containing a decree of the Governor-General of Franco-Indo-China, as follows:—

Art. 1. The export duties on paddy and on cargo rice containing more than 33 per cent. of paddy are increased by 9 centimes per 100 kilos. nett.

Art. 2. This increase is temporary only, and essentially revocable.

Art. 3. The Lieutenant-Governor of Cochin China is charged with the execution of the present decree, which comes provisionally into force from January 1, 1896, and will be submitted for the approval of the President of the Republic.

BELGIUM.

Tariff Modifications.

A despatch, dated March 5 last, has been received at the Foreign Office from Sir F. R. Plunkett, Her Majesty's Minister at Brussels, enclosing copy of extract from the “*Moniteur Belge*,” publishing the following modifications in the Belgian Customs tariff:—

“The undermentioned articles, hitherto classed under ‘*mercerie*’ and ‘*quincaillerie*,’ are ranged under ‘*produits divers pour l'industrie*,’ and dutiable at 5 per cent. *ad valorem*:—

- “1. Small wooden tubes used in connection with toy balloons.
- “2. Wooden capsules for corks, wood or china joints, and bone and china pieces serving as mountings for feeding bottles.
- “3. Bone shields used in connection with feeding bottles.”

SPAIN.

New Custom-house at Limpas (Santander).

A despatch, dated February 10 last, has been received from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, enclosing a translation of a Royal order authorising the establishment of a Custom-house at Limpas (Santander), to be administered by the officers of the Custom-house at Santoña.

Oleo-naphtha and Lubricating Oils.

According to the “*Journal Officiel*,” by a Royal order of December 10, 1895, oleo-naphtha, lubricating oils, and other articles comprised in paragraphs 8 and 9 of the tariff, imported in barrels or otherwise, will pay duty according to their net weight, tare being allowed for the recipients.

Flour and Groats.

The “*Journal Officiel*” states that by a recent Royal order it was decided that, when a shipment of flour contains less than 5 per cent. of groats, it shall be taxed as flour; otherwise it will pay the duty leviable on groats.

Consular Dues.

A report, dated 24th February last, has been received at the Foreign Office from Mr. W. Wyndham, Her Majesty's Consul at Barcelona, forwarding translation of a copy of a Departmental Order which had been received by the Collector of Customs at Barcelona from the Direction-General of Customs in Madrid, and which is already being carried out by the Barcelona Custom House.

The order throws upon the captain of each vessel, British or foreign, the responsibility of proving that he has paid the proper consular dues to the Spanish Consul at the port of loading, and authorises the Collector of Customs to charge them on the arrival of the vessel here if proof be not presented in the shape of a receipt from the Consul or a note by him on the manifest of the steamer, stating the amount of fees paid.

ITALY.

Payment of Duties in Small Coin and Paper.

The "Journal Officiel" states that by a decree of the 14th December 1895, the regulations of the decrees of 31st March, 26th July, 28th December 1894, and 19th June 1895, concerning the acceptance of small silver money and paper money in payment of import duties in Italy, will remain in force until 30th June 1896. Not more than 100 francs for each payment will be taken in small Italian silver coin.

Classification of Lace Goods sewn in Lengths.

A despatch, dated February 23 last, has been received at the Foreign Office from Sir Clare Ford, Her Majesty's Ambassador at Rome, with reference to the classification under the Italian tariff of lace goods when sewn together in lengths. The College of Customs Experts have decided that two or more pieces or strips of lace or tissue of the same quality when sewn together for the purpose of obtaining pieces or strips of greater length, do not constitute "sewn articles" (*cucitura*) in the sense of the Italian tariff.

AUSTRIA-HUNGARY.

Tare Regulations.

The "Bollettino di Notizie Commerciali" publishes extracts from ordinances of the Austro-Hungarian Customs, by which an additional 13 per cent. is to be added to the nett weight of liquid chlorate of zinc imported in tank wagons. With regard to raw coffee in bales, the tare is fixed at 3—5 per cent. of the gross weight.

UNITED STATES.

Customs Decisions.

The following decisions relating to the classification of articles in the Customs tariff and the application of the Customs laws of

the United States have recently been given by the Customs authorities in that country :—

Blocks of lignum-vitæ suitable for the manufacture of bowling balls are free of duty under paragraph 679.

Tiles with only one colour burned in, not being known commercially as encaustic tiles, are dutiable at 25 per cent. under paragraph 78.

Worsted yarn twisted with spun silk is assessed for duty at 45 per cent. *ad valorem* under paragraph 302, Tariff Act of 1894, as manufactures of silk, or of which silk is the component material of chief value, not specially provided for in said Act.

Jute webbing is dutiable under paragraph 263 as “webbing * * * made of cotton or other vegetable fibre,” at 45 per cent. *ad valorem*.

Plaster of Paris busts are assessed for duty at 55 per cent. *ad valorem* under paragraph 100 of Tariff Act of 1890.

Crude potassium metal is to pay duty at the rate of 20 per cent. under section 3.

Cotton anklets or cuffs, knit, are dutiable at 35 per cent. *ad valorem* under paragraph 264.

Magnesium powder is free of duty under paragraph 544.

GUATEMALA.

Tariff Modifications.

A despatch, dated 23rd January last, has been received at the Foreign Office from Mr. J. Frederick Roberts, Her Majesty's Chargé d'Affaires at Guatemala, stating, with respect to the modifications in the Customs tariff of Guatemala (*see* p. 295), the greatest increase in the tariff is on silks, satins, and articles made thereof. There is also an increase in the duty on corn, which, though small, will be a burden on the people, as increasing the price of breadstuffs. The object of this tax appears to be to try and get the people of Guatemala to grow more wheat and other cereals, for which the highlands of the republic are very suitable.

The reductions in the tariff are largest on drugs, hardware, and some cotton and linen goods.

Taking additions and alterations as a whole, they may be considered as beneficial to the community at large, the general tendency thereof being to increase the rates on “articles de luxe” and goods mostly in demand by the wealthier classes, and to reduce somewhat the tariff on the commoner class of goods and such as are of more general utility.

JAPAN.

Duties to be Charged on British Imports.

A despatch, dated February 6 last, has been received from the Foreign Office enclosing copy of “Treaty Series No. 2, 1896” (noticed in the Review of Trade Blue Books on p. 237 of the February number of the “Board of Trade Journal”) being the

Supplementary Convention between Great Britain and Japan, respecting the duties to be charged on British goods imported into Japan.

The articles of this Convention, which was signed at Tôkiô on July 16 last, and ratified on November 21 last, are as follows:—

1. The tariff annexed to this Convention shall be substituted for the *ad valorem* tariff annexed to the Protocol of July 16, 1894, it shall be subject to all the stipulations contained in Art. 1 of that Protocol in so far as these are applicable, and it shall come into force one month after the exchange of the ratifications of this Convention.

2. The specific duties established by this Convention shall be subject to triennial readjustment. Such readjustment shall be based on the difference between the average of the two quarterly rates of exchange adopted by the Japanese customs during the six months ending June 30, 1894, and the average of the rates of exchange adopted by the Japanese Customs for the four quarters preceding that in which each successive period of three years expires.

The schedule of readjusted duties shall be published by the Japanese Government three months in advance, and shall take effect immediately upon the expiration of the said period.

It is understood between the High Contracting Parties that the operation of this stipulation shall be subject to the acceptance of a similar arrangement by the other Powers with whom Conventional Tariffs are now being negotiated by Japan.

3. The quarterly rates of exchange mentioned in the preceding article are the rates determining the comparative values, as entered in the quarterly tables published by the Japanese Department of Finance, of the present Japanese silver yen on the one hand, and of the English pound sterling on the other.

4. The present Convention shall have the same duration as the Treaty and Protocol concluded on July 16, 1894, of which it is a complement.

5. The present Convention shall be ratified, and the ratifications shall be exchanged at Tôkiô as soon as possible, and not later than six months from the present date.

Tariff.

No.	Articles.	Duty.
		Yen
1	Caoutchouc, manufactures of - - -	10% <i>ad val.</i>
2	Cement, Portland - - -	100 catties 0·065
3	Cotton yarns, plain or dyed - - -	" 4·180
	Cotton tissues:	
4	Drills - - -	Square yard 0·016
5	Duck - - -	" 0·053
6	Handkerchiefs in the piece - - -	" 0·011
7	Prints - - -	" 0·012
8	Sateens, plain, figured or printed, brocades, Italians and figured shirtings - - -	" 0·017

No.	Articles.	Duty.
	Cotton tissues— <i>cont.</i>	Yen.
9	Shirtings, dyed - - - -	Square yard 0'013
10	" grey - - - -	" 0'006
11	" twilled - - - -	" 0'011
12	" white or bleached - - - -	" 0'010
13	T-cloths - - - -	" 0'009
14	Turkey red cambrics - - - -	" 0'012
15	Velvets or velveteens - - - -	" 0'041
16	Victoria lawns - - - -	" 0'006
17	All other sorts of pure cotton tissues, and all tissues of cotton mixed with flax, hemp, or other fibre, including wool, the cotton, however, predominating in weight, not specially provided for in this tariff - - - -	10 % <i>ad val.</i>
	NOTE.—It is expressly understood that ready-made clothing and other made-up articles are not included under the heading of cotton tissues.	
18	Glass, window, ordinary—	
	(a.) Uncoloured and unstained - - - -	100 sq. ft. 0'302
	(b.) Coloured, stained, and ground - - - -	10 % <i>ad val.</i>
19	Hats, including also hats of felt - - - -	10 % <i>ad val.</i>
20	Indigo, dry - - - -	100 catties 12'953
	Iron and mild steel—	
21	Pig and ingot - - - -	" 0'083
22	Bar and rod, exceeding $\frac{1}{4}$ -inch in diameter - - - -	" 0'261
23	Nails, including spikes, sprigs, tacks, and brads:—	
	(a.) Plain - - - -	" 0'573
	(b.) Galvanised - - - -	10 % <i>ad val.</i>
24	Pipes and tubes - - - -	10 % <i>ad val.</i>
25	Plate and sheet - - - -	100 catties 0'296
26	Rails - - - -	" 0'129
27	Screws, bolts, and nuts, plain and galvanised - - - -	10 % <i>ad val.</i>
28	Sheet, galvanised, both plain and corrugated - - - -	100 catties 0'740
29	Tinned plates:—	
	(a.) Ordinary - - - -	" 0'691
	(b.) Crystallised - - - -	10 % <i>ad val.</i>
30	Wire, and small rod not exceeding $\frac{1}{4}$ -inch in diameter - - - -	100 catties 0'503
31	Wire, telegraph or galvanised - - - -	" 0'256
	NOTE.—By the term "mild steel" as used in this tariff is understood mild steel manufactured by the Siemens, Bessemer, Bassic, or similar processes, and approximating in value to iron of the same class in this tariff.	
32	Lead, pig, ingot, and slab - - - -	" 0'316
33	Leather:—	
	(a.) Sole - - - -	" 5'690
	(b.) Other kinds - - - -	10 % <i>ad val.</i>
34	Linen yarns, plain or dyed - - - -	100 catties 6'527
	Linen tissues:—	
35	Canvas - - - -	Square yard 0'047
36	All other sorts - - - -	10 % <i>ad val.</i>
	NOTE.—It is expressly understood that ready-made clothing and other made-up articles are not included under the heading of linen tissues.	
37	Mereury or quicksilver - - - -	100 catties 5'048
38	Milk, condensed or desiccated - - - -	<div style="display: flex; align-items: center;"> <div style="font-size: 3em; margin-right: 10px;">{</div> <div style="text-align: center;"> Doz. 1 lb. tins and proportionately for tins of other weights - </div> <div style="margin-left: 10px;">}</div> </div> 0'123

No.	Articles.	Duty.
		Yen
39	Oil, paraffin - - - - -	10 % <i>ad val.</i>
40	Paint in oil - - - - -	100 catties 1·304
41	Paper, printing - - - - -	" 1·163
42	Saltpetre (nitrate of potash) - - - - -	" 0·490
43	Silk-faced cotton satins - - - - -	15 % <i>ad val.</i>
	NOTE.—It is expressly understood that all other mixed tissues of cotton and silk, and of wool and silk, where the cotton or wool predominates in weight, are to be classed for duty under Nos. 17 and 61 of this tariff respectively.	
	Steel (other than mild steel) :	
44	Ingot - - - - -	5 % <i>ad val.</i>
45	Bar, rod, plate, and sheet - - - - -	7½ % <i>ad val.</i>
46	Wire, and small rod not exceeding ¼-inch in diameter - - - - -	100 catties 1·819
47	Sugar, refined :	
	(a.) No. 15 to No. 20, inclusive, Dutch standard in colour - - - - -	" 0·748
	(b.) Above No. 20 Dutch standard in colour - - - - -	" 0·827
	Tin :	
48	Block, pig, and slab - - - - -	" 1·992
49	Plates - - - - -	10 % <i>ad val.</i>
50	Wax, paraffin - - - - -	100 catties 0·544
51	Woollen and worsted yarns, plain or dyed	" 9·169
	Woollen and worsted tissues, pure or mixed with other material :	
52	Alpacas - - - - -	sq. yd. 0·075
53	Blanketing and whipped blankets in plain weave - - - - -	100 catties 7·458
54	Buntings - - - - -	sq. yd. 0·031
55	Cloth :	
	(a.) Wholly of woollen or worsted yarn or of woollen and worsted yarns, such as broad, narrow, and army cloth, cassimeres, twceeds and worsted coatings - - - - -	" 0·093
	(b.) In part of woollen or worsted yarn and in part of cotton yarn, such as pilot, president, and union cloth - - - - -	" 0·039
56	Flannels - - - - -	" 0·044
57	Italian cloth - - - - -	" 0·029
58	Long ells - - - - -	" 0·036
59	Mousseline de laine - - - - -	" 0·021
60	Serges :—	
	(a.) Where the warp is worsted and the weft woollen - - - - -	" 0·056
	(b.) All other kinds - - - - -	10 % <i>ad val.</i>
61	All other sorts, pure or mixed with other material, the wool, however, predominating in weight, not specially provided for in this tariff - - - - -	10 % <i>ad val.</i>
	NOTE.—It is expressly understood that ready-made clothing and other made-up articles are not included under the heading of woollen and worsted tissues.	
62	Yarns, all sorts, not specially provided for in this Tariff - - - - -	10 % <i>ad val.</i>
	Zinc :—	
63	Block, pig, and slab - - - - -	100 catties 0·451
64	Sheet - - - - -	" 0·928

Weights Measures, and Coins.

The catty mentioned in this tariff is the Japanese weight. It is equal to 600 grms. of the metric system of weights, or 1·32277 lbs. English avoirdupois weight.

The pound is the English avoirdupois weight.

The square yard and square foot are the English Imperial surface measures.

The yen is the present Japanese silver yen of 900 fineness and 416 grains in weight.

Rule for Calculating ad valorem Duties.

Import duties payable *ad valorem* under this tariff shall be calculated on the actual cost of the articles at the place of purchase, production, or fabrication, with the addition of the cost of insurance and transportation from the place of purchase, production, or fabrication, to the port of discharge, as well as commission, if any exists.

Rule for the Measurement of Tissues.

In determining the dutiable width of any tissue the Customs shall discard all fractions of an inch not exceeding half an inch, and shall count as a full inch all fractions exceeding half an inch.

NOTE.—It is understood that selvages shall not be included in the measurement of tissues.

BRITISH INDIA.

Tariff Decisions.

By a Customs circular, dated December 21 last, the Government of India has decided that drosophores worked by steam may be held to be included in clause (a) of No. 14 of Schedule IV. of the Indian Tariff Act, 1894, as machinery employed in the spinning and weaving of cotton, and therefore exempt from duty. This ruling may be treated as applying to other apparatus similarly used in mills for humidifying the air.

Tariff Modifications.

The rates of duty and tariff valuations for the following articles when imported into British India have been changed since the passing of the Indian Customs Act 16, of 1894. The tariff valuations and duties now payable are shown in the following statement. The figures in brackets show the numbers of the articles in the old tariff:—

Articles.	Tariff Valuation.	Rate of Import Duty.
(44) Cotton, and articles made of cotton:—	R. a. p.	
Cotton twist and yarn - - -	—	Free
„ sewing thread - - -	—	„
„ piece goods, hosiery, and all other		
cotton goods not otherwise described -	<i>ad val.</i>	3½%
(83) Rose water - - -	Imp. gallon 2 0 0	5%

XV.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

THE NEW RUSSIAN PORT OF FEODOSIA.

A despatch, dated December 31 last, has been received at the Foreign Office from Mr. W. E. Goschen, Her Majesty's Secretary of Embassy at St. Petersburg, stating that it has long been the intention of the Russian Government to close the harbour of Sevastopol for commercial purposes. An ordinance was issued on the 11th (23rd) May 1890, instructing the Ministry of Ways to study the question of the construction of the port of Feodosia (principally in the interests of the export grain trade) and of the railway connection between it and the town of Djankoi on the Crimean line.

In August last, a special commission was sent to examine the works, and the port was formally inaugurated on the 9th (21st) September. It will not acquire its full importance till the trade has been definitely diverted from Sevastopol after the date fixed, namely, the 1st (13th) September 1899.

The harbour looks towards the north, and one of the principal tasks has been the construction of a narrow mole to protect it against the east and north-east winds. This mole runs approximately from south to north, and is not intended for the use of ships. It measures some 620 metres in length and possesses a lighthouse of the fourth dimension at its end. The entrance to the harbour lies between this point and another broader mole to the west of it, jutting out at right angles to the land and forming the northern boundary of the harbour, which is thus enclosed between them. The port lights are situated at its extremity. Ships can unload on both sides of this mole, and its exposed (northern) side is destined for the use of the coasting trade. There is also a wooden structure for the convenience of the Russian Navigation Company, and another one is being built for vessels of other private companies.

The total length of the quays round the harbour is about 1,234 metres. Its surface area is 109,300 square metres.

The depth has been wrongly given as 26—28 feet in several notices on the subject; the real depth is uniformly 24 feet, with the exception of a small area at the south-eastern extremity that measures only 19 feet. It is proposed to dredge this down to the same depth in the course of time. The quays are so constructed as to allow of further deepening.

There are large warehouses for the storage of grain along the quays, and a house for the electric light machinery. Other buildings are in connection with the railway. Sites have been chosen for future elevators.

The cost of the works hitherto has amounted to 3,642,571 roubles, about 400,000*l.*, which is below the credit of 4,200,000

roubles, about 450,000*l.*, assigned in 1891. The future expenditure to be incurred in the laying down of rails and of the necessary harbour works in connection with the railway, as well as in wharves, sheds, electric light, elevators, &c., is estimated at 5,000,000 roubles, about 550,000*l.*

In 1894 the trade of Feodosia attained something approaching 9 million pouds (145,084 tons), and this figure will probably be trebled when Sevastopol is closed. The population of the town has doubled within the last 10 years.

PEAT-COAL IN NORWAY.

A report, dated 21st February last, has been received at the Foreign Office from Mr. A. Stewart MacGregor, Her Majesty's Acting Consul-General at Christiania, enclosing specimen of a new fuel, peat-coal, together with report of the inventor, Mr. William Schöning, who claims to have discovered a practical means of converting peat into a cheap and valuable fuel.

Mr. W. Schöning states that after long continued experiments, he attempted the simplest and most evident manner of pressing the peat, at the same time carbonising (charring) it in the press. Both the ready-made product and the following patent claim ("Patent Krav"), viz., "the conversion of peat into coal by pressing the turf between heated plates or rollers,"—are already patented in *Norway*, *Germany*, and *Luxembourg*, and steps have been taken towards obtaining letters patent in the following countries:—Sweden, Denmark, Finland, Russia, Austria, Hungary, Italy, Switzerland, France, Belgium, Great Britain and Ireland, Spain, Canada, and the United States.

In order to test the heating capacity of the new fuel it was analysed by Mr. Schmelck, analyst to the City of Christiania, with the following results:—

				Per Cent.
Carbon	-	-	-	66.15
Hydrogen	-	-	-	6.60
Oxygen	-	-	-	17.75
Nitrogen	-	-	-	1.86
Sulphur	-	-	-	0.38
Ashes	-	-	-	3.76
Moisture	-	-	-	3.50
				<hr/> 100.00 <hr/>

According to Mr. Schmelck's further analysis it has a firing value of 6,556 degrees of heat ("Calorier") per kilog.

The Consul-General at Christiania, commenting on Mr. W. Schöning's invention, says that if, when thoroughly inquired into and tested, the worth of the various inventions be anything like

that which is claimed for them, it seems a pity, in these times of depressed agriculture and vexed land questions, that Great Britain, and especially Ireland, should not participate in the advantages accruing from the utilisation of their peat moors.

THE EXPORTS OF DANISH BUTTER.

The British Vice-Consul at Copenhagen says that the increase in the export of butter from Denmark in 1894 was very considerable, it being about 20,000,000 lbs. more than in the preceding year.

It thus proves that in spite of all competition the Danish butter has not only been able to maintain its position, but has gained a considerably larger market in England. The enormous increase in quantity is due to the cheapness of fodder. Of late years great quantities of cheap rye and barley—the latter especially from the Black Sea—have been imported into Denmark, and these cereals have mostly been used for fodder, thus very greatly increasing the produce of butter. It is also a fact that the middle classes in England seem nowadays more and more to adopt a continental mode of living, in which butter is a most important feature, thus naturally increasing the market in England, which it is hoped may be maintained as long as all efforts are made both by way of experience and science to keep up the first-class position of the Danish butter. The only complaint is the fall in price. In the interesting report of the Danish Chamber of Commerce for 1894 the farmers are most seriously advised to reduce the expenses connected with the manufacture of butter so as to enable them to sell cheaper, whereby, in the opinion of the Chamber, the market in England will be secured for a long time.—(No. 1,660, *Foreign Office Annual Series*.)

TRADE OF GERMANY WITH BRITISH AND GERMAN COLONIES IN AFRICA.

A despatch, dated February 18 last, has been received at the Foreign Office from Sir Frank C. Lascelles, Her Majesty's Ambassador at Berlin, stating that the "*Kolonial-Blatt*" of the 15th of February publishes an extract from "the Statistical Abstract for the several Colonial and Other Possessions of the United Kingdom for 1894," from which it appears that the trade with Germany of the several British colonies in Africa amounted in value to:—

Imports from Germany, 915,978*l.*; Exports to Germany, 621,728*l.*; together amounting to 1,537,706*l.*

From figures published in the "Statistisches Jahrbuch für das Deutsche Reich" it appears that the trade of Germany with the German colonies in Africa amounted in 1894 to:—

Imports, 213,000*l.*; exports, 269,000*l.*; or together, 482,000*l.*

REDUCTION OF RAILWAY RATES ON COAL FROM SILESIA TO BALTIC PORTS.

The Board of Trade have received, through the Foreign Office, a telegram from Her Majesty's Consul at Stettin, dated the 22nd February last, stating that a reduction of railway rates to 2*s.* 9*d.* per ton on coal from Silesia to that port was to come into force from the 1st March.

EXPORT TRADE OF TOGOLAND.

A despatch, dated February 18 last, has been received at the Foreign Office from Sir Frank C. Lascelles, Her Majesty's Ambassador at Berlin, stating that, according to the "Kolonial Blatt" of the 15th February the export trade of Togoland amounted in the first six months of the financial year 1895-6 to a value of 82,451*l.*, against 82,556*l.* in the corresponding months of the preceding year.

The export of palm oil and palm nuts decreased, owing to an unfavourable season, but the export of indiarubber for the six months increased from a value of 2,200*l.* to 6,550*l.*

The export of the latter article had already increased from 4,950*l.* in 1893 to 5,750*l.* in 1894.

LIQUOR TRAFFIC IN GERMAN SOUTH-WEST AFRICA.

A despatch, dated February 17 last, has been received at the Foreign Office from Sir Frank C. Lascelles, Her Majesty's Ambassador at Berlin, stating that in the "Kolonial-Blatt" of the 15th February the following statistics are published with reference to the traffic in spirituous liquors:—

(1.) Imported into the Cameroons, 1894 :

					Litres.	£
Rum	-	-	-	-	1,648,325	45,511
Liqueurs	-	-	-	-	32,000	3,541
Beer	-	-	-	-	---	3,109
Wine	-	-	-	-	---	2,882
						55,043

or a value of 55,000*l.* out of a total import of 324,000*l.*

(2.) Imported into German South-West Africa from Cape Town :

	Litres.	£
Beer - - - - -	12,824	718
Gin - - - - -	1,294	194
Pure spirit - - - - -	480	145
Rum - - - - -	328	33
Whisky - - - - -	48	16
Cognac - - - - -	868	294
		1,400

or a value of 1,400*l.* out of a total import of 22,000*l.* from Cape Town.

REGISTRATION OF FOREIGNERS IN THE PHILIPPINES.

A report, dated January 1 last, has been received at the Foreign Office from Mr. Montagu Loftus, British Vice-Consul at Madrid, in which he states that all foreigners have to be registered on arrival in the register of the Civil Governor of Manila, as also in the register of their respective consulates. They should therefore apply to the consulate for registration as soon as possible in order to obtain their "Cedula de Inscricpcion," without which they are prohibited from carrying on any business whatsoever. This Cedula also gives them the right of freely circulating through any part of the colony. The poll tax imposed on all Chinese landing in the Philippines has been raised to 50 dols. per head. (*No. 1664.—Foreign Office Annual Series.*)

CARGOES OF SALT, TOBACCO, &c. FOR ITALIAN PORTS.

A report, dated February 24 last, has been received from Mr. Charles A. Payton, Her Majesty's Consul at Genoa, transmitting copy of a notice issued by a respectable firm of shipbrokers and shipping agents at Genoa concerning the difficulties and delays to which vessels may be exposed which call at Italian ports with whole or part cargoes of salt on board, mention being made also of tobacco, coffee, sugar, tea, spirits, matches, and explosives.

The following is a copy of the notice in question :—

"*Salt and tobacco* are both Government monopolies as per law issued 15th June 1865, No. 2397, Art. I. :

"This law absolutely prohibits the entry into any ports of Italy (Sicily and Sardinia excluded) of steamers or sailing vessels with whole or part cargoes of salt on board, in transit or otherwise, whereas the law permits the entry and transit of steamers or sailing vessels having tobacco and cigars on board at the following ports : Ancona, Bari, Genoa, Leghorn, Messina, Naples, Venice

Civita Vecchia ; provided the tobacco and cigars are stowed in such manner in the holds as to be easily verified and controlled, and eventually weighed and discharged if required, the Italian Customs having this option.

"The law concerning the salt, although so old, has been allowed to remain dormant and has not been enforced, the Customs throughout the country having, until quite recently, allowed vessels with salt on board to touch at all ports without objection.

"We learn on inquiry that the Austrian steamer 'Pandora' recently touched at Barletta with 400 tons of salt on board in transit for the Brazils ; the importance of the quantity recalled the attention of the Government to the existence of the said law, the result being that the steamer was refused admission and obliged to return to its original port to get rid of the salt, severe instructions being forthwith telegraphed to all the Customs throughout the country (excepting Sicily and Sardinia) to insist in the future upon the strict observance of the law.

"In addition to the foregoing remarks *re* salt and tobacco, we would recommend your attention to the careful declaration of *coffee, sugar, tea, spirits, and matches*, which, although allowed entry into Italy, are subjected to the close inspection of the authorities in consequence of the heavy duty on same.

"We would also point out to you that *explosives* are also liable to special supervision, that is to say, they must be stowed in such manner as to be able eventually to be easily discharged on arrival, and deposited in the Government warehouse until steamer's departure."

NEW AUSTRIAN COPYRIGHT LAW.

A despatch, dated February 6 last, has been received at the Foreign Office from Sir E. Monson, Her Majesty's Ambassador at Vienna, transmitting the text (together with the translation thereof) of a law dated the 26th of December 1895, concerning the copyright in Austria of works of literature, art, and photography.

This law was published in the Official Gazette on the 31st December last.

CONCESSION FOR RAILWAYS IN SERVIA.

A report, dated February 20 last, has been received at the Foreign Office from Mr. Ranald D. G. Macdonald, Her Majesty's Consul at Belgrade, stating that a law has been published in the "Servian Official Gazette" conferring upon a certain Mr. Maxi S. Antorijevitch, a jeweller of Belgrade, a concession for the construction and working of a network of local railways whose object it is to connect a district having the town of Pojarevatz as its chief centre with the Danube, and perhaps, ultimately, with

the existing railway main lines. The district in question is rich in mineral resources, especially coal, and its wheat is said to be the best in Servia. Direct communication between Pojarevatz (upon which the chief country roads converge) and the Danube is secured by a road of about nine miles in length, terminating at the landing stage and Custom-house of Doubravitsa. Over two hours are required by country carts to cover this distance, and the high charges asked for transport have hitherto acted as a serious obstacle to the proper development of trade.

The railways which the concessionaire now proposes to construct are :—

From Doubravitsa to Pojarevatz, nine miles.

From Pojarevatz to the village of Male Isrnitché, on the south-east, six and a half miles.

From Pojarevatz to the village of Osipaonitsa, on the south-west, eight and a quarter miles.

From Pojarevatz to the town of Svilajnatz, on the south, 28 miles.

The total length of the network (calculated from the length of the roads as shown on the staff map) will therefore amount to about 52 miles.

The lines will run within the six-metre spaces which the law reserves on either side of the high roads, and the concessionaire is free to choose steam or electricity as the motive power. The State is pledged to grant no other private railway concessions on either side of the proposed line from Doubravitsa to Svilajnatz, within a distance of 10 miles on the east and as far as the River Morava on the west, a stipulation which has the effect of insuring to the concessionaire the possibility of ultimately connecting this railway with the State line running north and south along the western bank of the Morava. Liberal privileges are also granted for the free importation and conveyance on the State railways, at simple cost of transport, of material and rolling stock, and for exempting the concessionaire during 15 years from the payment of State and communal taxes, and for the utilisation of stone, gravel, timber, and water power.

The duration of the concession is fixed at 65 years from the opening of traffic. The plans for the Pojarevatz-Doubravitsa Railway are to be submitted to the Minister of Public Works within 10 months from the date of the concession, and the construction of this line must be completed within 16 months from the approval of the plans.

The State reserves to itself the right to take over the railways in case of war or any other emergency, and to purchase them from the concessionaire at the expiration of 30 years, or in event of the latter becoming bankrupt.

Freedom is given to the concessionaire to fix the tariff of charges for transport, which must not, however, exceed that which prevails on the Servian State Railways.

NEW RAILWAY IN SERVIA.

A despatch, dated February 12 last, has been received at the Foreign Office from Mr. Edmund V. Fane, Her Majesty's Minister at Belgrade, reporting that a Royal Ukase has been issued authorising the Servian Government to lay before the Skupshtina a "Projet de Loi" for granting a concession for the construction and working of a railway to connect the town of Nisch with Kladovo on the Danube. The question of a law to authorise the construction of a railway from Belgrade to Valjevo, in the west of Servia, is also likely to be discussed in the Chamber.

THE SHARE OF ENGLAND IN THE IMPORT TRADE
OF SALONICA.

Her Majesty's Consul-General at Salonica, in a recent report, says that a further diminution in imports to Salonica was experienced in 1894. The drop was, however, less considerable than that which occurred in 1893, and, taking the two years together, it may be said that the import trade was fairly brisk. The diminution in the two years is sufficiently explained by the stagnation in the grain export trade.

England still holds a larger share of the import trade in general than any other country.

Five-sixths of the cotton goods come from Manchester, and English manufacturers have of late years successfully imitated the striped cottons and coloured handkerchiefs which, until 1890, were supplied by Switzerland and other continental countries. A leading importer has furnished the following notes on the subject:—In addition to the rich assortment of handkerchiefs already imported from England, three favourite varieties of Swiss handkerchiefs are now copied there with satisfactory results. About 100,000 doz. of these three varieties are annually imported, at prices from 10*d.* to 4*s.* per doz. Even "yazmas" (kerchiefs used chiefly as a head-dress for native women) have begun to be imitated. The following articles are also now imported from England as well as from Switzerland, viz., "alajas," of which 70,000 pieces of 45 to 50 yards by 14½ to 19 inches are imported at prices from 1½*d.* to 2½*d.* per yard; and cotton trouserings, of which the annual import is 10,000 pieces of 60 to 70 yards by 20 to 22 inches, the price being 2*d.* to 3*d.* per yard.

England still keeps the leading place in the supply of cotton yarns, but is being hard pressed by Italian competition, especially in the coloured article. In red yarns Austria has the preponderance.

England is apparently losing ground in woollen goods. In cloth England sends only the first qualities, but there are still hopes for the development of the British cloth trade, as increasing

quantities are required in Macedonia. Flannels come almost exclusively from Germany.

It is satisfactory to note that the import of English hats is increasing, in spite of the cheapness of the inferior Austrian and Italian articles.

Until quite lately Austria held the market in gloves, but English competition is now beginning to make itself felt.

The Salonica boot and shoe trade deserves more attention on the part of British exporters. Imported boots and shoes, for which there is a growing demand, are now chiefly supplied by Germany and Austria.

The percentage of iron and iron goods imported from England continue to be strikingly small; it is not more than 5 per cent. of the total, while Belgium supplies three-fourths. Of the railway material imported, almost all the iron, including rails, &c., has come from Belgium. In machinery, which is not, however, a very considerable article of import, Austria-Hungary seems to be competing successfully with England. In the smaller articles of ironmongery English trade seems to have been practically ousted from the market, the cheapness of the Continental articles being the oft-repeated explanation.

Cutlery is often met with bearing English trade marks, the genuineness of which is, to say the least, doubtful. This matter seems to deserve serious attention.

The import of coffee is increasing, Salonica being a distributing centre for Macedonia, and the new railway affording facilities for distribution. The coffee market at Salonica is a brisk one, owing to the competition of numerous small importers. The greater part of the coffee still comes from Trieste and Fiume, but the dearer and better article from England maintains its position.

As anticipated in the last trade report from the Consulate-General of Salonica, the consumption of English rum has steadily increased. About 1,800 barrels, containing some 47,000 gallons, were imported in 1894.

The above noted small import from England of several articles in which a better trade might be expected, is of course, chiefly attributable to the greater cheapness of Continental goods, but the result is to a great extent due to the advantageous and regular steamship services between Salonica and Trieste, Fiume, Marseilles, and Hamburg. The Johnson steamers from Liverpool is the only British line which calls at this port with tolerable regularity. The neglect of Salonica by British commercial travellers may also be contrasted with the frequent visits of French, German, Swiss, and Austrian travellers.—(*No. 1662, Foreign Office Annual Series.*)

THE SALONICA-CONSTANTINOPLE RAILWAY.

A report, dated February 10 last, has been received at the Foreign Office from Mr. J. E. Blunt, Her Majesty's Consul-

General at Salonica, stating that the following further sections of the Salonica-Constantinople junction line have been completed, and were opened to public traffic on the second instant:—

Nousretli-Buk and Oktchilar-Khirka-Kevi, 23 and 108 kilometres long, respectively.

The remaining section of this line—Buk-Oktchilar, 40 kilometres long—will be completed about the middle of next April.

AMERICAN NEEDLE MACHINE.

A communication, dated February 16 last, has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, enclosing extract from the "Chicago Evening Journal," demonstrating the practicability and usefulness of a needle machine which has just been invented by Mr. Eugene Fontaine, of Detroit, Michigan, who is chiefly known for his pin machine of 1872.

It appears he completed his designs some two years ago, since which time he has been engaged in the work of putting up the machine. The inventor states that, within a very short time, the machine will be in a position to manufacture needles at the rate of 2,500 an hour, or 25,000 a day; the best quality to be manufactured to supply the whole world at a market price of about 50 cents (2s.) per 1,000. It is expected that the cheaper grades of needles will be made in the United States at about half the cost of production that prevails at present in Germany.

Mr. Fontaine has sold the right to manufacture and operate his machine to the X.L. Needle Company, and is to receive about 90,000 dollars (18,000*l.*) for the invention.

"SILICATE OF ALUMINIUM": A NEW PRODUCT.

A communication, dated February 13 last, has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, reporting that a new product has been discovered in the vicinity of Kansas City, Missouri, which is called "silicate of aluminium"; it is a soft stone and can be cut with a penknife. When placed in cold water for a period of about four hours it expands to about three times its size and becomes a natural paste; this paste is put on horses' feet that have become sore from over-travelling on hard roads, and it seems to have proved a speedy and effective cure in many cities in the United States.

It is said that some foreign countries are now trying it, and that one China house contemplates manufacturing different kinds of goods out of the substance. It has hitherto been used for bunions, sprains, and also for tooth powder, with good results.

The German Government have experimented recently with this "silicate of aluminium" on some of their artillery and cavalry

horses that had sore feet, and were so pleased with the result that they have since ordered 20 tons of it to be shipped to Hamburg.

In the event of the above information interesting British houses, inquiries might be addressed to Mr. Philip E. Burrough, British Vice-Consul at Kansas, who will be glad to supply full information on the subject and send samples if necessary.

OFFICIAL VALUE OF LANDS IN MEXICO.

A despatch, dated February 3 last, has been received at the Foreign Office from Mr. H. N. Dering, Her Majesty's Minister at Mexico, enclosing a statement, officially published, of the price of Government lands per hectare (nearly $2\frac{1}{2}$ acres), for the year 1896-7 in the various States of the Mexican Republic.

From this it appears that the present price per hectare varies from 50 centavos, or 1s. 1d., in Lower California, to 4.50 dols., or about 9s. 8d., in the fertile State of Morelos, and about 5.60 dols., or about 13s., in the federal district.

BOUNTIES ON SUGAR AND CONDENSED MILK MANUFACTORIES IN BOLIVIA.

A despatch, dated January 30 last, has been received from Captain H. Jones, Her Majesty's Minister at Lima, enclosing text and translation of a Decree of the Bolivian Government regarding bounties on sugar, sugar machinery, and factories for condensed milk.

The articles of the above Decree are as follows :—

Art. 1. Interest of 4 per cent. is guaranteed on the capital employed in the establishment in Bolivia of all steam machinery which it can be proved has been regularly in work during one year in the elaboration of sugar. This guarantee is assured for five years, and will be paid for each six months completed.

Art. 2. A bounty of 4,000 dols. (Bolivian) is created in favour of the sugar industry which may produce in one year up to 4,000 quintals of refined sugar.

Art. 3. Bounties are created of first, second, and third class of 2,000 dols., 1,000 dols., and 500 dols. (Bolivian), respectively, in favour of sugar-refining establishments which may introduce improvements into processes and maintain them in their works for more than one year.

Art. 4. A bounty of 3,000 dols. (Bolivian) is created in favour of whoever may establish the best factory for condensing milk, which may produce $34\frac{1}{2}$ kilogrammes daily during three months in any one of the agricultural departments of the Republic, it being necessary to prove its normal regular working during one year, in conformity with the preceding article.

Art. 5. The Executive, in carrying into effect the present law, will dictate all the necessary dispositions called for to prove that the industry which may be rewarded is a staple one, and has not been previously exploited by any other in the same form.

THE PORT WORKS OF MONTEVIDEO.

A despatch, dated January 28 last, has been received at the Foreign Office from Mr. Walter Baring, Her Majesty's Minister at Montevideo, stating that Messrs. Guerrard and Kummer, the French and German engineers, who were chosen to study the question of the port works of Montevideo, have drawn up a report in which they explain at considerable length the nature of the works they deem it necessary to have carried out.

The scheme proposed differs considerably from that known as the ante-project of Messrs. Luther & Co.

The cost of the works, including the two breakwaters, wet docks, a careening dock, wharves, &c., is estimated at 90 million francs, which sum can be reduced to about 65 millions if the construction of certain wharves is postponed. The time necessary for the completion of the works is calculated at eight years.

The minimum depth of the port at the lowest tide would be 23 feet.

XVI.—GENERAL TRADE NOTES.

SHIPPING THROUGH THE SUEZ CANAL.

According to the "Canal de Suez" bulletin for the 12th February last, the number of ships passing through the Canal in November 1895 was 252, of a gross tonnage of 898,776 tons, 243 of which showed a tonnage of 839,014 tons.

Of these 252 vessels 163 were English (602,493 tons), 29 French (104,788 tons), 25 German (79,832 tons), 17 Dutch (44,763 tons), 6 Italian (18,982 tons), 4 Austro-Hungarian (16,017 tons), 3 Spanish (12,297 tons), 2 Russian (10,946 tons), 2 Norwegian (6,623 tons), and 1 Ottoman (2,035 tons).

The bulletin for the 2nd March last contains statistics as to the number of ships passing through the Canal in December 1895, viz., 284, of a gross tonnage of 1,000,250 tons, 272 of which showed a tonnage of 917,507 tons.

Of these 284 vessels, 187 were English (689,039 tons), 29 French (98,101 tons), 22 German (78,030 tons), 16 Italian (42,641 tons), 15 Dutch (38,890 tons), 5 Austrian (17,127 tons), 3 Spanish (13,419 tons), 3 Russian (13,374 tons), 2 Norwegian (6,339 tons), 1 Ottoman (1,849 tons), and 1 Portuguese (1,442 tons).

THE ESPARTO-GRASS INDUSTRY OF TUNIS.

With reference to an article which was published on pp. 151-152 of the February number of the "Board of Trade Journal," it should be stated, on the authority of Mr. W. T. Thiselton-Dyer, C.M.G., C.I.E., Director of Royal Gardens, Kew, that the esparto of commerce is not, properly speaking, a fibre, and not characteristically marine, but a "grass" of the Agrostideæ tribe (*stipa tenacissima*).

COAL MINING IN THE DONETZ BASIN.

The "Handels-Museum" of January 23rd says that the rapid development of the Donetz coal industry is shown by the following statement of the quantities of coal sent by rail from the Donetz basin into the interior of Russia in the last fifteen years. The

quantities of coal thus sent were, in millions of pouds (one million pouds = about 16,000 tons) :—

Million Pouds.			Million Pouds.		
1880	-	38	1888	-	86
1881	-	45	1889	-	126
1882	-	57	1890	-	117
1883	-	59	1891	-	134
1884	-	58	1892	-	159
1885	-	70	1893	-	167
1886	-	79	1894	-	197
1887	-	89			

The average annual increase in the exports amounts to 12 per cent. In 1893 the production of coal was rather small, as the rich harvest caused a great demand for labour, and there was consequently a scarcity of men at the collieries. The increase of 18 per cent. in 1894 is also due to this cause. Coal is sent from the Donetz district to South and Central Russia, especially to Moscow, and occasionally to St. Petersburg. In the Donetz basin there are 127 collieries, a great number of them being worked with French or Belgian capital. When a new Belgian Joint-Stock Company, early in 1895, wanted to purchase several mines situated in the Taganrog district of the Don Province and in the Bachmut district of the Government of Ekaterinoslav, sanction was refused the undertaking, but a Russian company, with its headquarters in St. Petersburg was formed for the same purpose, under the title of "The Russian Donetz Company for the Coal and Manufacturing Industries." The shares of this company are, however, to a great extent in Belgian hands, and are quoted on the Brussels Bourse.

MINERALS IN SOUTHERN RUSSIA.

The "Revue Commerciale" of Berdiausk, states that vast deposits of iron, graphite, kaolin and other minerals have been found at the village of Grand Tounak and in German colonies; anthracite has been discovered near Orekhov at the village of Bélitskoïé. Specimens of kaolin have been already analysed at St. Petersburg, Berlin, and Marseilles; and it has been found that the quality is excellent for the manufacture of porcelain. As for the iron ore, it was said to contain from 62 to 72 per cent. of pure iron, and very little sulphur or phosphorus in which it resembles the celebrated ore of Krivoi-Rog.

EXHIBITION OF NIJNI-NOVGOROD, 1896.

The Brussels "Bulletin Commercial" states, that in order to make known in Russia the progress made in other countries in the construction of light railways and river and canal boats, the Russian Government has just decided that a special section, where foreign exhibits will be admitted, shall be annexed to the National Exhibition of Nijni-Novgorod to be opened in May next. The special building necessary will be erected at the expense of the exhibitors who will also pay costs of transport, &c.

In the railway transport of exhibits, the exhibitors will be allowed the usual reduction on condition that the same route is followed on leaving as on entering Russia.

With regard to light railways, any special or typical advantage of lines should be shown; and with this object it will suffice to send two sections of rails of about $1\frac{1}{2}$ metres in length, with fish-plates, bolts, and washers complete and fixed on portions of sleepers, if wooden, and entire sleepers if of metal.

As regards river-boats, it will suffice to exhibit plans and models with especial reference to any new system of traction.

THE PORT OF ST. PETERSBURG.

According to the "Revue d'Orient," a credit of 2 millions of roubles has, on the proposition of the Minister of Ways and Communications been granted for the enlargement and improvement of the Port of St. Petersburg. Of this amount, 500,000 roubles are in the Budget for the current year. The remaining $1\frac{1}{2}$ millions will be divided among the four following years, and paid out according to the necessity and progress of the work.

THE NORWEGIAN SUGAR TRADE.

The importation of sugar into Norway, according to the "Bulletin Commercial," has been steadily on the increase during the last few years. The quantity imported in the year 1894 was 20,506,000 kilogs as against 18,449,000 in 1893; 16,811,000 kilogs in 1892; 16,054,000 kilogs in 1891; and 14,951,000 kilogs in 1890. The principal countries exporting sugar to Norway in 1894 were Germany, 10,012,360 kilogs; United Kingdom, 4,887,960 kilogs; Netherlands, 4,405,640 kilogs; Belgium, 781,420 kilogs; and France, 315,040 kilogs. The duty is 0.20 kroner or 28 centimes per kilog for sugar of all kinds.

No tare is allowed on paper packing with or without string; on cases containing sugar candy, the tare is 12 per cent.; other sugars, 15 per cent.

Christiania is the principal port of entry for sugar. Of the 20,506,000 kilogs imported in 1894, 13,394,200 entered by this port.

The importation of molasses in 1894 was 10,747,000 kilogs; in 1890 it had amounted to only 6,918,000 kilogs. Molasses pay no duty in Norway.

FOREIGN TRADE OF GERMANY IN 1895.

The following statistics of the foreign trade of Germany in 1895 are taken from the German official monthly accounts for December and 12 months ending 31st December 1895, the values being in each case converted into £ sterling. The imports, including bullion and specie, increased in volume from 320,225,017 quintals in 1894 to 325,361,774 quintals in 1895, an increase of 5,136,757 quintals. According to value, however, the imports show a decline of 3,448,000*l.*, *i.e.* from 214,277,000*l.* in 1894 to 210,829,000*l.* in 1895. The volume of the exports, including bullion and specie, rose from 228,837,153 quintals in 1894 to 238,306,158 quintals in 1895, an increase of 9,469,005 quintals. The value of the exports shows the large increase of 18,233,000*l.*, having risen from 152,574,000*l.* in 1894 to 170,807,000*l.* in 1895. The figures as to value are calculated on the 1894 unit prices, and as the prices of many articles have risen to an appreciable extent in 1895, the definite figures of trade in 1895 will be still more satisfactory.

In the above large increase in the value of the exports, the following are the principal categories of merchandise which show an increase in value of more than a million pounds in 1895 as compared with 1894:—Wool and woollen manufactures increased 1,966,000*l.*; drugs, chemicals, and dyestuffs, 1,906,000*l.*; iron and ironwares, 1,883,000*l.*; cotton and cotton manufactures, 1,603,000*l.*; silk and silk manufactures, 1,319,000*l.*; Colonial produce, spices, beer, wines, spirits, &c., 1,176,000*l.*; and leather and leather wares, 1,167,000*l.*

PRODUCTION OF PIG IRON IN GERMANY.

The returns are now available regarding the production of pig iron in Germany, including Luxemburg, during December last, and during the whole of last year, as compared with the corresponding period of 1894. According to "Kuhlow's" the output in December last comprised 27,458 tons of Bessemer pig, 263,288 tons of Thomas pig, 77,984 tons of foundry pig, and 141,675 tons of forge pig and "spiegeleisen"; a total of 510,405 tons as against 489,822 tons in the preceding month, and 498,233 tons in December, 1894. The aggregate output for the whole of the year was 5,788,798 tons as compared with 5,559,322 tons in 1894.

THE COMMERCIAL UNION OF SAXONY.

The Consul for the United States at Chemnitz in a recent report to his Government says that the tenth annual report of the Commercial Union of Saxony tells a tale of 10 years' hard, well-done work. In the space of 10 years it has won a world-wide fame. It was given prizes, for its publications, at the Australian Exhibition, in 1888-89 and in Chicago in 1893. Besides, its president was made a member of the Chicago Exhibition general committee. The aid and counsel of the Union is sought from all sides and by all kinds of people. Many times its committee has acted as a court, or board of arbitration, in commercial quarrels and in deciding knotty questions; it has aided often and very materially to get foreign firms to pay large and long outstanding debts hitherto held to be hopeless. Its bureau of information, one of the best organised in the Empire, is always busy. It has had great success in finding out foreign swindlers and in scenting out industrial agents who come to collect and carry away the secrets of Saxony's commercial success. Its sample rooms have been enlarged and improved. They have been visited by business men from all parts of the world, and they have more than equalled the expectations of their projectors. The society has sent out seven travelling commissions to investigate trade prospects in Canada, Mexico, the West Indies, South America, Eastern Asia, and South and East Africa, thus spending 300,000 marks—all this to aid exports. It sent out 110,000 German and English catalogues; circulars in five languages; 48,000 reference books in five languages, calling attention to the industries of Saxony and Thuringia; 70 illustrated business albums, containing illustrations and references to Saxony's most important houses; and 150,000 other important articles, references, and notices. Since 1885, 8,000 merchants have seen the sample rooms. In ten years, answers have been given regarding the standing of foreign firms 9,000 times. Sample orders to the number of 13,000 covering a value of 7,000,000 marks, were sent out; these secured business to the value of an additional 30,000,000 marks. The membership was 476 at the end of the fiscal year; 28 firms have joined since. The finances of the society are based on the annual membership fees. The officers, except the secretary, all give their services free. The union has 46 principal agencies in continental Europe and 53 beyond seas.

In concluding his report, the United States Consul says:—
“ This year's catalogue is to consist of 15,000 copies, and is to
“ be sent to all parts of the world. Every effort is being made
“ and every energy is being employed, to extend the union's use-
“ fulness. It is not necessary to say success is certain; in some
“ cases it has been simply phenomenal. The 3,500,000 people
“ of Saxony are pouring out into the world's markets almost every
“ kind of article known. England, alarmed at the encroach-
“ ments in markets once monopolized by Manchester, Nottingham,

“ and London, is looking into its methods. All over the
 “ kingdom, such unions as the one referred to above are working
 “ to win away from France, England, and the United States
 “ part, if not all, of the foreign trade. Their work cannot be com-
 “ mended too highly.”

INTERNAL WATERWAYS IN PRUSSIA.

Internal traffic by water in Prussia requires from 16 to 36 millions of marks annually, while the railways are worked at a considerable profit.

According to “Le Monde Economique” there is an idea of levying taxes and tariffs on canals, so that they may pay the interest on the cost of their construction. The Rhine and Elbe and some of their tributaries, and the Upper Weser, are exempted by international and federal treaties.

MOVEMENT OF POPULATION IN FRANCE.

According to a report of the French Minister of Commerce, Industry, Posts, and Telegraphs, published in the “Journal Officiel” of the 23rd January last, the statistics of the movement of population in France in 1894 show 286,662 marriages, (6,419 divorces), 855,388 births, and 815,620 deaths, or a decrease, as compared with 1893, of 632 marriages, 19,284 births, and 51,906 deaths. (Divorces show an increase of 235.)

Leaving on one side the slight decrease (0·2 per cent.) in marriages and the increase (3·9 per cent.) in divorces, the chief characteristics of the year 1894 seems to be a slight decrease (2·2 per cent.) in births, and a notable decrease (6 per cent.) in deaths.

The excess of births over deaths which had been 7,146 in 1893, is increased to 39,768.

The figures for the four preceding years were—

				Births.	Deaths.	Excess of	
						Births.	Deaths.
1890	-	-	-	838,059	876,505	—	38,446
1891	-	-	-	866,377	876,882	—	10,505
1892	-	-	-	855,847	875,888	—	20,041
1893	-	-	-	874,672	867,526	7,146	--

From the preceding figures it would seem that any increase of the population in France becomes a question only of the decrease in deaths.

SLATE-QUARRYING IN FRANCE.

The "Journal Officiel" of the 17th February last publishes a Presidential decree with regard to the regulation of the slate quarries of Maine-et-Loire.

By this decree are established regulations as to preliminary declarations: regulations for quarrying, whether above or below the surface, and with regard to large masses of material; work in dangerous localities; rules as to descent into and ascent from the quarries, and other matters connected with the industry.

The "Journal Officiel," containing full text of the decree, can be seen by those interested at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, S.W., between the hours of 11 and 5.

BOUNTIES ON SILK, FLAX, AND HEMP CULTURE IN FRANCE.

The "Bulletin" of the French Ministry of Agriculture for December 1895 publishes a statement showing the amount of the bounties paid in France in the year 1894 on the cultivation of silk, flax, and hemp. From this it appears that the total weight of silk cocoons having the right to the bounty in 1894 was 10,578,945 kilogs., on which a bounty of 5,289,473 francs was paid.

With regard to flax and hemp, the area cultivated which was admitted to bounties in 1894 was:—flax 20,329 hectares; hemp, 13,351 hectares. For this area bounties were granted at the rate of 72 francs per hectare.

THE ECONOMIC CONDITION OF NOSSI-BÉ.

According to the "Journal des Tarifs et Traités de Commerce," the general trade of the island of Nossi-Bé continues to develop.

The value of the total trade in 1889 was 3,387,907 francs, which increased to 7,118,462 francs in 1894; (imports, 3,595,067 francs; exports, 3,523,395 francs).

Of the total trade of 1894, about 2,151,000 francs, or less than one third, represents French trade.

The chief articles of import are: muslin and silk, alpaca, cotton goods, linen hosiery and wine; of export: colonial goods, flour, wood, &c.

MINERAL AND METALLURGICAL PRODUCTION OF BELGIUM IN 1895.

The "Moniteur des Intérêts Matériels" publishes some statistics with regard to the production of minerals in Belgium in 1895, from which it appears that the total amount of coal produced was 20,415,000 tons (of 1,000 kilogs.) as against 20,535,000 tons in 1894.

The total amount of pig iron in 1895 was 829,000 tons, as against 819,000 in 1894; iron plates and other manufactures 1895, 453,000 tons, the same amount as in 1894; steel bars, rails, plates, &c., 848,000 tons in 1895 and 747,000 tons in 1894.

PRODUCTION AND TRADE OF LEMONS IN SPAIN.

According to a recent report of the Belgian Consul at Malaga, it is chiefly in the province of Malaga that lemons are cultivated for exportation. The trade is principally with the United States and England, but also with France, Germany, and Northern Europe.

In 1893, 35,000 half-cases and 80,000 quarter-cases (each containing on an average 700 and 350 lemons respectively) were exported from Malaga as against 28,000 half-cases and 60,000 quarter-cases in 1894.

There are two lemon crops in the year, one in July of so-called summer lemons, and another from the end of August to November. The former has hardly any importance, whilst the latter is the real lemon harvest.

The price of summer lemons varies from 40 to 45 francs per thousand, and of autumn lemons from 40 francs at the commencement of the harvest, to 25 francs at the close per 1,200 to 1,500 according to the quality.

There is thus, on an average, a difference of 10 francs per 1,000 between the value of the products of the two harvests.

All the sales are effected through the export houses, and growers very rarely negotiate for the sale of the whole crop before ripe, only having recourse to such a plan when in pressing need. The districts along the coast to the east and west of Malaga contain the principal lemon plantations.

The fruit is packed in "half-cases," containing from 600 to 800, or in "quarter-cases," containing from 300 to 400 lemons.

EXHIBITION AT TURIN IN 1898.

The "Bulletin Commercial" of Brussels states that it is announced that foreigners will be permitted to exhibit in the electricity section of the exhibition to be held at Turin in 1898.

THE EFFECT OF THE NEW HUNGARIAN LAW AS TO THE MARKING OF GOODS.

The "Handels Museum" of January 23 says that the provisions of the new Hungarian laws as to the marking of quantities on goods affect the trade in yarns. The Hungarian Ministry of Commerce now calls attention to the fact that the marking of quantities is not compulsory in Hungary in respect of yarns, but that in virtue of section 1 of the law in question when the quantity (weight or measure of length) is marked on the packet or on the yarn itself, and when the English system of numbering is retained, the marking must always be also expressed in metric measures. The Austrian Ministry of Commerce has informed the Chambers of Commerce by circular of this regulation.

PREPARATION OF PLUMS IN SERVIA

The processes employed in Servia for the preserving of plums are very primitive, and it has not yet assumed the form of a regular industry, although it is one of the chief resources of the country. At present it is only a necessary consequence of that cultivation of plums which is carried on by every one in his own orchard.

According to the "Journal Officiel" the Servians have adopted in the treatment of this fruit a sort of oven in two compartments one above the other, of which the first, formed by a perforated wooden tray, receives the plums while the other serves as a fire-place.

The success of the operation depends on the regulation of the heat during the progress of the drying.

The peasant only applies this process to plums destined for sale; as to those reserved for his personal use, he is content with simply notching them and leaving them to dry in the sun. The Government has indeed decided to introduce the drying apparatus in use in France and America, but these were soon abandoned, either on account of their expense or from the inconvenience of having to transport the plums to the drying place.

IMPROVED MEANS OF TRANSPORT IN MONTENEGRO.

The possession of Antivari has not hitherto been of great utility to Montenegro, whose trade is for the most part carried on via the Austrian port of Cattaro, which place, besides having a safe harbour for ships is connected with the interior of the country by a good road; while, on the other hand, Antivari has only an open roadstead and but defective communication with the chief highways of the principality.

According to a Report of the Italian Minister at Gravosa published in the "Bollettino di Notizie Commerciali" the formation of a good mercantile port at Antivari, or rather in the neighbouring bay of Val di Noce is an undertaking beyond the financial means of the country; but the Government wishes at least to improve the ways of communication with the place, and with this object has commenced the construction of the Visbazar-Rieka road, about 20 kilometres in length, which will place Antivari in direct communication with the principal inhabited districts.

But of more importance still for the trade of Montenegro is the proposed formation in Podgorizza of a National Steamship Company with a capital of 40,000 florins in 400 shares of 100 florins each. This Company is to perform, by means of small river steamers, a regular service between Rieka and the *bazar* of Scutari, the other side of the Lake.

Later on, when the improvements in the river Bojana shall be terminated, these steamers will probably reach Obotti where sea-going vessels can discharge cargo. The Government of the Principality, which looks upon the river Bojana as the best and most natural outlet of Montenegro to the sea, will encourage and favour the new enterprise as far as possible.

TELEGRAPHIC SERVICE OF THE OTTOMAN EMPIRE

The "Revue d'Orient" of the 4th February last, states that it has been decided to commence the re-organisation of the telegraphic service of the Ottoman Empire with the lines of Asia Minor, which will henceforth be divided into three principal sections: the line Constantinople-Bagdad will be extended to India; the Constantinople-Aleppo line to Egypt via Beyruth; and the Constantinople-Castambol line will be connected with the Russian system via Trebizonde. The first section, which is the largest, stands in need of repairs, and in future all damages will be seen to at once to avoid delay in the transmission of telegrams; the number of employés specially engaged in the superintendence of the lines will be considerably increased.

THE SYRIAN COCOON HARVEST.

The "Journal Officiel" states that the latest cocoon harvest in the districts round Tripoli (Syria) has not given the good results anticipated, and may be estimated perhaps at 25 per cent. less than the average.

The unfavourable climatic conditions of the year, added to speculation, have greatly contributed to this deficit. The scarcity of leaves and the abnormal quantity of eggs subjected to incubation, have influenced the yield, the average for the season having been 2 okes (oke = 2.8 lbs.) of cocoons for every drachma of

eggs (drachma = .11 oz.) as against 3 oke during the preceding year.

On the other hand, the losses experienced two years ago by spinners and persons who usually speculate in cocoons at the time of the harvest, having made the bankers more cautious, the market, at the beginning at least, has been quieter.

The exports of the season (31st June 1894 to 31st May 1895) reached the hitherto unrecorded figure of 639 bales, weighing 63,585 kilogs. net. On the 1st June 1895 it was estimated that 50,000 kilogs. of cocoons still remained unspun in stock. The production of the spinning factories of Tripoli has increased continually during the last five years, and the harvest of cocoons grows with ever-increasing number of mulberry trees in the province.

Hitherto the exportation of raw silk via the port of Tripoli has been entirely in the direction of France where the Lyons market absorbs the greater part. On the other hand, the importation of silkworms' eggs are almost all of French origin, chiefly from the department of Le Var, so that the silk production and industry in Syria form an essentially French branch of the European trade with this part of Asiatic Turkey. Italy is endeavouring to monopolise the Servian market; and the administration of the Ottoman Public Debt has several times endeavoured to introduce into Tripoli the yellow cocoons of Bagdad.

TRADE OF THE UNITED STATES FOR THE YEAR ENDED 31ST DECEMBER 1895.

From the monthly return relating to the "Finance, Commerce, and Immigration of the United States" for December 1895, which has recently been issued, it is shown that the value of the total imports of merchandise during the 12 months ended December 31, 1895, amounted to 801,663,000 dols. (167,013,000*l.*), as compared with 676,313,000 dols. (140,898,000*l.*) in the corresponding period of 1894, the figures for 1895 showing an increase of 125,350,000 dols. (26,115,000*l.*) The value of the exports of domestic merchandise was 807,740,000 dols. (168,279,000*l.*) in 1895 and 807,312,000 dols. (168,190,000*l.*) in 1894, these figures thus showing a slight increase of 428,000 dols. (89,000*l.*). The exports of foreign merchandise (re-exports) amounted to 17,122,000 dols. (3,567,000*l.*) in 1895, and 17,790,000 dols. (3,706,000*l.*) in 1894, or a decrease of 668,000 dols. (139,000*l.*)

In the total values of imports as shown above, the categories in which there were increases were as follows:—In articles in a raw state for domestic industry, an increase of 59,007,000 dols. (12,293,000*l.*); in manufactured articles for the mechanic arts, 32,978,000 dols. (6,870,000*l.*); in manufactured articles for consumption, 59,635,000 dols. (12,424,000*l.*); and in articles of voluntary use, luxuries, &c., 5,735,000 dols. (1,195,000*l.*). The

only category in which there was a decrease was in that of articles of food and live animals, this decrease amounting to 32,004,000 dols. (6,667,000*l.*).

With regard to the exports of domestic merchandise, there were increases in the following categories :—In manufactures, 23,352,000 dols. (4,865,000*l.*) ; in products of mining, 2,252,000 dols. (469,000*l.*) ; in products of the forest, 1,875,000 dols. (391,000*l.*) ; and in the products of the fisheries, 992,000 dols. (207,000*l.*). The decreases were in the products of agriculture, amounting to 27 970,000 dols. (5,827,000*l.*) ; and in miscellaneous products to 73,000 dols. (15,000*l.*).

BULLETIN OF THE DEPARTMENT OF LABOUR (U.S.A.)

A despatch has been received from the Foreign Office enclosing copy of No. 2 of the Bulletin issued every other month by the United States Department of Labour. The current number contains articles on The Poor Colonies of Holland ; The Industrial Revolution in Japan ; Trade-Unions in Great Britain and Ireland ; Wages and Hours of Labour in Great Britain and Ireland ; Strikes in Switzerland ; The Wealth and Receipts and Expenses of the United States, Decisions of Courts Affecting Labour, and Digest of Recent Reports of State Bureaus of Labour Statistics.

UNITED STATES RAILWAY EARNINGS IN 1895.

The "Railway Age" (U.S.A.) states that a partial idea of the results of the operations of American railways in 1895, in comparison with those of former years, may now be obtained. 122 roads in the United States, Canada, and Mexico, representing 104,036 miles, earned in the period of 12 months ended December 31 last, in round numbers, 553,458,000 dols., or about 29,800,000 dols. more than in 1894. Deducting the earnings of seven Canadian and Mexican lines, amounting to 56,728,000 dols., it is seen that 115 roads in the United States, aggregating 90,075 miles, or about 50 per cent. of the present mileage, earned 496,700,000 dols., or about 28,500,000 dols. more than last year. Supposing that the other roads in the country did as well on the whole, there was an increase last year of something like 54,000,000 dols. over the preceding year of bad results. The "Financial Chronicle" thinks that the improvement may amount to from 60 to 65 millions, as against an estimated loss in 1894 of 150 millions.

MINERAL PRODUCTION OF THE UNITED STATES IN 1895.

The total value of the production of minerals in the United States in 1895, according to "Le Monde Economique" amounted to 698,000,000 dols. as against 568 millions in 1894. The value of pig iron reached 112,000,000 dols. or 40 millions in excess of 1894. Copper, 39,000,000 dols. or 5 millions in excess of 1894; but silver 27 millions less than the previous year; the production of lead was about the same; but that of coal increased from 212 millions of dollars in value in 1894 to 240 millions in 1895, or an increased value of 28,000,000 dols.; the production of iron ore increased 31,500,000 dols. in value, or nearly cent. per cent.

THE DISPLACEMENT OF TRADE IN VENEZUELA.

In a report of the Chancellor of the French Legation at Caracas published in the "Moniteur Officiel du Commerce" of 6th February last a general displacement of trade in Venezuela is noticed.

The report states that in 1894 several representatives of important English and North-American firms visited Caracas to study its capabilities as a trading place, and, not content with confining themselves to the three traditional towns of Caracas, Valencia and Puerto-Cabello, they also visited Maracaibo, the great port for the region of the Andes, the richest district of Venezuela, and which supplies more than a fourth of the coffee, herds, and wood produced.

The trade of Maracaibo is almost entirely in the hands of Germans, and at Ciudad-Bolivar, though the chief element is French, the trade of the place is also dominated by Germans and North-Americans. Ciudad-Bolivar is the depôt for the mining regions of Guiana, and the market for various towns of the Upper Orinoco.

The commercial centre of Venezuela is gradually undergoing a change. At one time people flocked to Caracas from all parts of the Republic to make their annual purchases; now, as a result of the opening of certain lines of railway, Maracaibo, Puerto-Cabello, Carupano, and Ciudad-Bolivar have robbed Caracas of a part of its trade. Business houses of some importance have also been established at Valencia, Barquisimeto and San Fernando de Apure. This change was first noticeable about 1890-91, though checked in its movement by the revolution of 1892.

INSURANCE COMPANIES IN PERU.

A despatch, dated January 11 last, has been received at the Foreign Office from Captain H. M. Jones, Her Majesty's

Minister at Lima, enclosing translation of an Act recently passed by the Peruvian Chambers, which requires that all insurance companies having agents, offices, or branches in Peru must, in order to safeguard the interests of the public in its relations with them, have an effective capital for local operations of not less than 100,000 silver dollars, of which one-third must be invested locally in real estate or Government securities. The passing of this Act has led to the withdrawal by the British companies of all their agencies in Peru, and the only foreign companies now maintaining a branch in Peru are the "South British" of New Zealand, and the "New York Life" of New York.

JAPANESE TRADE IN 1895.

The "London and China Telegraph" notes that the total trade of Japan for the eleven months ending November 30, 1895, as compared with the corresponding period of 1894, was as follows:—

						1895.	1894.
						Dols.	Dols.
Exports	-	-	-	-	-	125,092,880	101,467,463
Imports	-	-	-	-	-	116,299,673	107,902,949
Total						241,329,553	209,370,412
Increase	-	-	-	-	-	-	31,959,141

IMPOSITION OF A TRADE TAX IN JAPAN.

On January 11th, a Bill relating to the Trade Tax was introduced in the Japanese House of Representatives.

According to an account published in the "Japan Weekly Mail" for January 18, the Bill is complicated, consisting of 39 articles. The Trade Tax is to be imposed, at different rates, on individuals or legal persons engaged in the sale of commodities of all sorts, in banking operations, in insurance business, in money lending, in manufacture of every description, in transport business, in warehousing, in working on canals and piers, in shipbuilding business, in the maintenance of anchorages for ships, and places for landing freight, in contracting for engineering works, in contracting for labour, in printing business, in photography, in keeping rooms for hire, in hotel business, in restaurant business, in real estate agency, in general

agency, in the business of middleman, and in brokerage. To give an idea of how the rates are fixed, it will be sufficient to cite a few cases. In the case of the sale of commodities, the tax is to be made up of $\frac{5}{10000}$ (for wholesale), or $\frac{15}{10000}$ (for retail) of the proceeds of sales, $\frac{40}{10000}$ of the rent of premises, and 1 yen per employé. For those engaged in banking, insurance, money lending, lending of articles and warehousing, the rates are $\frac{2}{10000}$ of the capital, $\frac{40}{10000}$ of the rent of premises, and 1 yen per employé. Those engaged in printing, manufacture, and photography, will have to pay $\frac{15}{10000}$ of their capital, $\frac{40}{10000}$ of the rent of premises, 1 yen each business employé, and 30 sen per hand. The Trade Tax will not be imposed upon persons engaged (1) in the sale of Government stamps of all sorts, (2) in the sale of minerals dug or collected by themselves, and (3) in the manufacture, repair, and sale of weights and measures. In the case of persons newly starting business, they are to be exempted from the tax during the first year. This exemption is to hold for the first three years, in the case of persons newly engaged in banking insurance, warehousing, manufacture, printing, transportation, working on canals, maintenance of piers, ship-building, and the maintenance of anchorages for ships. Mention need not be made of the provisions relating to appraisement of the rents of business premises, computation of the proceeds of the sales and other income, punishment for violations of the law, and so forth. The Trade Tax is to be paid in two instalments in May and November; and the present law is to come into force from the 1st of January 1897. It may also be of interest to refer to the existing system. The Trade Tax is now levied in the form of local tax, and its rate varies in different localities. In the opinion of the authorities, most of the inconveniences inseparable from the present system will be removed by carrying out the law just submitted to the House; while in view of the rapid growth of industry and commerce, this will prove a very profitable source of revenue to the State.

THE JAPANESE BUDGET.

The "Japan Weekly Mail" states that according to the Budget for the twenty-ninth fiscal year of *Meiji* (April 1896—March 1897), the aggregate Revenue and Expenditure are as follows:—

	Yen.			
Revenue	-	-	-	138,070,677.488
Expenditure	-	-	-	152,071,463.322
Deficit	-	-	-	14,000,785.834

The deficit is to be met by the various measures of taxation and by a Public Loan, which are to be introduced in the form of a Supplementary Budget.

The above-mentioned Revenue is made up of two parts, the ordinary and extraordinary, as follows :—

Revenue.

Ordinary :—

	Yen.
1. Taxes - - - -	73,792,890·980
2. Licences and fees - -	2,380,168·830
3. Receipts from Government works and property - - -	16,312,530·937
4. Miscellaneous receipts - -	716,176·776
5. Interest on deposits - -	1,201,566·000
Total - - - -	94,403,333·523

Extraordinary :—

1. Contributions - - - -	19,676·000
2. Proceeds of the sale of Govern- ment property - - -	577,973·305
3. Miscellaneous receipts - -	238,155·162
4. Appropriation from the fund reserved for the manufacture and issue of Public Loan Bonds	15,000·000
5. Appropriation from the Central Relief fund - - - -	160,900·000
6. "War vessel construction fund" -	1,812,250·822
7. Indemnity on account of the expenses of occupying Wei- haiwei - - - -	750,000·000
8. Appropriation from the war in- demnity - - - -	40,093,388·676
Total - - - -	43,667,343·965
Grand total - - - -	138,070,677·488

The expenditure is also divided into ordinary and extraordinary, as follows :—

Expenditure.

Ordinary :—

	Yen.
Civil List - - - -	3,000,000·000
Department of Foreign Affairs - -	1,010,947·262
Department of Home Affairs - -	7,818,165·573
Department of Finance - - -	39,379,486·452
Department of War - - - -	16,314,163·527
Department of the Navy - - -	7,663,986·701
Department of Justice - - - -	3,482,210·264
Department of Education - - -	1,357,948·092
Department of Agriculture and Com- merce - - - -	1,058,864·357
Department of Communications - -	8,123,904·011
Total - - - -	89,209,676·239

Extraordinary :—

	Yen.
Department of Foreign Affairs - -	61,087·556
Department of Home Affairs - -	3,497,221·163
Department of Finance - -	1,012,511·214
Department of War - -	19,268,360·990
Department of the Navy - -	29,456,335·675
Department of Justice - -	91,873·889
Department of Education - -	214,225·005
Department of Agriculture and Com- merce - -	720,305·075
Department of Communications -	8,539,866·516
Total - -	<u>62,861,787·083</u>
Aggregate expenditure - -	<u>152,071,463·322</u>

TOBACCO MONOPOLY IN JAPAN.

The "Japan Weekly Mail" states that, of the measures to be submitted to the Diet that which attracts keenest attention, after the army and navy bills, is the bill relating to the State monopoly of the sale of leaf tobacco. The first point kept in view by the Government in drawing up the scheme has, it is said, been to make the system as simple as possible. All the leaf grown in the country is to be delivered to the Government at prices fixed by the latter, and sold out to the manufacturers at specified rates. The intending growers are to send in by the end of April each year notices of the area of land to be devoted to the cultivation of tobacco, and must obtain the permission of the Government. In order to regulate the supply of the leaf, the Government will reserve the power of limiting, at its discretion, the area of land to be devoted to growing tobacco. The delivery of the produce to the Government is to be effected before the end of March every year. Although the grower is not to be allowed to hold back any part of the produce, either for his own use or for sale, that restriction is not to be applied to the case of leaf intended for exportation. In that case, the grower may, with the permission of the Government, sell his produce to others. The accumulation and sale of the leaves are to be effected by special offices, established for the purpose at convenient places throughout the country, and the new system is to come into force on and after the 1st January 1898. Simultaneously with putting it into operation, the existing tobacco tax regulations are to be abolished, so that the sale of manufactured tobacco will thenceforth not require the use of any stamps. A fund for the purchase of the leaf is to be created, to which end a special measure will be introduced under the title of "Project of Law relating to the Tobacco Leaf Monopoly Fund."

LIGHTHOUSE CONSTRUCTION IN FORMOSA.

The "Journal Officiel" states that, according to a communication of the French Minister at Tokio, the Japanese Government has decided to construct eight lighthouses in Formosa at the following places, viz.: Tsurikakesaki, Yahushima, Satsoutakasaki, Je Jima, Tsukatajima, Uscharazaki, Petao (Formosa promontory) and Fouki.

TRADE OF CAPE COLONY FOR 1895.

Returns have been received at the Board of Trade from the Agent-General for the Cape of Good Hope, from which it appears that the total imports into Cape Colony (exclusive of government importations and specie) for the year 1895 amounted in value to 13,285,000*l.*, or an increase of 2,397,000*l.* over the value for 1894. The port of the colony doing the greatest import trade during the year was Port Elizabeth, which imported 51 per cent. of the above total; Cape Town followed with 25 per cent., and East London with 22 per cent. of the same.

With regard to the value of the exports of Colonial products for 1895 this was 16,577,000*l.*, as compared with 13,503,000*l.* for 1894, or an increase of 3,074,000*l.* Cape Town is represented by 50 per cent. of the total, Port Elizabeth by 12 per cent., East London by only 5 per cent., and various inland stations together by 31 per cent.

The re-exports of imported goods in 1895 were valued at 221,000*l.*, or 27,000*l.* more than in 1894; in this total Cape Town figures for 80 per cent.

TRADE OF CANADA FOR 1895.

The "Monetary Times" of Toronto states that, according to the trade returns just issued at Ottawa, the general trade of Canada during the fiscal year 1895 amounted, outward and inward, to 224,420,485 *dols.*, as compared with 240,999,889 *dols.* in the 12 months preceding. There was thus a falling off equal to 16,579,000 *dols.* from the aggregate of 1894. The decline was in exports as well as imports, but principally in imports, which were 12,693,000 *dols.* less, while the exports were only 3,886,000 *dols.* less. A small table will make the comparison clear:

		Imports.	Exports.
		Dols.	Dols.
Fiscal year 1893-4	- -	123,474,940	117,524,949
" 1894-5	- -	110,781,682	113,638,803
Decrease last year	-	12,693,258	3,886,146

It is important to observe that the exports for the year ended with June 1895, exceeded the imports of the same period by 2,857,121 dols. This is the second instance in the history of the Dominion that this has occurred, the ordinary rule being the reverse. In 1879-80, which was a time of depression, the like happened, the imports of that year having been 86,489,747 dols., and the exports 87,911,458 dols.

The proportion of trade carried on in 1893 with Great Britain and the United States was 86·93 per cent.; in 1894 it was 85·01 per cent.; and last year it was 84·17 per cent.; that is to say, 41·43 per cent. with Great Britain and 42·74 per cent. with the States.

The total trade with principal countries, and the proportion of import and export, are indicated in the annexed table. The "imports" are the amounts entered for consumption:—

Imports and Exports for 1895.

				Exports to.	Imports from.
				Dols.	Dols.
Great Britain	-	-	-	61,856,990	31,131,737
United States	-	-	-	41,297,676	54,634,521
Germany	-	-	-	626,976	4,794,159
France	-	-	-	335,282	2,585,174
West Indies	-	-	-	3,725,426	4,956,196
Newfoundland	-	-	-	2,325,196	739,850
China and Japan	-	-	-	378,160	2,528,414
Spain	-	-	-	34,101	402,479
Belgium	-	-	-	251,402	441,617
South America	-	-	-	1,303,474	306,996
Italy	-	-	-	34,325	381,594
Holland	-	-	-	140,264	243,900
Portugal	-	-	-	58,781	57,140
Australia	-	-	-	417,124	—
Switzerland	-	-	-	—	259,400
Other countries	-	-	-	853,626	1,789,334
Total	-	-	-	113,638,803	105,252,511

The difference between the aggregate of these totals (218,891,314 dols.) and the 224,000,000 dols. given above as the total trade, arises from the fact that the entries for consumption during the year were some six millions less than the general imports.

Trade with the United States and the West Indies increased over the previous year, but with every other principal country it had decreased. Exchanges of commodities with China and Japan, which averaged 1,266,000 dols. a year in the 10 years ended with 1885, have grown to 2,601,000 dols. per year in the 10 years ending 1895, and the West India trade also shows some growth from the efforts made to stimulate it. Total trade with those islands was 7,438,000 dols. a year on an average during the last five years, while in the five years ending with 1890 it had been only 5,420,000 dols. The exports thither last year were 3,725,000 dols., and the imports thence was almost 5,000,000 dols., the aggregate of 8,681,000 dols. showing a marked increase, in which both imports and exports participate.

OPENING FOR BRITISH CAPITAL IN ONTARIO.

The "Canadian Gazette" of February 20 last states that attention has of late been directed to a line of industry in which British capital and enterprise may find a profitable outlet in Ontario, and, for that matter, in other parts of Canada as well, wherein the manufacture of the raw material could be effected in Canada for shipment to the United Kingdom, and there made into useful and necessary goods so as not only to be profitable to the firms or corporations engaged in the enterprise, but to build up a large and important trade. The industry relates to veneer and cut-stock. There are in Ontario considerable areas of suitable timber for both "slicing" and "rotary-cut" veneering—such as maple (four varieties), birch (three varieties), oak (three varieties), elm (three varieties), beech, sycamore, basswood, ash, balm, and white-wood—adjacent to a line of railway, so as to be convenient for shipping. The most improved machinery for this class of work can be obtained in Canada; and the veneering or cut-stock, properly dried, cut to size, and carefully bundled, can be shipped to Great Britain and made into furniture, house decoration, packing-boxes, nail kegs, barrels, butter-tubs, and an almost infinite number of lesser—though by no means small or unimportant—lines of goods. A great deal of attention and study has been given to this veneer industry, and its development and extension from one line of manufactured goods to another—such as pianos, organs, sewing machines, &c.—have been such that at this present time large lines that were formerly manufactured of solid material are now "built up," so as to be cheaper, more durable and substantial, and much lighter; and many other lines of industry are moving into the using of veneer for constructive material. A trade could, it is believed, be built up reaching into millions per annum; and as the industries provided for are daily necessities, so the business would increase from year to year. The capital for the industry on a large scale, however, would have to come from England. The few large lumbering and timber firms that are in Canada are all concerned with pine, and so special provision for capital would have to be made.

THE PROGRESS OF BRITISH COLUMBIA IN 1895.

The "Canadian Gazette" in a recent issue publishes an account of the position of the trade and industries of British Columbia in 1895, from which it appears that the colony continues to make satisfactory progress in all branches. In this article descriptions of the various mines and districts where gold is to be found occupy a prominent part.

During 1895, British Columbia contributed no less than 3,000,000 dols. in round figures to the world's supply of gold—a total which speaks well for the present and future. From

Kootenay alone 2,500,000 dols. of the precious metal was exported; and it is satisfactory to note that a large amount was during the year shipped to Montreal and other Eastern cities instead of to the States. At Golden Cariboo last year 70,000 dols. was taken in incomplete "wash-ups" on the claims of a Montreal syndicate. Other capitalists are also conducting hydraulic mining there, work has advanced to a stage where great returns are shortly to be expected, and in 1896 the gold output of Cariboo will, it is thought, again show a considerable increase.

Next to the mining industry the fisheries rank in importance. The seal catch in 1895 on the British Columbia coast, on the Japanese coast, on the Copper Island coast, and in Behring Sea, reached a grand total of 73,614 skins, of which the Canadian catch represented 70,739 skins. The catch was remarkable for large skins of fine colour and quality. Turning to the salmon supply, it is found that the number of cases put up last year was 600,881, as compared with 494,871 in 1894. During the year the rivers on the west of Vancouver Island made their first contribution to the pack, and this season will see three, and perhaps six, new preserving factories established on the west coast.

With regard to the lumber trade, although the demands of the United Kingdom, North and South America, and the new markets in South Africa absorbed a large quantity of wood during 1895, prices have not improved, but it is anticipated that the combination recently effected at San Francisco, by most of the mills of Oregon, Washington, and British Columbia will have the effect of adding to existing values. Full figures of the export of lumber are not available, but it is of interest to observe that trade with Australia is not neglected, and that several vessels took cargoes to Shanghai and Tientsin. The coal shipments from the Nanaimo mines aggregated last year 339,704 tons, a decrease of 54,068 tons as compared with 1895.

With regard to the trade of the principal cities, last year the total value of the exports from Victoria was 2,355,065 dols., and of imports 620,615 dols.; in 1894 the figures were respectively 3,013,213 dols. and 698,937 dols. Vancouver's exports totalled 897,035 dols. in 1895, while her imports reached 1,005,452 dols. New Westminster's exports were of the value of 2,939,447 dols., which is 1,090,758 dols. in excess of 1894, and twice that amount in excess of 1893. The imports of the Royal City totalled last year 593,492 dols., as against 531,703 dols. in 1894.

Turning lastly to agriculture, it is seen that the industry made remarkable progress in 1895. Full statistics are not yet available, but it is stated that the wheat crop was three times as large as in 1894, when the crop was 6,526 tons. Oats also gave a large crop, the yield being placed at double that of 1884, when it was 14,627 tons.

More attention is being paid to fruit than formerly, and the Horticultural Board is doing much to raise the quality of the production. One thing that has to be kept in mind, however, by

the farmers is the necessity of improved grading and packing to make their wares as attractive as possible. Besides the increased production of fruit, new markets were found, and a notable feature of last year was the shipments to the north-west territories of large quantities of plums and cherries. Co-operative factories of tinned fruits have been established to deal with this trade.

The farmers also seem to be at last awakening to the importance of the dairying industry. Up to a short time ago all the butter-making in the Province was in private hands, and not till 1895 did the first creamery start. Now it is being successfully operated at Ladners, turning out 500 or 600 lbs. a week, and not by any means able to supply the demand.

Taking everything into consideration, it would seem that the farming industry made decided advances during 1895, and the outlook for the future is still brighter.

PACKING BUTTER FOR EXPORT.

The "Australasian," in a recent issue, states that one of the chief obstacles in the way of the successful development of the dairying industry has hitherto been found in the difficulty of discovering a cheap, clean, and attractive way of packing butter, honey, and other perishable farm products. It will come as a surprise to many to learn that a simple process has been discovered, and is now in operation in Australia, by which the inventors claim that they can supersede costly refrigerating plant completely, and despatch butter, honey, jam, and similar comestibles to any part of the world with the certainty that when opened the article will be found in perfect condition.

How far these expectations are warranted by results already attained cannot as yet be stated definitely, but it is a fact that butter packed in Melbourne by the new process has been opened at Kimberley, South Africa, 700 miles from Cape Town, and found to be as good as when it left the butter factory in Victoria.

By the new process the butter is packed in a box made of six small sheets of ordinary glass, the points of juncture being covered with gummed paper. The glass casket is then covered with a mould of plaster of Paris about a quarter of an inch thick, which in turn is covered with specially prepared paper. The package then presents the appearance of a solid brick of plaster or cement, and as the plaster is a non-conductor of heat it is claimed that the butter packed hermetically inside is unaffected by any change of temperature. The prime cost of packing 1 lb. of butter in this way is about 1d., and the convenience and portability are obvious. The inventor states that these solid bricks of butter can be and have already been loaded on ship-board as ordinary cargo, and delivered in perfect condition in Coolgardie, Northern Queensland, and South Africa.

Cases manufactured in the same way, and capable of holding 2 cwt. of butter, have also been manufactured by the syndicate

at its large factory, where 40 hands, principally boys and girls, are engaged in making the glass boxes and covering them with plaster of Paris, all except the top, which is put on afterwards by a simple mechanical device, and removed by the purchaser in an equally easy manner.

It is claimed that a saving of 25 per cent. is effected on freight and packing as compared with butter frozen and carried in the usual way. In the future the new invention may revolutionise present methods of dealing with dairy produce for export.

TRADE OF NEW SOUTH WALES FOR 1895.

According to the "Australasian Trade Review," the returns of trade for the colony of New South Wales show that during the year closing on the 31st December 1895, imports totalled 13,902,568*l.*, against 13,840,499*l.* in 1894, or an increase of 62,069*l.* Apart from imports of gold, which decreased by 868,768*l.*, imports of merchandise showed an increase to the amount of 930,837*l.*

Exports, as will be seen in the appended table, showed an increase of 915,559*l.*, of which merchandise contributes 136,123*l.*, and gold, 779,436*l.*

				Twelve months ending 31st December.	
				1894.	1895.
				£	£
Imports :					
Gold	-	-	-	2,791,953	1,923,185
Merchandise	-	-	-	11,048,546	11,979,383
Total	-	-	-	13,840,499	13,902,568
Exports :					
Gold	-	-	-	2,131,635	2,911,071
Merchandise	-	-	-	11,259,651	11,395,774
Total	-	-	-	13,391,286	14,306,845

Comparing imports with exports, it will be seen that, while in 1894 imports exceeded exports by 449,213*l.*, exports exceeded imports in 1895 by 404,277*l.* Taking merchandise alone, the reverse will be seen to be the case, as in 1894 exports of merchandise exceeded imports of the same by 211,105*l.*, and in 1895 imports of merchandise exceeded exports by 583,609*l.*

THE EXPORT TRADE OF NEW ZEALAND.

The "New Zealand Trade Review" publishes a comparative table of the values of the exports of New Zealand for the years ending 30th September, 1895 and 1894, showing separately the proportions from each of the most important ports and from the North Island and South Island respectively.

The particulars respecting wool show that though the North Island exhibited a decrease and the South Island an increase as compared with the year 1893-4, yet that the North Island still had a slight excess over the South as to the weight of wool shipped. The figures were as follows:—

—		1894-5.	1893-4.
Quantity.—North Island	- Lbs.	64,970,226	67,261,754
South Island	- Lbs.	64,363,543	61,218,703
Value. — North Island	- £	1,994,132	2,174,509
South Island	- £	2,031,988	2,214,736

In value each island shows a falling-off, and the decline in the two divisions is approximately equal, in spite of the fact that in the one case there is an increase and in the other a decrease in weight. The only column which shows an increase is that of the minor ports of the North Island, the growth being almost entirely at Wanganui

Increased value is shown in frozen meat, especially in the South Island, where it compensates for the cessation of exports of wheat. The increased bulk of shipments of wool tends in the same direction, though, as regards the past year, the influence was neutralised by lower valuations. The increase in frozen meat is equal to about 21 per cent. in weight, and between 16 and 17 per cent. in value.

In tallow there is an increase of nearly 25 per cent. in the North Island and fully 35 per cent. in the South. The percentage of increase is very heavy at Dunedin and at Timaru. It was largely caused by the boiling down of sheep.

In the important industry of dairy produce, the values of the contributions of the two islands were as follows:—

—		Butter.		Cheese.	
		1894-5.	1893-4.	1894-5.	1893-4.
		£	£	£	£
North Island	-	192,530	214,239	64,409	28,669
South Island	-	41,192	60,246	94,465	76,060

Each division shows a decrease in butter and an increase in cheese. The percentage of the production of the North Island is, however, increased in both cases, that of butter having risen

from under 80 to over 82 per cent. of the whole, and that of cheese from under 28 to over 40 per cent. of the whole.

The figures of the gold exports are worth quoting. They are as follows :—

	1894-5.	1893-4.
	£	£
Auckland - - - -	343,647	202,099
Wellington - - - -	40	—
Dunedin - - - -	272,788	308,931
Bluff - - - -	30,411	17,481
Other S.I. ports - - - -	340,011	396,740
Total - - - -	986,857	925,251

NEW ZEALAND TIMBERS AND FOREST PRODUCTS.

A communication has been received from the Agent-General for New Zealand, transmitting copy of a pamphlet recently issued on New Zealand timbers and forest products.

In this pamphlet an effort has been made to collect from various sources reliable information concerning the timbers and forest products of the colony. No claim is made to expert knowledge of the subject treated, but extracts bearing on the commercial value of the New Zealand forests have been made from the writings of competent authors, which are not easily accessible to the ordinary reader. During the past year determined efforts have been made to bring New Zealand woods under the notice of the timber trade of Great Britain, and the samples of wood displayed at the Imperial Institute and at the New Zealand Government offices (13, Victoria Street, London), have sufficed to demonstrate the fine quality and varied beauty of New Zealand timber, and its suitability for the trade requirements of this country.

The matter collected in this pamphlet has been limited chiefly to the selection of information for timber traders in these markets.

XVII.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Barnsley.—The annual meeting of this Chamber was held on February 18 last, Mr. W. Batty (president) in the chair. The annual report was read and adopted, Mr. Batty was re-elected president for the year, and delegates were appointed to attend the meetings of the Associated Chambers of Commerce in London. There was no other business of importance.

Birmingham.—A meeting of the Council of the Birmingham Chamber of Commerce was held at the Exchange on February 12 last, Mr. T. J. Walsh presiding. A letter was read from Lord George Hamilton acknowledging receipt of the letter from the Chamber relative to railway communication between Burmah and China, and stating that there was every prospect that the Mandalay-Kunlon railway would be commenced at an early date, and that it would be so constructed as to meet the demands that were likely to be made on it in the matter of traffic.

A communication was received from the Worcester Chamber of Commerce asking the Birmingham Chamber to join with them in an endeavour to secure the abolition of the licence for selling gold and silver plate. Mr. Barker described the tax as very vexatious, and hoped that the Chamber would assist in obtaining its removal. Silver was so cheap that there should be free trade in it as in other metals. He believed that if the tax were abolished, it would result in the employment in Birmingham of a much larger number of men, as many other people would sell plate if there were no licence. At the present time he believed there were numbers of people who dealt in plate who did not pay the licence. He proposed that the action of the Worcester Chamber be supported. Mr. Tonks hoped that the Council would be careful how it interfered in the matter. The Birmingham Jewellers' Association had been very careful not to pronounce an opinion on the matter, but the National Retail Association of Traders had been formed, and one of its main objects was not only to see that the licence was enforced, but to maintain it. Jewellers who had a licence and who had paid it for a number of years did not agree with drapers and others opening up a business in plate goods by a side door, as it were. They felt that a gross injustice would be done if the plate licence were abolished. On a vote the amendment was lost, and the resolution carried. It was decided to support any action taken by Chambers of Commerce with regard to the opening of diplomatic relations between the British Government and Bolivia. The question of a national trade mark was mentioned, but it was resolved not to take any steps in the matter. There was no other business of importance.

Blackburn.—A meeting of the council of the Blackburn Chamber of Commerce was held on February 17th last, Mr. Henry Harrison, J.P., presiding.

The president, referring to the railway rates and charges, said that efforts had for some time been made to arrive at a satisfactory conclusion with the railway companies on the question of the overcharges of 5 per cent. upon the rates of 1893. A great many of the advances had been removed, and at the present time overcharges of 1s. 3d. per ton were only made on lots of cloth under two tons. He believed the railway companies were inclined to meet the traders of the district half way on this question, and several of the directors of the Chamber were in favour of that course. He hoped that at the next meeting he would be able to report that satisfactory terms had been arranged.

The president further reported that in accordance with the resolution passed at a previous meeting, the secretary had prepared a list of the looms and spindles in the area covered by the Chamber of Commerce. It was very desirable that they should have an accurate list. It was decided that the return be published in the annual report of the Chamber.

Mr. Harrison was re-elected president for the year.

Bradford.—The monthly meeting of the Bradford Chamber of Commerce was held on the 26th February last, Mr. T. A. Duncan in the chair.

The president read the following letter, which had been received from Mr. F. H. B. Skine, Collector of Customs at Calcutta :—

“SIR,

17th January 1896.

“I HAVE the honour to forward for your inspection a gross imitation of an English mark, believed to be a Bradford one, known as the “Greyhound,” 90,107. I shall be obliged by your ascertaining the owner, and placing him in communication with me. Also by your letting it be as widely known as possible that I am prepared to register English marks and stop foreign imitation. Application, with fee of one rupee currency (say 1s. 3d.) and sample with description of mark, to be addressed, ‘Collector of Customs, Calcutta.’ In this case I hear that the English trade has been nearly killed.

“I have, &c.,

“(Signed) F. H. B. SKINE,

“Collector of Customs.”

The president observed that the offer of the Collector of Customs to register trade marks should be of great service, and ought to be made known as widely as possible.

The secretary read a letter from a Bradford firm, complaining of the refusal of the French Custom-house authorities to reconsider the decision of experts as to the nature of some goods which had been sent to France.

Mr. Hoffmann said that this was a common cause of complaint against the French Custom-house, and it gave another illustration of the unwisdom in cases of dispute of allowing goods to leave the Custom-house until the question had been absolutely settled. If the goods had been left in the Custom-house, although the decision of the experts had been given, the Chamber could have appealed to the English Foreign Office, and by that means might have obtained another inquiry.

Bristol.—The usual monthly meeting of the council of the Bristol Chamber of Commerce was held on February 26 last, Mr. G. H. Perrin in the chair.

Instructions were given to the Bristol deputation as to voting upon the various questions in the official programme of resolutions for the London meeting of the Associated Chambers of Commerce next month.

Plans for the river improvements, extension of the free delivery boundaries of the railway companies, and tramways for goods traffic at the Bristol docks, also came up for discussion.

Halifax.—The monthly meeting of the council of this Chamber was held on February 26 last, Mr. T. Sutcliffe in the chair.

Mr. Clement Holdsworth said he had received a newspaper cutting from Melbourne stating that the collector of Customs had received instructions to enforce at once section 5 of the Land and Income Assessment Act Amendment Act, 1895, which provides that non-resident agents are to obtain a licence to carry on business in the colony in lieu of paying income tax. The fee payable in respect of such licence is 50*l.*, and every non-resident agent is liable to a penalty not exceeding 20*l.* for every day during which he carries on business without a licence. Mr. Holdsworth remarked that this would operate greatly to the disadvantage of English manufacturers who sent agents to Australia on business. The chairman said the matter would affect Bradford and other towns, and it would perhaps be advisable for all the Chambers of Commerce to take united action. The secretary was instructed to write to the Colonial Office to ascertain if the information in the paragraph be correct, and, if so, the General Purposes Committee were empowered to take what action might be deemed necessary.

Hebden Bridge.—The annual report of the Hebden Bridge and District Commercial Association, Ltd., just issued, reports on the losses of the Association during the past year from bad debts and trade losses, and comments generally on the increased railway rates, the new Factory Act, and other matters of local interest only.

Leeds.—A meeting of the Leeds Chamber of Commerce was held on February 26 last, Mr. J. Pease in the chair.

The council considered the agenda for the coming meeting of the Associated Chambers, the Rating of Machinery Bill, postal communication with the south-west, the question of a national trade mark, and the metric system.

Liverpool.—A meeting of the committee of the African trade section of the Liverpool Chamber of Commerce was held on March 6 last, Mr. John Holt in the chair.

A report by Sir Claude Macdonald respecting the adulteration of produce in the Niger Coast Protectorate, and the steps taken by the Administration in this matter was read, and a discussion followed, part of the committee being of opinion that the Government should be asked to extend the local regulations to the European traders, and to punish anyone found purchasing adulterated produce. It was, however, carried by a majority that no further alteration in the law should be advised, but that the Government should be requested to enforce the existing regulations more strictly.

The affairs of Benin, the Lagos Bar, and the Sierra Leone spirit duties also came up for discussion.

The monthly meeting of the Chamber was held on March 6 last, Mr. Charles M'Arthur in the chair. The subjects discussed were, cablegrams by telephone, Companies' Acts Amendment, High Court of Justice Continuous Sittings Bill.

London.—A meeting of the Council of the London Chamber of Commerce was held on Thursday, February 13 last, Mr. W. J. Thompson, junr., in the chair.

Reference was made to the steamboat service on the Thames, telegraphic and telephonic communication with lightships, &c., overside delivery in the port of London, horseless carriages, light railways, Merchandise Marks Act Committee, and other subjects.

At the last meeting of the leather trade section of the Chamber attention was called to the case of "*Clarke v. the Owners of the Silver Stream*," in which his firm represented the plaintiff, in whose favour there was a verdict with costs. The pelts were buried in damp earth in the lower hold of the ship for five months, and on being landed proved to be in bad condition, fully 40 per cent. being damaged, and many being without value. Mr. Justice Bruce gave judgment to the effect that if a shipowner stowed casks of pelts in damp ballast, which was the case in this instance, they were stowed improperly. The opinion of the meeting was that the decision was of the greatest importance to the trade, and that the attention of members should, therefore, be called to the matter.

A large number of replies were submitted from Colonial Chambers of Commerce, in answer to the section's circular calling attention to the great depreciation in the value of hides caused by the present system of branding cattle in the colonies. The latest communication—from the Maryborough (Queensland) Chamber—stated that the Colonial Government intended to introduce into Parliament a measure permitting owners to brand on portions of the hide of least value to the tanner, and a code of earmarks is being arranged by all the Colonies, which, when adopted, will render branding in prominent portions less necessary than at present.

The annual meeting of the cement trade section was held on February 20 last, of the tobacco trade section on January 23, of the South African trade section on February 4, and of the timber trade federation on February 11.

Manchester.—At the annual meeting of the Chamber, held on February 3 last, Mr. W. H. Holland in the chair, the annual report was adopted, and new directors elected for the current year.

An ordinary meeting of the Board of Directors was held on February 12 last.

The minutes of the African sectional committee contained a reference to correspondence upon special difficulties encountered in the introduction of British cotton goods into British East Africa, which had been under the consideration of the committee. It appears that American drills and sheetings have long been known to the Arab traders at Zanzibar and Mombassa, and even in Uganda. Within the last two years British cloths of equal quality, but bearing new marks, have been introduced, but their distribution is checked, because the Administration is in the habit of paying the troops, partly at least, by means of American goods, which pass as a kind of currency in British East Africa. Inasmuch as there are parts of the country where neither British nor

American cloths have yet become known, and the effect of the exclusive use of the former by the Administration for these purposes constitutes in effect an official preference in their favour, it was resolved, on the recommendation of the committee, that the Secretary of State for Foreign Affairs should be requested to instruct Her Majesty's Administration in British East Africa to employ also, as far as possible, goods of British manufacture for military or official purposes.

Reference was also made to the Registration of Firms Bill, the metric system, imports of silver into Lagos, railway connection between Burmah and Siam, and the Indian duties on cotton goods.

Nottingham.—A meeting of the council of the Nottingham Chamber was held on February 24 last, Mr. George Wigley in the chair.

Mr. Carver submitted the report of the foreign trade committee. He said the committee had held a meeting since the last meeting of the council, for the purpose of receiving the samples that had been sent by the British Consul-General at Siam for the inspection of manufacturers, to see how far such samples could be imitated by manufacturers in this country. Manchester, Bradford, and Blackburn had had the samples, and the committee had been informed by the Associated Chambers that if any members of the Nottingham Chamber wished to have the facility of seeing those samples, they could do so by visiting the offices of the Associated Chambers in London on certain dates. The committee, however, thought it desirable, in the interests of the town, that they should request that the samples should be forwarded to Nottingham. With the permission of the Foreign Office—which was readily granted—these samples had been sent down, and the foreign trade committee thought it was desirable to invite certain manufacturers, whether members of the council or not, to inspect the samples.

Reference was made to an express service between London and Nottingham and horseless carriages, and the council then proceeded to consider the resolutions to be considered at the conference of the Associated Chambers. There are 31 propositions in all on the agenda, having reference to various subjects. Upon 20 of the resolutions the council expressed no opinion, the only one upon which opposition was directed to be offered being that from Bradford, which runs as follows:—"That the regulation
 " that all packages transmitted by pattern post must have printed
 " or stamped thereon the name, address, and trade of the sender
 " (without which such packages are subject to the letter rate of
 " postage) is unnecessary and is a great hindrance to trade, and
 " that the Postmaster-General be therefore urged to rescind the
 " same."

Oldham.—At the last meeting of the council of the Oldham Chamber of Commerce, the president and Messrs. Emmott and Beardoe Grundy were appointed to represent the Chamber at the annual meeting of the Associated Chambers of Commerce. In considering the programme for that meeting each resolution

was dealt with seriatim. On the question of consular reports Mr. Emmott said that the secretary had recently put before him some official instructions and correspondence in regard to this matter dating back for a period of 10 years. He found that the instructions that had been sent to Her Majesty's Consuls were really very full, and practically gave directions on a great many of the points that the Chamber had urged for a long time. There was, however, one question of importance which he thought might be considered. In some part of these documents of which he spoke, the question was raised as to the difference between the American Consul and the English Consul. It was there stated that the American Consul was or had been a commercial man, and he was only appointed as a rule for a few years—for a President's term of office, or it might possibly be for a shorter period. Then he went back to business. He had, therefore, a grasp of commercial questions not usually possessed by our own Consuls. If we imitated America in having commercial men as our Consuls it would add considerably to the expense of the consular service of this country; and it was a definite point how much we intended to pay in addition to what was now spent on that service. Further, another practical point called attention to, and one he had heard mentioned in that Chamber, was that all consular representatives of this country should not be commercial travellers and must not be expected to be commercial travellers. People who were interested in the trade of some particular districts should read the consular reports from those districts and indicate precisely where they were short of the information they required. It was decided that support should be given to resolutions standing in the name of the Executive Council urging that more attention should be paid to consular reports, and requesting the Foreign Office to obtain through the Consuls samples of goods of foreign manufacture with a view to such samples being circulated among Chambers of Commerce interested in such manufactures.

The yarn contracts question was reported on, and Mr. S. R. Platt was re-elected president of the Council.

Sheffield.—A meeting of the council of the Sheffield Chamber of Commerce was held on February 26 last, Mr. S. E. Howell in the chair.

A letter read from the Associated Chambers of Commerce pointed out that the Foreign Office was prepared to receive practical suggestions from Chambers of Commerce as to improvement in the system of consular trade reports, and also as to how consular officers can assist British traders. It was announced that the subject would be discussed at the annual meeting of the Associated Chambers, and that suggestions could be brought before that body.

The council considered a communication from Mr. W. T. Wheatley, advocating the merits of a national trade mark. After consideration, it was resolved to take no action thereon.

Reference was also made to the Companies Acts, and the metric system.

Walsall.—At the annual meeting of this Chamber, the report for the past year was read and adopted, the election of the council carried out and the remaining business of the meeting was to decide what steps the Chamber should take in regard to the various matters to be brought up for discussion at the next meeting of the Associated Chambers of Commerce.

It was resolved to support a proposition of London that early information should be obtained of proposed international exhibitions, and that British commerce should be directly represented in connection with them, and also to support a proposition by Worcester to prevent railway companies increasing their rates without first submitting them to the Board of Trade.

The next proposition considered was one by Bradford, requiring the public prosecutor to take up cases of false marking under the Merchandise Marks Act, and the other by Sheffield to obtain a common definition of a trade mark. With regard to the first, the chairman expressed his conviction that the Merchandise Marks Act had been a most serious injury to British trade, and he strongly opposed it being made more stringent. Mr. Scott, however, pointed out that this was not what was intended, but that what was wanted was that the public prosecutor should be required to take up such cases, as this Chamber had had before them when they found a common cast-iron bit marked as “best steel.” The chairman urged that any interference between buyers and sellers must be injurious and must lead to endless difficulties as to where it should stop. This question was, in the end, left open, but it was decided to support the Sheffield proposition with regard to trade marks.

With regard to a proposition by Hickmondwike that the system of dealing in the options and futures of produce was detrimental to the country, Mr. Leckie pointed out that 90 per cent. of the transactions in wheat were mere speculation, there being no intention either to deliver or accept the wheat which was pretended to be bought. The result of the system was that wheat had fallen 26s. per quarter, while barley, which was not subject to such speculation, had only fallen 9s. 3d. What was wanted was that a man who sold what he had not got should be subject to a heavy penalty. Mr. Rathbone pointed out that in some things—cotton for instance—purchases must be made for further delivery or the manufacturers could never send out their estimates. He thought they would be right in opposing the resolution. Mr. Greatrex supported this view, but Mr. Leckie, in reply, said that such transactions would not be touched. In the cotton for future delivery it was intended that the goods should be delivered, but in the wheat gambling nothing of the sort was ever intended. It was resolved to leave the question open.

Amongst other propositions brought forward it was resolved to support one to the effect that the various chambers be asked to institute local inquiries with a view to ascertain what amount of foreign competition existed in their district, and to report in detail what means could be taken to obtain the substitution of British goods for foreign ones.

XVIII.—RECENT TRADE BLUE BOOKS.

1. *Metropolitan Water Companies. Return of the Accounts, as they are respectively made up, of the Metropolitan Water Companies to the 30th day of September and the 31st day of December 1894 (in continuation of Parliamentary Paper, No. 294, of Session 1894).* 448. Sess. 2. Price 2½d.

The present return is issued under the direction of the Local Government Board, and gives the accounts of the various metropolitan water companies between the above dates.

The water companies whose accounts are dealt with are as follows:—Chelsea Waterworks Company; East London Waterworks Company; Grand Junction Waterworks Company; Kent Waterworks Company; Lambeth Waterworks Company; New River Company; Southwark and Vauxhall Water Company; and West Middlesex Waterworks Company.

2. *London Water Companies Supply. Return showing the Average Amount of Water supplied Daily by each of the London Water Companies, from various sources, in each Month during the past two Years, specifying the Amount from each source separately.* 452. Sess. 2. Price 1d.

This is a further return issued under the direction of the Local Government Board, giving the average amount of water supplied daily by each of the metropolitan water companies. The companies whose returns are here given are the same as in the preceding paper. The return deals with the period from September 1893 to August 1895.

3. *Metropolitan Water Companies. Return relating to.* 459. Sess. 2. Price 2½d.

This is a return also issued by the Local Government Board. It shows, as regards each water company supplying water in the metropolis—(a) the name of each metropolitan parish where the supply throughout the parish is a constant supply; (b) the name of each metropolitan parish where the supply is a constant supply only in part of the parish, and, in the case of each such parish, a description of the part of the parish where the supply is constant; (c) with respect to each parish at whose instance the constant supply was ordered or given; (d) the name of each metropolitan parish where the supply throughout the parish is intermittent; (e) the name of each metropolitan parish where the supply is intermittent only in part of the parish, and, in the case of each such parish, a description of the part of the parish where the supply is intermittent; and (f) with respect to each parish or part of a parish where the supply is intermittent, the period during which water is supplied daily to the premises of consumers.

The return is dated September 3, 1895.

4. *Tramways (Street and Road). Return of Street and Road Tramways authorised by Parliament, showing the Amount of Capital Authorised, Paid up, and Expended; the Length of Tramway Authorised, and the Length open for the Public Conveyance*

of Passengers down to the 30th day of June 1895; the Gross Receipts, Working Expenditure, and Net Receipts; the Number of Passengers conveyed, and the Number of Miles run by Cars during the Year ended the 30th Day of June 1895; together with the Number of Horses, Engines, and Cars at that date (in continuation of Parliamentary Paper, No. 270, of Session 1894. 393. Price 3d.

This is the annual return issued by the Board of Trade, the present number being for the year ended 30th June 1895. It shows that the total capital expended on the 30th June 1895 was 14,956,343*l.*, as compared with 14,388,698*l.* in 1894. The total length of line open for public traffic was 982 miles in 1895, against 975 miles in 1894; the total number of passengers carried in 1895 was 661,760,461, as compared with 616,972,830 in 1894; the gross receipts were 3,733,690*l.*, against 3,615,837*l.* in 1894; the working expenses 2,878,490*l.* and 2,859,056*l.*; while the net receipts amounted to 855,200*l.* in 1895 and to 756,781*l.* in 1894.

5. *Twenty-fourth Annual Report of the Local Government Board, 1894-5.* (C.7867.) Price 5s. 2d.

This report, which is issued by the Local Government Board, contains the customary information with regard to the proceedings of the Board during the year 1894-5. The statements and observations submitted in this report are divided into five parts, relating respectively to—(1) the Local Government Act, 1888, and County Councils; (2) the Local Government Act, 1894; (3) Relief of the Poor and the Poor Rate; (4) the Public Health and Local Administration; and (5) Local Taxation and Valuation.

6. *Building Societies Return*, 395. Price 1s. 9d.

This return, which is issued by the Home Office, is dated the 6th of July 1895, and is in continuation of Parliamentary Paper, No. 222, of Session 1894. It contains an abstract of the accounts furnished by building societies incorporated to the 31st day of December 1894, and includes both Great Britain and Ireland. It is made pursuant to the Building Societies Act 37 & 38 Vict. c. 42, s. 40, in the annexed form:—

Register Number.	
Year when Incorporated.	
Name of Society, and Chief Office or Place of Meeting.	
Year of the Society's Existence.	
Date to which Accounts are made up.	
Number of Members (where stated).	
Total Receipts during the last Financial Year.	£
To the Holders of Shares.	£
To Depositors and other Creditors.	£
Balance of Unappropriated Profit.	£
Balance Deficit (if any).	£
Balance due on Mortgage Securities (not including Prospective Interest).	£
Amount Invested in other Securities, and Cash.	£

7. *Railway, &c., Bills. Report by the Board of Trade upon all the Railway, Canal, Tramway, Gas, Electric Lighting, and Water Bills and Provisional Orders of Session 1896.* 54. Price 6d.

This is an annual return issued by the Board of Trade. It contains statements relating to the number of railway, canal, tramway, gas, electric lighting, and water Bills, the total proposed capital, and the total length of proposed railways and tramways; also to the number of applications to the Board of Trade for Provisional Orders under the Tramways Act, the Gas and Water Works Facilities Acts, and the Electric Lighting Acts, together with Bills for railways, &c., by existing and new companies, and Bills for tramways, gas, electric lighting, and water.

Lists are also appended of Bills for which reports are required to be made by the Board of Trade with regard to tolls, rates, and charges under Standing Order of the House of Commons 145A; also of Bills containing powers of purchase, &c., in respect of which it may be necessary for the Board of Trade to issue certificates in compliance with Standing Orders 122, House of Lords, and 163, House of Commons, as regards the paid-up capital and expenditure of the companies; also of Bills for which reports are required to be made by the Board of Trade with regard to abandonment and release of deposits, under Standing Order 158A, House of Commons; and also of Bills for which reports are required to be made by the Board of Trade with regard to level-crossings and inclination of roads, under Standing Orders 154 and 155, House of Commons.

8. *Fifty-Seventh Annual Report of the Registrar-General of Births, Deaths, and Marriages, in England.* 1894. (C.—7768). Price 1s. 5d.

In submitting his report for the year 1894, dated the 5th of December 1895, to the President of the Local Government Board, the Registrar-General remarks that the population of England and Wales, as enumerated on 6th April 1891, consisted of 29,002,525 persons. The natural increase, by excess of births over deaths, from the beginning of April 1891 to the middle of the year 1894, amounted to 1,121,145, and this would raise the population to 30,123,670 at the latter date. In the absence of precise information regarding emigration and immigration the estimated populations published in the annual reports are based on the assumption that the rate of increase which had prevailed in the intercensal period immediately preceding has since been maintained. Estimated by this method the population in the middle of the year 1894 was 30,060,763, including 14,565,660 males and 15,495,103 females. It is on this population that the rates for 1894 in the present report have been calculated.

The number of marriages registered in the year 1894 was 226,449, corresponding to a rate of 15.1 persons married per 1,000 living. This is a slight recovery from the very low rate of 1893, which had been 14.7, and also on the average rate (14.9) in

the 10 years 1884-93. The lowest marriage-rates in registration counties during 1894 were 10·5 in Rutlandshire, 11·6 in Middlesex, 11·9 in Huntingdonshire, 12·1 in Westmoreland, 12·2 in Hertfordshire, 12·3 in Buckinghamshire, and 12·4 in Surrey and in Essex. The highest rates were 15·6 in Staffordshire, 15·7 in Warwickshire, 15·8 in Nottinghamshire and in Monmouthshire, 15·9 in the West and East Ridings of Yorkshire, 16·2 in Lancashire, 16·7 in South Wales, and 17·0 in London. Speaking generally, the counties which had shown the highest or the lowest average marriage-rates in the 10 years 1884-1893 occupied similar positions respectively in the scale of marriage-rates in 1894.

The births registered during the year numbered 890,289, and were in the proportion of 29·6 only per 1,000 persons living; this rate being the lowest on record. The birth-rate in 1894 showed a defect of 2·0 per 1,000 below the mean rate in the decennium immediately preceding. The lowest birth-rates were 23·5 in Westmoreland and in Rutlandshire, 24·4 in Surrey, 24·7 in Sussex and in Huntingdonshire, and 25·3 in Devonshire. The counties with the highest rates were Nottinghamshire and Derbyshire 31·7, South Wales 33·1, Monmouthshire 33·7, Staffordshire 35·0, and Durham 35·2.

The deaths registered in England and Wales in 1894 were 498,827 in number, and in the proportion of 16·6 per 1,000 of the population; the average rate in the ten years immediately preceding having been 19·2. The death-rate in 1894 was considerably lower than that in any previous year on record, the nearest approaches to so low a rate having been 18·9 in the year 1881, 18·1 in 1888, and 18·2 in 1889. In these three years only previous to 1894 had the rate of mortality in England and Wales been below 19·0 in a thousand.

9. *Administration Report on the Railways in India for 1894-95, by Lieutenant-Colonel T. Gracey, R.E., Officiating Director-General of Railways.* (C. 7845.) Price 3s. 3d.

This return gives the detailed account of the railways of India for the year 1894-95. The report states that the following lengths of new line opened for public traffic between the 1st April 1894 and the 31st March 1895 compare, with the figures in the period from 1st April 1893 to 31st March 1894, as follows:—

—	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	Miles.	Miles.	Miles.	Miles.
1894-5 - - -	284½	75½	1½	360½
1893-4 - - -	247	182½	22¼	451¾

The total length of railway open for traffic, after minor corrections, was on the 31st March 1895 as follows:—

	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	Miles.	Miles.	Miles.	Miles.
State lines worked by companies -	3,529 $\frac{1}{2}$	5,238	—	8,767 $\frac{1}{2}$
State lines worked by the State -	4,039 $\frac{1}{2}$	1,310	28	5,377 $\frac{1}{2}$
Lines worked by guaranteed companies -	2,586 $\frac{3}{4}$	—	—	2,586 $\frac{3}{4}$
Assisted companies -	183 $\frac{3}{4}$	172 $\frac{3}{4}$	51	407 $\frac{1}{2}$
Lines owned by Native States and worked by companies -	412 $\frac{1}{2}$	188 $\frac{1}{2}$	71 $\frac{3}{4}$	672 $\frac{1}{2}$
Lines owned by Native States and worked by State railway agency	124	—	22 $\frac{1}{4}$	146 $\frac{1}{4}$
Lines owned and worked by Native States -	—	744 $\frac{1}{4}$	94 $\frac{1}{2}$	838 $\frac{3}{4}$
Foreign lines -	—	58 $\frac{3}{4}$	—	58 $\frac{3}{4}$
Total -	10,875 $\frac{3}{4}$	7,712	267 $\frac{1}{2}$	18,855 $\frac{1}{4}$
Total length open at end of previous year }	10,596 $\frac{1}{4}$	7,636 $\frac{3}{4}$	267	18,500

In addition to the above, there were 91 $\frac{1}{4}$ miles of steam tramways, working outside municipal limits.

Including unfinished lines, the total capital expenditure on all railways up to the 31st December 1894 amounted to Rs. 246·88 crores, thus:—

	Standard Gauge.	Metre Gauge.	Special Gauges.	Unclassified Expenditure.	Total.
	Rs.	Rs.	Rs.	Rs.	Rs.
Crores -	185·22	59·96	0·88	0·82	246·88

In addition to this, Rs. 0·78 crores were expended on collieries and on surveys, completed and in progress, making up the total expenditure of railways under all heads to Rs. 247·66 crores. Of this, the capital outlay on open lines, including steamboat service and suspense accounts, amounted to:—

	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	Rs.	Rs.	Rs.	Rs.
Crores -	180·53	56·38	0·88	237·79

The average cost per mile of railway open was as follows:—

Standard Gauge.	Metre Gauge.	Special Gauges.
Rs. 1,60,020	Rs. 70,725	Rs. 32,253

The cost per mile of single track, including sidings, was—

Standard Gauge.	Metre Gauge.	Special Gauges.
Rs. 1,23,619	Rs. 62,699	Rs. 30,616

The gross earnings during the calendar year were Rs. 25,50,88,564, against Rs. 24,08,75,468, during 1893, as follows :—

—	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	Rs.	R.	Rs.	Rs.
1893 - - -	17,82,77,908	6,13,12,800	12,84,760	24,08,75,468
1894 - - -	18,89,05,257	6,48,74,147	13,09,160	25,50,88,564
Increase -	1,06,27,349	35,61,347	24,400	1,42,13,096

Of the total gross earnings during 1894, 74·77 per cent. were earned by the standard gauge lines, 25·06 per cent. by the metre gauge, and the balance, 0·17 per cent., by the special gauge lines.

The number of passengers carried, as compared with the previous year, was as follows :—

—	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	No.	No.	No.	No.
1893 - - -	83,454,834	46,245,635	819,978	135,520,447
1894 - - -	94,128,215	50,698,172	900,710	145,727,097
Increase -	5,673,381	4,452,537	80,732	10,206,650

The total working expenses amounted in 1894 to Rs. 11,98,39,200, against Rs. 11,35,48,059 in the previous year, as follows :—

—	Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
	Rs.	Rs.	Rs.	Rs.
1893 - - -	8,14,78,987	3,13,57,326	7,11,746	11,35,48,059
1894 - - -	8,74,16,899	3,16,27,581	7,94,720	11,98,39,200
Increase - -	59,37,912	2,70,255	82,794	62,91,141

The net earnings realised were Rs. 13,52,49,364, against Rs. 12,73,27,409 in 1893, as under :—

— — —			Standard Gauge.	Metre Gauge.	Special Gauges.	Total.
			Rs.	Rs.	Rs.	Rs.
1893	-	-	9,67,98,921	2,99,55,474	5,73,014	12,73,27,409
1894	-	-	10,14,88,358	3,32,46,566	5,14,440	13,52,49,364
Difference	-	-	+ 46,89,437	+ 32,91,092	- 58,574	+ 79,21,955

The statistical return on the capital expenditure on open lines, including steamboat service and suspense accounts, compares as follows :—

— — — —				Standard Gauge.	Metre Gauge.	Special Gauges.	Average of all Lines.
1893	-	-	-	5·48	5·38	6·63	5·46
1894	-	-	-	5·62	5·90	5·84	5·69

10. *Board of Trade. Strikes and Lock-outs (Labour Department). Report by the Chief Labour Correspondent on the Strikes and Lock-outs of 1894. (C.—7901). Price 2s. 9d.*

In presenting this, the seventh annual report, by Mr. Burnett, the Chief Labour Correspondent of the Department, Mr. Llewellyn Smith, the Commissioner for Labour, remarks that while the general arrangement of the volume does not greatly differ from that of last year, certain alterations and re-arrangements have been made (the nature of which is explained in the report), with a view to make the tables more generally useful.

The Commissioner for Labour desires to call attention to the evidence supplied by the report of the greatly diminished intensity of labour disputes in 1894 as compared with the two previous years. Although, doubtless partly owing to the improved machinery of the Department, a larger number of actual outbreaks was recorded, the aggregate number of working days lost through disputes (which is the best test of their importance) was less than one-third of the corresponding number for 1893, and not much more than half of that for 1892, and also to point out the fact that far the greater part of the lost time is due to a very small number of disputes. Three-quarters of the time lost in all the 1,061 disputes of the year was accounted for by 16 disputes, no less than 60 per cent. being due to a single strike.

With Mr. Burnett's report is incorporated a special section prepared in the Department, dealing in greater detail than in previous years with the settlement of disputes and other questions by arbitration and conciliation.

11. *Report on Formosa, by Mr. N. Perkins, Assistant in Her Majesty's Consular Service. (Commercial No. 1, 1896.) (C.—7922.) Price 2½d.*

The present report, which is dated Peking, June 25, 1895, is stated to be in continuation of Commercial No. 11 (1893), C.—7104. It deals with the position and natural divisions of the island of Formosa, its population, means of communication, ports, and harbours, lights and beacons, administration, missions, products and exports, imports thereto, and other general information with regard to the position of the country. An appendix is attached containing a statement by Mr. A. Henry, F.L.S., on the flora and botany of Formosa.

12. *Banking and Railway Statistics, Ireland. December, 1895. (C.—7958.) Price 2d.*

This is a half-yearly return prepared under the direction of the Registrar-General for Ireland. It gives the statistics of deposits and cash balances in Joint Stock Banks; deposits in Post Office Savings Banks and Trustee Savings Banks; amount of the Government Funds, India Stocks, and Guaranteed Land Stock; and Bank Note circulation in Ireland, December, 1895; also weekly traffic receipts of Irish railways for the years 1894–95; and railway traffic statistics for each of the ten years, 1885–94.

The present return shows that the deposits and cash balances in the Joint Stock Banks, in December 1895, stood at 39,008,000*l.* (exclusive of 1,383,000*l.* Government and other public balances in the Bank of Ireland), as compared with 37,607,000*l.* at the corresponding period in the year 1894, being an increase of 1,401,000*l.*, or 3·7 per cent., following an increase of 1,755,000*l.* or 4·9 per cent., at the close of 1894, as compared with the amount for December, 1893, and showing an advance of 9,237,000*l.*, or 31·0 per cent., on the amount for December 1887, since which period, comparing December with December, there has been a continuous annual increase.

On the 31st December 1895 the estimated balances in the Post Office Savings Banks in Ireland amounted to 5,603,000*l.*, as compared with 4,977,000*l.* for the corresponding date in 1894, being an increase of 626,000*l.*, which, although slightly under the large increase (634,000*l.*) in 1894, is more than double the highest increase in any previous year since the establishment of these banks.

The amount of deposits at the end of each year in Trustee Savings Banks reached, in December 1894, 1,982,000*l.*, and in 1895 2,075,000*l.*, being an increase of 93,000*l.* following an increase of 106,000*l.* in 1894, and a decrease of 91,000*l.* in 1893, and being the highest amount for December in any year since 1884, when the balance was 2,097,000*l.*

It will be seen that, as stated in the report, there has been a very large increase in savings during the year, the total amount in the savings banks in Ireland being 7,678,000*l.* in December 1895, as compared with 6,959,000*l.* for the corresponding period of the year 1894, being an increase of 719,000*l.* for the year, following the large increase of 740,000*l.* in the year 1894.

XIX.—QUARANTINE NOTICES.

DENMARK.—The Government of the Danish Antilles has substituted a sanitary inspection for the quarantine imposed on vessels arriving from ports of Brazil.

PORTUGAL.—Arrivals from Volhynia and Podolia are free from cholera since 10th January.

Macao is suspected of bubonic plague. Peruvian ports are reported free from yellow fever from the 15th February last.

Ceuta and ports of Morocco are reported free from cholera since February 14. Pernambuco is declared infected with yellow fever, and all the other ports of the Province suspected of the same disease.

SPAIN.—Arrivals from Casa Blanca are admitted to free pratique if they have a clean bill of health signed by the Spanish Consul, or, in default, by the Consul of some other nationality, the general health on board being satisfactory, and no suspicious case having occurred, provided they do not otherwise come under existing quarantine regulations.

Similarly, vessels from ports declared compromised by the Royal Order of January 7, shall be admitted to free pratique if the general state of health on board is satisfactory.

Likewise merchandise specified in the Royal Order of October 29, 1886, which left Casa Blanca since January 24, and arrives at Spanish ports after March 4, shall be admitted without disinfection.

The provisions of the Royal Order of July 28, 1893, issued exclusively for the port of Vigo, has been extended to the ports of Corunna and Santander.

TURKEY.—48 hours' observation on arrivals at Constantinople from Alexandria.

GREECE.—The following alterations have been made in the quarantine regulations at Athens, viz. :—

1. The sanitary examination of all steam and sailing vessels arriving from any European port is suppressed.

2. In future only the steam and sailing vessels from Turkish ports shall be subjected to a severe sanitary examination.

3. The quarantine of five days' observation against Egyptian ports, as well as the ten days' against Damietta, are maintained.

XX.—POST OFFICE NOTICES.

MAILS FOR THE WEST AND SOUTH-WEST COASTS
OF AFRICA.

The following table shows the places for which Mails will be despatched by the Mail Packets leaving Liverpool for the West and South-West Coasts of Africa during the latter part of the month of March 1896 :—

Wednesday, 18th March.—Sierra Leone, Sherbro, Accra, Lagos, Fernando Po, Rio del Rey, Cameroons, Batanga, Eloby, Gaboon, Mayumba, Loango, Landana, N'Gove, Nyanga, Cape Lopez, Fernan Vaz, Sette Cama, Black Point, Cabenda, Banana, Boma (Congo), Muculla, Ambrizette, Mussera, Kinsembo, Ambriz, Loanda, Victoria, and Bibundi.

Saturday, 21st March.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Benin, Sapelli, Warree, Brass, and Akassa.

Wednesday, 25th March.—Madeira, Teneriffe, Goree, Dakar, Rufisque, Bathurst, Isles do Los, Conakry, *Sierra Leone, *Sherbro, Monrovia, Sinoe, Grand Bassa, Kroo Coast, Half Jack, Lahou, Cape Lahou, Grand Lahou, Drowin, Grand Bassam, Assinie, Axim, *Cape Coast Castle, Elmina, Salt Pond, *Accra, Pram Pram, Addah, Quitta, Jellah Coffee, Lome, Little Popo, Grand Popo, New Calabar, Abonema, Bakana, and Degama.

Saturday, 28th March.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Bonny, Old Calabar, Opobo, Benin, Warree, New Calabar, Bakana, and Brass.

In London the correspondence should be posted on the previous day in each case.

Correspondence for Senegal (Goree, Dakar, Rufisque), the Congo (Banana, Boma), and Portuguese possessions on the West and South-West Coasts of Africa is not sent via Liverpool unless specially so superscribed.

These vessels carry Parcel Mails for Bathurst, Sierra Leone, Monrovia, Axim, Cape Coast Castle, Salt Pond, Winnebah, Accra, Addah, Quitta, Lagos, Benin, Bonny, Brass, New Calabar, Old Calabar, Opobo, and Warree.

The Parcel Mails are made up at Liverpool on the afternoon of the day before the departure of each vessel.

* Correspondence for Sierra Leone, Sherbro, Cape Coast Castle, and Accra will not be sent by the steamer of the 25th of March, unless specially so superscribed as the direct packet leaving on the following Saturday is due at those ports some days earlier.

XXI.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month and Two Months ended 29th February 1896, compared with the corresponding periods of the year 1895.

MONTH ENDED 29TH FEBRUARY.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 29th February		Increase.	Decrease
	1896.	1895.		
	£	£	£	£
I. Animals, living (for food) - -	846,517	492,806	353,711	—
II. (A.) Articles of food and drink, duty free - - -	11,400,923	8,668,872	2,735,551	—
(B.) Articles of food and drink, dutiable - - -	1,749,032	1,628,354	120,678	—
Tobacco, dutiable - - -	300,212	183,663	116,549	—
III. Metals - - -	1,663,857	1,302,753	367,104	—
IV. Chemicals, dyestuffs, and tanning substances - - -	917,247	626,393	296,854	—
V. Oils - - -	777,085	404,087	372,998	—
VI. Raw materials for textile manufactures - - -	7,317,791	6,751,169	566,622	—
VII. Raw materials for sundry industries and manufactures - - -	2,746,428	1,875,018	871,410	—
VIII. Manufactured articles - - -	6,532,296	5,376,394	1,155,902	—
IX. (A.) Miscellaneous articles - - -	1,125,915	769,201	356,714	—
(B.) Parcel post - - -	93,433	65,979	28,154	—
Total Value - - -	35,476,736	28,134,489	7,342,247	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - -	50,793	36,423	14,370	—
II. Articles of food and drink - - -	730,222	698,442	131,810	—
III. Raw materials - - -	1,220,075	1,090,528	138,547	—
IV. Articles manufactured and partly manufactured, viz.:—				
(A.) Yarns and textile fabrics - -	9,461,438	8,060,324	1,401,114	—
(B.) Metals and articles manufactured therefrom (except machinery) - -	2,589,972	1,844,973	777,969	—
(C.) Machinery and millwork - -	1,212,024	960,965	251,059	—
(D.) Apparel and articles of personal use - -	918,987	709,355	209,132	—
(E.) Chemicals, and chemical and medicinal preparations - - -	717,387	613,476	103,911	—
(F.) All other articles, either manufactured or partly manufactured - -	2,660,224	2,005,897	654,327	—
(G.) Parcel post - - -	113,331	85,242	28,092	—
Total Value - - -	19,683,456	15,973,095	3,710,361	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	5,638,232	4,407,824	1,230,408	—

I.—FOREIGN TRADE OF THE UNITED KINGDOM—*cont.*

TWO MONTHS ENDED 29TH FEBRUARY.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Two Months ended 29th February		Increase.	Decrease.
	1896.	1895.		
	£	£	£	£
I. Animals, living (for food)	1,617,354	967,533	649,821	—
II. (A.) Articles of food and drink, duty free	23,913,731	20,537,510	3,376,221	—
(B.) Articles of food and drink, dutiable	3,641,932	3,579,059	62,873	—
Tobacco, dutiable	617,074	411,157	205,887	—
III. Metals	3,351,300	2,903,415	447,885	—
IV. Chemicals, dyestuffs, and tanning substances	1,733,719	1,220,976	512,743	—
V. Oils	1,600,248	1,080,963	519,285	—
VI. Raw materials for textile manufactures	16,318,977	16,376,402	—	57,425
VII. Raw materials for sundry industries and manufactures	5,936,140	4,568,525	1,367,615	—
VIII. Manufactured articles	12,619,988	11,060,309	1,559,679	—
IX. (A.) Miscellaneous articles	2,384,030	1,978,048	405,982	—
(B.) Parcel post	216,099	194,043	22,056	—
Total Value	73,950,592	64,877,970	9,072,622	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURES.

	£	£	£	£
I. Animals, living	105,478	75,537	29,941	—
II. Articles of food and drink	1,607,027	1,329,808	277,219	—
III. Raw materials	2,760,777	2,399,031	361,746	—
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics	19,547,381	17,300,529	2,246,852	—
(B.) Metals and articles manufactured therefrom (except machinery)	5,148,806	3,845,571	1,303,235	—
(C.) Machinery and millwork	2,548,439	2,032,382	466,057	—
(D.) Apparel and articles of personal use	1,909,826	1,492,687	417,139	—
(E.) Chemicals, and chemical and medicinal preparations	1,479,246	1,276,143	203,103	—
(F.) All other articles, either manufactured or partly manufactured	5,473,309	4,226,518	1,246,791	—
(G.) Parcel post	230,335	169,125	61,210	—
Total Value	40,810,624	34,197,331	6,613,293	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value	10,463,939	8,296,032	2,168,857	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 1st February 1896 -	46,576 3	106,076 0	25,601 1
„ 8th „ -	44,954 2	81,884 2	24,915 7
„ 15th „ -	45,100 7	76,396 2	23,063 7
„ 22nd „ -	41,204 7	67,018 1	21,578 5
„ 29th „ -	36,300 5	64,729 5	21,315 5
February 1896 - -	214,137 0	396,104 2	116,475 1
Corresponding month in 1895 -	207,709 1	366,502 5	77,209 4
„ „ 1894 -	205,683 0	232,614 1	67,213 3
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 1st February 1896 -	26 3	23 1	14 1
„ 8th „ -	26 4	22 5	14 0
„ 15th „ -	26 7	21 11	14 0
„ 22nd „ -	26 3	21 10	13 9
„ 29th „ -	25 6	21 10	13 10
February 1896 - -	26 2	22 2	13 11
January 1896 - -	25 7	23 10	13 10
December 1895 - -	24 11	23 11	13 11
November „ - -	25 9	25 1	14 2
October „ - -	24 6	25 5	13 5
September „ - -	22 10	24 0	13 7
August „ - -	24 2	20 3	15 7
July „ - -	24 9	19 8	16 1
June „ - -	25 9	19 7	16 0
May „ - -	22 5	20 5	15 2
April „ - -	20 5	21 0	14 2
March „ - -	19 11	21 3	13 10
February 1895 - -	20 0	21 9	13 8
„ 1894 - -	25 0	28 5	18 0
„ 1893 - -	25 9	25 1	17 9
„ 1892 - -	32 6	28 2	19 11

III.—EMIGRATION.—FEBRUARY 1895.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out of EUROPE during the Month ended 29th February 1896, and the Two Months ended 29th February 1896, compared with the corresponding Periods of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.	Total for corresponding Month, 1895.
Month ended 29th February.							
English - -	2,741	487	741	1,612	781	6,362	5,143
Scotch - - -	487	32	55	128	94	796	535
Irish - - -	920	33	38	26	36	1,053	1,034
Total of British } origin - - }	4,148	557	834	1,766	911	8,216	6,712
Foreigners - -	2,873	202	9	629	99	3,812	2,782
Nationalities not } distinguished - }	2	—	—	—	263	265	180
Total - -	7,023	759	843	2,395	1,273	12,293	9,674
Total for corre- } sponding } Month, 1895 - }	6,396	496	625	1,296	861	9,674	—
Two Months ended 29th February.							
English - - -	4,830	800	1,232	3,048	2,009	11,919	10,818
Scotch - - -	868	63	85	253	208	1,477	1,017
Irish - - -	1,557	69	56	42	64	1,788	1,876
Total of British } origin - - }	7,255	932	1,373	3,343	2,281	15,184	13,711
Foreigners - -	4,956	416	10	1,157	233	6,772	5,485
Nationalities not } distinguished - }	3	—	—	—	524	527	426
Total - - -	12,214	1,348	1,383	4,500	3,038	22,483	19,622
Total for corre- } sponding } Months, 1895 - }	12,446	1,038	1,478	2,490	2,470	19,622	—

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION.—FEBRUARY 1896.

RETURN of the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM during the Month and Two Months ended 29th February 1896, compared with the corresponding Periods of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.
Month ended 29th February.												
Aliens not stated to be en route to America:*												
Arrived at London -	720	310	175	34	91	46	—	—	62	60	1,048	497
" Grimsby -	75	34	37	45	15	6	—	—	23	8	150	60
" Hull -	115	38	70	43	39	23	—	—	101	49	325	123
" Tyne Ports -	8	35	14	55	188	104	—	—	93	93	303	287
" Leith -	28	47	38	18	10	—	—	—	31	10	107	45
" Newhaven -	—	—	—	—	—	—	372	238	1	—	373	238
" Other ports -	95	83	51	41	35	32	—	—	184	144	365	240
Total -	1,041	541	385	493	378	241	372	238	495	334	1,267	1,490
Aliens en route to America:												
Arrived at Grimsby -	12	47	42	57	442	245	—	—	51	69	547	398
" Hartlepool, } -	96	69	—	—	—	—	—	—	—	—	96	69
" West -												
" Hull -	134	3	8	18	294	412	—	—	474	347	910	780
" Leith -	46	67	—	25	2	—	—	—	28	4	76	97
" Other ports -	1	2	2	4	3	6	—	—	290	234	296	263
Total -	289	458	52	415	741	633	—	—	843	674	1,925	1,607
Total of aliens en route and of aliens not stated to be en route to America* -	1,330	672	437	338	1,119	844	372	238	1,338	1,005	4,596	3,097
Two Months ended 29th February.												
Aliens not stated to be en route to America:*												
Total No. -	1,952	1,116	705	512	671	634	784	644	1,053	688	15,165	13,594
Aliens en route to America:												
Total No. -	612	409	91	267	1,063	911	—	—	1,390	1,356	3,156	2,943
Total of aliens en route and of aliens not stated to be en route to America* -	2,564	1,525	796	779	1,734	1,545	784	644	2,443	2,044	18,321	16,537

* The distinction made in this Return between "Aliens not stated to be en route to America," and "Aliens en route to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be en route to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 213, Sess. 1895, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Goole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the aliens who arrived at ports in the United Kingdom not en route to America in the month of February 1896 was 688, and 520 in the same month of 1895; in the two months ended February 1896 the number was 1,233, and in the same months of 1895 the number was 1,150.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER of RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	February		Two Months ended February	
	1896.	1895.	1896.	1895.
Total gazetted	No. 337	No. 340	No. 721	No. 723
Number gazetted in principal trades and occupations :—				
Grocers, &c.	24	19	61	48
Publicans and hotel keepers, &c.	19	35	52	66
Builders	24	20	48	46
Farmers	21	30	44	53
Butchers	13	11	23	18
Greengrocers, fruiterers, &c.	4	10	17	17
Decorators, painters, plumbers, &c.	5	5	14	14
Boot and shoe manufacturers and dealers	7	10	13	17
Tailors, &c.	7	7	13	5
Confectioners	5	1	13	3
Drapers, haberdashers, &c.	8	10	12	11
Auctioneers	7	3	12	8
Solicitors	5	2	12	4
Bakers	3	8	11	21
Clerks, commercial and general	4	4	10	9
Agents, commission and general	5	6	9	13
Jewellers, watchmakers, importers, silversmiths, &c.	7	1	9	4
Travellers, commercial, &c.	3	1	8	7
Coal and coke merchants and dealers	5	1	7	11
Schoolmasters and schoolmistresses	3	3	7	4
Clothiers, outfitters, &c.	1	1	6	4
Carpenters and joiners	—	5	6	7
Merchants	2	3	6	4
Saddlers and harness makers	4	5	6	9
Corn, flour, seed, hay, and straw merchants and dealers	5	7	6	9
Restaurant, coffee, and eating-house keepers	2	2	6	3
Fishmongers, poulterers, &c.	2	3	5	9
Provision merchants, &c.	2	2	5	6
Carriers, carmen, lightermen, and hauliers	2	1	5	6
Cabinet-makers and upholsterers	4	—	5	2
Printers and publishers	1	2	5	3
Millers	1	4	5	5
Lodging-house keepers	2	2	5	6
Gardeners, florists, &c.	2	2	5	5
Ironmongers	1	1	4	5
Wine and spirit merchants, &c.	1	3	4	6
Furniture dealers and makers	2	1	4	3
Milliners, dressmakers, &c.	2	1	4	2
China, glass, and earthenware, &c. dealers	3	1	4	5
Engineers and founders, &c.	—	1	3	6
Stationers	3	3	3	4
Dairymen, cowkeepers, &c.	1	2	3	2

	February		Two Months ended February	
	1896.	1895.	1896.	1895.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.	No.	No.
Clerks in holy orders - - -	2	2	3	6
Contractors - - -	1	—	3	2
Carriage, &c. builders - - -	1	1	3	3
Officers in Army - - -	2	—	3	1
Timber merchants and wood dealers -	2	2	3	2
Blacksmiths, farriers, &c. - -	2	1	3	2
Architects and surveyors - -	—	1	3	2
Brokers, stock and share - -	2	—	3	2
Cattle and pig dealers - - -	1	—	3	3
Tobacconists, &c. - - -	—	3	2	5
Chemists, druggists, and chemical manufacturers - - -	1	1	2	2
Wheelwrights - - -	1	—	2	4
Hosiery, gloves, &c. - - -	2	—	2	2
Hair-dressers - - -	—	1	2	3
Curriers, tanners, and leather merchants	2	—	2	1
Cab, coach, and omnibus proprietors -	1	1	2	1
General dealers - - -	1	5	1	7
Fishing net, smack owners, and masters	—	1	1	4
Directors and promoters of public companies - - -	1	2	1	5
Stone, marble, and monument masons -	1	—	1	3

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 29th February 1896, and during the first Two Months of the Year 1896, compared with the corresponding Periods of the Year 1895.

Animals.	February		Two Months ended February	
	1896.	1895.	1896.	1895.
	No.	No.	No.	No.
Cattle - - - - -	44,362	31,794	83,463	71,024
Sheep - - - - -	10,313	6,464	21,329	22,564
Swine - - - - -	76,124	67,464	144,416	143,393
Goats - - - - -	6	19	15	22
Horses - - - - -	3,149	925	4,807	1,870
Mules or Jennets - - -	1	—	4	—
Asses - - - - -	19	1	23	4
Total - - - - -	133,974	106,667	254,057	238,877

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month and Two Months ended 29th February 1896, compared with the corresponding Periods of the Year 1895.

	February		Two Months ended February	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Brill	1,729	1,449	3,453	3,109
Soles	7,845	8,905	15,952	14,654
Turbot	5,966	5,810	12,602	11,464
Prime fish not separately distinguished	1,979	1,848	4,300	3,644
Total prime fish	17,519	18,012	36,312	32,541
Cod	53,743	49,073	98,481	85,946
Haddock	220,611	207,945	433,309	377,414
Hake	1,417	2,186	3,154	4,359
Halibut	3,947	3,272	5,545	4,803
Herrings	775	493	22,006	5,393
Ling	10,890	11,034	19,225	18,301
Mackerel	432	55	1,495	222
Pilchards	1,324	—	5,619	279
Plaice	53,845	54,406	107,813	99,200
Sprats	6,297	11,759	48,412	25,893
Fish not separately distinguished, except shell fish	89,638	80,504	185,910	161,865
Total	460,438	438,706	967,281	846,246
Shell fish:—	No.	No.	No.	No.
Crabs	332,314	14,056	421,067	45,497
Lobsters	23,082	8,004	35,170	18,354
Oysters	2,022,000	663,000	3,835,000	1,632,000
Other shell fish	Cwts.	Cwts.	Cwts.	Cwts.
	42,729	31,197	93,036	60,683
VALUE.				
	£	£	£	£
Brill	4,505	3,923	9,818	9,223
Soles	46,718	50,444	97,422	97,320
Turbot	23,230	23,125	51,827	49,072
Prime fish not separately distinguished	6,676	5,683	14,505	12,290
Total prime fish	81,129	83,475	173,572	167,905
Cod	33,892	27,120	65,637	58,783
Haddock	124,187	114,897	251,187	214,023
Hake	1,640	2,172	3,509	4,653
Halibut	8,527	6,836	12,976	11,711
Herrings	372	121	4,860	1,830
Ling	7,488	6,874	13,818	12,488
Mackerel	957	129	2,102	491
Pilchards	348	—	1,653	65
Plaice	64,945	65,373	137,517	125,922
Sprats	936	888	7,301	2,777
Fish not separately distinguished, except shell fish	60,962	49,824	123,185	106,503
Total	385,383	354,709	796,817	707,401
Shell fish:—				
Crabs	2,825	246	3,714	688
Lobsters	971	453	1,510	950
Oysters	7,394	1,719	13,921	5,488
Other shell fish	11,110	6,764	22,877	13,075
Total	22,300	9,182	42,022	10,904
Total value of fish landed	407,683	363,891	838,839	727,002

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month and Two Months ended 29th February 1896, compared with the corresponding Periods of the Year 1895.

	February		Two Months ended February	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles)	828	743	1,917	1,708
Turbot	264	199	498	364
Cod	61,864	48,130	106,257	87,344
Eel	2,480	2,957	3,648	4,533
Flounder Plaice, Brill	13,618	6,191	25,407	12,059
Haddock	127,115	129,175	261,914	212,365
Halibut	1,052	996	1,406	1,401
Herrings	57,731	45,427	89,554	84,875
Ling	8,058	21,943	12,344	26,779
Mackerel	53	59	55	61
Saith (Coal Fish)	5,253	9,222	9,708	14,820
Skate	6,892	10,136	11,098	12,591
Sparling	32	40	72	12
Sprats	331	735	29,929	1,036
Torsk (Tusk)	179	185	246	236
Whiting	4,309	2,755	8,873	4,940
Fish not separately distinguished, except shell fish	4,627	4,661	8,359	8,577
Total	294,686	283,524	571,285	478,701
Shell Fish:—	No.	No.	No.	No.
Crabs	178,003	12,685	375,958	96,993
Lobsters	35,773	29,282	88,325	54,501
Oysters	32,068	62,600	71,168	90,500
Clams	Cwts.	Cwts.	Cwts.	Cwts.
Mussels	2,846	2,488	5,609	4,993
Other shell fish	30,174	21,828	59,074	45,179
	6,060	4,945	11,082	7,902
VALUE.				
	£	£	£	£
Soles (Lemon Soles)	1,898	1,662	4,517	4,290
Turbot	805	572	1,624	1,322
Cod	21,602	18,338	39,413	37,816
Eel	1,212	1,747	1,762	2,534
Flounder, Plaice, Brill	12,905	5,643	25,971	13,258
Haddock	40,356	43,669	88,630	86,548
Halibut	1,727	1,394	2,334	2,238
Herrings	9,041	10,827	17,908	21,922
Ling	2,666	5,995	4,325	8,015
Mackerel	32	22	34	24
Saith (Coal Fish)	740	1,063	1,372	1,919
Skate	1,913	2,203	3,265	3,032
Sparling	82	11	180	22
Sprats	16	49	1,755	80
Torsk (Tusk)	29	32	43	46
Whiting	1,666	1,257	3,445	2,490
Fish not separately distinguished, except shell fish	921	1,015	2,059	2,014
Total	97,611	95,529	198,637	187,571
Shell Fish:—				
Crabs	715	62	1,411	406
Lobsters	1,570	1,385	3,829	2,567
Oysters	135	315	293	444
Clams	396	344	779	693
Mussels	2,076	1,372	4,302	2,621
Other shell fish	1,370	1,040	2,410	1,647
Total	6,262	4,518	13,024	8,383
Total value of fish landed	103,873	100,047	211,661	195,954

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month and Two Months ended 29th February 1896, compared with the corresponding Periods of the Year 1895.

	February		Two Months ended February	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles	95	102	163	214
Turbot	60	57	125	124
Total prime fish	155	159	288	338
Cod	7,511	7,307	12,903	11,416
Haddock	2,861	3,595	7,575	6,102
Hake	164	309	327	695
Herrings	673	769	1,536	3,754
Ling	3,291	4,590	4,983	6,412
Mackerel	45	768	375	8,484
Sprats	—	—	—	—
Whiting	1,346	1,024	2,906	2,004
Fish not separately distinguished, except shell fish	3,001	2,484	5,436	4,372
Total	19,047	21,205	36,329	43,574
Shell Fish :—	No.	No.	No.	No.
Crabs	657	1,542	2,914	3,642
Lobsters	4,467	2,475	8,585	4,332
Oysters	93,950	12,800	135,350	77,850
Other shell fish	Cwts. 3,250	Cwts. 1,500	Cwts. 4,832	Cwts. 2,427
VALUE.				
	£	£	£	£
Soles	455	406	742	902
Turbot	237	245	510	449
Total prime fish	692	621	1,252	1,351
Cod	3,132	2,915	5,476	4,919
Haddock	1,887	2,319	4,544	3,953
Hake	92	165	183	343
Herrings	131	103	270	697
Ling	1,182	1,599	1,814	2,136
Mackerel	25	563	166	3,713
Sprats	—	—	—	—
Whiting	734	644	1,509	1,142
Fish not separately distinguished, except shell fish	1,460	931	2,868	1,834
Total	9,335	9,860	18,082	20,088
Shell Fish :—				
Crabs	5	11	20	24
Lobsters	136	88	301	163
Oysters	188	38	272	124
Other shell fish	489	216	772	375
Total	818	353	1,365	686
Total value of fish landed	10,153	10,213	19,447	20,774

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.—COTTON RETURNS.—FEBRUARY 1896.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month and Two Months ended 29th February 1896, compared with the corresponding Months of the Year 1895.

	Month of February		Two Months ended February	
	1896.	1895.	1896.	1895.
IMPORTS.				
	No.	No.	No.	No.
American - -	302,943	503,245	596,744	1,178,818
Brazilian - -	1,484	162	11,866	162
East Indian - -	15,226	703	24,486	2,003
Egyptian - -	40,227	45,079	106,910	97,655
Miscellaneous - -	5,757	4,768	7,271	11,021
Total - -	365,637	553,957	747,277	1,289,661
EXPORTS.				
	No.	No.	No.	No.
American - -	10,218	13,726	27,601	40,681
Brazilian - -	786	751	1,550	1,004
East Indian - -	5,557	1,751	9,508	4,076
Egyptian - -	10,834	7,350	28,150	20,462
Miscellaneous - -	2,228	2,336	5,392	4,922
Total - -	29,623	25,914	72,201	71,145
FORWARDED from PORTS to INLAND TOWNS.				
	No.	No.	No.	No.
American - -	212,849	211,692	*458,413	473,748
Brazilian - -	5,870	716	12,876	1,888
East Indian - -	3,371	1,109	6,142	3,560
Egyptian - -	31,598	24,982	*71,182	58,286
Miscellaneous - -	10,023	12,856	20,522	25,576
Total - -	263,711	251,355	569,135	563,058
FORWARDED from INLAND TOWNS to PORTS.				
	No.	No.	No.	No.
American - -	334	270	433	270
Brazilian - -	—	—	7	—
East Indian - -	1	—	4	—
Egyptian - -	49	10	49	10
Miscellaneous - -	46	117	140	128
Total - -	430	397	633	408

* Corrected figures.

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note.—Rouble = 2s. ; Franc = 9 $\frac{6}{10}$ d. ; Milreis = 4s. 6d. ; Lira = 9 $\frac{6}{10}$ d. ; Gulden = 1s. 8d. ; Drachma = 9 $\frac{6}{10}$ d. ; Lew = 9 $\frac{6}{10}$ d. ; L. Egyptian = 1l. 0s. 6 $\frac{1}{4}$ d. ; Dollar = 4s. 2d.

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe	Nov.	Roubles 39,498,000 (1896)	47,464,000 (1895)	453,536,000 (1896)	472,549,000 (1895)
France	Jan.	Frs. 325,836,000 (1895)	266,689,000 (1894)	—	—
Portugal	July	Milreis 3,648,000	2,909,000	23,298,000	22,480,000
Italy†	Dec.	Lire 111,647,000 (1896)	121,871,000 (1895)	1,187,258,000 (1896)	1,094,600,000 (1895)
Austria-Hungary	Jan.	Gulden 56,744,000 (1895)	57,271,000 (1894)	—	—
Greece	Nov.	Drchms. 10,781,000	11,848,000	97,378,000	99,371,000
Bulgaria	Nov.	Lew 6,332,000	8,754,000	63,120,000	89,889,000
Egypt	Sept.	L. Egypt 623,000	718,000	5,647,000	6,621,000
United States	Dec.	Dollars 62,190,000 (1896)	62,135,000 (1895)	801,663,000 (1896)	676,313,000 (1895)
	Jan.	„ 64,603,000 (1895)	67,548,000 (1894)	—	—
Mexico*	Sept.	„ 2,816,000	2,439,000	8,321,000	7,170,000
	Oct.	„ 3,510,000	2,989,000	11,831,000	10,159,000
British India†	Dec.	Rupees 5,62,94,801	5,79,59,778	50,65,00,955	52,98,92,796

II.—EXPORTS.

Russia in Europe	Nov.	Roubles 55,114,000 (1896)	58,846,000 (1895)	641,144,000 (1896)	610,793,000 (1895)
France	Jan.	Frs. 262,025,000 (1895)	248,569,000 (1894)	—	—
Portugal	July	Milreis 2,506,000	2,228,000	15,224,000	13,512,000
Italy†	Dec.	Lire 114,219,000 (1896)	99,008,000 (1895)	1,038,278,000 (1896)	1,032,799,000 (1895)
Austria-Hungary	Jan.	Gulden 52,433,000 (1895)	55,636,000 (1894)	—	—
Greece	Nov.	Drchms. 7,443,000	5,598,000	64,336,000	68,925,000
Bulgaria	Nov.	Lew 11,370,000	6,309,000	72,488,000	65,487,000
Egypt	Sept.	L. Egypt 374,000	406,000	6,738,000	8,164,000
United States	Dec.	Dollars 92,531,000 (1896)	84,877,000 (1895)	824,862,000 (1896)	825,102,000 (1895)
	Jan.	„ 87,108,000 (1895)	81,230,000 (1894)	—	—
Mexico*	Sept.	„ 7,364,000	6,288,000	21,745,000	20,236,000
	Oct.	„ 9,720,000	6,549,000	31,465,000	26,785,000
British India†	Dec.	Rupees 10,15,21,526	8,61,73,365	80,59,62,821	78,37,29,728

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the “special” imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are “general.” “Special” means, in the case of imports, imports for home consumption ; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

‡ The figures for Italy for December 1895, and also the aggregate, are based on the official valuation for 1895 ; the particulars published for the previous months of 1895 having been based on the valuation for 1894.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Nine Months ended 31st December		Nine Months ended 31st December	
	1895.	1894.	1895.	1894.
	R.	R.	R.	R.
I.—Animals—living - -	28,63,664	24,76,417	11,11,364	9,17,461
II.—Articles of food and drink -	6,35,08,741	5,84,50,639	21,36,27,083	19,92,30,747
III.—Metals and manufactures of—				
A.—Hardware and cutlery (including plated-ware)	1,04,22,623	94,67,240	1,33,209	78,795
B.—Metals - -	4,50,90,144	3,38,27,986	6,82,415	5,50,844
C.—Machinery and mill-work	2,15,44,811	1,73,63,287	790	2,435
D.—Railway plant and rolling-stock (other than Government stores) -	1,21,63,187	1,25,12,711	—	—
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	1,55,27,069	1,32,85,301	10,19,49,818	10,04,51,367
V.—Oils - - - -	2,42,78,981	1,38,64,447	55,21,412	53,95,239
VI.—Raw materials and unmanufactured articles - -	3,73,63,643	3,32,87,550	29,62,55,018	30,72,82,500
VII.—Articles manufactured and partly manufactured—				
A.—Yarns and textile fabrics	21,48,10,156	27,53,71,579	10,13,07,483	8,90,20,761
B.—Apparel - - -	1,20,84,157	1,09,27,753	15,22,052	12,49,419
C.—Other articles factured - -	4,68,41,909	4,00,67,886	5,05,33,340	4,21,05,031
TOTAL - -	50,65,00,955	52,08,92,796	77,26,43,984	74,62,84,599

Exports of Indian Produce and Manufacture only.

XXII.—LIST OF DIPLOMATIC AND CONSULAR
REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1896, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.
1657	Réunion - - - - -	1 <i>d.</i>
1658	Tokio - - - - -	1½ <i>d.</i>
1659	Maranham - - - - -	1 <i>d.</i>
1660	Copenhagen - - - - -	1 <i>d.</i>
1661	Berlin - - - - -	1½ <i>d.</i>
1662	Tehran - - - - -	2½ <i>d.</i>
1663	Salonica - - - - -	1½ <i>d.</i>
1664	Manila - - - - -	½ <i>d.</i>

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources :—

No.	—	Price.
387	<i>Portugal</i> .—Report on the East Coast Route to Mashonaland -	1 <i>d.</i>
388	<i>Austria-Hungary</i> .—Report on the Iron Industry in Styria and the Betler Ironworks in Hungary - - -	5 <i>d.</i>

XXIII.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
27	Mauritius	1890	85	Hong Kong	1892
28	Gambia	"	86	St. Lucia	"
29	Hong Kong	"	87	Falkland Islands	"
30	Ceylon	1889	88	Gold Coast	"
31	Lagos	"	89	Basutoland	1892-93
32	Lagos	1890	90	Ceylon	1892
33	Ceylon	"	91	Zululand	"
34	Newfoundland	"	92	Mauritius	"
35	Jamaica	1889-91	93	Labuan	"
36	Victoria	1890	94	British Honduras	"
37	British New Guinea	1890-91	95	Lagos	"
38	St. Vincent	1891	96	Sierra Leone	"
39	Bermuda	"	97	Fiji	"
40	Seychelles and Rodrigues	1889 and 1890	98	Rodrigues	"
41	Gambia	1891	99	St. Helena	"
42	Barbados	"	100	British Bechuanaland	1892-93
43	Turks Island	"	101	British Guiana	"
44	Bahamas	"	102	Newfoundland	1892
45	Fiji	1890	103	Jamaica	1892-93
46	Seychelles	1891	104	Victoria	1892
47	British Bechuanaland	1890-92	105	Bermuda	1893
48	Malta	1890 and 1891	106	Gambia	"
49	Gibraltar	"	107	Hong Kong	"
50	Falkland Islands	"	108	St. Vincent	"
51	Leeward Islands	"	109	Gibraltar	"
52	Grenada	1890 and 1891	110	Bahamas	"
53	St. Lucia	"	111	Turks and Caicos Islands	"
54	Labuan	"	112	Leeward Islands	"
55	British Guiana	"	113	Barbados	"
56	Zululand	"	114	Malta	"
57	Mauritius	"	115	Zululand	"
58	Lagos	"	116	British Honduras	"
59	Straits Settlements	"	117	Trinidad and Tobago	"
60	Ceylon	"	118	Falkland Islands	"
61	Natal	1891-92	119	Rodrigues	"
62	Basutoland	"	120	Mauritius	"
63	St. Helena	1891	121	Straits Settlements	"
64	Sierra Leone	"	122	Labuan	"
65	Jamaica	1891-92	123	Basutoland	1893-94
66	Trinidad and Tobago	1891	124	St. Lucia	1893
67	Newfoundland	"	125	St. Helena	"
68	British New Guinea	1891-92	126	Ceylon	"
69	Victoria	1891	127	Fiji	"
70	Rodrigues	"	128	Grenada	"
71	Bermuda	1892	129	Sierra Leone	"
72	Fiji	1891	130	British Bechuanaland	1893-94
73	British Honduras	"	131	British New Guinea	1892-93 and 1893-94
74	Turks and Caicos Islands	1890	132	Lagos	1893
75	Gibraltar	1892	133	British Guiana	1893-94
76	Leeward Islands	"	134	Jamaica	"
77	Bahamas	"	135	Newfoundland	1893
78	Trinidad and Tobago	"	136	Gold Coast	"
79	Malta	"	137	Zululand	1894
80	Gambia	"	138	Bermuda	"
81	Straits Settlements	"	139	Bahamas	"
82	Grenada	"	140	Barbados	"
83	Barbados	"	141	Turks and Caicos Islands	"
84	St. Vincent	"	142	Malta	"
			143	Gambia	"

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast	Economic Agriculture.
2	Zululand	Forests.
3	Sierra Leone	Geology and Botany.
4	Canada	Advantages to Emigrants.

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OF
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AND
MISCELLANEOUS COMMERCIAL INFORMATION.

Vol. XX.]

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[No. 117.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

There has been an improvement in the general state of employment as compared both with last month and with March 1895. The improvement affects most of the important branches of industry, except mining, which shows a very slight decline.

In the 108 trade unions, with an aggregate membership of 415,731, making returns, 14,717 (or 3·5 per cent.) are reported as unemployed at the end of March, compared with 3·8 per cent. in February, and with 6·5 per cent. in the 83 unions, with a membership of 387,907, from which returns were received for March 1895.

Coal Mining.—There was a slight falling off in employment during March as compared with a month and a year ago. In the second fortnight of the month there was, however, a recovery in the great export districts. The average number of days worked by 1,061 pits, at which 296,661 workpeople were employed, was 4·81 per week in March, as compared with 4·91 in February and 4·93 in March 1895.

Iron Mining.—Employment in this industry has fallen off slightly since the previous month, but continues better than a year ago. At the mines included in the returns an average of 5·77 days per week was worked, as compared with 5·85 in February and 5·59 in March 1895. The total number employed at the mines was 13,317, or about 500 more than a year ago. Of these 68·6 per cent. worked full time.

In the *Pig Iron* industry employment is better than in February, and considerably better than a year ago. At the end of March the ironmasters making returns had 339 furnaces in blast, being 3 more than at the end of February, and 40 more than at the corresponding date last year. The number of workpeople employed at these furnaces was 21,522, an increase of 78 over the previous month, and of 1,559 over March 1895.

Employment in *Steelworks* maintains its improvement as compared with a year ago, the number of workpeople employed in 107 works being 32,193 at the end of March, or 11·5 per cent. more than at the end of March 1895.

At 85 *Puddling Furnaces and Rolling Mills* the number employed at the end of March was 16,767 or 4·3 per cent. more than a year ago.

The *Tin-plate Trade* has been much disturbed, and fewer mills are working than in February. The number of mills in operation in the 89 works included in the returns was 286 at the end of March, as compared with 339 at the end of February. Only 40 of the 89 work were giving full employment.

A further improvement has taken place in employment in the *Engineering* and kindred trades, and the percentage of unemployed union members has fallen from 3·4 in February to 2·9 at the end of March, compared with 7·8 per cent. in March 1895.

The improvement in employment in the *Shipbuilding* trades has also been well maintained, the percentage of unemployed union members at the close of the month being 7·2, compared with 7·8 in February and 12·4 in March of last year.

Employment in the *Building* trades continues good, the percentage of unemployed in unions making returns being 2·6 compared with 2·5 in February and 4·9 in March 1895.

The *Furnishing* trades are busy, the percentage of unemployed union members being only 1·0, compared with 2·6 in February and 4·9 per cent. in March 1895.

Employment in the *Printing* and *Bookbinding* trades continues steady, the percentage of unemployed members being 4·6, compared with 4·9 in February and also in March 1895.

In the *Paper* trade employment has fallen off, and the percentage of unemployed union members at the end of March was 4·7, compared with 2·8 in February.

In the *Glass* trade 14 per cent. of union members are reported as unemployed.

In the *Leather* trades employment has slightly improved and the percentage of unemployed union members is 5·9, compared with 6·2 in February and 5·7 in March 1895.

Employment in the *Boot and Shoe* trade is only moderate in most of the principal centres. In Leeds, however, it is brisk.

Employment in the ready-made branch of the *Tailoring* trade is very good, some overtime being worked, and there is the usual seasonal improvement in the bespoke branch.

Employment for *Cotton* spinners has improved: weavers are better employed in some districts, but are slacker in others. The *Woollen* trade continues very busy, but the *Worsted* trade is not quite so well employed. Employment in the *Hosiery* trade generally is good, in the *Silk* trade it is fair.

As regards the employment of women in the *Textile* trades, information respecting 490 mills, employing about 90,000 women and girls, shows a slight decline, 86 per cent. being in mills

giving full employment during the month, compared with 88 per cent. among those for whom returns were received for February. The slight decline was most marked in the worsted trade.

Trade Disputes.—The number of fresh disputes occurring in March was 95, as compared with 78 in the month of February last and 49 in March 1895. Thirty-four disputes took place in the mining industry, 16 in the textile trades, 14 each in the building and metal trades, 7 in the engineering and shipbuilding trades, 2 in the clothing trades, and 8 in the miscellaneous group of industries. In 78 of the disputes, 19,407 workpeople were involved. Seventy-two new and 18 old disputes, involving altogether about 22,000 workpeople, were settled in March. Of these disputes, 34, involving about 10,000 persons, resulted in favour of the workpeople; 41, involving about 8,000 persons, in favour of the employers; and 8, involving 2,860 persons, in a compromise; the results of the remaining 7 being indefinite or not known.

Changes in Rates of Wages and Hours of Labour.

The changes in rates of wages reported during March affected about 128,000 workpeople, of which number about 56,000 received increases and 72,000 sustained decreases. Among those affected by increases were about 47,000 workpeople engaged in the engineering, metal, and shipbuilding industries, and about 5,500 building operatives. The decreases reported were mainly confined to coal miners and tin-plate workers. In Scotland 67,000 miners had their wages reduced, in most cases by 6*d.* per day; and in South Wales and Monmouthshire about 4,900 workpeople were affected by further reductions, in the tin-plate trade.

During the month about 1,200 persons were reported to have had their hours of labour reduced.

MERCHANDISE MARKS ACT.

Further Discretionary Powers given to Officers.

A copy of the following General Order ($\frac{1896}{1889}$) has been received at the Board of Trade from the Commissioners of Her Majesty's Customs.

In addition to the powers conferred upon them by General Order $\frac{7}{1889}$, the Board now give collectors and surveyor at the out-ports, and inspectors and surveyors in London, discretionary powers in the following cases:—

(1.) They may deliver coverings, such as capsules, paper bags, &c., marked with indications of British origin, such as the Royal arms, trade descriptions in the English language, &c., when they are satisfied that the coverings are to be used in connexion with British goods, or with goods of a kind not produced in the United Kingdom, and that the indications on the coverings have reference to the goods with which the coverings are to be used, and not to the coverings themselves.

Coverings marked with the names of firms in the United Kingdom, with whose goods the coverings are to be used, and

consigned directly to such firms, may be delivered. If not so consigned, the written consent of the firms whose names they bear must be produced before delivery is allowed.

(2.) Immediate reports of detentions of goods under the Merchandise Marks Acts, required by General Order $\frac{39}{1888}$, may be dispensed with when such detentions are made in consequence of the absence of evidence that the marks on the goods are correct, provided that the officers have reason to believe that such evidence will be forthcoming within a reasonable time. On production of the evidence, the officers, if satisfied therewith, will deliver the goods. This direction will include British returned goods bearing marks which would be open to objection on goods of foreign origin; also goods bearing marks indicative of origin in some country other than that from which the goods are imported, when such marks are believed to be authentic.

(3.) Goods bearing a mark registered in this department under paragraphs 12—15 of General Order $\frac{99}{1887}$ may be delivered by the officers without reference to the Board on receipt by them of the authority of the proprietor of the mark, or of his duly appointed agent, provided the mark be properly qualified.

OFFICIAL ENQUIRY UNDER THE BOILER EXPLOSIONS ACT.

A formal investigation was held at Stockport on the 31st ultimo into the circumstances attending the explosion of a boiler at the Hill Top Farm, Woodford, near Stockport, on February 24 last.

The boiler, one of the Cornish type, was purchased second-hand about 31 years ago; it was worked for 15 years and sold to the lessee of the Hill Top Farm. It was then fitted with new ends and a new flue, and it was set up in brickwork outside the farm buildings, and consequently exposed to the weather.

There is little doubt that the farmer was ignorant of boiler management. About 12 years ago the crown of the furnace came down about $2\frac{1}{2}$ inches from overheating, but he continued to work the boiler; and at a much later period, as the safety-valve leaked, a second weight was put on the lever, increasing the load from 31 to 52 lbs. per square inch. The boiler was never examined since it was repaired 16 years ago, and as it was exposed to the weather, the shell gradually corroded.

In December last, the farmer told his son that he could go on working the boiler in the same manner as he had worked it, and on the 21st of that month the farmer died, leaving his property to a trustee, to be realised and divided between his eight children.

The son then worked the farm on behalf of the trustee, and in the interests of his brothers and sisters. He only used the boiler once, and when steam was got up on the second occasion the boiler exploded. No one was injured.

The Court found that the explosion was due to the boiler having become so wasted by external corrosion as to be no longer able to withstand the pressure to which it was subjected at the

time of the explosion, the plate in the vicinity of the fracture being reduced to the thickness of paper. That whilst in possession of the farmer the boiler had been grossly neglected. The son had not sufficient time to overhaul the plant on the farm after he took possession of it.

The Court condemned in the strongest terms the manner in which this boiler had been used, but in all the circumstances, they made no order as to costs.

REGISTRATION OF TRADE MARKS IN EGYPT.

The Board of Trade have received information through the Foreign Office that it is possible to register trade marks at the Registry of the Mixed Tribunals in Egypt. A certain advantage is found to accrue from such registration in the event of legal proceedings having to be taken for infringement.

The Board of Trade desire, therefore, to press on the attention of those of Her Majesty's subjects who may desire or intend to engage in trade with Egypt, the necessity of promptly securing registration of their trade marks in Alexandria.

GOVERNMENT CONTRACTS

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply—

1. Brass boiler tubes.
2. Mild steel boiler tubes.
3. Steel rails.
4. Fish-plates.
5. Iron dog-spikes.
6. Axle-box brasses.
7. India-rubber blocks and cushions, and
200 feet spans.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that office by *two o'clock p.m.* on Tuesday the 21st April 1896, after which time no tender will be received.

LOAN CONTRACT FOR PORTO RICO WATERWORKS.

Her Majesty's Secretary of State for Foreign Affairs has received from Her Majesty's Consul at Porto Rico a despatch, reporting that the Municipality of St. John the Baptist of Porto Rico has put up to public tender the contract for a loan destined for the Waterworks of Porto Rico, for the sum of 400,000 dols. money current. Further particulars of this loan may be seen on application to the Commercial Department, Foreign Office, between the hours of 11 and 6.

TENDERS FOR THE EGYPTIAN GOVERNMENT.

Her Majesty's Secretary of State for Foreign Affairs has received from Her Majesty's Agent and Consul-General at Cairo a copy of the Egyptian Official Gazette of February 29 last, in which tenders are invited by the Daira Sanieh administration for olive oil, rangoon oil, linseed oil, tallow, grease, and other commodities before 11 a.m. on April 20, 1896; and also for 20 waggons and various other articles such as buckets, drain pipes, shovels, &c. before 11 a.m. on April 30, 1896.

The Gazette may be seen on application to the Commercial Department of the Foreign Office between the hours of 11 a.m. and 6 p.m.

TENDERS FOR WORKS AT LOURENÇO MARQUES.

The Secretary of State for Foreign Affairs has received from Her Majesty's Consul at Lourenço Marques information to the effect that the Improvements Commission of that port will, up to the end of April next, receive plans and designs for a screw-pile lighthouse for the Cockburn Bank, and for a tug-boat for the pilot service. The range of the light must be 10 miles, and the lighthouse must contain accommodation for the keepers. The tug-boat must be of the compound system, 100 feet in length, 8 feet draught at stern, 500 indicated horse-power, and not less than 10 miles an hour speed. The boat must accommodate captain, engineers, crew of eight, and two life boats.

Further particulars can be obtained of the Commercial Department, Foreign Office, S.W., between the hours of 11 and 6.

INSPECTORS OF WEIGHTS AND MEASURES.

The following Inspectors of Weights and Measures have, during the month of March 1896, passed the examination provided for in the 11th Section of the Weights and Measures Act, 1889:—

Beirne, Patrick	Lockhart, Richard	} (Royal Irish Constabulary) Ireland.
Brosnan, Patrick	McGouran, James	
Connell, John	McGurk, William	
Connor, David	McMahon, James	
Farrell, William	O'Donnell, Henry	
Frain, Patrick	O'Sullivan, Timothy	
Healy, Francis	Price, Henry	
Horgan, Michael	Robinson, Henry W.	
Linnin, Michael	Sullivan, John	
Laughran, Hugh	Wilson, William H.	
Collett, William Rickford	-	} Pembroke Township (Ireland).

II.—THE FOREIGN TRADE OF THE UNITED KINGDOM.*

The total value of the imports into the United Kingdom from foreign countries and British possessions during the month ended 31st March 1896 was 38,344,750*l.*, as compared with 35,959,890*l.* for the corresponding period of 1895, or an increase of 2,384,860*l.*, equal to 6·63 per cent.; and the exports of British and Irish produce and manufactures were valued at 20,422,419*l.*, as compared with 18,523,030*l.*, an increase of 1,899,389*l.*, or 10·26 per cent.

The exports of foreign and colonial merchandise amounted in value to 4,623,326*l.*, as against 4,846,599*l.* for March 1895, a decrease of 223,273*l.*, or 4·61 per cent. This is due chiefly to smaller shipments of indigo, jute, and leather.

The classes of goods imported which show a higher value in March 1896, as compared with March 1895, are: articles of food and drink (duty free), which show an improvement of 1,441,005*l.*; manufactured articles, 1,360,199*l.*; animals, living (for food), 325,286*l.*; metals, 201,586*l.*; raw materials for sundry industries and manufactures, 199,058*l.*; tobacco (dutiable), 121,358*l.*; chemicals, dyestuffs, and tanning substances, 68,441*l.*; and parcel post, 8,865*l.*

In the class of manufactured articles the chief increases are found in silk broad stuffs (378,233*l.*), leather (165,655*l.*), iron manufactures, unenumerated (74,154*l.*), silk or satin ribbons (73,835*l.*), leather gloves (67,082*l.*), woollen yarn for weaving (55,439*l.*), and woollen cloths (47,545*l.*). In this class there is a decrease of 10,018*l.* in the value of linen yarns. In Class II., articles of food and drink (duty free), the improvement is seen principally in sugar, refined, and candy (360,280*l.*), Indian corn or maize (306,442*l.*), butter (269,992*l.*), sugar, unrefined (262,635*l.*), wheat meal and flour (261,348*l.*), and fish, cured or salted (124,670*l.*). Decreases are found in the value of bacon (182,982*l.*), and lard (183,611*l.*). In animals, living (for food), there is an increase of 320,636*l.* in the value of oxen and bulls imported.

Other articles showing noteworthy increases are cotton, raw (548,647*l.*), caoutchouc (185,184*l.*), and iron ore (130,908*l.*).

The value of the imports for March 1896 of raw materials for textile manufactures, in spite of the increase of 548,647*l.* in the value of cotton, raw, imported, and of 82,290*l.* and 58,906*l.* in flax and hemp respectively, shows a total decrease of 955,660*l.*, as compared with the amount for March 1895, which must be chiefly attributed to the fall in the value of the imports of sheep and lambs' wool and of jute, 1,329,587*l.* and 328,878*l.* respectively less than last year.

* See also Statistical Tables, p. 498.

With regard to the exports of British and Irish produce and manufactures, improvements are seen in every class with the exception of raw materials and articles of food and drink, which have declined 103,595*l.* and 12,346*l.* respectively. Yarns and textile fabrics have improved 837,645*l.*; metals and articles manufactured therefrom, 455,320*l.*, of which iron, wrought and unwrought, represents 340,745*l.*; machinery and mill-work, 219,340*l.*, and in this class the amount of mining machinery sent to South Africa and Australasia is particularly noticeable; apparel and articles of personal use, 160,329*l.* (these articles were much in demand for South Africa and Australasia); chemicals and chemical and medicinal preparations, 128,305*l.*; other manufactured articles, 163,615*l.*; parcel post, 38,171*l.*; and animals, living, 12,605*l.*

The most important decrease in the exports of raw materials was in coal and patent fuel (73,727*l.*), and in the class of articles of food and drink, fish of all descriptions (30,230*l.*).

Taking the trade for the first quarter of the present year, ending 31st March 1896, it is seen that the net increase in the value of imports, as compared with the corresponding period of 1895, amounts to 11,457,482*l.*, of which articles of food and drink represent 4,817,226*l.*, manufactured articles, 2,919,878*l.*; raw materials for sundry industries and manufactures, 1,566,673*l.*; animals, living (for food), 975,107*l.*; and metals, 649,471*l.*

There is a decrease of 1,013,085*l.* in the value of the imports of raw materials for textile manufactures, and this is almost entirely due to the decline in the imports of wool.

The exports of British and Irish produce for the three months ending 31st March 1896 show a total increase of 8,512,682*l.* over the amount for the corresponding period of 1895, the increase being in all classes. Yarns and textile fabrics have improved 3,084,497*l.*; metals and articles manufactured therefrom (except machinery), 1,758,555*l.*; machinery and mill-work, 685,397*l.*; apparel and articles of personal use, 577,468*l.*; chemicals and chemical and medicinal preparations, 331,408*l.*; raw materials, 258,151*l.*; articles of food and drink, 264,873*l.*; and other articles, either manufactured or partly manufactured, 1,410,406*l.*

The exports of foreign and colonial merchandise for the three months ended 31st March 1896 amounted in value to 15,087,265*l.*, as against 13,141,681*l.* for March 1895, or an increase of 1,945,584*l.*

III.—SEA FISHERIES OF THE UNITED KINGDOM.

A publication* has recently been issued by the Board of Trade containing statistical tables and memorandum relating to the sea fisheries of the United Kingdom, including a return of the quantity of fish conveyed inland by railway from each of the principal ports of England and Wales, Scotland, and Ireland, during each of the years from 1890 to 1895. The tables give the results of the collection of fishery statistics for England and Wales during the year 1895, the collection itself having been commenced in the spring of 1885.

The chief facts brought out by the return as to fish landed on the coasts of England and Wales during the year 1895 are as follows:—The total quantity of fish landed, exclusive of shell-fish, amounted to 7,263,000 cwts., or, roughly speaking, to 363,000 tons, of the value of 5,129,000*l.*; which, with the addition of the value of shell-fish, viz., 309,000*l.*, makes a total value for the year of 5,438,000*l.* The corresponding values for the years 1893 and 1894 having been 5,171,000*l.* and 5,291,000*l.* respectively, it would appear that there is a slight increase in the value of the fish landed in each year. And here, it should be remarked, that the values throughout the tables, as well as the average prices stated, are in all cases those taken at the time of landing, which are believed generally to be fairly accurate.

For Scotland the returns show a total quantity of 6,104,000 cwts. landed, exclusive of shell-fish, the value being 1,764,000*l.* To this should be added 66,000*l.*, the value of the shell-fish landed, making a total of 1,830,000*l.* The corresponding figures for Ireland show a total of 629,000 cwts. landed and valued at 256,000*l.*, exclusive of shell-fish, the total value, including shell-fish, being 269,000*l.* It should be remarked that in the year 1895, as compared with the year 1894, there is in Scotland a slight decrease in the quantity, and at the same time an increase in the value of the fish landed, whilst in Ireland there is a decrease both in the total quantity and the total value of the fish landed.

The aggregate results for the whole of the United Kingdom show that in the year 1895, 13,996,000 cwts. of fish, exclusive of shell-fish were landed, or approximately 700,000 tons valued at 7,149,000*l.* at the point of landing. If to this the value of the shell-fish, amounting to 388,000*l.*, be added, a total of 7,537,000*l.* is arrived at as the value of the sea fisheries of the United Kingdom at the place of landing in the year 1895, compared with a total value of 7,260,000*l.* in the year 1894.

A comparative table is given showing the quantities and value of the fish landed at each of the principal ports in England and Wales during the last 10 years. This table shows at a glance

* Parliamentary Paper 75, 1896.

the various fluctuations in the trade in fish done at each port. Taking first the East Coast, it will be seen that Grimsby steadily maintains the first place as a fishing port, indeed the amount returned as landed at that place in 1895, viz., 83,000 tons, is in excess of that landed in any previous year. This last observation applies also to Hull, where 46,000 tons of fish were landed during the past year. Some way behind comes Lowestoft with 31,000. The amount landed at London, viz., 52,000 tons, is still larger than that landed at the two last-named places, although it is to be remarked that this total was considerably exceeded by the amounts landed in the earlier years of this collection, viz., 1886 to 1888, when the amount averaged 64,000 tons.

The fluctuations in the amounts of fish landed on the South Coast are in no way remarkable, the amounts throughout being small. As regards the West Coast, the most salient facts are the fluctuations in the amount of fish landed at Neyland, although the amount at the same time landed at Milford keeps fairly steady. Neyland is supplied with fish, chiefly drift net fish (mackerel), coming almost entirely from the Irish coasts, although not landed before in that country. Milford, on the other hand, although it receives also some fish from the Irish coasts, is by no means entirely dependent on such fish for its supply; indeed, by far the larger proportion of the fish landed at Milford is trawl fish caught in the St. George's Channel.

As regards the quantity and value of fish landed in each month of the year, it would appear from a table given that the months of April and October were the most important in the year 1895, whilst in the preceding year, 1894, the most important months were May and November. The fourth quarter of the year was, in 1895, the most important to the fishing industry; in the years 1893 and 1894 it was the second quarter that was most important. In all three years the first quarter of the year was the least important as regards the value of the fish landed.

It appears from tables showing for a series of years the total quantities and values of the fish imported into this country, distinguishing the proportion of such fish retained for home consumption, as well as the exports of fish from this country to foreign parts during the same period, that the value of the fish imported into this country and retained for home consumption now exceeds 2,400,000*l.* a year, having increased from 1,750,000*l.* in 1886 to 2,450,000*l.* in 1895; whilst, on the other hand, the total value of British and Irish fish exported from this country, which, for many years averaged about 1,700,000*l.* a year, has in the year 1895 risen to 2,275,000*l.* Taking these figures in conjunction with the value of the produce of the sea fisheries of the United Kingdom during the last five years, it will be seen that the total value of the fish consumed in the United Kingdom has materially increased during that period, having amounted to 7,712,000*l.* in 1895, as compared with 6,365,000*l.* in 1894.

IV.—SUICIDES IN THE ENGINE-ROOM STAFF OF THE MERCANTILE MARINE.

A memorandum recently issued by the Registrar General of Shipping and Seamen, deals with the question of suicides in the engine-room staff of the mercantile marine.

In this report the Registrar General states that it is only since the beginning of 1892 that the system has been in full working order by which officers at the ports at home and abroad hold a special inquiry into cases of deaths of firemen and trimmers reported to have committed suicide or as missing at sea, and that the memorandum is confined to deaths from above causes of firemen and trimmers which occurred since 1st January 1892, on vessels registered under the Merchant Shipping Act in the British Islands.

An analysis of the reports of the inquiries of the officers shows that 30 firemen and trimmers serving under ordinary agreement, and 19 Asiatic firemen and trimmers serving on vessels trading to the United Kingdom under "Asiatic" agreement (*i.e.*, special agreements terminating in India or China), committed suicide or are supposed to have committed suicide in 1894, as against 27 and 14 respectively in 1893.

The figures also show that but few suicides occur in the home or coasting trade, where the voyages are short and a man feeling unwell can remain on shore.

As regards the foreign-going trade, the suicides occur in no very marked degree in any particular locality, nor can it be said that suicides occur with frequency on a particular vessel or line of vessels. It should be noted, moreover, that no suicides are reported to have occurred in the record-making mail steamers running between the United Kingdom and United States.

It appears that the suicides cannot be attributed to ill-usage either by their superiors or fellow-workmen, the reports in all cases being to the effect that there was no ill-treatment.

The heat of the stokehold and coal bunkers is undoubtedly very great, running in some cases to 120° F., but to give any comparative table on this point would be misleading, as the temperature is not taken upon any system.

It also appears from the reports that the number of deaths occurring either when the man was actually drunk or suffering from delirium tremens or from the effects of a recent drinking bout including those suffering from mental derangement was 12 in each of the years 1894 and 1893; and the conclusion would appear to follow that the prevalence of suicide amongst firemen may to some extent be attributed to the necessity of turning to work in a hot stokehold whilst still suffering from the effects of drink.

The proportion of suicides to number employed is (as is the case among the general population) larger amongst the men over 30 than amongst the number under that age.

With regard to the question as to what are the number of firemen and trimmers employed in the mercantile marine of the United Kingdom amongst whom these suicides occur—when an attempt was made to obtain the numbers employed on the 5th April 1891 on merchant ships registered in the British islands, it was found that on that day there were actually serving as firemen and trimmers (not including Lascars) on these vessels the following numbers :—

In the home trade	-	-	4,207
In the foreign trade	-	-	17,258
In the fishing trade	-	-	399
Yachting	-	-	97
			<hr/>
			21,961
			<hr/>

These figures—allowance being made for variation in tonnage employed—represent approximately the numbers at sea at any one time; and the total strength of the whole body of firemen and trimmers required for the mercantile marine service is obtained by adding a certain proportion to cover the number of men on shore.

The Registrar General calculates that the rate of suicide amongst firemen and trimmers in the mercantile marine is about 1 in 900. Suicide amongst stokers in the Royal Navy is very rare.

The Registrar General concludes his report as follows :—

“There can be little doubt that the figures as regards ‘suicide’ or ‘supposed suicide’ fairly represent the actual condition of things, and that though the rates of suicide I have given, based as they are on estimates, may be criticized, no amount of criticism will obscure the fact that suicide amongst firemen in our mercantile marine service is prevalent to a degree, calling for serious attention.”

The rate of suicide amongst *all* males between the ages of 20 and 55, which practically cover the active life of seamen, appears from the reports of the Registrar General of Births and Deaths to be about 1 in 5,000.

V.—THE EXPLOITATION OF THE MEKONG VALLEY.*

The Anglo-French agreement which has just settled the question of the Upper Mekong, so long undecided as far as France, Great Britain, and Siam are concerned, appears to have a certain economic importance, for it not only fixes the conditions under which Siamese trade can be extended northwards, but also the routes through the densely populated Chinese province of Yunnan which are open to the two great western powers.

At first sight these conditions seem to give France a greater chance of being the first to penetrate commercially into this important part of Southern China. The interesting details of the voyage of Prince Henry of Orleans point out the difficulties of the Brahmapootra side, and a recent despatch mentions the cordial relations existing between the Chinese authorities at Muang-Huo and the French authorities in the valley of the Nam-Oo, caravan exchanges being already frequent.

The little rails of the Lang-Son line may well serve as a model for those of the first railway to be laid down between the lands watered by the Upper Mekong and the Salween of the Chinese Empire. This narrow gauge line, with its diminutive waggons, was planned by Frenchmen, and its opening preceded any serious idea of a line between Burmah and Yang-Tse-Kiang. France, then, appears to have gained the first advantage in that economic struggle which is going on in the regions of the Upper Mekong, and the question is—will she, profiting by her privileges in the diplomatic situation she has created in China, have the first fruits of this vast opening into Yunnan and succeed in diverting some of its wealth into Indo-China? That will depend on the energy to be hereafter displayed, which will not be wanting, to judge by the action of Lieutenant Simon and also of Prince Henry, who has been the first to traverse the shortest and most direct route from China to India—a route searched for in vain by so many Englishmen on both frontiers, and, indeed, at present not practicable for trade.

So far as Siam is concerned, shut up as it henceforth will be in the valley of the Menam, the Anglo-French agreement must be of some importance. This country cannot remain outside the influence of the economic struggle entered into between France and England in the north, which will have the effect of bringing through Siam at least a part of the trade of those northern regions and the *tra los montes* of the Chinese frontier.

In this case it seems likely that the English will have the advantage, as they are already doing a good business with Siam, especially in rice and wood, the exports of the latter

* Based on an article in the "Economiste Français."

product in 1894 having been almost double those of 1893. These exports were, for England, 13,000 tons; Bombay, 17,000 tons; Hong-Kong, 7,000 tons; and Singapore, 2,000 tons. In French Tonquin, however, a demand for Siamese woods has sprung up: in 1894 the imports there amounted to 390 tons, worth 137 francs per ton, and in October 1895 a French official commission was in negotiation at Bangkok for the purchase of 1,300 tons of Siamese wood for Toulon, from which it would appear that the French Admiralty has no prejudice against the hard grains so abundant in Siam and so valued in British trade. France can by this means divert to her advantage a part, at any rate, of the trade which has hitherto gone to England or India.

Siamese rice has generally found its destination in British ports. But in 1894 South America doubled its importations, Rio de Janeiro taking 22,500 tons, as against 11,000 tons only in 1893. There is a tendency, therefore, in the importance of English trade, to decrease in the case of the two chief Siamese products, rice and wood, the first going to Brazil, the second to France. If this double current increases, it will have a great effect on the foreign trade of Siam, as rice represents 70 per cent. of the country's export trade and rice, wood, fish, and pepper represent 83 per cent., according to the statistics for the year 1894.

One difficulty which French trade has to overcome in its relations with Siam rests in the monetary system, or rather in the necessity of falling in with the monetary system of a country where the imports of specie in 1895 amounted to 11,108,225 francs (444,329*l.*) Mexican dollars and Japanese yen are the foundation of this importation of specie by foreign merchants and bankers, and then exchanged by them at a fixed rate for the ticals of the Siamese treasury. This native money is the only medium which the traders in rice will accept. The Japanese yen is not worth quite as much as the Mexican dollar in Siam, although the two pieces contain about the same amount of silver, and have the same value at Singapore and Hong-Kong. The Siamese treasury gives 80 ticals for 48 Mexican dollars or 49 Japanese yen. It was at one time feared that the British dollar, put into circulation at the Straits Settlements, Hong-Kong, and Labuan in 1895 would undergo a like depreciation, but events have not justified this belief. Great Britain, then, has an advantage in having already conformed to the monetary system of Siam.

With regard to the imports into Siam, the trade with Singapore, Hong-Kong, and Bombay, valued respectively at 20,559,500 francs (822,380*l.*), 9,400,000 francs (376,000*l.*), and 5,105,625 francs (204,225*l.*), in 1894 represented 82 per cent. of the total trade of that year, and the same three places absorbed about the same proportion of exports. France, therefore, will have all she can do, not to eliminate, but to diminish in any noticeable degree the English element of the foreign trade of Siam. The

"Messageries Fluviales" have already made their presence felt, as they are developing a traffic between Bangkok and Saigon in passengers especially.

A British company is exploiting the sapphire and ruby mines of the Chantaboon region, while the Ottoman Bank and a French syndicate have taken in hand—the former, the gold deposits of Kabia, and the latter, those of Wattana. The first results of these three mining enterprises are not yet known.

The foreign trade of Siam ought to be remunerative enough, as the chief houses occupied in it are always increasing their staff and extending their operations.

With such a field of operations opened up in the north of this country by the Anglo-French agreement, this part of the Far East should witness great changes in the near future.

VI.—RAISING OF LIVE STOCK IN RUSSIA.*

Of the 60 millions of horses, estimated by the Agronomic Department of Washington, U.S.A., to be the total equine population of the globe, 30 millions, or about one-half, are to be found in the Russian Empire. The greater part (about 86 per cent.) belong to the peasants, and for these there appears to be a feeling in the country that something will have to be done, for the situation at present is a most serious one.

The economic crisis has impoverished the Russian rural population; village horses are becoming daily more and more rare and the race is degenerating. At present 30 per cent. of the peasant establishments are without horses, and the latest animal census is not at all reassuring, for it appears that in 422 districts of 41 governments of Russia only 12,675,657 horses were registered, which means for the last six years an increase of 619,757 only.

It is evident that assistance should be granted to the rural population in this question of horse-breeding, in order to increase the number and improve the breed; and this might be effected by giving an impetus to private studs, by means of a State supply of stallions.

At present only 25 per cent. of the stallions in the Government studs are used for the improvement of working horses, instead of making them the chief object of an institution, the wealth of which may be seen from the fact that the stables of Bélovodsk and Khrenovoe alone have 59,680 dessiatines (161,136 acres) of ground, which cannot be worth less than 6,660,000 roubles.

In a country where there are so many lovers of sport, race-horses are never wanting, and the question of a supply of horses for the army need not be considered either. According to estimates in time of peace, the Russian army possesses 130,000 horses, Germany only requires 94,000, and the Austrian and French armies 60,000 and 140,000 respectively. Germany has need of but 9,180 horses annually, or 0.24 per cent. of the total number of horses in the Empire, which last year amounted to 3,836,256. Austria purchases annually for the army 6,422 horses (0.18 per cent.); France, 18,000 (0.59 per cent.); in Russia, with a total number of 30 million horses, the annual amount required to be purchased is only 9,000, or 0.03 per cent. of the whole.

The "St. Petersburg Gazette" (Russian) concludes by pointing out that the State studs will have enough to do to produce the number of stallions necessary, and expresses a hope that the Ministry of Agriculture will undertake the direction of private studs.

* Based on an article in the "Gazette (Russe) de St. Petersbourg," republished in the "Journal de St. Petersbourg."

An Archangel correspondent of the same paper speaks of the raising of livestock in the Petchora region. Owing to the abundance of meadow-land there is no village, no matter how small it may be, where there are not several households possessing 10 or 12 head of horned cattle, five horses, and half a score of sheep; in that district households without any livestock are the rare exceptions. It may be asserted that, on an average, every peasant possesses two or three horses, three or four cows, and from five to seven sheep. Swine-breeding is almost unknown.

Latterly, however, the raising of livestock has been neglected, due to the fact that the meadow-land is not worked enough.

At the time of the haymaking nearly all the population goes fishing and the haymaking is left to the children and the old people. It can be understood that under such conditions forage is sometimes scarce in winter, a state of affairs which leads to the loss of part of the stock and the degeneration of the rest.

At one time, Petchora horses were celebrated in the north of the Empire; on the feast of St. Nicholas, hundreds of these animals were brought to the Pinega fair, where they were snapped up at once. Now the race has become smaller and the demand has decreased. The causes of this may be looked for not only in the want of forage, but also in the bad habits of the peasants, who work the animals from the time they are two years old, without considering the hindrance to their development and the consequent degeneration of the race. And yet a horse of two or three years old will fetch from 30 to 75 roubles, a good price for a beast which has cost nothing to rear.

It is the same as regards the meat of horned cattle, which fetches at Pinega (frozen or salted) $1\frac{1}{2}$ roubles to 2 roubles per pound (36 lbs. avoirdupois).

One special branch of the agricultural industry of the country is the raising of reindeer in the *toundras* of Bolschézémelsk and Timansk, in the Petchora region. A century ago, these animals formed the patrimony of the nomadic Samoyès; these now look after the flocks, the property of a few Zyrianes, especially of the village of Ijemtsy, who are very rich, some owning as many as 5,000 reindeer.

It is not easy to establish with accuracy the number of these animals in the country, the owners themselves being ignorant of the exact figure; but according to approximate calculations made by traders, they should amount to about 600,000. The fifth part of these are slaughtered annually, furnishing about 120,000 skins, which fetch 2 roubles each, the meat selling at about 1 rouble per pound (36 lbs). There are other profits to be made out of the reindeer, so that it may be said that they represent in the Petchora region an annual revenue of 500,000 roubles, which, however, does not benefit the people at all, but only serves to still further enrich some of the wealthy Zyrianes.

VII.—GERMAN TRADE IN AFRICA.

A report, dated the 4th April, has been received at the Foreign Office from the Hon. Chas. S. Dundas, Her Majesty's Consul-General at Hamburg, on the subject of the German trade with Africa. The report states that for some years the trade of Germany with Africa has been continually increasing. According to the Imperial statistics, which comprise only the goods trade in direct intercourse between German and African ports, the value of the German exports to and imports from Africa amounted in the year—

	£
1889 to	2,880,000.
1890 „	3,504,000.
1891 „	4,080,000.
1892 „	4,464,000.
1893 „	5,088,000
1894 „	5,280,000.

The goods traffic with Egypt has risen from 240,000*l.* value in the year 1889 to 960,000*l.* in 1894, and in the same period that to the West Coast from 1,368,000*l.* to 2,352,000*l.*, while that to the East Coast from 240,000*l.* to 528,000*l.* The figures of the South African trade are of particular interest. In the seven years from 1889 to 1895 the importation into Germany of raw wool from the Cape is stated to have amounted to 5,280,000*l.* value, of which 960,000*l.* is put down to the past year.

The exportation of German manufactures to the Cape during the same period is valued at 3,000,000*l.*; to the Transvaal at 1,032,000*l.* In the year 1892 the exportation to both these countries amounted to no more than 395,000*l.* In 1895 it had risen to 1,056,000*l.* This exportation embraces various branches of industry, especially iron, machinery, textiles, and chemicals. Further important articles are sugar, beer, clothing, explosives, timber goods, pianos and other musical instruments, and leather goods.

In the last two years, for instance, the following goods have been exported:—

	Cwts.
Dyed cotton goods - - - -	8,250
Cyanide of potassium - - - -	2,200
Explosives - - - -	74,400
Railway rails and sleepers - -	154,000
Wire - - - -	110,000
Hardware - - - -	70,000
Machinery - - - -	12,000
Pianos and other musical instruments	12,000
Beer, in bottles - - - -	32,540
Refined sugar - - - -	21,200

The growing importance of German commercial interests in South Africa is evidenced particularly by the increase of exports to the Transvaal. During the years 1891 and 1892 the exportation of the German Customs Union to the South African Republic had reached an average value of no more than 72,000*l.* In 1893 it rose to 192,000*l.*; in 1894 to 288,000*l.*; and in 1895 to about 336,000*l.*

The following table gives the comparative increase in certain goods in the two years 1891 and 1895:—

	1891.	1895.
	Cwts.	Cwts.
Railway rails and sleepers - - - -	53,800	151,600
Locomotives - - - -	8,400	23,800
	No.	No.
Railway trucks - - - -	12	657

In the same degree the exportation of hardware and machinery has increased.

The increase in the exportation of cyanide of potassium is very noticeable, and this is due to its demand for use in the production of gold. The exportation of this article dates only from the year 1892. In that year 1,658 cwt. of the value of 11,372*l.* was exported to the Transvaal; in 1895 the exportation of this article had increased to 18,484 cwt. and 147,000*l.*

The development of the gold production of the Transvaal, has given a great impetus to the export of this article as is evidenced by the fact that whereas the whole German exportation of cyanide of potassium to the Transvaal in 1891 only amounted to 820 cwt., in 1895 it had reached the above figures.

VIII.—FRENCH TRADE IN BRITISH COAL.

According to a report to the Foreign Office by Mr. H. E. O'Neill, Her Majesty's Consul at Rouen, a question that has been agitating the coal trade for the past 12 months or more has been settled at last, and in a manner not altogether unsatisfactory to the importer of British coal, viz., "Shall France continue to be dependent upon Great Britain for a portion of her coal supply, whilst she possesses coal measures of her own whose output might be largely increased?" A sharp battle has been fought over this question, and the end has been a compromise. British coal is one of the chief imports of Rouen. During the past 10 years the average import has been 400,000 tons. The whole of Normandy and Brittany is largely dependent upon British coal for the working of its industries and for domestic consumption. In 1893 and 1894 the imports of British coal into French ports extending from Calais to La Rochelle reached 3,500,000 tons. The matter was one, therefore, that seriously affected a large British interest. Now, coal was one of the few articles left untouched by the Customs tariff of 1892, and as cheap coal is a necessary condition of success in all industries, there seemed reasonable ground for hope that its free import would not be further interfered with. The duty of 1.20 fr. per ton fixed upon all foreign coal many years back already benefitted the home production, and as a fact the consumption of French coal was steadily increasing, and if not displacing, it was at least checking the advance of British coal in French markets. At Rouen the amount of British coal consumed, in proportion to French, was in 1884 as 9 to 1; and 10 years later the relative quantities were as 4 to 1. But the extreme protectionists were not satisfied with this steady advance of French coal. They argued that the development of the French northern collieries was directly hindered by the competition of British coals; that in the case of international complications a sudden check might be given to the import of foreign coal by the declaration of coal as contraband of war, and consequently that great suffering and loss might be caused to all industries now dependent upon foreign coal, for French collieries could not suddenly fill the void that would thereby be created in the French coal market; and they contended that the price of British coal might and would be still further reduced to the benefit of all consumers if it had to sustain a more effective competition with that of the northern French collieries.

The measure proposed by the protectionist party to bring this about was a great reduction in the railway rates from the northern collieries to Normandy and Western France. The Government was pressed to approve of a new Zone Tariff (No. 107) with a different rate per ton per kilometre in each zone, by which coal from the Departments of the Nord and Pas de Calais was to be transported into Normandy and Brittany at 5 frs. per ton for a

waggon load of 10 tons to any station 100 kiloms. distant; at 7 frs. 50 c. for the same quantity at 200 kiloms.; at 9 frs. 50 c. at 300 kiloms.; at 11 frs. at 400 kiloms.; and at 13 frs. 50 c. at 500 kiloms. And for every despatch of over 100 tons, or of 10 waggons, this tariff was to be still further reduced by 15 per cent.

These were the proposals of the protectionists. They were opposed with unusual unanimity at Rouen. Even the Chamber of Commerce protested strongly against a measure that, if it had been taken alone, might have dealt a heavy blow at an industry that brought about 200 ships yearly to Rouen laden with coal from British ports. Careful calculations showed that if the above rates were enforced without compensation, the imports of British coal might be expected to diminish by more than 100,000 tons. This would mean a loss to the port of about 100 vessels, with a corresponding fall in the revenue that accrues from tonnage and pilotage dues, and brokers' fees, besides a loss of employment to hundreds of labourers upon the coal quays. It was argued further, that the ultimate effect of the rates proposed would be not so much to benefit the French collieries as those of Belgium, and even of Germany, which now supply France with coal. For France consumes 12,000,000 tons more of coal than she is able to produce, and this excess of demand is supplied by about 5,000,000 tons of British, 5,000,000 tons of Belgian, and 2,000,000 tons of German coal. It was impossible for the French collieries to replace the coal now sent from Great Britain to Normandy, and Brittany by an increase in their own output. They would only send there a portion of the coal they now supplied to districts nearer the mines, and if this were done the result would be that those districts would draw their coal from Belgium and from Germany. Even the advantages to be gained by the French mines would, it was said, be shared by foreigners, for one-third of the miners employed in the northern collieries are Belgian; whereas the suffering and losses to be borne in the western ports would fall upon men of French blood. Again, the rates proposed were so low that they could not but result in a loss to the Western Railway Company, which would be called upon to carry over the longer distances to the extreme west of France, and this loss would react upon the State in the form of interest guaranteed, or ultimately upon the taxpayer.

These opposing views have been thoroughly ventilated during the past 12 months in the press, by deputations to ministers, and lastly, in a special debate of the Chamber of Deputies, on November 24 last, and the result of it all has been a compromise.

The reduced rates on Tariff 107 have been approved by the Government, and they came into force at the beginning of this year. But with them there has been a re-arrangement of the rates for the carriage of coal into the interior from Rouen and other northern ports. These have been reduced, though in a lesser proportion than have the rates from the French mines westward, and British as well as French coal will benefit therefrom.—(*Foreign Office Annual Series*, 1668.)

IX.—THE CONDITION OF THE FRENCH MERCANTILE MARINE.

Referring to the project of the French Government to meet the supplementary charge of 1,400,604 frs. for the "invalides de la marine" in the Navy Estimates of 1896-97 by a tax on ship-owners, a writer in the "Journal des Debats" deals at some length with the present condition of the French mercantile marine.

According to this authority the mercantile marine of France is in a far from satisfactory condition. The encouragement given under the form of shipping bounties has not had the wished-for effect in restoring to it its former prosperity. On the contrary, the decay with which it is threatened is daily on the increase, and it is to be feared that the French mercantile marine will, ere long, have declined into complete ruin in face of the formidable foreign competition which is destroying it.

This would be nothing short of a national disaster for France. The maritime industry is an inexhaustible source of power and wealth. Without going back farther than the middle ages, it may be said that in the 16th century Holland owed to its "sea rovers" the prosperity which made that little country the centre of the world as regards trade, and that in the present day England owes her influence and wealth to her mercantile marine. In addition to the enormous material profit which a nation derives from the development of its transports, in addition to the benefits which are assured to her by the work of her dockyards, the openings found for the products of her forges—iron, copper, and wood—the amount of wages which she can pay out to her working classes, in addition to the certainty with which she can construct war vessels from her own resources, and to the valuable aid which the great mercantile transports, armed as cruisers, lend her at the time of need, in addition to all these advantages, an importance of quite a special kind, but of an inestimable value, arises from the presence of the national flag flying on all parts of the waters of the world.

A mercantile marine is not only an instrument of wealth for a country, it is a signal of credit and strength, and a proof of prosperity and power to all nations.

The shipping industry should, therefore, be developed as far as possible, and on comparing the conditions of the French mercantile marine with its rivals, the conclusion, from a French point of view, is not a satisfactory one. At the beginning of the century French shipping occupied a good position, disputing even with England the monopoly of the export trade of the world, and it was under the French flag that a good half of the European trade was carried on; but whilst French activity was being exhausted in questions of politics, England was steadily (as M. Lourdelet stated in his Report at the Paris Chamber of Commerce on the 19th of February last) "developing her mercantile marine, urging forward the construction of her steam vessels,

“ and finding in the Colonial Empire of Canada, Australia, and the Indies an incomparable opening for her trade, and a stimulant for her maritime activity.”

Other nations, especially Germany, have followed the example set by England, and the United States and Norway have effected a considerable development in their trading fleets. The result to-day is that the tonnage of all French vessels, sailing and steam combined, is less than that of either of the above-named countries. France now holds the fifth place, and this has come about during the last eight or ten years.

From the statistics of naval construction the same unsatisfactory conclusions may be drawn.

The correspondent of the “*Journal des Debats*” goes on to say: “ France built in 1895, 35,825 tons, 14,009 of which were steam vessels. During the same period, the output of North-American yards was 84,877 tons, only reckoning vessels of over 100 tons; that of German yards was 101,402 tons, and of English yards 1,139,544 tons, of which 994,292 tons were for the mercantile marine. France is thus in the fourth rank only. It must be noted, likewise, that 93½ per cent. of the vessels from the English yards are steam-vessels, and that, consequently, the English output is comparatively more *efficient* than the French, which includes more than 50 per cent. of sailing vessels. On the 31st December last, in English yards there were 711,652 tons in process of construction for trading vessels alone, and machinery has attained to such a pitch of perfection, that in five years (1889–1894) the number of workmen employed has been diminished one-sixth without affecting the amount of work done, a fact which places the shipping industry of the United Kingdom in an absolutely privileged position. The result of foreign competition with regard to the French mercantile marine has been such that according to M. Lourdelet, in his report already quoted, out of a total of 16,100,670 tons representing the movement of shipping between France and other countries, only 4,082,000 tons was carried under the French flag; the remainder, or nearly three-fourths of the whole, was taken by foreign vessels. It would be unjust to deny that efforts have already been made by the French Government to regain a little of the lost ground, viz., partly by the establishment of construction bounties, and partly by subventions to the postal lines.”

As far as the former of these points is concerned, there scarcely seems to be a way of improving the existing system; it is hardly worth noting the defect in one of the articles of the system, namely, that which concerns the case of vessels built abroad, but sailing under French flag. Previous to the year 1893 vessels of this category had the right to half the bounty, but the law of 30th January last took away this privilege, and it would not appear as if this law, intended really to favour the mercantile marine, would have effect. French shipbuilding yards are very inferior to English ones; construction in France costs more, and

the difference is so great that French shipowners having to choose between the half-bounty and the extra expense which shipbuilding in their own country involves, do not hesitate to give up the bounty. It remains to be seen, therefore, if by a temporary law for a few years, that is, until the time when French yards can compete on equal, or at any rate less unequal, terms with foreign ones, it would not be well to return to the system of half-bounty.

The second means whereby the State has lent her aid consists in granting an annual subvention to maritime postal companies, and this is a really efficacious means of helping the mercantile marine.

In an article published in the "Gazette de Cologne" of the 5th November last, it was asserted that the German Transatlantic fleet had been developed contemporarily with the Imperial postal lines, or even under the influence of the development of the latter. These companies, powerfully equipped, possessing a considerable fleet of swift vessels are, so to speak, the skeleton of a nation's mercantile marine, and around them are grouped the fleet of smaller vessels, sailing and other. They form the means of opening up new outlets, and the ways of communication, and commercial relations with other lands. Hence the duty of the State to undertake part of the charges which the formation and development of these huge organisations necessitate, and by the contribution which it makes to the expenses, attains two results of great importance in the furtherance of its own interests. On the one hand it can exact from the company receiving subventions (as to hours of departure, &c.), an absolute regularity in its services, so necessary to trade; on the other hand it has at its disposition an auxiliary fleet, which can be of the greatest use in the event of hostilities. These big vessels, capable of being rapidly changed into armed cruisers, serviceable alike for their size and speed (usually greater than that of war vessels), allow of the free disposition by the State of its maritime attack and defence forces.

X.—OPENING FOR PASSENGER STEAMER SERVICE TO BEIRA (PORTUGUESE EAST AFRICA).

A report, dated February 18 last, has been received at the Foreign Office from Mr. A. Carnegie Ross, Her Majesty's Consul at Beira (Portuguese East Africa), on the opening there appears to be for a passenger steamer service to that port.

Mr. Ross observes in his report that the establishment of a good passenger steamer service between Beira and South Africa is of great importance to British interests (1) for the trade of the port, so largely in the hands of British merchants; and (2) for British shipowners, as a properly organised service would be profitable and undoubtedly assist in preventing the shipping trade of the place from drifting into other hands.

It must be observed (1) that the business connection of Beira with East Africa preponderates immensely over that with other places; (2) there is practically no outward cargo; and (3) it is more important that vessels should be punctual in taking passengers away than in bringing them to Beira. There is a weekly service from Beira to the interior, and people going up country, who so wish, are not required to spend more than one week in the low country whereas people coming down to the coast, if they miss a steamer are obliged to remain doing nothing sometimes for three weeks in a hot and malarious climate.

These delays often cause loss of health as well as of valuable time.

In 1895 an arrangement was come to between the Cape Mail Companies and the German Line, and the vessels of the former ceased running north of Delagoa Bay. As soon as Beira was opened as a port all the steamers running northward made it a port of call, and in addition during 1893–95 the Castle Mail Packets Company, Limited, ran a steamer once a fortnight between Durban and Beira, first the steamship “Melrose” and then the steamship “Courland.”

The services of these boats all came to an end at once, and the port which had really been over-supplied with regular steamers was suddenly abandoned to the German Line. Trade was rapidly increasing and those who having done good work for little profit in the earlier days might have had some chance of recouping themselves, instead of holding on a little longer, dropped the business entirely.

The actual steamship service of the port is as follows:—

(1.) The steamship “Induna,” a vessel capable of carrying about eight first-class passengers and as many second, runs between Delagoa Bay and Quilimane, calling at all ports both in going north and south. It does not run according to any time table and is poorly equipped in every way. It comes to Beira about once a fortnight on the way north, returning three or four

days later from Chinde. Sometimes it goes direct from Chinde to Delagoa Bay without calling at Beira. If this boat ran up to time and had a reasonable amount of accommodation it would help to fill the gap. It has light draught and being well and boldly handled loses no time in running in and out of the ports at all states of the tide. It belongs to the Aberdeen line of steamers.

(2.) The steamers of the British Colonial Steam Navigation Company, Limited, of Messrs. Bullard, King, and Company, and of the Clan Line call at Beira at most irregular intervals on the way to India, Australia, or any other place but Europe. They bring passengers and cargo but are, of course, absolutely useless for people wishing to get home or to go to South Africa.

(3.) The British India Steam Navigation Company, Limited, sends a steamer once a month from Bombay to Delagoa Bay and back, calling on the South going voyages. It is proposed to alter this service so that the vessels call on both North and South voyages. Unfortunately these vessels call on the way South about the same time as the German boats. Three successive times it has happened that passengers after waiting for a steamer for three weeks have had a choice of two in the same day or within a couple of days of each other.

A passenger wishing to go home northwards by this line would have to tranship at Zanzibar and again at Aden. The steamers of this Company are quite uncertain in their movements.

(4.) The German East Africa Company's steamers are supposed to call at Beira once a month going South and once going North. They run South to Durban. These steamers are of about three thousand tons. They bring a great deal of cargo for Delagoa Bay, and as there are invariably delays at that port in discharging, it is the usual thing for them to leave Beira two or three days before the advertised time. The people on the coast are aware of this peculiarity, but travellers from the interior, timing their arrival in accordance with the time table, are, of course, sure to miss their boat and be obliged to wait a month for another. The January boat was five days after its advertised time. The December, January, and February South going steamers were full when they arrived, but each took away from Beira some 25 first-class passengers and 30 others. On all of these occasions the steamers netted some 300% to 400% of passage money.

Such are the present unsuitable arrangements. What is required is a regular service, say, every 14 days, of a good coaster running between Durban and Beira, calling at the intermediate ports, and connecting either in Delagoa Bay or Durban with some line of home-going steamers. If possible, it should be made to call at Delagoa Bay in time to take or land overland mails and any passengers who may have chosen that, the quickest, route to Beira.

The want of a regular periodical communication is acutely felt by all classes of the community, and it is highly probable that the Mozambique Company on having a proper scheme by a responsible company presented to them would be willing to pay a

subsidy for the conveyance of mails between Delagoa Bay and Beira.

The following table shows the progress of the passenger trade since Beira was opened as a port :—

The figures are taken from official returns.*

Six Months of	Passengers Arrived.	Left.
1892	685	280
1893	1,417	914
1894	1,098	893
1895	1,216	845

XI.—THE INDUSTRIES OF VALPARAISO.*

For some time past the *Sociedad de Fomento Fabril* (Society for the promotion of manufactures) has been engaged in the compilation of departmental industrial statistics, and a copy has recently been issued of those relating to the Department of Valparaiso. It appears that the number of industrial establishments in the Department is 417, concerning which the following statistics are given :—

Value of the raw material consumed in 1895	\$20,057,573
Number of operatives	12,616
Do. machines	2,203
Do. steam engines	162
Horse power of do.	1,766
Cost of fuel per diem	\$3,642,065
Number of lights	4,822

The rate of progress in the establishment of industries is given as under :

Number of industries founded previously to 1870	58
From 1870 to 1880	56
Do. 1880 to 1890	172
Do. 1890 to 1895	131
Total	417

With respect to the classification of these industries, 51, returned as “alimentary,” consumed in 1895 raw material of the value of 8,124,245 dols., and gave employment to 1,527 workmen. Of this class of industries the sugar refinery at Viña del Mar occupies the first place. It contains 150 machines operated by steam, 20 steam and four gas engines, aggregating 500 horse-power, and furnishes employment to 500

* Based on an article in the “Chilian Times.”

operatives. Its annexes are described as "a factory of animal charcoal, a distillery, and a gasworks for lighting the refinery and also the town."

The refinery does not appear to have been always in prosperous circumstances, but under its present able management, it seems, at length, to have turned the corner, and embarked on an era of prosperity. Of the other industries that are included in the same classification as the refinery, the report says but little. Bakers want more power over their workmen; biscuit makers, and chocolate manufacturers want higher duties on the imported articles; while the vermicelli men want the Government, by subsidies or otherwise, to procure them such freights as will enable them to export their products to Panama and Central America.

The next heading relates to lighting and does not include Viña del Mar gasworks. The two Valparaiso gasworks have amalgamated since the report was published. Together they consumed raw material to the value of 424,000 dols. in 1895, and they employed 150 workmen. The value of the production of the old company in 1894 was 739,424 dols.; and the cost of the electric lighting plant of this company is given at 132,000 dols. Of the coal consumed native figured for 69,000 dols., and foreign for 355,000 dols. Of the persons employed 12 were foreigners and 138 Chilians. The highest monthly wage paid is given at 500 dols., and the lowest at 100 dols.; whilst the highest daily is four dols., and the lowest 1 dol. 20 cents. Next after lighting comes *La fabricacion de Bebidas*, and under this heading there are included breweries, mineral and ærated waters, liquors, ice, &c. Ten factories and two deposits are classified. The value of the material consumed is given at 991,178 dols., and the number of persons employed at 359. The most important of these establishments is the *Fabrica Nacional de Cerveza*. This concern was formed by the amalgamation, in 1890, of Plagemann and Co., of Valparaiso, and the *Fabrica de Cerveza de San Francisco de Limache*. The former establishment is now used as a depôt, and for malting and ice making, whilst the brewing is done at the latter. The company is domiciled in Valparaiso, and that is, perhaps, the reason why this concern is included in the Department of Valparaiso, but, in reality, the brewery belongs to the Department of Limache. The capital of this concern is given at 1,124,638 dols.; a year's sales at 567,903 dols.; a year's profits at 114,315 dols.; and a year's wages at 75,318 dols. Of the liquor, mineral and ærated waters factories little or no information is vouched except that they want more patronage from consumers and more protection against the imported articles. Of carriage and cart building establishments, exclusive of the large general concern of Hardie and Co., there are two, employing 83 hands, and consuming 43,000 dols. worth of raw material.

Next come saw mills, furniture factories, and establishments in which wood is used up. There are 57 of these establishments employing 764 hands, and working up raw material of

the value of 1,473,492 dols. The owners of these establishments want protection in a variety of forms, such as, for instance, increased duties on imported articles; better railway service, and lower rates; repression of drunkenness among workmen, &c. After these establishments come the mechanical industries, which are the most important of all. There have been classified 59 establishments, employing 2,614 hands, and working up raw material of the value of 2,456,250 dols. Among these establishments are the extensive and well-known concern of Balfour, Lyon and Co.; Hardie and Co.; Lever, Murphy and Co.; and Morrison and Co. The first-mentioned firm, it is said, employ 600 hands, possess 83 machines, and have five steam engines aggregating 100 horse-power. Hardie and Co. give employment to 500 hands, and they possess 62 machines, an 80-horse steam engine, &c. Lever, Murphy and Co. employ in their two works 600 hands, and they have 160 machines of different kinds, and seven steam engines aggregating 125 horse-power. No returns are given of Morrison and Co. A revision of the Customs tariff in favour of all the industries in this classification is insisted upon, and the report concludes thus: "The condition of the working man occupies the attention of Valparaiso manufacturers. They consider it to be necessary to educate the working classes, and to improve their habits and manner of living, and they propose as a means to this end the construction of healthy dwellings, and a restriction of the sale of alcoholic beverages together with the exercise of proper vigilance over their manufacture."

XII.—THE ECONOMIC CONDITION OF ABYSSINIA.

It is astonishing what an unimportant economic role Abyssinia has played for centuries past, when from other points of view she has shown herself to be not a little remarkable; for it may be said that this East African Switzerland, as it has been called, has never exhibited that increase in wealth which is seen in all other countries in their progress towards a civilisation, however rudimentary. Although emerged from barbarism for many centuries, the Abyssinians, Gallas, and Shoans do less trade and develop fewer industries than the ancient Visigoths, who were infinitely more a nation of warriors. From the times of antiquity to the present day one has still to learn, says a writer in the "*Economiste Français*," what amount of industrial products have been exported by Ethiopia outside her boundaries, although the industries have existed from a very remote age. The country has never been known to have any foreign trade under any form, and still remains somewhat of an economic enigma. The few inhabitants, who are traders, and who have been held in subjection so long, have not yet received their freedom as did similar communities of the Middle Ages, and this may be taken as one explanation of the meagre development of Abyssinia up to the present day.

Other explanations are not hard to find. One, not the least important, arises from the fact that the country is almost impenetrable from the sea, the natural route for all economic developments, especially at the present day. Abyssinia has thus been perpetually shut out, or rather "walled up," and this condition of affairs does not seem yet on the point of change. Such impenetrability is not only maritime, it is also fluvial, the country being altogether deprived of that water transport which plays such an important part in commercial transactions and industrial enterprises. Finally, what may be styled the Abyssinian littoral, with the exception of the portion which has fallen to Ras Kasar and of the French possessions, is occupied by Mohammedans, Arabs, and Somali, who forbid the Abyssinians access to this maritime zone. These are more than enough reasons in explanation of the negative progress of the country from an economic point of view.

The elements of wealth are not, however, wanting. The country has an area of about 400,000 square kilometres (154,400 square miles), and contains a population reckoned at about 3,000,000. Including Shoa, the southern part, which has an area almost double that of France, the area is equal to that of many of the most important European countries. The population is

most hardy and, though not as yet numerous, increasing. Shoa appears to be the richest part, and is essentially an agricultural and pastoral country.

The products of Abyssinia, including Tigré, Amhara, and Shoa have a certain importance in spite of an imperfect exploitation and impossible trade outlets. The chief productions are gold, iron, cotton, coffee, cane-sugar, &c. The gold deposits are said to be of considerable extent.

Erythrea, the Italian colony to the north and east of Abyssinia, includes the coastal belt of Ras Kasar and the more distant parts, as far as the Abyssinian frontier and the boundary line fixed by the Treaty of 15th April 1891. Its area is about 247,300 square kilometres (95,400 square miles), with a population of about 450,000 inhabitants, of which the chief town, Massowah, contains about 16,000.

On the Indian Ocean, the portion of the coast of Somaliland, between the mouth of the Youba river and the eighth degree of north latitude, is under an Italian protectorate. The ports belonging to the Sultan of Zanzibar have been placed, since the 16th July 1893, under the administration of the Italian authorities for a period of three years with, according to recent consular reports, a most favourable result.

To the west, the sphere of Italian influence has been limited by treaties concluded in 1891 with England. The total extent of country over which Italy exercises any kind of influence, including the French and English coastal belts and those parts on the Gulf of Aden and to the N.E. of Somaliland which have been abandoned, amounts to about 1,651,820 square kilometres (637,600 square miles).

The trade of Massowah has developed since the year 1892, the average importation by land and sea being of a value of 10,903,015 lire (436,121*l.*), including precious metals to the value of 416,162 lire (16,646*l.*)

In these Ethiopian regions two opposing influences are actually at work with regard to the economic development of the country, the one being in favour of progress, the other in favour of the *status quo*. General interest would demand the triumph of Latin, that is to say, foreign influence, which indeed in the past and under the Jewish form has made Abyssinia what she is. The soldiers and clergy, in fact, belong to a race of immigrants, the traders and cultivators to the primitive stock.

XIII.—INDUSTRIAL REVOLUTION IN JAPAN.*

Japan is becoming less and less dependent upon foreign nations for the necessities and comforts of life, and is making her own goods with the greatest skill and ingenuity. Since their release from the exclusive policy of the feudal lords, the people have studied the methods of all civilized nations, and have adopted from each those which seem to them the most suitable for their own purposes and convenience. They have found one thing in Switzerland, another in Sweden, another in England, others in Germany, France, and the United States, and have rejected what is of no value to them as readily as they have adopted those things which are to their advantage. The Japanese workman can make anything he has ever seen. His ingenuity is astonishing. Give him a piece of complicated mechanism—a watch or an electrical apparatus—and he will reproduce it exactly, and set it running without instructions. He can imitate any process and copy any pattern or design more accurately and skilfully than any other race in the world. It is that faculty which has enabled Japan to make such rapid progress, and will place her soon among the great manufacturing nations of the world.

It was only 40 years ago that the ports of Japan were forcibly opened to foreign commerce. It was only 28 years ago that the first labour-saving machine was set up within the limits of that empire. Now the exports and imports exceed 23,958,000*l*.

The industrial revolution that is now going on in Japan is quite as remarkable as the political revolution that occurred there 30 years ago, and equally important to the rest of the world. Until recently all the manufacturing done in Japan has been in the households, and 95 per cent. of the skilled labour is still carried on in the homes of the people, and in a measure independent of the conditions that govern wage-workers in other lands. The weaver has his loom in his own house, and his wife, sons, and daughters take their turn at it during the day. It has always been the custom for children to follow the trade of their parents.

The finest brocades, the choicest silks, the most artistic porcelain, cloisonné, and lacquer work are done under the roofs of humble cottages, and the compensation has heretofore been governed usually by the quality of the piece produced.

In the olden times the feudal lord, or daimio, who ruled a particular province, was always expected to purchase the highest grades of industrial art that were produced in his jurisdiction, and if he did not wish them they were sold to some of his sub-

* Based on a report by Mr. W. E. Curtis, made from personal observations in Japan in 1895, and published in the "Bulletin of the Department of Labour" of the United States.

ordinates who had money to invest in such luxuries ; but now there are middlemen who stand between the artisans and the public markets, and buy for both the local and the export trades, taking all sorts of goods and disposing of them wherever they can do so to the best advantage. These middlemen are the same who used to handle similar wares during the days of the daimios, and their fathers followed the same business before them. The relations between the producer and his agent have gone on for centuries in the same family, and the weaver sells his brocades to the great-grandson of the merchant who bought his great-grandfather's product. When a large order is to be placed, say, for 1,000 lacquer trays or 10,000 embroidered shawls, the middleman distributes it among the families of his acquaintance who are in the habit of doing such work. If they are poor he advances them money and furnishes them with materials. He is usually their banker, and they keep an open account with him, being credited for whatever they furnish and charged with whatever money or supplies he gives them. If they get a little ahead he advises them in making investments, and acts as a friend and counsel in financial affairs. The relations between these commission men and their clients have nothing to resemble them among the working classes of Europe or America. They are the outgrowth of the feudal system, and to the credit of Japan it may be said that the confidence which the working people have placed in their agents has seldom been betrayed. There has not been a fixed price for anything except the staples of the country, but the producers have relied upon the middlemen to represent their interests and get as good prices as possible for their wares, just as a large real estate owner depends upon his agents to rent and sell his property to advantage.

It will be seen that under this system organisations for the purpose of affecting wages and hours of work have not been practicable in Japan, but guilds have been known for several centuries, and were introduced by the Dutchmen who occupied the island of Deshima, in the harbour of Nagasaki, the western-most port of Japan. During the 400 years that the policy of isolation and exclusion was enforced in Japan, these Dutchmen were the only foreigners with whom the inhabitants of the country were allowed to communicate, and the few modern ideas that filtered into the country came through them.

The guild or association of men who are engaged in the same kind of employment which originated in Holland was readily accepted in Japan, and long before the country was open to foreign commerce this system included all trades and occupations. The silk growers and silk buyers, the men who raise tea and those who sell it, the spinners and weavers, artists and decorators, carpenters, screen makers, confectioners, peddlers, doctors, lawyers, merchants, and even thieves have had guilds to promote their mutual interests. Among the mechanics and tradesmen the guilds have often been extended to include life insurance and aid to those who are ill or infirm, like mutual benefit societies

of other countries. Assessments were made upon the living to pay the doctors who have attended the sick and the undertakers who have buried the dead.

There have been but two strikes in Japan. One of these occurred among a railway construction gang, who were hired for certain wages to work six days a week, and were required to work seven without additional compensation. When their protests were unheeded they laid down their tools, and appealed to the police authorities for the enforcement of the law which makes six days a week's labour, and provides that no employé of the Government or any corporation or private individual shall be compelled to work more than six days in a week without extra compensation. Sunday is the usual day of rest in Japan. Its selection is not due to law nor to religious scruples, but to public convenience and, perhaps, out of respect to foreign nations. When what is known as the six-day law was passed the Government set the example by closing its offices on Sunday, and all other institutions followed suit. That law was originally suggested for sanitary reasons.

The second strike in Japan occurred in Tokio in the summer of 1895. A party of bricklayers engaged in building a factory near Tokio had their hours of labour extended from 12 to 13, because of a desire on the part of the management to complete the job and start the machinery as soon as possible. The men did not object to this increase of time, but asked a corresponding advance of wages, and, as the contractor refused, they left work. He got other bricklayers to take their places, but they were induced to abandon him also, and as he persisted in his refusal to do what the men considered simple justice, it was decided to send emissaries to all the other bricklayers in the city, and ask them to join in a sympathetic strike. Public sentiment was aroused by the disturbance, and the contractor who caused the trouble finally compromised with his men and went back to 12 hours' work for 12 hours' pay.

The ancient system of household labour is being rapidly overturned by the introduction of modern methods and machinery. The older artisans are offering a vain resistance, and cannot be drawn from their antique looms and forges by any inducement that has yet been offered; but the younger generations are rapidly acquiring a knowledge of the use and value of labour-saving machinery, and factories are being built in all parts of the empire. The greatest progress thus far has been made in cotton spinning and weaving, but several iron mills have been established, and machine shops are springing up all over the country. In four years the new treaties go into effect, when foreigners will be allowed to engage openly in manufacturing enterprises. Then their capital and experience will give a decided stimulus to mechanical industry, and the increase in the productive power of Japan will be even more rapid than now.

The first manufactory established in Japan was a cotton mill, down in the south-western corner of the empire, in the province

of Satsuma, which has produced the best pottery and some of the greatest men. Prince Shimazu was its patron. Having learned something of modern arts and sciences from the Dutchmen who were allowed to remain on the island of Deshima, he started a laboratory on his estates, in which he learned telegraphy, photography, and how to make glass, coke, and gas for illuminating purposes. A few years later he built a factory near his summer villa, which was half arsenal and half iron foundry. He made guns there and other articles of iron, and experimented with explosives.

All the work in both institutions was conducted under his personal supervision, with the assistance of Dutch chemists. From these he heard that much could be learned about such matters from books; so he started a retainer to Nagasaki, charged with the duty of securing whatever books on chemistry, natural philosophy, and other scientific subjects could be bought or borrowed, and an order was left with a merchant at Deshima to procure for him a copy of every scientific publication that was issued. In this way a considerable library accumulated, and the books were translated to the prince, as fast as they came, by a schoolmaster who had learned English at Deshima, and whose services were secured.

One of the books contained a description of the Arkwright spinning jenny, and the prince was so fascinated that he lost no time in ordering a machine through the Dutchmen. When it came and was set in operation, he decided to introduce it among his people as a substitute for the old-fashioned spinning wheel. He built a stone factory, and set up a plant of 3,030 spindles, at a cost of 30,000 yen (about 3,125*l.*), in a suburb of Kagashima. The machines were throstles and mules made by Platt and Bro., of Bradford, who sent an English engineer over to set them up and educate the operatives. The prince treated him like an equal, built him a fine large stone house, which is now the high-school building at Kagoshima, surrounded him with luxuries of every kind, and paid him a handsome salary. As the enterprise was not intended for profit, but for the purpose of introducing the art of spinning among the people, everything was conducted on a most elaborate and expensive scale, and the yarns produced were of superior quality.

The next factory was set up by Mr. Kajima, of Tokio, in 1867, while the country was still disturbed by the war. It originally had but 720 spindles, but now works 82,000, and is the largest in the empire. These were the only factories in Japan until 1879, when the Government undertook to encourage such enterprises, and established two well-equipped plants in different parts of the country, to educate operatives and demonstrate the superiority of modern machinery. It set up four more in 1880, four in 1881, one in 1882, another in 1883, and still another in 1884. They served their purpose, made machine-spinning popular, and have since been handed over to private companies who are working them with great profit.

The industry has grown so rapidly that, according to statistics gathered by the Osaka Board of Trade, there are now 61 factories in operation, with 580,564 spindles, employing 8,899 men and 29,596 women. The factories in course of construction, and which will be in operation during the present year, will bring the total number of spindles up to 819,115. Thirty-seven of these factories are at Osaka; the largest in the empire has 82,000 spindles and the smallest 1,136. There are four with more than 50,000 spindles, and 13 with more than 25,000.

Most of the raw cotton used in Japan comes from India and the United States, but very little is shipped direct from the latter country. The import statistics do not disclose this trade, it is merged with that of Great Britain.

The first genuine foreign factory to be established in Japan is the Osaka Tokei Seizo Kubushiki Kwaisha, familiarly known as the American Watch Company. It was started on 1st January, 1895, and turned out its first finished watch on 10th April. The organizer and promoter of this company was Mr. A. H. Butler, of San Diego, California, who took an outfit of watch-making machinery to Japan, and induced a number of jewellers and watch dealers in Osaka to furnish 33,000*l.* capital to pay the cost of a building and the running expenses of the business.

The highest wages paid to the skilled native workmen in the factory are only 40 sen a day, which is equivalent to 10*d.* in English money. The lowest wages are 10 sen (2½*d.*) a day. The capacity of the factory when fully in operation will be 150 watches a day, and owing to the low price of labour, they can be sold with a profit for 50 per cent less than the market price in Western countries.

Embroidery women, who make the work that is so much prized by Americans and Europeans for decorative purposes seldom receive more than 7½*d.* or 10*d.* a day in English money, although in any other land they would be estimated as artists.

Investments are as safe in Japan as anywhere else in the world, although there is comparatively little foreign capital represented. All the railroads, which now represent a total of about 3,000 miles and a capital of 15,625,000*l.*, were built with local money. No money was borrowed abroad, and there are very few shares or bonds of Japanese corporations held by foreign investors. At the same time the people are not rich. There are very few men of large fortunes. It is said there are only two millionaires in Japan. One made his money in coal mining and the other is the principal owner of a great steamship company, which has 100 vessels in its service and a monopoly of the coasting trade. But there are many men with small fortunes, and although wages are very low nobody is very poor. There is not an almshouse in the entire country, and you never see a beggar in the streets. Occasionally some poor leper stretches out his hand as you enter one of the gilded temples, but there is not a country in Europe or America so free from street begging as Japan.

There is no protection for foreign patents in Japan, but an article or instrument or machine that comes into the country or that is seen abroad may be manufactured without interference or the payment of royalty; but no Japanese can obtain a payment upon a foreign invention. He must show that his idea is not only original in Japan, but original with him, and if it appears thereafter that he is mistaken, or has practised a deception, his patent is cancelled. The processes and implements used in all the industries of Japan have been inherited from generations far removed, and nearly all of those recently adopted are copied from foreign models.

While the Japanese will soon be able to furnish themselves with all they use and wear and eat without assistance from foreign nations, they will be compelled to buy machinery and raw material, particularly cotton and iron. Therefore, foreign sales will be practically limited to those articles, and, in addition, the market for machinery will be limited as to time. The Japanese will buy a great deal within the next few years, almost everything in the way of labour-saving apparatus, but they are already beginning to make their own machinery, and in a short time will be independent of foreign nations in that respect also. Another important fact—a very important fact—is that they will buy only one outfit of certain machinery: this they will copy, and supply all future demands themselves. They will go on until the new treaties take effect, when foreign patents will be protected.

They have very little wood-working machinery, and very little shoe-making machinery, for the people do not wear shoes. The same is true of knitting machinery, for they do not wear hosiery. Probably not more than 20,000 out of the 41,388,313 people who compose the population of Japan wear shoes and stockings; 90 per cent. go barefooted and barelegged—women, children, and men protecting their feet from the stones by wooden and straw sandals. The higher classes have the same sort of foot-gear, but it is made in a more finished manner, and they wear little cloth affairs that they call “*tabis*” upon their feet. These are made of white or blue cotton, and do not go above the ankle-bone. At the same time the use of shoes and hosiery is increasing, and the people will grow into it as they have grown into other foreign notions.

The Japanese make all the woodwork about their houses by hand, and most of their houses are entirely of wood. They are very skilful in all kinds of cabinet and joiner work. Their hand-made tools are well adapted for doing close work, and are kept very sharp; besides, they give a great deal more labour and patience to an article than foreign carpenters and cabinet makers. You never see scratches from nicked tools on their planed work. They use very few nails, but mortise almost everything. It is usually so well done that it is difficult to detect the joints, except by the grain, and it lasts for ever. Some of their ships are made without a bit of iron in their composition—everything is mortised.

Every variety of agriculture is carried on, and the soil is in constant use. A couple of acres is considered a large tract of land for farming purposes. Most of the farms are of smaller area, and the crops are greatly diversified. Upon such a little spot of land will be grown almost everything known to the vegetable kingdom—a few square feet of wheat, barley, corn, and millet; a plat of beans perhaps 10 feet wide by 20 feet long, an equal amount of potatoes and peas, and a patch of onions, about as big as a grave; beets, lettuce, turnips, sweet potatoes, and other varieties of cereals and roots occupy the rest of the area.

The farmer looks up on his growing crop every morning, just as an engineer will inspect the movements of his machinery, and if anything is wrong remedies it. If a weed appears in the bean patch he pulls it up; if a hill of potatoes or anything else fails it is immediately replanted. And when he cuts down a tree he always plants another to take its place. The artificial forests of Japan cover many hundreds of square miles, and by this accuracy, economy, and care the prosperity of the country is permanently assured. As one crop is harvested the soil is worked over, fertilized, and replanted with something else.

The largest area of agricultural land in Japan is devoted to raising rice, perhaps as much as nine-tenths of the whole, and as that crop requires a great deal of water, the paddy-fields are banked up into terraces, one above the other, and divided off into little plats 25 or 30 feet square, with ridges of earth between them to keep the water from flowing away when they are flooded. All farming lands are irrigated by a system that is a thousand years old. Some of the ditches are walled up with bamboo wickerwork and some with tiles and stone.

The official statistics of Japan show that there are 11,400,008 men and 10,948,053 women engaged in agriculture, which is nearly one-half the total population.

The working men of Japan have no reason to complain that the women do not carry their half of the load. Whatever may be the position of the gentler sex in the household, although she is not allowed to hold property or share in the responsibilities that are usually divided between husbands and wives in Western countries, she is at least admitted to an equality with men when there is any hard work to be done.

Silk and tea, the two chief exports of Japan, are raised almost entirely by the labour of women, and in the mechanical arts she appears to participate equally in the labour, although she gets little or none of the credit. Her deft fingers fashion many of the choicest pieces of cloisonné and the ceramics, and in the decoration of lacquer that which comes from her hands is equal and often superior to the work of men; she weaves mats and other articles of straw; she braids bamboo baskets, and a thousand-and-one other articles that are made from that useful tree; she goes out with her husband in fishing boats and dries and salts the catch he brings home; she assists in house-building and cabinet-making, and in various other occupations which in the Western

countries are not considered suitable to her sex. She does almost everything that man can do quite as well and as rapidly as he, although her wages in every employment are only a little more than half of his. She is always present in the shops and stores, usually as bookkeeper and cashier. Some of the largest stores are managed by women, and a few are owned by them. And, although the laws and social regulations of the country prohibit it, sometimes you find a woman whose force of character defies both courts and customs, and directs the financial affairs and the business of her family as well as the matters that pertain to the household.

With reference to more recent developments of Japanese industries, the "Japan Weekly Mail" publishes, in a recent number, the profit and loss account for the last six months of 1895 of most of the spinning factories of Japan, the rate per spindle of the leading factories being also given. The following table shows the figures :—

	Profit for the last Half-Year.	Profit per Spindle.
	Yen.	Yen.
Koriyama spinning factory - - -	36,345·887	6·761
Himeji spinning factory - - -	22,242·706	5·388
Kishiwada spinning factory - - -	58,529·406	5·255
Owari spinning factory - - -	75,552·494	4·808
Sakaye spinning factory - - -	25,542·369	4·751
Wakayama spinning factory - - -	51,053·953	4·629
Uwajima spinning factory - - -	19,381·630	4·453
Settsu spinning factory - - -	155,294·199	4·395
Hirano spinning factory - - -	120,322·841	4·388
Iyo spinning factory - - -	22,817·937	4·257
Miye spinning factory - - -	204,860·988	3·984
Fuyuma spinning factory - - -	30,236·176	3·936
Shimozuke spinning factory - - -	18,020·619	3·929
Amagasaki spinning factory - - -	101,132·869	3·746
Meiji spinning factory - - -	27,363·806	3·555
Kamagafuchi spinning factory - - -	147,357·105	3·432
Osaka spinning factory - - -	188,333·806	3·403
Tokyo spinning factory - - -	78,374·346	3·393
Kurashiki spinning factory - - -	49,949·551	3·233
Fukushima spinning factory - - -	35,149·289	3·032
Kofu spinning factory - - -	6,166·444	2·987
Senshu spinning factory - - -	54,005·912	2·785
Miike spinning factory - - -	55,802·874	2·691
Okayama spinning factory - - -	48,360·911	2·507
Naniwa spinning factory - - -	56,316·743	2·231
Tamashima spinning factory - - -	30,920·489	2·206
Asahi spinning factory - - -	30,624·261	2·052
Hiroshima spinning factory - - -	10,524·774	1·853
Noda spinning factory - - -	7,252·781	1·453
Average profit per spindle - - -	- - -	3·561

It will be seen, says the "Shogyo," from which the above figures are taken, that the profit per spindle ranged from 6·761 yen to 1·435 yen, and that the average profit at the factories cited was as high as 3·561 yen. Japanese spinners are certainly to be

congratulated on such success. No wonder that capitalists are eager to embark in this enterprise. During the last six months of 1895 the price of the raw material and that of coal, as well as the wages of mechanics, were comparatively low, yet the market price of yarns rose steadily, reaching, at the beginning of winter, a figure without precedent in recent years. The table given below shows the cost of raw material and labour, as well as the price of yarns, for the two months of July and December of last year :—

	December.	July.
Cotton (per 100 catties) :—	Yen.	Yen.
Broach - - - - -	21·95	18·50
Pachora - - - - -	20·50	17·75
Middle quality - - - - -	26·40	22·30
T'ung ginned - - - - -	24·15	19·25
Shanghai ginned - - - - -	23·50	18·35
Coal (per 10,000 lb.) - - - - -	17·31	16·01
Wages, per day :—	Sen.	Sen.
Male mechanics, average - - - - -	18·14	18·05
Female mechanics, average - - - - -	10·30	9·83
	Yen.	Yen.
Yarns (per bale) - - - - -	93·240	81·80

A glance at the table shows at once why the spinning industry proved so successful during the second half of 1895. Whether similar success can be attained during the present half-year cannot be predicted with any certainty. The price of raw material and the wages of mechanics show an upward tendency.

The increase in the value of last year's exports of agricultural produce, amounting to 501,449 above that of 1894, is due chiefly to the improvement in ostrich feathers, mohair, wool, and goat and sheep skins.

There was at the beginning of the year a large quantity of wool awaiting export at the ports of the Colony, and the moving off of this surplus may account for some of the increase in this year's exportation; but still the increase in quantity is very satisfactory, especially when account is taken of the severe droughts and other drawbacks to sheep farming.

Unfortunately, as may be seen from the above table, there is not an increase in value proportionate with the increased weight.

The increases in the values of mohair and ostrich feathers are most satisfactory features in the export table. The value of the two together make up a total amount of more than three-fourths the value of our wool export.

The growth of these two industries, both comparatively new, may account in some good degree for the slow increase in the wool export on the average over the last 20 years.

The capital prices now obtainable for mohair may not be maintained in the future, but there is no doubt that there is an increasing demand for this textile fibre, caused by the greater number of uses and manufactures to which it is now applied. The total quantity of mohair now supplied by all the Angora farmers of the world does not probably exceed twenty-five million pounds, while the wool-clip of the world will exceed two thousand million five hundred thousand pounds, or one hundred times as much. It is not over-production the Angora farmers have to fear but a change in the fashions.

The total value of the imports of agricultural produce into Cape Colony in 1895 was 929,030*l.*, as against 672,710*l.* in 1894, or an increase of 260,711*l.* The principal increases are in wheat (69,168*l.*), spirits of all sorts (52,915*l.*), wine (48,355*l.*), and cigars (37,402*l.*).

With the exception of a decrease in the value of chicory (243*l.*), and of flour (4,238*l.*), there is an increase in all imported articles of farm produce. A decrease in flour is more than made up by the increased importation of wheat, and it is to be feared that from the failure of crops through drought and locusts over large areas of the Colony, the imports of 1896 may show a still higher figure.

XV.—THE CUSTOMS TARIFF OF GUATEMALA.

(Continued from page 300.)

No.	Articles.	Duty.
		Dols. cts.
1260-61	Lamps of copper and its alloys, put together or in pieces whether gilt or not, silvered or nickelled, <i>gross weight</i> -	Kilos. 0 60
1262	Pencil cases of copper or its alloys or white metal of all kinds, with or without pencil, with weight of package -	" 2 0
1273	Handles of copper or its alloys, or white metal, of all kinds, though they have glass or porcelain, with or without bolts, with weight of package -	" 0 60
1277	Medallions or <i>bas reliefs</i> of copper or its alloys of all classes for the <i>rosettes of ceilings or other places in the house</i> , silvered, nickelled, or gilt, with weight of package -	" 0 70
1282	Springs of copper or its alloys of all kinds, for furniture, with weight of package -	" 2 0
1302	Purses of copper or its alloys, with weight of package -	" 2 0
1305	Tacks of copper or brass, gross weight -	" 0 50
1309	Lamps of copper or its alloys or white metal of all classes, <i>gross weight</i> -	" 0 60
1315	Watches of all kinds of metal, not gold or platinum, not repeaters -	Each 0 1
1318	Castors of copper or its alloys for furniture, <i>gross weight</i> -	Kilos. 0 50
1326	Tacks of copper or brass though the spike be of iron, up to 24 millimetres long, gross weight -	" 0 50

SECTION 9.

Articles of Wood and Wood and Iron.

1362	Busts and half-relief of wood or its imitation when its <i>nett weight</i> does not exceed 10 kilos. each, with weight of package -	" 2 0
1362	Busts of half-relief of wood or its imitation when its weight exceeds 10 kilos each, with weights of package -	" 1 0
1389	Wooden staves for casks or barrels, gross weight -	" 0 03
1393	Wooden statues, when their net weight does not exceed 10 kilos. each, with weight of package -	" 2 0
	Wooden portable steps, though part iron, gross weight -	" 0 10
1394	Wooden statues, weight (<i>nett</i>) exceeding 10 kilos. each, with weight of package -	" 1 0
1396	Wooden figures, weight not over 10 kilos. each, with weight of package -	" 2 0
1397	The same <i>nett weight</i> over 10 kilos. each, with weight of package -	" 1 0
1403	Frames of wood, <i>put together or in pieces, of all classes and signs, whether gilt, silvered, bronzed, varnished, or painted, or containing ornaments of other material, gross weight</i> -	" 0 50

SECTION 10.

Articles of Leather and Hides.

1451	Trunks and portmanteaus of leather or lined with leather, without straps, with or without metal ornaments, of at least 40 centimetres long inside, with weight of package -	" 0 75
1454	Leather bags for travelling, with weight of package -	" 3 0

No.	Articles.	Duty.
1469	Belts of leather, or part leather, for sportsmen and travellers, <i>made specially to carry cartridges and money, with weight of package</i> - - -	Dols. cts. Kilo. 1 0
1469	Leather belts, with or without buckles not of silver or gold, with weight of package - - -	„ 2 0
1486-7	Leather gloves of all kinds, with weight of package - - -	„ 4 50
SECTION 11.		
<i>Articles of China, Clay, and Crystal.</i>		
1508	Services of glass, china, with or without metal ornaments, with weight of package - - -	„ 2 0
1513	Services of glass, china, or porcelain, with ornaments of metal, with weight of package - - -	„ 2 0
1515	Manufactures of porcelain, china, or clay, not specified, with weight of package - - -	„ 1 0
1526	Busts of crystal, glass, china, porcelain, or clay, with weight not exceeding 10 kilos. each, <i>with weight of package</i> - - -	„ 1 0
1527	The same when over 10 kilos. each, <i>with weight of package</i> - - -	„ 0 50
1528	Empty boxes of all kinds of crystal, glass, porcelain, or clay, even if ornamented with silk cloth, or with leather or metal ornaments, with weight of package - - -	„ 3 0
1529	Chandeliers of crystal or glass, with or without pedestal of other material, <i>gross weight</i> - - -	„ 0 50
1530	Candlesticks of crystal glass, with or without pedestal of other material, <i>gross weight</i> - - -	„ 0 40
1531	The same of china or porcelain, <i>gross weight</i> - - -	„ 0 30
1538	Crystal or glass in pieces, with mountings or settings of metal, <i>and the pieces of crystal prepared for mounting, with weight of package</i> - - -	„ 3 0
1550	Figures of glass, china, porcelain, or clay, <i>with weight not exceeding 10 kilos. each, with weight of package</i> - - -	„ 1 0
1551	The same, weighing over 10 kilos. each, <i>with weight of package</i> - - -	„ 0 50
1554	Bottles of glass, narrow-necked, empty, all sizes, <i>gross weight</i> - - -	„ 0 10
	Bottles of glass or crystal for panniers or cruet stands, <i>with weight of package</i> - - -	„ 3 0
1574	Porcelain or its imitation made in pieces of all sizes, and forms with mountings or settings of metal <i>and the pieces of porcelain or its imitation ready for mounting, with weight of package</i> - - -	„ 2 0
1575	Breast pins of crystal glass, china, or porcelain of all kinds though having ornaments of metal, <i>with weight of package</i> - - -	„ 2 0
SECTION 12.		
<i>Miscellaneous Articles.</i>		
1584	Fans of tortoise-shell, mother-of-pearl, or ivory, without cases - - -	Each 5 0
1591	Personal ornaments of horn, celluloid, gutta-percha, bone, or wood, with or without metal ornaments, <i>with weight of package</i> - - -	Kilo. 2 0
1592	Ornaments of feathers though <i>containing other materials, with weight of package</i> - - -	„ 10 0
1594	Ornaments of straw, <i>with weight of package</i> - - -	„ 1 50
1604	Gymnastic apparatus, <i>gross weight</i> - - -	„ 0 10
1609	Earrings of horn, celluloid, gutta-percha, bone or wood, with or without metal ornaments, <i>with weight of package</i> - - -	„ 1 0
	Pillows or mattresses of feathers, down, or its imitations, <i>with weight of package</i> - - -	„ 2 0
	Pillows or mattresses of wool, <i>with weight of package</i> - - -	„ 1 0
1612	Harmonicas of all classes, <i>gross weight</i> - - -	„ 0 35
1617	Manufactures not specified, of cane, osier, or straws, though containing ornaments of other materials, <i>with weight of package</i> - - -	„ 1 50

No.	Articles.	Duty.
		Dols. cts.
1636	Boas of feathers, with weight of package - - -	Kilo. 8 0
1641	Mouthpieces or pipes, of meerscham or amber or their imitations, with or without cases, with weight of package - - -	" 8 0
	Bags of cloth covered with gutta-percha or other material, for school children, with weight of package - - -	" 1 0
	Studs and sleeve-links of all kinds for shirts, excepting those of gold, silver, or platinum, with weight of package - - -	" 2 0
1652	Ropes of aloe, hemp, and other fibres, at least 36 millimetres in circumference, if not destined for the Ports and Lakes of the Republic, gross weight - - -	" 0 10
1658	Musical boxes of all kinds, with weight of package - - -	" 1 0
	Shoe brushes, with weight of package - - -	" 0 50
	Shoemakers' wax, with weight of package - - -	" 0 75
1690	Clarionets of all kinds, with weight of package - - -	" 2 0
	Baskets of osier for agriculture, gross weight - - -	" 0 05
1733	Statues of marble or alabaster, with or without pedestal of other material, with weight not over 10 kilos each, gross weight - - -	" 0 30
1734	Do., exceeding 10 kilos. each, gross weight - - -	" 0 20
1735	Do., exceeding 50 kilos. each, gross weight - - -	" 0 05
1737	Matting of hemp, cocoa, jute, palm, or henequén in pieces, gross weight - - -	" 0 30
	Cases (étuis) of any material, not specified, containing silk, with weight of package - - -	" 3 0
	Bands of gutta-percha for the transmission of motive force, when not forming part of special machinery, although combined with pulleys and axles, gross weight - - -	" 0 30
1745	Figures of marble or alabaster, with or without pedestal of other materials, nett weight exceeding 10 kilos. each, gross weight - - -	" 0 30
	Pins of celluloid, gutta-percha, bone, horn, or paste, for the hair, with weight of package - - -	" 2 0
	India-rubber in plates, with or without mixture of cloth, gross weight - - -	" 0 20
1778	Jewellery of silver or silver and steel, whatever fineness, with pearls or precious stones, nett weight - - -	" 50 0
1780	Do., without pearls or precious stones, whatever fineness, nett weight - - -	" 10 0
1797	Manikins of all kinds not specified, for dressmakers or exhibition of dresses, gross weight - - -	" 0 20
	Hose of rubber not specified, with or without mixture of cloth, for pumps, pipes, and other uses, gross weight - - -	" 0 20
	Longitudinal measures of rubber cloth with weight of package - - -	" 1 0
	The same of cloth or metal in leather cases with weight of package - - -	" 1 0
1805	Dolls of all kinds not dressed, with weight of package - - -	" 1 0
1809	Oboes with or without cases, with weight of package - - -	" 2 0
1811	Organs of all kinds and their supplementary pieces, with weight of package - - -	" 0 35
1831	Loose supplementary pieces for watches, with weight of package - - -	" 1 0
1851	Portemonnaies of other materials not specified, not being of gold, silver, platinum, or aluminium, with weight of package - - -	" 2 0
	Screens of all classes, not specified, with or without metal mountings, with weight of package - - -	" 4 0
	Straw hats, with weight of package - - -	" 0 50
	Dresses and articles for dresses of all classes, for dress, with weight of package - - -	" 20 0

No.	Articles.	Duty.
SECTION 13.		Dols. cts.
<i>Articles of Stationery, Cardboard, and for General Office use.</i>		
1891	Exfoliated almanacs, mounted or ornamented on card, <i>though having advertisements</i> , with weight of package -	Kilo. 0 80
1903	Memorandum books of card or lined with rubber, covered cloth or any stuff or other vegetable fibre of all kinds, with weight of package -	" 0 50
1908	Cards for lottery games, with weight of package -	" 1 0
1913	Copybooks for writing, drawing, or mathematics, with special ruling or with models of writing or drawing, for the use of schools, gross weight -	" 0 10
1923	Invoices in blank, with headings or printed memoradums or lithographed, with weight of package -	" 1 0
1946	Handles of tortoiseshell, mother-of-pearl, ivory, or ordinary metal, for pens, with or without cases, with weight of package -	" 4 0
1965	Brown paper, except that used for filtering, blotting paper, or of manilla for wrapping up, <i>although with advertisements</i> , gross weight -	" 0 05
1975	Straw paper, imitation of China paper, for wrapping up, gross weight -	" 0 15
	Serviettes or handkerchiefs of paper, though with advertisement, with weight of package -	" 0 50
	Cards for menu or ball programmes, with or without pencils, with weight of package -	" 2 0
SECTION 14.		
<i>Wines, Liqueurs, and Nutritive Articles.</i>		
2058	Fruits <i>preserved</i> in their juice or candied, gross weight -	" 0 30
2076	Shell fish, preserved, in any package, gross weight -	" 0 25
2079	Nuts (dried fruits), gross weight -	" 0 15
2094	Refined salt, ground, in any package, gross weight -	" 0 06
2103	Wheat, gross weight -	" 0 02 $\frac{1}{4}$
2104	Grapes, apples, pears, quinces, peaches, cherries, plums, and strawberries in syrup, or in their juice, gross weight -	" 30
SECTION 15.		
<i>Drugs, Medicines, &c.</i>		
2118	Cod-liver oil, prepared as emulsion creosoted with hypophosphites, with iodine, with bark, or other medicinal substances, in packages of any capacity, gross weight -	" 0 12
2130	Oils for preparing leather, gross weight -	" 0 10
2135	Acetate of copper (verdigris), nett weight -	" 0 30
2171	Alkaloids of vegetable and animal origin, such as: Aconitine, atropine, berberine, caffeine, cannabine, cantharidine, cicutine, cocaine, codeine, cubebine, curarine, daturine, digitaline, delphine, strychnine, narceine, narcotine, nicotine, morphine, piperine, veratrine, and others not denominated, and those similar and those derived from them, nett weight -	Gramme 0 10
2173	Alkaloids, organic, artificial, such as trimethylamine and their salts and similar, not denominated, nett weight -	Kilo. 5 0
2176	Artificial camphors prepared with other essences than turpentine, such as eucalyptol, menthol, thymol, and thumenol, &c., with weight of package -	" 6 0
2202	Antipyrine and its compounds, not specified, nett weight -	" 14 0
2207	Arsenate of quinine, nett weight -	" 6 0
2228	Balsams of copaiba, liquid or solidified, and balsam of Canada, nett weight -	" 1 0

No.	Articles.	Duty.
		Dols. cts.
2229	Balsam of Mecca, of Peru, or black of Tolu, or white, storax, liquid, amber, and other natural balsams, not denominated, and sap of sea pine, nett weight - - -	Kilo. 0 50
2236-7	Essence varnish or oil varnish (solution of resin, or gum resin, or elastic gum, dissolved in the essence of turpentine, benzine, or similar products, or in some drying oil), gross weight - - -	" 0 25
2248	Bicarbonate of soda, in crystals or lumps, with weight of package - - -	" 0 10
2252	Bovinine (liquid extract of meat), in bottles up to 125 grms. - - -	Dozen 1 50
2260	Pitch, liquid or black (vegetable pitch) - - -	Kilo. 0 10
2263	Hydrated bromal, nett weight - - -	" 10 0
2269	Bromide or bromo-hydrate of quinine, nett weight - - -	" 10 0
2270	Bougies of Reinald or other manufacturers, in boxes of one dozen, per dozen boxes - - -	— 2 0
2275	Boxes of reagents for chemical, pharmaceutical, metallurgical, and medical laboratories, with weight of package - - -	Kilo. 0 30
2279	Cantharides, entire or in powder, nett weight - - -	" 3 0
2281	Capsules or gelatinous pearls and wafers containing liquid or solid medical substances, in packages of half a kilo. at least, nett weight - - -	" 6 0
2282	The same, in packages of less than half a kilo, with weight of package - - -	" 3 0
2294	Carbonate of lithyne, nett weight - - -	" 1 80
2301	Curaine (specific), in bottles of 150 grains - - -	Doz. 2 0
2309	Carmines of Florence (carmine of fine cochineal) and carmine of indigo, or other, nett weight - - -	Kilo. 12 0
2310	Carmines of cloves (red lac of cochineal) and lac of other colours, excepting litmus (blue lac), nett weight - - -	" 1 0
2312	Caseine, nett weight - - -	" 4 0
2313	Castoreums, complete, nett weight - - -	" 10 0
2314	" in powder, nett weight - - -	" 12 0
2322	Medicinal cerates in lesser packages of less than 500 grms. with weight of package - - -	Kilo. 1 0
2328	Cyanide of iron or Prussian blue, nett weight - - -	" 0 25
2340	Chloralamide, nett weight - - -	" 3 0
2341	Hydrated chloral in cakes or crystals and similar products not denominated, nett weight - - -	" 2 0
2344	Chlorodyne, of Brown or other manufacturers, in bottles up to 30 grms. - - -	Doz. 1 50
2345	Chlorodyne, in bottles up to 60 grms. - - -	" 2 25
2346	Hydrochlorate of quinine, nett weight - - -	Kilo. 8 0
2373	Congeaers or refrigerators, Class 2, those refrigerating by use of ice or a frigorific mixture made of any material, gross weight - - -	" 0 15
2374	Cones of menthol with weight of packages - - -	" 6 0
2378	Coraline or Corsican moss, nett weight - - -	" 0 25
2381	Cuttings of bark, entire, of any kind, nett weight - - -	" 0 50
2382	The same in powders, nett weight - - -	" 0 70
2390	Creosote, white or yellow, nett weight - - -	" 2 0
2395	Chloral croton, nett weight - - -	" 5 0
2403	Dermatol (subgalate of bismuth), nett weight - - -	" 12 0
2407	Diastase or maltine, nett weight - - -	" 12 0
2415	Elixir of pepsine, papaine, peptone, and other digestive elixirs and wines as that of Tisy, in bottles up to 360 grms. - - -	Doz. 3 0
2419	Polybromurated elixirs, of Baudry, or other manufacture in bottles up to 360 grms. - - -	" 4 0
2422	Emulsion of tar in bottles up to a litre - - -	" 3 0
2427	Plasters of any composition in cylinders, nett weight - - -	Kilo. 1 0

No.	Articles.	Rate.
		Dols. cts.
2429	Adhesive plasters made on cloth of silk pure or mixed, (court plaster) up to 25 centimetres wide - -	Metre 0 50
2430	The same (cut in sheets), with weight of package - -	Kilo. 5 0
2431	Porous plasters (perforated), with weight of package - -	„ 2 0
2437	Ergotine and extract of rye, nett weight - -	„ 5 0
2458	Specifics of secret composition not denominated nor similar to those denominated, with weight of package - -	„ 2 0
2459	The same of declared composition neither denominated nor similar to those denominated, with weight of package - -	„ 1 50
2490	Eugenol, sulphonal, and hydroquinone, nett weight - -	„ 3 0
2501	Extract of malt, pure or mixed with other medicinal substances in bottles up to half kilo. - -	Doz. 4 0
2502	The same in bottles up to 250 grammes - -	„ 2 25
2508	Ethereous extract of male fern - -	Kilo. 16 0
2519	Flowers of lavender, gross weight - -	„ 0 15
2532	Fruit preparations, purgative, or Tamar-Indian, of Grillon or Julien or other makers, in bottles of at least half a kilogramme, nett weight - -	„ 4 0
2533	The same in bottles or boxes of up to 12 fruits - -	Doz. bot. 2 0
2551	Gotaries (instruments for counting drops) of any form or system - -	Doz. 0 50
2552	Sugar plums, comfits, and froth of santonine, in bottles of at least 500 grammes, nett weight - -	Kilo. 1 50
2555	Dosiometric granules in bottles and tins of 250 grammes at least, nett weight - -	„ 8 0
2556	The same in package of less than 250 grammes, with weight of package - -	„ 6 0
2557	Homeopathic granules or globules, saccharine, nett weight - -	„ 3 0
2558	Homeopathic granules or globules, active or impregnated, in packages of 250 grammes at least, nett weight - -	„ 6 0
2559	The same in packages of less than 250 grammes, with weight of package - -	„ 5 0
2575	Hemoglobin, nett weight - -	„ 4 0
2592	Hypophosphates and phosphates of ammonia, lime, iron potash, and soda, nett weight - -	„ 4 0
2595	Medicinal leaves and herbs, with weight of package - -	„ 0 30
2599	Indigo, nett weight - -	„ 0 20
2605	Hypodermic injections not denominated liquid and in tablets or other forms, with weight of package - -	„ 10 0
2606	Iodine, iodol, iodoform, and other chemical compounds of iodine, not denominated, nett weight - -	„ 5 0
2607	Iodides of sulphur, iron, lead, potash, sodium, and zinc, nett weight - -	„ 3 0
2608	Iodide of starch, nett weight - -	„ 3 0
2609	Iodide of mercury (proto and deuto), nett weight - -	„ 5 0
2616	Soap of lead (simple plaster) of small rolls, nett weight - -	„ 1 0
2619	Jellies, gelatines, and pastes, medicinal, not denominated, with weight of package - -	„ 1 0
	Reuter's curative syrup, with weight of package - -	„ 0 75
2635	Blue lac or litmus, nett weight - -	„ 0 50
2639	Lactopeptin, nett weight - -	„ 12 0
2669	Feeding-bottles, complete, or with their respective tubes and nipples - -	Dczen 2 0
2678	Machines and apparatus for aerated water, second class Letter B. Bottles, and syphons of glass of any system or mark, for containing and carrying the aerated water for retail sale, gross weight - -	Kilo. 0 05
2703	Nitrates of ammonia, baryta, copper, strontia, iron, lead, and zinc, nett weight - -	„ 0 30

No.	Articles.	Duty.
		Dols. cts.
2706	Nitrate of silver crystallised and melted, in cylinders, pure or mixed with other nitrates, net weight - -	Kilo. 16 0
2715	Opium, in paste, nett weight - - -	" 7 0
2716	Opium, in powder, nett weight - - -	" 9 0
2731	Pancreatine, pepsine, papaine, papayotine, bromeline, and other digestive ferments in pure state or mixed with other substances, with weight of package -	" 4 0
2737	Medicinal paper, not specified, with weight of package -	" 2 50
2751	Compressed tablets for hypodermic injections, with weight of package - - -	" 10 0
2752	Pastilles of secret composition (specifics of all kinds in form of pastilles), not denominated, with weight of package - - -	" 1 50
2757	Pectine or vegetable jelly, pectic acid, and pectates of ammonia, potash, and soda, with weight of package -	" 1 0
2758	Pectoral of anacahuited, simple or composite, in bottles containing up to 250 grammes - -	Doz. 3 0
2776	Pills, balls, sugar pills (with the exception of those of santonine granules or globules (with exception of dosimetric and homeopathic) of any manufacture, whether or not mixed or carried with inert substances, in packages of 250 grammes at least, nett weight -	Kilo. 6 0
2777	The same in packages of less than 250 grammes, with weight of package - - -	" 3 0
2780	Pilocarpine and its salts, nett weight - - -	Grains 0 10
2789	Aperient powders : Tarrant's and others similar, such as Eno's fruit salt, Chanteau, Seidlitz, in powder or granulated, in bottles, up to 200 grammes - -	Doz. 4 0
2791	Disinfectant powders (sawdust, gypsum, or other similar substances impregnated with picric acid or mineral creosote, impure), gross weight - - -	Kilo. 0 5
2795	Medicinal powders, official or of known composition such as Looch's white powders, and those of theriaca or theriacal powders, with weight of package - -	" 2 0
2806	Pots, bottles, and boxes of glass or china, without ornaments, adapted for pharmaceutical uses, and capacity not exceeding 250 grammes - - -	" 0 10
2842	Salts composed expressly for preparing artificial mineral waters, as those of Vichy, Apollinaris, &c., net weight -	" 3 0
2880	Salts of quinine, not specified, net weight - - -	" 8 0
2851	Salicylamide, somnal, thyol and creosotal, net weight -	" 4 0
2853	Salicylate of bismuth, net weight - - -	" 5 0
2859	Salicylate of quinine, net weight - - -	" 8 0
2861	Salipyrine (salicylate of antipyrine), alfol (salicylate of naphthol), betol or salinaftol (salicylate of naphthol) and cresalol (salicylate of creosol), net weight - -	Grms. 0 02
2863	Santonine, in crystals or powder, net weight - -	Kilo. 16 0
2887	Solution of Larozyène's, in bottles up to 360 grammes -	Doz. 4 0
2891	Van Buskirk's zozodant, with weight of package - -	Kilos. 1 0
2892	Sozoidol, alkaline or metallic, net weight - - -	" 12 0
2898	Sulphates of alumina, simple or double (alums), of copper, ordinary iron, magnesia, and soda, with weight of package - - -	" 0 06
2915	Sulphur of antimony hydrated or mineral kermes, diaphoretic antimony and yellow sulphur of antimony, net weight - - -	" 2 75
2932	Tartrate of quinine, net weight - - -	" 8 0
2939	Tinctures of alkaloids (solutions of alkaloids or their salts in water and alcohol), with weight of package -	" 10 0
2944	Homeopathic tinctures, and solutions of same, with weight of package - - -	" 6 0

No.	Articles.	Duty.
		Dols. cts.
2947	Ordinary turpentine, gross weight - - -	Kilo. 0 01
2950	Tricopherus, Barry's and Kemp's Oriental Tonic, or their imitations, and similar preparations for the hair, in common bottles, with weight of package - - -	„ 0 75
2951	Tripoli, in <i>powder or bricks</i> , nett weight - - -	„ 0 15
2955	Vulcanised rubber tubes, or not vulcanised, up to 2 centimetres of exterior, nett weight - - -	„ 2 00
2956	Tubes of rubber <i>or glass</i> for drainage, with weight of package - - -	„ 2 00
2958	Ointments and medicinal pomades of any composition and mark, in packages of less than 500 grammes, with weight of package - - -	„ 1 00
2959	Medicinal ointments, in packages of 500 grammes at least, nett weight - - -	„ 1 50
2964	Valerianates of ammonia, in solution or dry, of bismuth, of iron, or zinc, nett weight - - -	„ 6 00
2965	Valerianate of quinine, nett weight - - -	„ 8 00
2966	Vaseline (petroleum, petroleum jelly), white or yellow, in tins of 500 grammes, or more, nett weight - - -	„ 0 50
2967	Vaseline in tins, pots, bottles, or other package of less than 500 grammes, with weight of package - - -	„ 0 35
2977	Wines of San Juan and of San Rafael, and other similar natural wines advertised as medicinal, and in bottles of a litre or half a litre - - -	Litre 0 25

XVI.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

SWEDEN.

Increased Import Duty on Bacon.

A despatch, dated March 22 last, has been received at the Foreign Office from Mr. G. E. Welby, Her Majesty's Secretary of Legation at Stockholm, stating that at a recent meeting of the two Chambers for the purpose of voting together on various questions on which their respective majorities were not in accord, a majority of 196 to 167 decided in favour of raising the present duty on smoked bacon from 25 öre to 30 öre per kilogramme, and also from 10 to 20 öre per kilogramme that on bacon of other kinds.

This increase has been made in the interests of the small peasant proprietors in the southern provinces of Sweden, but it has met with considerable opposition owing to the large amount of bacon consumed by the inhabitants of the more northern provinces and their preference for the quality of the bacon imported from the United States.

These new rates of duty will come into force on 1st January next, unless the Government should in the meanwhile decide otherwise.

NETHERLANDS.

Free Entry of Sulphuric Ether.

A despatch, dated 25th March last, has been received at the Foreign Office from Sir Horace Rumbold, Her Majesty's Minister at The Hague, enclosing translation of a law published in the "Official Gazette" of March 25 last, by which sulphuric ether, used in the manufacture of gilding for the decoration of porcelain or earthenware, is exempted from import duties under certain conditions, which apply exclusively to the importer.

The translation of the above law is as follows:—

Art. 1. Sulphuric ether used in the manufacture of gilding for the decoration of porcelain or earthenware is admitted free of duty under the following conditions:—

Art. 2. The person desiring the exemption from duty should address himself to the Minister of Finance, with a statement of the parish in which the gilding manufactory is situated, the cadastral section and the number of the manufactory, and the quantity of sulphuric ether estimated as required for one year. The Minister determines the quantity which can be laid in without duty for each year, as well as the minimum of each supply.

Art. 3. When the exemption from duty for the importation of sulphuric ether is demanded, notice of the demand should be made in the declaration prescribed by Art. 120 of the General Law of August 26, 1822 (*Staatsblad* No. 38). On that declaration, after security for the duty has been lodged, a certificate will be granted in which the said demand is stated.

The fluid will be transported to the manufactory under seal.

Art. 4. The storage of the fluid in the factory will take place in the presence of Customs and Excise officials, in whose presence one kilogramme of balm of sulphur ("zwavelbalsem") will be mixed with the sulphuric ether in the proportion of one to ten. The officials will place on the certificate a declaration respecting the quantity laid in and the mixture of the sulphur. The balm of sulphur required for the mixture will be supplied by the person requiring the exemption.

Art. 5. The mixture may be used for no other purpose than for the manufacture of gilding and may not be sold out of the manufactory. No distilling apparatus is permitted in the factory.

Art. 6. The tax collector, in whose district the factory is situated, will keep an account of the exempted sulphuric ether which has been laid in with the manufacturer. If a larger quantity is laid in than that for which exemption is allowed for the current year the certificate will not be passed, and the duty on the excess quantity will be recovered from the security already lodged.

Art. 7. The person receiving the exemption will deliver to the office of the collector of taxes at the end of the year a signed and written statement of the quantity of sulphuric acid laid in exempt from duty, which remains unused at the end of the year. This quantity will be considered as the first provision in the next year and as such will be entered in the account mentioned in Art. 6. As long as the above-mentioned statement has not been delivered, no new provision will be permitted.

Art. 8. The officials are authorised, under instructions from the collector of taxes, to examine the surplus sulphuric ether remaining in the factory within eight days after the expiration of the year whether the statement mentioned in the preceding article has been delivered or not.

Art. 9. In case of fraud or attempted fraud, or on account of the non-fulfilment of the regulations contained in this decree, the Minister of Finance can withdraw from the manufacturer the privilege of further free importation of sulphuric ether.

The Administration of the New Customs Law.

According to the "Handels Museum" of January 23, a number of firms established at Marien-Gladbach who are interested in the export trade to Holland, have addressed to the German Imperial Chancellor a memorial complaining of the way in which the new Dutch Customs regulations, which came

into force on the 1st August last, are administered by the Dutch officials. The memorialists also request that these regulations may be carried out in accordance with the commercial treaty between Germany and Holland. The following statements are set forth in the memorial:—That before the new regulations came into force goods imported into Holland, which in the opinion of the Customs officials were declared at too low a value, were seized and their declared value, increased by 10 per cent., paid to the importers. This procedure is in accordance with the regulations customary in other countries where import duties are levied *ad valorem*. From the 1st of August last, however, the Dutch Customs officers ceased to make use of this right of seizure, but, if the goods appear to them to have been declared at too low a figure, they require the amount to be increased. In case the importer does not consent to this the consignment is detained and handed to a committee to ascertain its value. If this committee decide that the goods have been undervalued, 10 times the duty on the deficiency has to be paid in addition to the ordinary duty. No objection would be raised to this procedure if the goods were fairly valued by the officers and the committee. Cases in which the value has thus been fixed clearly show, however, that these officials have not a sufficient technical knowledge.

It is also declared that the composition of the committee is unsuitable, as it consists of a Customs inspector as chairman and of two members. The committee alone is authorised to call in experts, whilst in other countries where *ad valorem* duties exist each side can nominate an expert, the interests of each party being thus properly guarded. But the experts called in by the committee have in most cases an insufficient technical knowledge of the merchandise, and it is chiefly owing to this that their valuations do not agree with the invoices of the sale of the goods, and that the committee, although legalised accounts are placed before them, usually place a higher value on the goods than is shown in the invoices; in support of these statements the memorialists adduce numerous individual cases. It is claimed that in consequence of this procedure the importer pays more than the 5 per cent. import duty provided for in the commercial treaty between Germany and Holland, as the tenfold duties imposed on the increase of valuation determined by the committee, bring the total duties up to from 15 to 50 per cent. *ad valorem*, even on the valuation of the committee. The memorialists also complain that the proceedings of the committee are very dilatory and usually cause a delay of from a fortnight to a month. These delays cause great disadvantages to the exporter as well as the consignee, as in many cases the latter refuses to accept the goods and they remain on the exporter's hands. The Marien-Gladbach Chamber of Commerce has decided to support this memorial before the Imperial Chancellor and the Minister of Commerce.

FRANCE.

Imports of Cereals.

The "Journal Officiel" of the 20th March last publishes the following Presidential Decree relative to the entry of cereals into France.

Art. 1. Grain (*blés tendres*) for the manufacture of starch may be temporarily imported free of duty under conditions set forth in the law of 5th July 1836.

Art. 2. Every 100 kilogs. of corn must be represented by 55 kilogs. of starch.

Art 3. Re-exportation or storage of starch must be effected within six months.

Art 4. Declarations of temporary importation as well as declarations of re-exportation or storage must be made in the name and on account of the manufacturers.

Art 5. The operations for import and export can at present be carried out only at Marseilles, Paris, and Lyons. But the Financial Minister has power to authorise such operations in other towns where the customs has establishments, if manufactories of starch have been established in such places.

Tariff Decisions.

A despatch, dated March 6 last, has been received from the Foreign Office, enclosing copy of a French Customs circular to the following effect:—

"Thorium salts, especially nitrate or azotate, solutions of which are employed in the preparation of incandescent gas-burners, belong to the class 'Chemical Products not denominated, other than of an alcoholic basis,' and pay a duty of 5 per cent. *ad valorem*. The estimation of the value of these products having given rise to many disputes, the Departments of Trade and Finance have decided to convert the duty of 5 per cent. into an equivalent specific rate, and considering that the value of thorium salts, once 2,000 frs. per kilog. is now only about 400 frs. on account of recent discoveries of extensive deposits of thorium ore, it has been decided to substitute for the duty of 5 per cent. *ad valorem*, a specific rate of 20 frs. per kilog."

Warehouse Charges, &c. at Rouen and Dunkirk.

A despatch, dated April 1 last, has been received from the Foreign Office enclosing extract from the "Journal Officiel" containing two French Presidential Decrees modifying the charges leviable for the use of certain warehouses at Rouen and of a careening slip, &c. at Dunkirk.

Tonnage Dues at Fécamp.

A despatch, dated April 7 last, has been received from the Foreign Office enclosing extract from the "Journal Officiel" of

April 1 last, containing text of a French Presidential Decree modifying the tariff of tonnage dues levied at the port of Fécamp.

The above can be seen on application at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, between the hours of 11.0 and 5.0

FRANCE—ALGERIA.

Tariff Decisions.

A despatch, dated February 6, has been received from the Foreign Office, enclosing copy of a French Customs circular containing the following articles of the law of December 28 last:—

Art. 11. The following goods are liable on importation into Algeria to two-thirds of the duty levied in the Metropolitan tariff:—Sugars (including French Colonial and Metropolitan sugars), pepper (ground and whole), pimento (ground and whole), cinnamon, cassia lignea, nutmegs (whole and shelled), mace, vanilla, cloves (in bunches and separate).

Art. 12. The products of extra European countries included in Table A. of the law of 17th July 1867 are liable in Algeria (when imported from European depôts) to the surtax of the Metropolitan tariff. Foreign sugars remain liable to the surtaxes applicable to foreign sugars in the mother country.

Art. 13. The statistical tax, established by Article 3 of the law of 22nd January 1872, will be levied in Algeria.

The following are exempted from this tax:—1. Products imported from France into Algeria and *vice versa*. 2. Goods exempted from Customs duty by Table D. of the law of 17th July 1867.

Art. 14. Quay dues, such as are established by Article 6 of the law of 30th January 1872, will be levied in Algerian ports.

Navigation between France and Algeria will be exempted from this duty.

The laws of the 20th March 1875 and 12th March 1877 are abrogated.

PORTUGAL.

Commercial Arrangements with Russia the Netherlands and Norway.

The Board of Trade have received through the Foreign Office copies of the Portuguese white books of the Legislative Session of 1896, containing the commercial arrangements concluded by Portugal last year with Russia, the Netherlands, and Norway, respectively. The following statement, which has been prepared at the Board of Trade, shows the modifications in the Customs tariffs of the countries named which will come into operation as soon as the different arrangements have been ratified.¹

1. *Modifications in the Customs Tariff of Portugal.*

Articles.	Rates of Import Duty at present leviable under the Tariff.		Rates which will be leviable when new Arrangements come into force.	
A.—UNDER COMMERCIAL CONVENTION WITH RUSSIA.	Reis.		Reis.	
Hides or skins, tanned - - -	Kilog.	300	Kilog.	240
Do. do., curried—				
Morocco leather - - -	"	600	"	480
Cow hides, dyed or blackened - -	"	500	"	360
Varnished, smooth or grained - -	"	500	"	400
Staves - - -	3 % <i>ad val.</i>		1 2 % <i>ad val.</i>	
Wooden hoops for casks - - -	3 % <i>ad val.</i>		1 2 % <i>ad val.</i>	
Flax and hemp, raw - - -	Kilog.	9	Kilog.	5
Common wood—				
In beams, joists, and planks, more than 75 mm. in thickness, and at least 25 cm. in width - - -	Cub. met.	1,400	Cub. met.	1,200
Sawn, in planks or deals, &c., of a thick- ness of more than 35, but not more than 75 mm. - - -	"	2,800	"	2,500
Tar and mineral pitch - - -	Ton	100	Ton	10
Mineral oils, light, for illuminating purposes (density, 0·780 to 0·820; point of ignition from 37° to 49°) - - -	Kilog.	67	Litre	46
Do., do., average (density 0·820 to 0·860; point of ignition from 50° to 150°) - -	"	60	Kilog.	52
Tea - - -	"	1,000	"	900
Codfish, in any condition - - -	"	29	"	34
Cheese - - -	"	300	"	*220
Candles of all kinds, except those made principally of paraffin - - -	"	120	"	90
B.—UNDER COMMERCIAL DECLARATION BETWEEN PORTUGAL AND THE NETHERLANDS.				
Jute damasks - - -	Kilog.	1,000	Kilog.	†1,000
Coarse jute cloth and packing cloth of jute -	"	150	"	†150
Coarse cloth and packing cloth of linen or of jute mixed with linen or hemp - -	"	210	"	180
Jute tissues not specially tariffed, unbleached or bleached - - -	"	650	"	†650
Bags or sacks made of coarse jute cloth or of jute packing cloth - - -	"	450	"	225
Sugar refined by the Portuguese method and sugar above No. 20 Dutch standard -	"	145	"	†145
Sugar not otherwise specified - - -	"	120	"	†120
Cheese - - -	"	300	"	200
Tin manufactures - - -	"	200	"	†200
Tin capsules for bottles - - -	"	200	"	160
Pipes of clay or chalk without ornamenta- tions or incrustations of other materials -	"	500	"	80
Candles of all kinds, except those made principally of paraffin - - -	"	120	"	90

* A lower rate (200 reis. per kilog.) is provided for by the Declaration between Portugal and the Netherlands.

† No change in duty is to be made in these cases, but the maintenance of the existing rates is guaranteed for the duration of the Declaration.

1. Modifications in the Customs Tariff of Portugal—continued.

Articles.	Rates of Import Duty at present leviable under the Tariff.	Rates which will be leviable when new Arrangements come into force.
C.—UNDER COMMERCIAL TREATY WITH NORWAY.		
Fish manure and other natural manure for agricultural purposes - - -	Reis. Kilog. 0·3	Reis. Kilog. *0·3
Wood-pulp for paper making - - -	1	*1
Codfish, salted or dried - - -	39	*39
Fish powder - - -	200	20
Condensed milk - - -	200	*200
Horseshoe nails, polished - - -	150	100

2. Modifications in the Customs Tariff of Russia.

Articles.	Rates of Import Duty at present leviable under the Tariff.	Rates which will be leviable when new Arrangements come into force.
Cork, unwrought - - -	Rbbs. cop. Poud 0·12	Rbbs. cop. Poud 0·10
Cork, wrought— Partly manufactured (in sheets, cubes, &c.)	2·00	1·60
Manufactured - - -	3·00	2·40

The Declaration between Portugal and the Netherlands does not provide for any alterations in the Customs duties leviable on goods imported into Holland.

3. Modifications in the Customs Tariff of Norway.

Articles.	Rates of Import Duty at present leviable under the Tariff.	Rates which will be leviable when new Arrangement comes into force.
Cork, in the rough - - -	Kron. öre. Free.	Kron. öre. *Free.
Corks for bottles, not mounted, and cork soles - - -	Free.	*Free.
Other manufactures of cork - - -	Kilog. 0·13	Kilog. *0·13
Cooking salt, raw and refined - - -	Free.	*Free.
Wine (including quinine and other medicinal wines):		
† Testing not above 23 degrees—		
In casks or jars - - -	Kilog. 0·11½	Kilog. 0·11½
In bottles - - -	Litre 0·11½	Litre 0·11½
‡ Testing above 23 but not above 25 degrees—		
In casks or jars - - -	Kilog. 0·36	Kilog. 0·36
In bottles - - -	Litre 0·36	Litre 0·36

* No change in the rates of duty is to be made in the above cases, but the maintenance of the existing rates on the articles mentioned is guaranteed for the duration of the treaty.

† This is a raising of the limit from 21 to 23 degrees.

‡ This is a raising of the lower limit from 21 to 23 degrees.

Tax on the Manufacture or Consumption of Sugar.

A despatch, dated March 27 last, has been received at the Foreign Office from Sir H. G. Macdonell, Her Majesty's Minister at Lisbon, transmitting copies and translation of a law dated 26th March last, levying a tax on the manufacture or consumption of sugar, either clarified or refined, at the rate of 15 reis per kilogramme on any sugar which may not be stored in any Custom-house in Portugal or in the adjacent islands, or else shipped *in toto*, or actually at sea, for any Portuguese ports, on the 27th March 1896.

The said tax will be kept in deposit pending the decision of the Cortes with regard to the Bill laid before them on the 16th March last for the imposition of the tax in question.

SPAIN.

Consular Dues.

With reference to a notice published on p. 310 of the March number of the "Board of Trade Journal," a despatch, dated March 17th last, has been received from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, stating that the circular to Spanish Custom houses has now been published in the "Boletin Oficial de las Adunas."

ITALY.

Importation of Maize.

According to a circular of the Minister of Finance, published in the "Bollettino di Notizie Commerciali," maize is now permitted to be imported, both by land and sea, into Italy from European States if it be accompanied by a certificate of good condition signed by the local authorities of the place whence it comes instead of by the Consuls; that is, when the maize itself does not arrive damaged or spoiled.

GREECE.

Modification of Greek Currant Bill.

With reference to an article on the Greek Currant Bill on page 133 of the number of the "Board of Trade Journal" for August 1895 a despatch, dated March 10 last, has been received at the Foreign Office from Mr. E. Egerton, Her Majesty's Minister at Athens, stating that the Greek Chamber has passed a Bill modifying Article 2 of the law of July last respecting the tax on currants.

Exporters of currants of the 1895 crop will no longer have the option of paying the tax of 15 per cent. in money or in kind as heretofore, but will be required to do so in kind.

The following is a translation of the article of the law as now modified :—

“Currants exported within the time specified in the preceding article are liable to a tax, payable in kind; this tax in kind is fixed at 15 per cent. for the crop of the year 1895, and it is payable in currants either of the same kind as those actually exported or different in respect to quality or place of production at the choice of the taxpayer.

“Currants not perfectly dry or mixed with other substances, or in a crushed condition, will not be accepted; currants not well cleaned will likewise not be accepted.

“The force of the present law begins from the day following the day of its insertion in the Official Gazette.”

UNITED STATES.

The following decisions relating to the classification of articles in the Customs tariff and the application of the Customs laws of the United States have recently been given by the Customs authorities in that country :—

French mustard, ground, preserved, and prepared with other ingredients is dutiable at 25 per cent. *ad valorem* under paragraph 234.

Small cakes of paint, about 1 by $\frac{1}{2}$ by $\frac{1}{4}$ inch in dimensions, stamped “Chinese white” are dutiable at 1 cent per pound under paragraph 47.

Sheet gelatine, coloured, is assessed for duty under paragraph 19, which provides for “gelatine, glue, isinglass or fish glue, and prepared fish bladders or fish sounds” at 25 per cent. *ad valorem*.

Music rolls for mechanical pianos, consisting of sheets or rolls of perforated paper, especially designed and exclusively used in the operation of mechanical pianos, are to pay duty at 25 per cent. under paragraph 326 $\frac{1}{2}$.

Wire rope, with hemp core, is dutiable at 1 $\frac{1}{4}$ cents a pound, and 1 cent a pound additional upon the total weight of the rope.

Antimacassar cloth, consisting of woven fabrics of cotton about 18 ins. in width, with an open woven centre resembling netting, is assessed for duty under paragraph 264, at 35 per cent. *ad valorem*.

Renaissance or Battenburg linens are entitled to entry at 35 per cent. *ad valorem* under paragraph 277.

Sachet powder is dutiable at 40 per cent. under paragraph 61 as an article of perfumery.

Lifebuoy royal disinfectant soap is to be assessed for duty at 35 per cent. under paragraph 63.

Scrap mica, consisting of pieces of mica of irregular shape, varying in sizes from 2 or 3 inches square to areas still smaller, is dutiable at 20 per cent. *ad valorem* as mica under paragraph 167 $\frac{3}{4}$.

Lemon squash, consisting of lemon or lime juice, containing sugar, in glass bottles, and used to make lemonade by the addition of water to suit the taste, is assessed for duty at 20 per cent. *ad valorem* under section 3 of the Tariff Act of 1894, as an unenumerated manufactured article.

Calcined magnesite, or furnace cement, is dutiable at 10 per cent. *ad valorem* under paragraph 79 as "cement" other than Roman, Portland, or hydraulic cement.

Asbestos fibre is entitled to free entry as asbestos in an unmanufactured state under paragraph 388.

Crocus is to pay duty as a colour at 25 per cent. under paragraph 61, Act of October 1890.

Drawback on Sugar and Syrup.

A despatch has been received from the Foreign Office enclosing copies of the following circulars issued by the Treasury Department of the United States with reference to drawbacks on sugar and syrup.

On the exportation of refined sugars made wholly from imported raw sugars, and on the exportation of syrups made wholly from imported raw cane sugars, drawback will be allowed equal in amount to the duties paid on the imported sugars so used, less the legal deduction of 1 per cent.

When the exported sugars are "hard refined" of standard test, commercially known as loaf, cut loaf, cube, granulated, crushed, or powdered, and are made wholly from imported raw cane sugars, the amount of drawback shall be determined by allowing for each 100 lbs. of the exported article, the duties paid on the respective grades and quantities of material used, as indicated in the following schedule:—

Test of Raw Sugar used.		Allowance for 100 Lbs., Hard Refined.	
Degrees.	Lbs.	Degrees.	Lbs.
100	100·00	87	124·27
99	101·87	86	126·14
98	103·73	85	128·01
97	105·60	84	129·88
96	107·47	83	131·74
95	109·34	82	133·61
94	111·20	81	135·48
93	113·07	80	137·35
92	114·94	79	139·21
91	116·81	78	141·08
90	118·67	77	142·95
89	120·54	76	144·82
88	122·41	75	146·68

When "soft refined" sugars made from imported raw cane sugars are exported, the quantity of "crystallisable sugar" contained therein shall be determined by reference to weight and polariscope test of the exported article, and drawback allowance

shall be based on the quantity of "crystallisable sugar" so found, as herein-before provided for a like quantity of "hard refined" sugar.

When the "hard refined" sugars herein-before described are made wholly from imported raw beet sugars, the dutiable value of the material used for each pound of the exported article shall be determined by dividing the dutiable value of 1 pound of the material used by the decimal denoting the "net analysis" of such material.

The amount of drawback which may be allowed on the exported syrup product of raw cane sugars shall be determined by allowing for each gallon of the exported article, valued at 5 cents in condition as "thrown" from the "centrifugal," the duties paid on the respective grades and quantities of material used as indicated in the following schedule :—

Test of Raw Sugar used.	Allowance for One Gallon Syrup.	Test of Raw Sugar used.	Allowance for One Gallon Syrup.
Degrees.	Lbs.	Degrees.	Lbs.
99	1.36	86	1.56
98	1.37	85	1.58
97	1.39	84	1.60
96	1.40	83	1.62
95	1.41	82	1.64
94	1.43	81	1.66
93	1.44	80	1.68
92	1.46	79	1.70
91	1.48	78	1.72
90	1.49	77	1.74
89	1.51	76	1.77
88	1.53	75	1.79
87	1.55		

The value of the syrup in condition as thrown from the centrifugal shall be declared by the manufacturer on the drawback entry, which declaration shall be verified by the collector, and if the declaration so verified shows a value per gallon above or below 5 cents, the allowance shall be determined by increasing or reducing the schedule allowance in proportion to the increase or reduction above or below the 5 cents per gallon, but in no case shall the allowance be based on a value of syrup exceeding 8 cents per gallon without special authority from the Department.

For fractional tests of the raw cane sugar used in the manufacture of either refined sugar or syrup, the allowances of quantity of material shall be computed in proportion to the schedule allowances for the degrees next above or below such fractional test.

The quantities of sugar and syrup exported shall be ascertained by United States weighers and gaugers, respectively, and samples shall be taken as ordered by the collector, to be submitted to the appraiser for report of polariscope test and such other expert inspection as may be requisite.

On requisition of collectors, appraising officers shall furnish polariscope tests, "net analyses," and other conditions of valuation of raw sugars not found on the import invoice for use in liquidation of drawback entries.

Bounties other than those on Shipping.

With reference to a notification on page 187 of the February number of the "Board of Trade Journal," a despatch has been received from the Foreign Office, containing copy of a report, dated January 21 last, by Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, on the question of sugar bounties in the United States. Sir Julian Pauncefote states that in a previous despatch he had reported that, by the law of the 2nd March 1895, granting bounties in certain cases, the producers of sugar had partially recovered the benefits of which they were deprived in 1894 by the repeal of the Tariff Act of the 1st October 1890, generally known as the McKinley Act, which had granted a bounty from the 1st July 1891 until the 1st July 1905.

It now, however, appears that the sugar producers have not profited by the above Act of 1895, because, under a decision of the Court of Appeal of the district of Columbia, holding that the bounty originally given by the Act of 1890 was unconstitutional, the Comptroller of the Treasury has been unable to pay out any money, but has referred the question of the constitutionality of the bounty provision to the Court of Claims, from which an appeal may be taken to the Supreme Court of the United States.

The reduced bounty, which has not been paid, is "eight-tenths" of a cent per lb. on sugar testing not under 80 degrees, and not "one-tenth" of a cent per lb.

The Comptroller's decision in the above case presents features of remarkable interest both in regard to bounties in general and to the restrictions placed by the Constitution on the powers of the Legislature in respect of taxation and grants of money.

It is stated that, except in one doubtful instance, no direct money bounty Act was ever passed by Congress until 1890, when the McKinley Act granted bounties on sugar.

Free Entry of Theatrical Scenery, Properties, &c.

Copies of the following circular, dated March 10 last, with regard to the free entry of theatrical scenery, properties, and apparel have been received at the Foreign Office from the Treasury Department of the United States:—

To Collectors and other Officers of the Customs:—

Paragraph 596 of the Act of August 28, 1894, provides as follows:—

"Professional books, implements, instruments, and tools of trade, occupation, or employment in the actual possession at the

time of persons arriving in the United States ; but this exemption shall not be construed to include machinery or other articles imported for use in any manufacturing establishment, or for any other person or persons, or for sale, nor shall it be construed to include theatrical scenery, properties, and apparel, but such articles brought by proprietors or managers of theatrical exhibitions arriving from abroad for temporary use by them in such exhibitions and not for any other person and not for sale and which have been used by them abroad shall be admitted free of duty under such regulations as the Secretary of the Treasury may prescribe ; but bonds shall be given for the payment to the United States of such duties as may be imposed by law upon any and all such articles as shall not be exported within six months after such importation : Provided, that the Secretary of the Treasury may in his discretion extend such period for a further term of six months in case application shall be made therefor."

The following regulations will hereafter govern the importation of theatrical scenery, properties, and apparel, under the foregoing paragraph.

The invoice filed with the entry of the articles must describe them in such manner as to insure their identification at the time of export, and the values thereof must be duly set forth. Entries of such merchandise will be made under the general provisions of Article 349 of the Customs Regulations of 1892, with the necessary modifications to meet the case.

The exportation of the merchandise covered by said bond shall be made only at the port of importation and shall be on regular withdrawal and under Customs supervision.

The bond account of such transactions shall be kept separate and distinct from the ordinary warehouse account.

At the expiration of the bond it shall be sent to the district attorney for collection, unless the required proofs of exportation shall have been furnished by the importer or consignee.

BRAZIL.

Tax on Bills of Lading

A communication, dated March 4 last, has been received at the Foreign Office from Mr. E. Kanthack, Her Majesty's Consul at Para (Brazil), stating that the decree exposing shippers of goods on "Bills of Lading to order," to an annual tax of Rs. 300 \$000 refers only to goods of Brazilian origin shipped in Brazil to Para, and is intended as a precautionary measure to prevent small tradespeople from evading the payment of local commercial taxes.

The decree referred to is not to be applied to goods shipped to Para from foreign countries.

BRITISH INDIA.

Exemptions from Duty.

By Customs circular, dated 14th February last, and in exercise of the power conferred by section 23 of the Sea Customs Act, VIII. of 1878, the Governor General in Council is pleased to exempt from import duty the under-mentioned articles used in the manufacture of cotton, namely:—

Bobbins, warping; forks for looms; healds; heald cords; heald knitting needles; laces; lags for dobbies; needles for dobbies; pickers, buffalo and others; picking bands, levers, and sticks, over and under; reed pliers; reeds; shuttles (for power-looms); springs for looms; strappings; weft forks; so far as the said articles are liable to duty under Schedule IV. (Import Tariff) of the Indian Tariff Act, VIII., of 1894, as amended by Acts XVI. of 1894 and III. of 1896.

Orders may also be issued to the collectors of Customs to pass, without levying import duty, the articles specified in the subjoined list, when such articles are imported by the owner of a cotton-weaving mill, and are shown to the satisfaction of the collector to be intended for use in the weaving of cotton or the baling of woven cotton goods.

List of Articles to be exempted.

Aniline blue, bisulphate of soda, china clay, chloride of magnesium, chloride of zinc, Epsom salts, farina, flannel taping, Glauber salts, glutina, glycerine substitute, heald varnish, hoop iron, rivets for bales, sewing needles, sizing paste, sizing wax, soda ash, starch.

LAGOS.

Amendment of Duties on Spirits.

An Ordinance to amend the law relating to Customs duties upon spirits entering Lagos has been passed in the Legislative Council of that Colony, by which all spirits imported into the Colony are liable to a duty of 2s. instead of 1s. per Imperial gallon.

NEW PORT OF ENTRY IN SIERRA LEONE.

A despatch, dated March 30 last, has been received from the Colonial Office, enclosing copy of an Order passed at an Executive Council held at Fort Thornton, in the colony of Sierra Leone, by virtue of which Kukuna, situated at or near the Great Skarries River, is appointed a port of entry under the Sierra Leone Customs Ordinance, 1880.

TRINIDAD AND TOBAGO.

Tax in Aid of Immigration.

A despatch, dated February 7 last, has been received from the Colonial Office, enclosing copy of an "Ordinance for raising Funds in aid of Immigration for the year 1896," passed by the Legislature of Trinidad, which states—

1. There shall be raised, levied, collected, and paid for the use of Her Majesty, Her heirs and successors in aid of immigration upon the several kinds of produce herein-after mentioned, which shall have been raised or manufactured in the island of Trinidad and which shall be shipped for ports or places beyond the limits of the Colony on any day between the 1st day of January and the 31st day of December, both inclusive, of the year one thousand eight hundred and ninety-six, the several taxes, rates, and charges herein-after mentioned ; that is to say—

Articles.	Duty.
	<i>s. d.</i>
Upon all sugar:	
For every hogs-head - - - - -	5 0
For every tierce - - - - -	3 4
For every barrel or bag - - - - -	0 7½
Upon all molasses:	
For every puncheon - - - - -	1 8
For every half puncheon - - - - -	0 10
For every package less than a half puncheon - - - - -	0 6
Upon all rum:	
For every puncheon - - - - -	3 4
Upon all cocoa:	
For every bag - - - - -	1 5
Upon all coffee:	
For every bag or barrel - - - - -	1 5
Upon all cocoanuts:	
For every thousand - - - - -	0 10

2. For the purposes of this Ordinance, every cask whereof the truss is 32 inches or more shall be deemed to be a hogshead, and every cask whereof the truss is 18 inches and less than 32 inches shall be deemed to be a tierce, and every cask whereof the truss is less than 18 inches shall be deemed to be a barrel.

3. Where any such produce is contained in any package except those above mentioned, such taxes and charges shall be raised, levied, collected, and paid at the following rates, that is to say :

On sugar, at the rate of 2*s.* 9*d.* for every 1,000 pounds ;

On rum, at the rate of 3*s.* 4*d.* for every 100 gallons ; and

On cocoa and on coffee at the rate of 8½*d.* for every 100 pounds.

4. The several taxes, rates, and charges herein-before specified shall be payable by the shipper of such produce at the time of such shipment aforesaid, and the shipper thereof shall deliver to Her Majesty's Collector of Customs or Sub-Collector in Trinidad, or to the officer performing the duties of Her Majesty's Collector

of Customs in Tobago an account of the produce in such form and containing such particulars as the Governor may from time to time direct.

5. If such account is not delivered or if the taxes, rates, and charges payable in respect of such produce are not paid to the Collector of Customs, Sub-Collector, or Officer in Tobago aforesaid within 24 hours after the departure of the vessel in which the produce was shipped, or such further time not exceeding four days as the Collector of Customs may allow, the shipper thereof shall forfeit the sum of 100*l.* in addition to the amount of taxes, rates, and charges payable.

WESTERN AUSTRALIA.

Customs Duties Repeal Act, 1895.

A communication has been received at the Board of Trade from the Agent-General for Western Australia enclosing copy of the Customs Duties Repeal Act of 1895, by which the duties on the articles enumerated in the following schedule are repealed.

Schedule.

Arrowroot, sago, tapioca, cornflour, and other farinaceous foods, n.o.e.	Iron-wire netting, iron and steel fencing wire, and also standards, droppers, and staples—for fencing only.
Atlases, maps, charts, and globes.	Lead—sheet, tea, scrap, and pig.
Bags and sacks, including corn and flour sacks, gunny bags, ore bags and bagging in the piece for wool bales and sacks.	Molasses and golden syrup.
Blankets and rugs.	Oil, mineral.
Cocoa and chocolate, n.o.e.	Paraffin wax.
Cocoa in slabs.	Photographs.
Cocoa nibs.	Picture cards for school use.
Coffee (raw).	Rice, paddy, ground and meal.
Copper—wire, rod, sheet, and ingot.	Sheep dip.
Cream separators, and parts thereof.	Sugar.
Explosives of all kinds; not including powder (gun and sporting) ammunition, and fireworks.	Sulphur.
Galvanised iron (corrugated and plain).	Tea.
	Tinplates and tinned sheet iron.
	Wool bales.
	Zinc—sheet, perforated, and plain—and ingot.

XVII.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

OPENING OF NAVIGATION AT RIGA.

A despatch has been received from the Foreign Office, enclosing copy of a telegram from Her Majesty's Consul at Riga, dated March 23 last, which states that navigation at that port was then practically open, although the bay was full of drift ice.

OPENING OF THE PORT OF NICOLAIEFF.

A despatch, dated the 3rd March, has been received at the Foreign Office, from Her Majesty's Consul at Odessa, reporting as follows:—

“The port of Nicolaieff is now open for traffic, although there is still a good deal of ice in the River Boug.”

OUTPUT OF COAL AND PRODUCTION OF IRON AND STEEL IN BELGIUM, 1895.

A report, dated February 23 last, has been received at the Foreign Office from Sir F. R. Plunkett, Her Majesty's Minister at Brussels, stating that, from a tabular statement of the output of coal and production of iron and steel in Belgium for the year 1895, which was published in a recent issue of the “*Moniteur Belge*,” it appears that for the whole year 1895, the output of coal in Belgium was 20,092,412 $\frac{3}{4}$ tons, as against 20,210,175 tons in 1894, a falling-off of 117,762 $\frac{1}{4}$ tons in 1895.

The stocks of coal remaining in hand at the pits' mouth on December 31 were respectively, 647,209 $\frac{3}{4}$ tons in 1895, and 696,763 $\frac{1}{2}$ tons in 1894, showing a decrease in 1895 of 49,553 $\frac{3}{4}$ tons.

The amount of coal (excluding coke and pressed coal-dust) exported from Belgium are stated, in the commercial tables published by the Ministry of Finance, at 4,573,584 $\frac{3}{4}$ tons in 1895, and 4,467,827 tons in 1894, an increased exportation for 1895 of 105,757 $\frac{3}{4}$ tons.

France is the chief market for Belgian coal, and the exports to that country amounted to 3,561,432 tons in 1895, as against 3,503,319 tons in 1894.

The statement of the production of iron and steel in Belgium is divided into three sections, which respectively deal with smelting works, iron factories, and steel works.

The total production of pig-iron in 1895 was 816,039½ tons, as against 805,668 tons in 1894, an increase of 10,371½ tons during 1895.

Taking into account the differences in the quality of the product the above totals are distributed under the heads of "Foundry pig-iron," "Forge pig-iron," and "Steel pig-iron."

With regard to foundry pig-iron, for the whole year the production reached 84,100½ tons in 1895, and 78,844¾ tons in 1894 an increase of 5,255¾ tons in 1895.

The total annual production in 1895 of forge pig-iron was 324,444⅔ tons, as against 372,074 tons in 1894; a diminution for 1895 of 47,629⅝ tons; while of steel pig-iron it amounted to 407,494⅝ tons in 1895, and 354,749⅓ tons in 1894, an increase of 52,745½ tons.

The production of manufactured iron amounted to 446,219¼ tons in 1895, as against 446,130¾ tons in 1894, an increase of 88½ tons.

The amount of sheets and plates produced was 99,876⅓ tons in 1895, and 116,722⅞ tons in 1894, and the decrease consequently amounted to 16,846¾ tons; while as regards sundry iron goods, the figures were 346,343 tons in 1895, and 329,407¾ tons in 1894, showing an increase of 16,935¼ tons.

In the 12 months ending December 31, 1895, the amounts of cast steel produced were, respectively, 448,364¾ tons in 1895, and 399,253⅞ tons in 1894, an advance of 49,110⅞ tons.

In wrought steel, the production for the same period was 386,135⅔ tons in 1895, and 335,927⅓ tons in 1894, an increase for the twelvemonth of 50,208¼ tons.—(*Foreign Office Miscellaneous Series, No. 389.*)

RETURNS OF BRITISH SHIPPING AT ANTWERP FOR 1894 AND 1895.

A communication, dated March 13 last, has been received at the Foreign Office from Mr. G. R. de Courcy Perry, Her Majesty's Consul-General at Antwerp, enclosing returns of British shipping at that port for the years 1894 and 1895. From these it appears that the total number of British vessels which entered the port of Antwerp in direct trade with Great Britain and her colonies in 1895 was 1,695, of a total tonnage of 1,095,522 tons, as against 1,615 of a tonnage of 1,033,139 tons in 1894. The total number of British vessels trading direct with Great Britain and her colonies cleared in 1895 was 2,274, of a tonnage of 1,687,908 tons, as against 2,252 with a tonnage of 1,548,793 tons in 1894.

With regard to the indirect or carrying trade in British vessels from and to other countries, the number of vessels entered and cleared in 1895 was:—entered, 979 of a total tonnage of 1,289,700 tons; cleared, 389 vessels (694,969 tons), as against 1,615 vessels (1,325,570 tons) entered and 471 vessels (705,392 tons) cleared in 1894.

REDUCTION OF RAILWAY RATES ON COAL FROM SILESIA TO
BALTIC PORTS.

With reference to a note on p. 319 of the March number of the "Board of Trade Journal," it should be stated that the reduction of railway rates on coal from Silesia to Baltic ports was by 2s. 9d. per ton, the reduced rate thus being 7s. 6d. instead of 10s. 3d. per ton.

SUBMARINE TELEGRAPH BETWEEN FRANCE, NORTH AMERICA,
AND THE FRENCH ANTILLES.

A despatch, dated March 9 last, has been received from the Foreign Office, enclosing extract from the "Journal Officiel," containing the report of the discussion on March 3 last in the French Chamber of Deputies upon the proposal to establish telegraphic communication between France, North America, and the French West Indian Islands.

The convention concluded by the French Government and the French Cable Company was adopted by the Chamber.

A further despatch, dated March 26 last, has been received at the Foreign Office from Mr. Henry Howard, Her Majesty's Secretary of Embassy at Paris, stating that the above proposal was voted by the Senate on the 24th March last without discussion; and a despatch from the Foreign Office, dated April 2, transmits extract from the "Journal Officiel" giving the text of the law in question.

MINING CONCESSIONS IN MADAGASCAR.

A communication, dated February 17 last, has been received at the Foreign Office from Mr. McMillan, Her Majesty's Consul at Tamatave, reporting that, on the Resident-General of France being approached on the subject of grants of concessions in the island, M. Laroche stated that the rules and regulations to be introduced regarding mining concessions and the exploitation of mineral reefs, have not yet been promulgated, but that, as soon as they are framed these regulations will be made known.

In the meantime, persons desirous of devoting themselves to preliminary investigation, may obtain licences to prospect in the offices of His Excellency the Prime Minister at Antananarivo.

These licences are generally accorded on payment of an ordinary fee of 25 frs.

PROHIBITION OF FOREIGN SILVER COINS IN ANGOLA.

A communication, dated February 13 last, has been received at the Foreign Office from Mr. W. C. Pickersgill, Her Majesty's Consul at St. Paul de Loanda, reporting that in obedience to a royal decree, dated December 22nd, 1895, the Governor-General of Angola had proclaimed the introduction of foreign silver coins

into the province to be illegal, and that such as then existed in the country could only become lawful tender by being officially stamped with their value in reis before the 15th March 1896, after which date they might continue in circulation for six months, or be sold to the Government. The price offered is at the rate of Rs. 4,500 for 20s. or 20 marks, Rs. 4,300 for 25 frs., and Rs. 320 for 1 rupee, in notes of the Banco Nacional Ultramarino.

The true reason for this order has not been made public, nor is it explained by the state of the currency. But the object of the Government is to enable the bank above named to dispose of an accumulation of about 27,000*l.* worth of non-Portuguese silver, and thus to leave the directors of the corporation without excuse for the unsatisfactory condition of their Loanda coffer, which contains no proper means of redeeming the notes they have issued, amounting to over 290,000*l.* Practically, there has been nothing in circulation in Angola but copper and paper for several years past, and Loanda notes, whether "bank" or treasury, are subject to discount, even in the minor provinces of St. Thomas and Cape Verd, where silver is fairly plentiful.

A supply of white coinage sufficient to cover the issue referred to is about to arrive from Portugal, and it is understood that in future the bank will be kept alive to its responsibilities. The contract by which it acquired a monopoly of colonial business expired about four years ago, and does not seem likely to be renewed, but whether the Government will encourage competition or not remains to be proved. At all events they are taking away the power which the Colonial Bank has hitherto held of swamping opposition.

GOVERNMENT MONOPOLY OF PETROLEUM IN MONTENEGRO.

A despatch, dated March 25 last, has been received at the Foreign Office from Mr. A. T. Waugh, Acting British Chargé d'Affaires at Cetinje, reporting that the Montenegrin Government has established a monopoly of the importation of petroleum, to date from March 1 last.

Petroleum will now be imported only by the Ministry of Finance and sold at the rate of 2.25 florins (equal to 3s. 9*d.*) per tin.

THE DEVELOPMENT OF SUGAR-CANE CULTIVATION IN EGYPT.

A despatch, dated March 12 last, has been received at the Foreign Office from Lord Cromer, Her Majesty's Agent and Consul-General at Cairo, transmitting copy of a Khedival Decree published in the Official Gazette sanctioning the institution of a joint-stock company under the title of "Société Foncière d'Egypte," which has for its chief object the development of sugar-cane cultivation in Egypt.

INDIA-RUBBER FROM THE SOUDAN.

A despatch, dated February 6 last, has been received from Colonel A. H. Maclean, British Vice-Consul at Dakar, Senegal, enclosing a table showing the values and description of certain balls of india-rubber submitted to Monsieur Sampain, President of the Chamber of Commerce of St. Louis, for examination.

These specimens of india-rubber were received from the Soudan by the Governor-General of "Soudan-Senegal" on his return from the Soudan.

In order to encourage the cultivation of plants producing india-rubber this substance is now received in payment of taxes in the districts mentioned in the report.

The table can be seen on application at the Commercial Department, Board of Trade, 7, Whitehall Gardens, S.W., between the hours of 11 and 5.

PROPOSED RUNNING OF NETHERLANDS S.S. LINE TO PORT ARTHUR (TEXAS).

A communication has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, stating that it is reported that the Kansas City, Pittsburg, and Gulf Railway at Kansas City, Mo., are trying to effect an arrangement with the Royal Mail line (Holland) and Netherlands American line, with a view to run some of their ships from Dutch ports, and Southampton and Liverpool, to Port Arthur, Texas, which latter place would become a new port. Mr. Burrough, British Vice-Consul at Kansas, reports that, as the arrangements have not yet been settled, it might be expedient to advise British steamship companies of the same.

There appear to be lots of pine trees suitable for shipbuilding on the line of the Kansas City, Pittsburg, and Gulf Railway, in the Indian territory, and on borders of Louisiana. The trees are very tall and well formed, with very few limbs. The United States have a reservation of the same trees in the vicinity for shipbuilding purposes.

PROPOSED SHIP CANAL FROM THE GREAT LAKES TO THE HUDSON RIVER (U.S.A.).

A despatch, dated February 27 last, has been received from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, transmitting copy of a report from the House of Representatives on Railways and Canals, dated February 18; also an Act passed by the Senate on February 24 providing for the construction of two steam revenue cutters of the first class for service on the Great Lakes, and a report from the Senate Committee on Commerce accompanying the above Act.

The subject of the first of these enclosures is a proposed ship canal from the Great Lakes to the navigable waters of the Hudson River.

Particulars are given in the Report of the great traffic of the Sault Canal at the outlet of Lake Superior and of the trade of the lakes generally, of the total tonnage of shipping built on the Great Lakes in the United States in certain years, and of the rates on all freights carried on the lakes; and it is stated that with a canal from the lakes to the Hudson, of sufficient capacity so that certain vessels, such as "whaleback" steamers, could pass through without breaking bulk, the cost of carrying wheat from the head of Lake Superior to the city of New York will be less than 5 cents per bushel.

The report concludes with a strong recommendation that a survey be made to determine the feasibility and probable cost of the desired canal.

CONCESSION FOR RAILWAY IN GUATEMALA.

A despatch, dated February 13 last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Acting Consul-General at Guatemala, transmitting copy and translation of a concession granted to Mr. Henry Neutze (a prominent German financier in Guatemala) for the construction of a railway from the port of Ocos to Santa Catarina, with powers of extension to San Marcos.

The port of Ocos lies about 150 miles north-east of San José de Guatemala (the port of Guatemala City), and is coming from the eastward, the last Guatemalan port before touching Mexico. The proposed line of rail will take a northerly direction from the port of Ocos, trending slightly east to Santa Catarina, which is about due east from Tuxtla Chico and Tapachula on the Mexican frontier, and will be quite near to Rodeo, a town of some size in the department of San Marcos, Guatemala, the approximate length, drawing a straight line from Ocos, is about 15 leagues, but of course detours have to be made that would make it longer.

Mr. Neutze receives a subvention from the Government of Guatemala of 15,000 dols. per English mile, all materials necessary for the construction and for use of the labourers employed may be imported free of duty, all lands through which the railway may require to pass shall be conceded by the Government.

The concession is for a period of 75 years, at the expiration of which the railway becomes State property without any payment on the part of the Government; during the aforesaid 75 years Mr. Neutze may dispose of his rights to any person (having, however, first to give the Government the option of purchase), but should he do so to foreigners, they, in the event of any questions arising, are barred from recourse to diplomatic intervention.

The rails are to be of steel and weigh not less than 36 lb. per lineal metre ; the sleepers can be of either steel or lumber.

CONCESSION FOR PORT WORKS AT OCOS, GUATEMALA.

With reference to the above new railway in Guatemala, a further despatch, dated February 13 last, has been received from Mr. J. F. Roberts, Her Majesty's Acting Consul-General at Guatemala, enclosing copy and translation of a concession granted to Mr. Henry Neutze and Mr. Adolph Stahl for the construction of a new pier at the Port of Ocos, in connection with the Ocos Railway of which Mr. Neutze is also concessionaire.

In connection with this pier the Government of Guatemala will at once become great gainers by having all the necessary buildings for customs purposes erected for them at the expense of the pier company ; and the general public will be gainers by a decrease in the existing wharf charges.

The new pier becomes Government property at the date of the expiration of the Ocos railroad concession, namely in 75 years.

NEW RAILWAY TARIFF IN COSTA RICA.

A despatch, dated February 17 last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Acting Consul-General at Guatemala, enclosing copy and translation of the new tariff that has come into force on the Pacific Railway of the Republic of Costa Rica, starting from the Port of Punta-Arenas.

The above can be seen on application at the Commercial Department, Board of Trade, 7, Whitehall Gardens, between the hours of 11 and 5.

CONTRACT FOR IMPROVEMENTS AT THE PORT OF LIMON (COSTA RICA).

A despatch, dated February 17 last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Acting Consul-General at Guatemala, enclosing a communication from Mr. Harrison, Her Majesty's Consul at San José, Costa Rica, stating that the executive, considering it necessary to carry out some sanitary works in the Port of Limon—not provided for by the contracts entered into with Mr. M. C. Keith on March 28, 1892, and October 7, 1893—have decided, in accordance with the second clause of the latter, to make an additional contract with the said Mr. Keith for the required improvements, and have applied to the Permanent Commission for authority to contract same for a sum not exceeding 25 per cent. of the amount of the original contract.

INCREASE OF YELLOW FEVER AT RIO DE JANEIRO.

A despatch, dated February 3 last, has been received at the Foreign Office from Mr. E. C. Phipps, Her Majesty's Minister at Petropolis, stating that the mortality statistics for the first fortnight of January have just been published and show a considerable increase of deaths from yellow fever.

During that period there were 245 declared deaths from that disease, exclusive of such deaths as have occurred at the Maritime hospital on the opposite side of the Bay, especially assigned to yellow fever cases.

Since the expiration of the fortnight in question the yellow fever mortality has rather increased and has reached as much as 30 in a day.

This is a feature which should not pass unnoticed.

The year 1894 is described in the official annual report of the Director of the Federal Sanitary Institute as that of the "Most deadly and extensive yellow fever epidemics ever experienced" since the disease was, in the year 1850, imported from New Orleans; it killed only 371 persons during the whole of the month of January that year, whereas last month not less than 490 persons can have fallen victims to the scourge. February and March are far more deadly, as in 1894, 1,350 and 1,929 victims were reported in those two months. It is, moreover, to be noticed that during the past year the epidemic was never stamped out, whilst in 1894, during the cooler months of September, October, and November, only five persons succumbed to it. The absolutely unsanitary condition of the city is daily subject for animadversion in the press. Mr. Phipps concludes his report as follows:—

"In view of the considerable passenger traffic passing through Rio de Janeiro *en route* to the River Plate and other ports, I think it would be useful that a warning should be made public as to the sensible risk which British passengers or emigrants incur, especially during the summer months, by passing a single night in the city or in the suburbs of Rio de Janeiro.

"The disease is peculiarly fatal to foreigners and especially to fresh arrivals, natives being almost free from it. During the past year several cases have come under my notice of foreign-born persons who have contracted the disease immediately after their first arrival.

"In the neighbourhood of the capital there is no complete immunity from the disease except at high altitudes; in the towns of Petropolis, Theresopolis, and Novo Friburgo, though deaths occasionally are due to its having been contracted in the city, it is never propagated."

 THE FOREIGN TRADE OF URUGUAY FOR 1895.

A despatch, dated February 9 last, has been received at the Foreign Office from Mr. Baring, Her Majesty's Minister at

Monte Video, enclosing extract from the "Montevideo Times," containing statistics issued by the Customs-house as to the import and export trade of 1895.

It will be seen that, as compared with 1894, there was an increase in imports of 795,823 dols. (169,320*l.*), while, on the other hand, exports show a decrease of 935,868 dols. (200,000*l.* nearly), the total trade of the country diminishing by the sum of 140,043 dols. (29,800*l.*).

The year 1894 was notable for a large increase in exports, so that, as the "Montevideo Times" justly points out, "the decrease in 1895 does not necessarily indicate a relaxation of the productive powers of the Republic."

Imports are classified under eight heads, of which five show an increase, viz., drinks, comestibles, &c., raw materials and machinery, various and live stock.

Exports are classified under five heads as the division "other products" may be left out. Of these only one, viz., live stock shows an increase.

The prospects of the country as far as regards agriculture, &c. appear this year to be fairly favourable. The harvest has been good, the price of wool has, fortunately for sheep farmers, risen since last season, and the clip is reported to be clean and of good quality. Cattle are fetching good prices. Sheep, however, are reported to be suffering greatly from foot rot and also from maggots, both diseases being brought on by the unusual dampness of the season and the consequent over luxuriant growth of grass.

MONTEVIDEO CUSTOMS RECEIPTS FOR JANUARY 1896.

A despatch, dated February 6 last, has been received at the Foreign Office from Mr. W. Baring, Her Majesty's Minister at Montevideo, enclosing an extract from the "Montevideo Times," in which the Custom-house receipts for January 1896 are given.

These show an increase over those for December last, which amounted to 919,829 dols. (195,703*l.*), and it will also be seen that they are in excess of the receipts for January in the two past years.

In January 1895 quarantine seriously interfered with the trade of the country, and caused the Customs receipts to be low.

January is always a busy month, but this year the increase in the receipts may be partly due to the fact that importers have been clearing goods in considerable quantities in order to avoid the new import duties.

XVIII.—GENERAL TRADE NOTES.

WINE PRODUCTION OF THE WORLD IN 1895.

The "Economiste Français" publishes the following statement taken from the "Moniteur Vinicole" as to the amount of wine produced in various countries in the year 1895.

	Hectolitres.
France - - - -	26,687,600
Algeria - - - -	3,797,700
Tunis - - - -	179,800
Italy - - - -	21,343,400
Spain - - - -	17,250,000
Portugal - - - -	1,995,000
Azores, Canaries, Madeira - -	210,000
Austria - - - -	3,000,000
Hungary - - - -	2,865,000
Germany - - - -	3,645,000
Russia - - - -	720,000
Turkey and Cyprus - - - -	2,400,000
Bulgaria - - - -	1,200,000
Greece - - - -	1,600,000
Roumania - - - -	3,120,000
Switzerland - - - -	1,250,000
United States - - - -	850,000
Argentina - - - -	1,350,000
Chile - - - -	1,500,000
Australia - - - -	150,000

SILK PRODUCTION OF THE WORLD IN 1895.

The "London and China Telegraph" notes that, pending the final results of the inquiry made each year since 1871, the Lyons Syndicate of silk merchants has published some approximate and provisional statistics of the world's production of silk during the year 1895. These figures confirm the estimates made in the autumn, viz., that Western Europe would show an important deficit upon the previous year, that several countries of the Levant would furnish a production scarcely equal to 1894, but that the Far East would make up for the loss. The total production of the silk harvest of 1895 reaches 14,685,000 kilogrammes of raw silk, against 14,369,000 kilogrammes in 1894. These totals are made up as follows :—

	1894.		1895.	
	Cocoons.	Raw Silk.	Cocoons.	Raw Silk.
	Kilos.	Kilos.	Kilos.	Kilos.
Western Europe -	58,040,714	4,701,000	52,851,261	4,005,000
Levant -	18,793,000	1,244,000	17,688,000	1,163,000
	Bales.		Bales.	
Far East—Shanghai	71,243	3,787,000	78,000	4,153,000
Canton -	28,820	1,354,000	30,000	1,410,000
Yokohama	51,396	3,084,000	60,000	3,600,000
Calcutta -	2,472	199,000	4,194	354,000
Total -		8,424,000		9,517,000
Grand Total -		14,369,000		14,685,000

PIG-IRON PRODUCTION IN GREAT BRITAIN, GERMANY, AND THE UNITED STATES.

The "Iron Age" publishes the following table showing in thousands of tons, the annual output of pig-iron in Great Britain, Germany, and the United States respectively from 1870 to 1895 inclusive. The production of Germany, which includes Luxemburg, is given in metric tons of 2,204 English pounds, while the production of the other countries is expressed in gross tons of 2,240 pounds, sufficiently close for this comparison. The amounts are given in thousands of tons.

Years.	Great Britain.	Germany.	United States.
	Thousand Gross Tons.	Thousand Metric Tons.	Thousand Gross Tons.
1870	5,963	1,391	1,665
1871	6,627	1,563	1,706
1872	6,741	1,988	2,548
1873	6,566	2,240	2,560
1874	5,991	1,906	2,401
1875	6,365	2,029	2,023
1876	6,555	1,846	1,868
1877	6,608	1,932	2,066
1878	6,381	2,147	2,301
1879	5,995	2,226	2,741
1880	7,749	2,729	3,835
1881	8,144	2,914	4,144
1882	8,586	3,380	4,623
1883	8,529	3,469	4,595
1884	7,811	3,600	4,097
1885	7,415	3,687	4,044
1886	7,009	3,528	5,683
1887	7,559	4,023	6,417
1888	7,998	4,337	6,489
1889	8,322	4,524	7,603
1890	7,904	4,658	9,202
1891	7,406	4,641	8,279
1892	6,709	4,937	9,157
1893	6,976	4,986	7,124
1894	7,427	5,559	6,657
1895	8,000	5,788	9,446

It will be observed that, while the German annual output exhibited some irregularity up to 1876, from that time there has been a steady increase from year to year, almost without a break. It has, in fact, proceeded with wonderful steadiness, when contrasted with the figures for the United States and Great Britain. The United States has surpassed both the other countries in the expansion of its pig-iron trade, but the progress made has been spasmodic, sometimes going forward with marvellous rapidity and then retrograding seriously. Great Britain also has exhibited striking irregularity in its annual output. In 1870 it was producing about the same quantity that Germany is turning out to-day, but in the interval the United States has forged to the front and Germany bids fair soon to attain second place in the race for supremacy.

IMPROVEMENT OF RUSSIAN PORTS.

According to the "Bulletin Commercial," it is stated in the "Odesskiu Listok" that the Russian Minister of Ways and Communications has just granted subsidies for the improvement of the principal ports, viz.:—Odessa, 677,250 roubles; Marioupol, 40,000 roubles; Taganrog, 100,000 roubles; Novorossiisk 307,000 roubles; Poti, 197,500 roubles; Batoum, 436,500 roubles (exclusive of a sum allowed for the damages occasioned by the recent storms); Libau, 319,700 roubles; Riga, 272,000 roubles. A sum of 600,000 roubles has been appointed for the purchase of a dredger for the harbours of the Black Sea, and 3,200,000 roubles will be reserved for works connected with the dredging and regulation of the course of the river Volga.

THE FAIR OF NIJNI-NOVGOROD.

According to the "Journal de St. Petersburg" the annual fair of Nijni-Novgorod this year, instead of being opened on the 15th July, as usual, will, by special permission of the Emperor, be inaugurated on the same date as the Nijni-Novgorod Exhibition.

FINLAND STEAM NAVIGATION COMPANY.

The Italian Consul at Helsingfors in a recent report, noticed in the "Bollettino di Notizie Commerciali," refers to the development and flourishing condition of the Finland Steam Navigation Company.

This Company proposes to establish a regular line between the northern ports and the chief Mediterranean ports.

With this object, two new vessels have been built which can take cargoes of 2,000 tons and attain a speed of 11 knots an hour when so loaded, while the largest of the other vessels of the Company can only carry 1,400 tons.

These ships, which will be ready early in the summer, are principally for the transport of merchandise, but will also contain, besides officers' cabins, 11 small cabins of one berth each for passengers. They will be fitted up with all the most recent improvements, including two steam cranes for loading and discharging cargo, as well as a double bottom for water ballast in order to increase their steadiness and security.

THE SAMARCAND-TASHKEND RAILWAY.

The "Journal de St. Petersburg" states that, according to a recent rumour, when the Samarcand-Tashkend line is finished, one of the stations will be connected with the Trans-Siberian line with the view of affording an outlet into the Central Asian provinces for Siberian grain.

UTILISATION OF WATER POWER IN NORWAY.

"The Engineering and Mining Journal" of New York states that, according to a Reuter despatch from Christiania, the estate of Hafslund, near the great waterfall known as the Sarpsfos, between Christiania and Göteborg, has been acquired by a syndicate, chiefly consisting of German and American capitalists, for the sum of 800,000 kroner. The purchasers intend to form a large company, with a capital of 3,000,000 to 5,000,000 kroner, in order to utilise the water power of the falls for electrical force, and establishing aluminum works on the same principle as those now being constructed at the falls of Foyers, in Scotland. The Sarpsfos is one of the finest falls in south-eastern Norway, being 74 ft. in height and 116 ft. in width. The water power is already utilised, however, by numerous sawmills and cellulose factories, and the railway crosses the fall, so that the proposed new works will probably not interfere to any great extent with the artistic aspect of the place.

GERMAN IRON AND STEEL TRADE IN 1895.

The following particulars of the import and export trade of Germany in iron and steel in the year 1895, as compared with

1894, are taken from the German Official Monthly Accounts for December 1895 :—

Description,	Imports.		Exports.	
	1894.	1895.	1894.	1895.
	Tons.	Tons.	Tons.	Tons.
Iron ore - - -	2,093,007	2,017,136	2,558,729	2,480,136
Pig and semi-manufactured :				
Pig iron - - -	203,948	188,217	154,647	135,289
Old iron - - -	7,900	11,339	77,723	84,814
Ingots, &c. - - -	719	757	41,992	61,808
Total - - -	212,567	200,313	274,362	281,911
Manufactured products :				
Rails - - -	3,542	1,831	119,410	116,627
Bar iron - - -	19,966	19,777	300,558	277,991
Plates and sheets - - -	4,409	4,968	90,012	124,015
Wire, ordinary - - -	4,573	5,085	123,900	115,633
Do., coppered, polished, &c. - - -	315	498	85,918	89,699
Large castings - - -	4,246	5,121	16,239	19,066
Anchors, &c. - - -	1,415	1,389	644	729
Tubes, forged - - -	1,989	2,886	28,552	32,592
Large unfinished castings, &c. - - -	5,040	4,133	103,818	116,326
Finished castings, &c. - - -	5,021	4,789	15,874	18,823
Hardware, forged iron - - -	1,255	1,324	—	—
All other - - -	5,977	6,116	280,163	334,408
Total - - -	57,748	57,917	1,165,088	1,245,909
Machinery, &c. :				
Locomotives, locomobiles - - -	2,538	2,003	5,686	7,988
Machines for wood-working - - -	3,033	3,221	1,698	1,548
Do., for foundries - - -	32,146	32,909	90,842	97,185
Do., for forges - - -	2,988	3,116	15,121	16,078
Do., for other metals - - -	267	287	666	846
Boilers - - -	312	211	2,912	3,376
Sewing-machines, cast-iron - - -	2,981	5,249	8,020	9,625
Do., forged iron - - -	27	39	5	4
All other - - -	226	172	206	222
Total - - -	44,518	47,207	125,156	136,872

COAL TRADE OF GERMANY IN 1895.

According to a recent number of "Kuhlew's Trade Review" the imports of coal into Germany in 1895 amounted to 5,117,356 tons; of coke, to 461,779 tons; of lignite, to 7,181,050 tons; and of briquettes, to 59,806 tons. The exports of coal from Germany in 1895 amounted to 10,360,838 tons; of coke, to 2,293,328 tons; of lignite, to 18,814 tons; and of briquettes, to 200,374 tons. The consumption of coal at Berlin in 1895 is estimated at 1,426,726 tons, showing a falling-off of 31,473 tons as compared with 1894. The consumption of lignite and briquettes at Berlin in 1895 is estimated at 779,100 tons, showing

an increase of 63,213 tons as compared with 1894. The consumption of English coal at Berlin during the same period was estimated at 230,501 tons, or 41,297 tons more than in 1894.

GERMAN SHIPBUILDING IN 1895.

According to a report of the French Consul-General at Hamburg ("Moniteur Officiel du Commerce") the tonnage of ships built in German yards in 1895 amounted to 101,402 tons as against 117,621 tons in 1894. Notwithstanding this decrease of about 6,000 tons, the result should be considered satisfactory, the year 1894 having been one of exceptional activity. In 1893 and 1892 the figures were 66,474 and 48,208 tons respectively.]

GERMAN CEMENT INDUSTRY.

According to the "Engineering and Mining Journal" of New York, there are about 63 cement works in the whole of Germany. The Rhine is the principal centre for this manufacture, but in the neighbourhood of Hamburg there are three or four establishments for the production of the article. The annual production of Germany amounts to nearly 11,000,000 barrels, giving employment to some 1,800 hands, whose annual earnings amount to some 698,780*l*. The largest customers for this article in Europe are Russia and Norway, and of transatlantic countries, the United States, Brazil, Chile, and Venezuela. The exportation to Great Britain and British Possessions is comparatively small. The following table gives approximately the quantity and value exported to European and other countries:—Norway, 58,500 cwt., 6,700*l*.; Russia, 34,353 cwt., 2,900*l*.; United States, 1,386,872 cwt., 168,000*l*.; Brazil, 446,340 cwt., 40,200*l*.; Chile, 131,000 cwt., 13,000*l*.; Venezuela, 103,000 cwt., 9,800*l*.

CONSTRUCTION BOUNTY ON SHIPS IN ITALY.

The "Bollettino di Notizie Commerciali" publishes the text of a law which passed the Italian Senate in December last, by which it is decreed (*a*) that the articles of Chapters I. and II. of the law of 6th December 1885, on the mercantile marine; and (*b*) the law of 30th June 1889, concerning construction bounties in shipbuilding, are to remain in force until the 30th June 1896, provided that no new law affecting the mercantile marine be passed previous to that date.

AUSTRO-HUNGARIAN MINT.

The French "Bulletin de Statistique," in a recent number, states that the total value of new coins struck at the various mints of the Austro-Hungarian Empire up to the 31st July 1895, according to the law of the 2nd August 1892 was 385,506,000 florins that is, 288,763,000 florins of gold crowns, 67,277,000 of silver crowns, 26,714,000 of nickel hellers, and 2,752,000 of bronze hellers.

Of the total value of 385,506,000 florins, 232,342,000 florins were for Austria proper, and 153,164,000 for Hungary.

PRODUCTION OF TOBACCO IN ROUMANIA IN 1895.

The tobacco crop in Roumania of 1895, according to the "Journal de la Chambre de Commerce de Constantinople" amounted to 4,202,820 kilogs., as against 2,247,944 in 1894. The improvement is not only in the quantity but in the quality of the crop and, calculating on the basis of the sale prices, the average yield was of a value of 359 francs per hectare in 1895, as against 212 francs per hectare in 1894. The number of planters in 1895 was about 20,000 (*15,802 in 1894*), and of hectares cultivated about 7,000 (*5,562 in 1894*).

PROJECTED FLOATING DOCK AT GALATZ.

The "Bulletin Commercial" states that, according to a Bucharest journal, the Minister of Public Works has submitted to the Roumanian Chamber a Bill authorising the immediate construction at Galatz of a floating dock for ships' repairs. The total cost is estimated at 1,000,000 francs, divided as follows:—Construction of dock, 650,000 frs.; construction and fitting of repairing shed, 100,000 frs.; and sundry expenses, 250,000 frs.

THE FISH TRADE OF ROUMANIA.

The fish trade of Roumania, says the "Handels Museum" of February 27, forms the subject of an interesting article in the periodical "Societatei Centrale de Agricultura," from which the following figures are extracted:—The imports of salted and fresh fish of all kinds, dried or smoked, sturgeon, sterlet, eels, pike, stockfish, herrings, and mackerel, is continually increasing, and in 1894 amounted to 6,778,902 kilogs. of the value of 4,185,604 frs. The exports have fallen off; in 1880 they amounted to 3,095,942 kilogs. (value 1,900,225 frs.), but in 1894 they had fallen to 1,309,106 kilogs. (value 785,672 frs.) Tunny fish, sardines in vessels, rough caviar, black caviar, and other fish products, which are less important articles of trade, Roumania annually imports to the value of about half a million francs and exports about 140,000 frs.

The average annual importation of fish into Roumania in the 18 years, 1877 to 1894, was 5,052,617 kilos. of the value of 3,639,072 frs., whilst the average of the exports in the same period was 1,550,310 kilos., valued at 974,182 frs., so that Roumania pays annually 2,664,890 frs. for her fish supply. In the 18 years in question this excess of value of the imports over that of the exports amounted to 48,068,007 frs.; on this account the competition of Russia is regarded as calamitous, as large quantities of Russian fish, favoured by the tariff, are thrown on the Roumanian markets, and this commodity competes with the Roumanian in Servia and Austria-Hungary.

THE FINANCES OF THE TRANSVAAL.

The "Natal Mercury" states that the Transvaal revenue returns for 1895, which have just been published, give a clear idea of the very great progress made by the State during the past year. The gross income for the year totalled 4,358,912*l.*, and the net income 3,539,955*l.* In 1894 the net income was 2,247,728*l.*, so that the increase for the year has been 57 per cent. It is interesting to look at the rate of progress made during the past few years, and the following table will show better than any other way how marked this progress has been, and how greatly the revenue has increased within the past year or two especially :—

Year.				Revenue.	Expenditure.
				£	£
1887	-	-	-	668,433	621,073
1888	-	-	-	884,440	720,492
1889	-	-	-	1,577,445	1,201,135
1890	-	-	-	1,229,060	1,386,461
1891	-	-	-	967,191	1,350,073
1892	-	-	-	1,225,829	1,188,765
1893	-	-	-	1,702,684	1,302,054
1894	-	-	-	2,247,728	1,734,738
1895	-	-	-	3,539,955	2,679,095

It would be still more interesting if we had the statistics as far back as the date of the foundation of the present Republic. We should then see that in less than 15 years the revenue of the country has risen from a few tens of thousands, insufficient to pay the expenses of administration, to nearly as many millions, giving each year a handsome surplus over a fairly heavy expenditure. At present there cannot well be less than a surplus of 2,000,000*l.* in the State Treasury, and, of course, the whole of this remarkable if not unprecedented progress in the rise of a country from practical bankruptcy to opulence has been due to the discovery of the goldfields, and the energy, capital, and perseverance thrown into their development by the alien population.

THE FOREIGN TRADE OF NEW YORK CITY.

One of the functions of the New York Chamber of Commerce is the preparation of an analysis of annual reports of exports and imports of the United States for the fiscal year, separating totals for New York city from those for other ports. In this way some measure is obtained of the fluctuation in New York city's proportion of foreign commerce.

A comparison of total annual values of foreign commerce at New York and at remaining ports, separately, during 11 years shows, according to "Bradstreet," that the proportion of the total foreign trade at New York and elsewhere has been fairly constant, continuing about 54 per cent. of the whole during this time, as contrasted with 56 per cent. 11 years ago.

Of the imports at New York one-sixth only in value were received in American vessels, and one-ninth of the imports only went out in American vessels. Interest is lent by the fact that of the importations at other ports one-fifth arrived in American vessels, a larger proportion than at the city named, while of domestic exports sent out from other ports than New York about one-tenth in value went abroad in American vessels, a trifle less than the proportion so going from New York.

Total foreign commerce of the United States for the fiscal year 1894-95 was 1,709,867,836 dols., of which, as explained, New York city reported 934,290,314 dols., or 54 per cent. The year before the value of total foreign trade at all ports was 50,432,355 dols. larger than last year, so the decrease from the year 1893-94 is about 3 per cent. Of this falling off the decrease at New York is 27,028,077 dols., also about 3 per cent.

PROPOSED SUGAR BOUNTY BILL IN IOWA.

According to the "Sugar Cane" for April 1896, the Iowa Legislature is considering a Bill for the payment of a bounty of 1 cent per pound on all sugar manufactured in the State, such sugar to contain at least 90 per cent. of crystallised sugar, and in the case of beet sugar a minimum of 5 dols. per ton must have been paid on the raw material.

MEXICAN IMMIGRATION.

The "Mexican Herald," in an article on the subject of Mexican immigration and the investment of foreign capital in Mexican concerns, notes that capital in Europe, and especially in the great banking centre of the world, London, is steadily accumulating, and it will not be very long before it will be looking afield for investment.

Possibly the trend for investments may now turn to Mexico. There are few other countries that are naturally so rich, and where such large returns for invested capital may be realised.

It is true that, not speaking of the railways, a good deal of English money has been from time to time thrown away as much in Mexico as ever it has been in the Argentine, but that has not been the fault of the country. Every year sees the formation of London companies to undertake impossible things and controlled by impossible managers, and every year sees them go out of existence.

Undoubtedly Mexico requires capital to develop her latent industries, to exploit her immense mineral wealth, to hew into her rich forests, to spin, to trade, to breed cattle and delve into her extraordinary tropical resources; but the flow of British capital in the form of companies to work up improbable undertakings, will in the long run recoil upon the country and will give it no good name.

What Mexico does want from Europe, and especially from Great Britain, is immigration and that of a certain class.

Within the British Isles to-day are thousands of young men, of education and good blood, whose parents are rich enough to give each a small capital to start in life with, say from 500*l.* upwards—and who in fact leave their homes yearly in vast numbers to carve out for themselves fortune in a new country—in Australia, in Africa, in the United States, and in the North West Territories, and it would be well for Mexico if a few of these young men would tempt fortune there.

A class of young men, well educated, possessed of the tact and politeness of breeding and good education, with a sufficiency of capital of their own, and who would embark into tropical cultivation, &c., would indeed be a boon to the country. A pauper immigration from Europe or any other quarter is not needed. What is required is the small capitalist of the very best type and one with *thw*s and *sinews*, and blessed with confidence in himself and common sense and pluck.

There is a great field for this class of immigrant in Mexico, and it may be hoped that their attention may be turned to this, a comparatively new field, for their energy and capital.

There are other lands besides England where the same class of emigrants may be obtained, but none where there are so many, and those so adventurous.

If Great Britain would give Mexico more men of such a type it would be a good thing for the country.

THE MEXICAN CATTLE INDUSTRY.

According to the "Mexican Herald," the profits of cattlemen in the United States are very greatly curtailed by the item of transportation. The principal grazing regions are at long distances from the points of consumption, and the freight by rail, or the expense by driving, together with the natural shrinking resulting from such long transportation, makes a very considerable

loss to the owner of the cattle. Moreover, the variation in the climate, not only from day to day, but from one season of the year to another, prevents the rapid fattening of stock. In the winter season the snow-covered pastures necessitate the maintenance of all stock intended for the market with grain and fodder, and shelter from the cold, while that which is left to range barely manages to subsist from one cold autumn to the next wet and blustering spring. The high prices of grazing land which is at all desirable necessitates a very large investment of capital in the business, entirely independent of the amount in cattle itself. Yet, in spite of these difficulties, the exportation of beef is an industry in which large fortunes are yearly made. This is due, not to the natural advantages of the country, but to the enterprise, energy, and ability of those interested in it, many of whom have made immense fortunes.

In Mexico, on the other hand, the best cattle ranges are in the foothills near the coast, and in immediate proximity to lines of transportation. The pastures are green almost the entire year. There is no cold weather necessitating the sheltering or feeding of stock, no fodder to be purchased nor stores of grain to be laid away for use when the pastures are unproductive. Grazing lands are cheap, and will support more head of cattle per acre the year round than those of the United States will in the summer months. And yet, in spite of these advantages, there is almost no cattle export compared with what might be if the lands adapted to that purpose were all utilised. The Mexican cattlemen can compete on most favourable terms in the markets of the world with those of any other country, as taxes are low and all expenses paid in silver. The cultivated perennial grasses of the foothill regions are luxuriant growers, most nutritious, and can be used the entire year through. At any season advantage can be taken of the markets to obtain high prices when less favoured regions further north cannot supply them with fat stock. Although, at present, the foreign market is confined to the United States, the time is coming when the European markets will be supplied from Mexico direct, as the natural advantages of the country are much greater than those of any other section. No effort is, however, being made to build up a foreign market, due partly to lack of capital and partly to lack of enterprise. The technical knowledge necessary to successfully carry on the exportation of beef, either on the hoof or in cold storage, is lacking in Mexico, and native capitalists are afraid to venture funds in, to them, an unknown industry.

The present production of cattle is to a certain extent limited by home markets, but the possibilities of production have practically no limit. Whoever first occupies the field with the necessary energy, knowledge, and capital will establish an industry rivalling in importance, and exceeding in profits, that of the Armors and Swifts of America.

Argentina and Australia are exporting much to Europe. They are thousands of miles further away from market than Mexico,

and their cold storage ships have to pass under the Equator, while their pasturage is more subject to drouth and frost. The margin between cost of production in Mexico and market price in Europe is so great that only the most gross mismanagement or ignorance can prevent the greatest pecuniary success resulting from the development of the grazing lands; but the Mexican capitalist is timid, and hesitates long before embarking in a new industry, especially when so many opportunities in known and tried industries are presented to him. As long as the human race continues to use meat as an article of diet, so long will the grazier of the foothills of eastern Mexico reap the greatest profits from the industry, for it can be produced better and cheaper, and nearer to market there than elsewhere.

PRODUCTION OF CAOUTCHOUC IN BRAZIL.

The "Bulletin Commercial" states that, according to American statistics, the production of caoutchouc in the province of Para (Brazil), which is constantly on the increase, has doubled in the last 12 years, and quintupled in the last 30 years. The production of 1895 was 13·4 per cent. greater than that of 1894 and 10·2 per cent. greater than that of 1893.

The following statement shows the value of the exports in certain years :—

Years.	Value.
	£
1865	8,243,000
1870	10,528,000
1875	15,144,000
1880	18,889,000
1885	29,310,000
1890	36,300,000
1895	46,363,000

The imports of caoutchouc into the United States have increased in about the same proportion as the exports from Brazil out in 1895, for the first time, the amount of caoutchouc sent from Para to Europe was greater than that sent to the United States. The increase is specially noticeable in England, France, and Germany, where Para caoutchouc is in great demand.

CONSULAR INVOICES IN VENEZUELA.

According to the "Journal Officiel" merchandise imported into Venezuela must be accompanied by consular invoices, to be drawn out in quadruplicate, one copy of which must be viséd by the Venezuelan Consul.

The viséd copy is sent under cover to the consignee of the goods, who must present it at the Custom-house within three days

after the arrival of the ship, in order to ensure the release of the goods: two copies remain with the Venezuelan Consul to be forwarded to the Venezuelan authorities at Caracas.

The cost of the consular invoice, whatever be the contents, varies from 20 to 24 francs according to exchange.

The invoice should be drawn up with the greatest care.

The gross weight must be set down *in writing* and the contents of the packages must be distinctly stated. Goods are divided into classes and pay duty on the gross weight, consequently the question of "packing" is an important one. A false declaration or even an error as to the contents of a package involves payment of the highest class of dues.

The following may serve as a model for a consular invoice:—

Venezuela.

No. Folio
 Invoice of goods shipped by, in the port of,
 on the steamer, Captain, for consign-
 ment to, in the port of, on account
 of

Marks.	Nos.	Packages.			Contents of Packages.	Gross Weight in —.	Value in —.
		Numbers in Figures.	Numbers in Writing.	Descrip- tion.			

CUSTOMS RECEIPTS OF URUGUAY.

According to the "Moniteur des Intérêts Matériels," the Customs receipts for Uruguay for January 1896 show a marked improvement on the figures for the corresponding period of previous years, amounting to 1,065,819 piastres, as against 885,103 for January 1895, 924,716 for the same period of 1894, and 769,197 for the corresponding month of 1893.

The total value of the Customs receipts for the year ending December 31, 1895, was 10,632,328 piastres, as against 10,256,902 for 1894, and 9,004,160 for 1893.

ARGENTINE WOOL CLIP.

A recent number of the "Australasian" states that, according to the estimate of Argentine authorities, the wool clip of 1895 will be the greatest ever obtained; and the advanced price will make the money return, much greater than it has been in any

recent year. In 1884 Argentina exported 114,000 tons of wool for 32,000,000 dols. gold; and in 1894 the exports amounted to 162,000 tons, worth only 29,000,000 dols. gold.

OUTPUT OF BEEF AND MUTTON IN ARGENTINA.

The following figures, obtained from a private source, may, according to the "South American Journal," be accepted as correct; they represent the output of beef and mutton in 1895. Of frozen meat the year's output is shown in the subjoined table:—

Company.	Carcasses of Mutton.	Quarters of Beef.
Sansinena Company - - - -	773,262	14,976
Las Palmas Produce Company - -	626,953	4,876
The River Plate Fresh Meat Company -	522,677	—
Total - - - -	1,922,892	19,852

Of preserved meat, 13,556 boxes, weighing in all 1,264,539 lbs., were exported. In addition, the Argentina Preserved Meat Company slaughtered and prepared 58,000 bullocks and 8,500 wethers. Of jerked or sun-dried salted beef, 10,800 bales, weighing 1,392,558 lbs., were sent to Brazil, and 23,830 bales, weighing 3,073,212 lbs., to Cuba. It is likely that, in the course of a few years, the sun-dried beef trade will die out, though, on the whole, it is a cheap and wholesome way of preserving beef, and, for tropical countries, it is at any rate safe. The first Spanish colonists learnt the art of sun-drying and preserving beef from the Incas, but the origin of the system has never been cleared up from the shadows of American tradition.

TOBACCO IN SUMATRA.

According to a report of Mr. E. Spencer Pratt, Consul-General for the United States at Singapore, the crop of tobacco from the east coast of Sumatra which, since 1869, had been gradually increasing, reached a total of over 236,000 bales of 174 lbs. each in 1890. This was evidently too large a crop for the consumption, for heavy loss was sustained by planters, many of whom, working with slender means, had to close their estates, and even the Deli Maatschappij suffered a loss that year. In 1891, the crop was about 10,000 bales less, and prices improved, but although the big companies made comparatively small profits, many private planters had to close. This reduced the crop of 1892 to about 150,000 bales, which brought good prices and evidently stimulated

the production, for the crop of 1893 rose to 160,000 bales, and the 1894 crop, which is now being disposed of, is estimated at about 200,000 bales or more.

The opinion is that there is at present no demand for more than 200,000 bales a year at the most, and when the production surpasses this quantity, another crisis will be experienced.

As far as can be ascertained at present, there are on the east coast of Sumatra 26 public companies and about 25 private planters engaged in tobacco cultivation.

As to the mode of cultivation, the planting is done by Chinese coolies, imported chiefly direct from China. When the number from China proves insufficient, Chinese are engaged in the Straits Settlements on contract.

Each Chinese coolie gets a field of three-fourths of a bouw (1 bouw = $1\frac{3}{4}$ acres). He has to plant tobacco on that field, and he gets payment for his labour when the tobacco is ripe and delivered by him to his master. When he delivers his tobacco, the coolie is paid per 1,000 plants, according to quality, from 1 dol. to 8 dols. During the planting season, however, he receives an advance of 5 dols. per month.

When the tobacco is in the fermenting sheds, the Chinese work in the sheds on contract until the planting season commences again. They are chiefly employed at sorting tobacco, and receive three-fourths of a cent for each bundle of 35 to 40 leaves. The payment of coolies on the estates is made on the 1st and 16th of the month, when they are paid what they have earned, or given advances.

Besides Chinese, Japanese coolies are employed, who, in the beginning, get 6 dols. a month, and the women 3 dols. per month. They are used for roadmaking, shed building, and jungle cutting or clearing work. These wages must be considered as a minimum; as soon as they learn their work, they enter into contracts for piecework, and a good workman can earn as much as 12 dols. per month.

THE JAPAN MAIL STEAMSHIP COMPANY.

With reference to an article on pp. 161-162 of the February number of the "Board of Trade Journal," the "Bombay Gazette" states that Messrs. Tata and Sons, the Bombay agents of the Nippon Yusen Kaisha (Japan Mail Steamship Co.), have just learnt by wire that the directors have resolved to extend the operations of this company to Europe. The first steamer of the new monthly service, the "Tosa Maru," 5,789 tons gross, was to leave Yokohama on the 15th of March, and is due about the 15th of April. After her arrival she will engage cargo for European ports. The "Tosa Maru," formerly known as the "Islam," has been recently renovated, and fitted up to take a few passengers.

ESTABLISHMENT OF A STEEL FOUNDRY IN JAPAN.

The "Engineering and Mining Journal" of New York, states that the Japanese are now manufacturing a considerable amount of mining machinery and with success. This, of course, applies only to the lighter forms, as their works are not yet of sufficient magnitude nor of the necessary equipment to turn out the heavier pieces.

It is quite evident that their ambitions go beyond this, and they propose to be independent of other countries in their requirements for ordnance and small arms, by the aid of their own iron mines, their own fuel and their own skill. The Japanese Government has recently arranged for the establishment of a steel foundry in Japan with the firm of Sir W. G. Armstrong & Co., of London, on the following terms :—The materials at present to be imported from England ; not more than 20 per cent. of the workmen to be English, the balance to be Japanese ; if a new arm is invented in England it is to be manufactured also at the Japanese works ; a subsidy is provided for a stated number of years, at the expiration of which period the Japanese Government will acquire the works by purchase.

RISE OF PRICES IN JAPAN.

The "Bulletin Commercial" (Brussels) states in a recent issue, with regard to the competition of Japan with European countries in certain industries, that hitherto many kinds of Japanese goods had the advantage, owing to cheap labour, of costing much less than similar European articles, but it appears that this will not continue to be so. The native Japanese press announces that an extraordinary rise of prices has lately taken place in Japan, and that the movement does not yet seem to have lost its upward tendency. The rise has especially affected articles of luxury. In the case of some of these, among which may be mentioned silk and cotton articles, the prices have risen as much as 30 and 40 per cent. in comparison with those of 1893. Rice, barley, salt, sugar, coal, wood and metal articles are also to be found in a list of some 22 different kinds affected by the upward movement.

It is noticeable also that the rise in prices has been accompanied by a considerable increase in the rate of wages, consequent on the departure of a large number of Japanese artisans and workmen for Formosa and Wei-Hai-Wei.

TRADE BETWEEN JAPAN AND AUSTRALIA.

According to a report of the Austrian Consul at Yokohama, published in the "Handels-Museum" for February 27, the question of establishing a regular exchange of commodities between Japan and Australia has been lately raised in the former

country more than once, the project being to exchange the productions of Japanese industry for the raw materials of Australia.

Several representatives of Australian export syndicates have visited Japan within the last few months. In October 1895 samples of the products of Australia were exhibited in the native Chamber of Commerce and Industry at Tokio by a representative of the South Australian export trade. This exhibition made little impression, as it did not form a good collection of samples. Besides the photographs of Australian breeds of horses and horned cattle—which might perhaps be useful to Japan for the improvement of her system of cattle breeding, the samples of wool seemed to be the only articles of importance to Japanese trade. Samples of flour, tallow, wine (characterised by the Consul as dear, and of medium quality), eucalyptus oil, dried fruit, preserves and ostrich feathers were also exhibited. As the Japanese woollen industry develops, the importation of Australian wool should be capable of considerable expansion, and the same may be said of the exports of Japanese silk and cotton goods to Australia. It is also said that the Japanese are about to establish a subventioned line of steamers to Australia.

RAILWAYS IN INDIA.

A memorandum has been received from the Public Works Department of the Government of India notifying that the Ennur branch of the Bezvada-Madras Railway, a length of 8·52 miles, was opened for the public carriage of passengers on the 22nd February 1896.

The section of the Wazirabad Lyallpur railway from Sangla to Lylpur, a distance of 27·18 miles, was opened for the public carriage of passengers and goods on the 15th February 1896; and the section of the East Coast State railway from Rambla station to Khurda road station, a distance of 61·53 miles, was opened for the public carriage of passengers and goods on the 1st March 1896.

THE BOMBAY SALT TRADE.

According to the "Pioneer Mail," which quotes from the report on the administration of the Bombay Salt and Continental Customs Department for the past year, the 1894-95 salt season was generally favourable for manufacture throughout the Presidency; the number of works was increased, and the works at Sanikatta were reopened with improved arrangements for storage. At the Runn works the quantity of salt manufactured and brought to account was, with the exception of one year, the largest on record by four lakhs of maunds. The most marked improvement was, however, in the Konkan works, where the previous year's figures were improved on by 35 per cent. At

Kharaghoda, it is shown, experiments were continued for the purpose of discovering fresh sources of supply, and resulted in a substantial addition to the known brine-yielding area, while the whole of the ground was surveyed and demarcated with a view to the elaboration of a scheme for the systematic development of the works. The Imperial revenue under salt increased by close upon 18 lakhs in the year, a fact which is attributed to the growth of the issues from all works in the Bombay Presidency, to larger imports from Goa, and also to some extent to an increase in the duty recovered under the credit bond system on salt issued in the previous year. Meanwhile charges were the same, and the expense of working unaltered.

BREEDS OF CATTLE IN THE N.W. PROVINCES AND OUDH.

A copy of Bulletin No. 2 of the Veterinary Series of the Department of Land Records and Agriculture, being a Note on Breeds of Cattle in the District of Bahraich (Oudh), has been received at the Board of Trade from the India Office.

The bulletin deals with the Suitability of the District for Cattle Breeding; Breeds of Cattle; Stall Feeding; General Management of Cattle; Uses to which Cattle are put; Cattle Markets; Breeding Bulls; Cattle Diseases, &c., &c.

REVENUE RETURNS OF CAPE COLONY.

A communication, dated March 9 last, has been received from the Agent-General for the Cape of Good Hope, enclosing copies of comparative statements, from which it appears that the revenue received in the month of January last was 597,427*l.*, showing a net increase of 80,118*l.* on the corresponding amount for 1895. For the seven months, July—January 1895–96, it amounted to 3,810,292*l.*, showing a net increase of 751,997*l.* on the amount for the corresponding period of 1894–95.

CANADIAN TIMBER IN 1895.

The “Monetary Times” of Toronto states that the year 1895 has not been by any means a satisfactory one for makers or sellers of Canadian lumber. Demand from the United States was, in the early part of the year, limited, and prices declined. The insurrection in Cuba, which is a considerable market for the coarser grade, restricted exports thither; and to Argentina a disappointingly small quantity has been sent. Building being dull in the Western States, the lumber producers there sent unusual shipments eastward. As a result of all these things, added to the quietness of business in the States, prices have

declined. But there is still another feature which is a very important one. The development of the Southern States has resulted in an enormously increased output of Southern pine. And this pine goes to the Northern and Eastern States, taking the place of the coarser grades of Canadian and North-Western pine at lower prices. Of course it does not affect the choice pine of Canada, which it cannot equal, nor are their prices of square timber lower than Canadian. But cargoes of pitch pine boards have reached Oswego, N.Y., which have been carried cheaply from New York by the Erie Canal, and have been sold at prices Canadian traders cannot touch.

The Canadian supply is abundant; the mill men on the Georgian Bay and in the Ottawa District are carrying more coarse lumber, probably, than they have done for years, and it is no exaggeration to say that prices of common lumber are 10 to 15 per cent. lower than they were 12 months ago. But it is agreeable to learn that the supply of choice is not in excess of the ordinary demand, and the prices are firm. So that, when revival of business comes, some activity may be expected.

SISAL INDUSTRY OF THE BAHAMAS.

A paper was read on March 18 last before the Society of Arts on the Sisal Industry of the Bahamas by Dr. D. Morris, C.M.G., Assistant Director of the Royal Gardens, Kew, who has recently returned from a visit which he paid to that colony at the request of the Colonial Government.

With reference to this subject a despatch, dated March 30, has been received at the Board of Trade from the Colonial Office, forwarding a copy of the Journal of the Society of Arts for the week ended March 20, containing Dr. Morris' paper, and stating that it is proposed to present it to Parliament among the Annual Colonial Reports.

RAILWAY TRAFFIC EARNINGS IN NEW SOUTH WALES.

From a communication received from the Agent-General for New South Wales it appears that the earnings of the New South Wales railways in December last amounted to 250,681*l.*, as compared with 278,388*l.* in December 1894. In the 12 months ended 31st December 1895, the aggregate earnings were 2,843,074*l.*, as compared with 2,835,084*l.* for the same period of 1894. The length of line open is 2,531 $\frac{1}{4}$ miles.

As regards the tramways, the receipts for December last amounted to 26,208*l.*, as against 27,131*l.* in 1894. The length of line open is 61 miles.

The railway earnings for January 1896 amounted to 247,455*l.* as against 240,763*l.* in the same month of 1895, and the tramways receipts for January 1896 were 26,605*l.*, as compared with 25,690*l.* for January 1895.

REVENUE OF WESTERN AUSTRALIA.

According to a recent number of the "British Australasian," the revenue of Western Australia for the year ended December 31, 1895, amounted to 1,438,717*l.*, as compared with 863,679*l.* for the previous year. The receipts of the various departments as compared with those in 1894 were:—Customs, 621,825*l.*, an increase of 206,742*l.*; land, 153,876*l.*, an increase of 48,187*l.*; mines, 77,885*l.*, an increase of 68,425*l.*; postages, &c., 51,143*l.*, an increase of 16,172*l.*; telegrams, 61,943*l.*, an increase of 36,278*l.*; railways, 348,741*l.*, an increase of 150,187*l.*; and stamps, 43,667*l.*, an increase of 29,190*l.* The export of gold from the colony during the same period amounted to 231,512 ozs.; valued at 879,748*l.* Of this amount Coolgardie produced 125,105 ozs.; Yilgarn, 19,747 ozs.; Wilbarra, 19,522 ozs.; and Murchison, 65,477 ozs., the balance being from other fields. The Government has authorised the expenditure of 100,000*l.* at Coolgardie for the purpose of supplying the local centres with drinking water. The population of the colony on December 31 was estimated at 101,000, as against 82,000 on the corresponding date of the previous year.

TRADE OF QUEENSLAND FOR 1895.

According to the Melbourne "Journal of Commerce" the trade of Queensland for the year ending December 31, 1895, was as under:—Imports, 5,334,273*l.*, against 4,264,635*l.* for 1894. Exports, 8,714,701*l.*, against 8,402,646*l.* The principal exports were:—Gold, 2,276,753*l.*, decrease, 105,163*l.*; hides and skins, 448,790*l.*, increase, 178,583*l.*; meat, preserved and frozen, 1,098,229*l.*, increase, 299,839*l.*; sugar, 799,336*l.*, decrease, 87,498*l.*; tallow, 593,290*l.*, increase, 124,970*l.*; and wool, 2,983,804*l.*, increase, 65,232*l.*

EXPORTS OF FROZEN MEAT FROM NEW ZEALAND PORTS.

The "New Zealand Trade Review" publishes the following comparative table of the exports of frozen meats from New Zealand ports during the years 1895 and 1894, compiled from particulars furnished by the agents of the Shaw Savill and Albion line of steamers.

The totals for the Colony compare as follows:—

				1895.	1894.
				Lbs.	Lbs.
Mutton	-	-	-	99,838,929	98,716,617
Lamb	-	-	-	26,619,737	17,819,885
Beef	-	-	-	1,580,856	192,602
Total	-	-	-	128,039,522	116,729,104

The respective proportions of the shipments from the North Island and the South Island are as follows:—

	North Island.		South Island.	
	1895.	1894.	1895.	1894.
	Lbs.	Lbs.	Lbs.	Lbs.
Mutton	43,515,170	46,720,090	56,323,759	51,996,527
Lamb	3,388,079	2,186,209	23,231,658	15,633,676
Total	46,903,249	48,906,299	79,555,417	67,630,203
Beef	1,570,400	192,602	10,456	—

There is thus an increase of over 1,000,000 lbs. in the total of mutton, nearly 9,000,000 lbs. increase in lamb, and nearly 1,500,000 in beef.

The North Island shows a decrease of 3,000,000 lbs. in mutton, an increase of 1,000,000 in lamb, and an increase of nearly 1,500,000 in beef; a net decrease of some 625,000 lbs.

The South Island has a total increase of nearly 12,000,000 lbs., of which over 4,000,000 lbs. is in mutton and less than 8,000,000 lbs. in lamb.

Shipments of beef are, as usual, practically confined to the North Island.

The falling-off in shipments of mutton from the North Island is attributable to the poor prices ruling in London in the months of March, April, and May, which induced shippers to turn their sheep into canned meat and tallow, in preference to freezing.

The heavy shipments of lambs did not give results very encouraging to shippers, and it is doubtful if so many will be sent forward this year.

The great increase in the exports of beef is connected with the experiments in chilling which however did not prove a success.

TAX ON COMMERCIAL TRAVELLERS IN NEW ZEALAND.

According to the "Australasian Trade Review," the colony of New Zealand has imposed a tax on commercial travellers from other places. The apparent object of this new form of "protection" is to secure business for the local agent, and to prevent travellers coming to sell direct to distributing houses. As an alternative, the object of the tax is to raise revenue.

There is nothing so mutually beneficial to the trade of two friendly countries, or sister colonies, as the free coming and going of commercial people. The correspondence of agent and principal is confined to necessary communications concerning business in hand; but when people come into contact personally, ideas are exchanged, and trade is facilitated in a thousand ways. The effort to prevent this free interchange of amenities is another example of the narrowing effect of the policy of protectionism. In the case of this particular tax, Victorian wine-makers dealing with those colonies which make no wines themselves, find it necessary to send representatives periodically to interview their customers and promote trade. Now that a tax is to be levied, some of these firms will cease to visit New Zealand, and it remains to be seen if the latter will gain materially by the change. Probably loss of trade will result, whereby both sides will be the losers, and the consumers of New Zealand will not be so well catered for as before.

Needless to say, the measure is very unpopular with the hotel keepers of New Zealand, as the travellers have been good customers to them, and the loss of their custom will mean a considerable item to them. The tax is 50*l.*, with a penalty of 20*l.* per day for non-payment.

XIX.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Birmingham.—The annual meeting of this chamber was held on March 18 last, Mr. T. J. Walsh presiding. The chairman moved the adoption of the annual report in a speech of some length, in the course of which he said there was a tendency to underrate the importance of foreign competition, which, if carried to any extent, would undoubtedly be prejudicial to any attempt to compete against it. No doubt other countries did fence themselves around with fiscal arrangements, which the policy of England, at any rate for the present, seemed to render totally impossible. Where they did so they paid the penalty in increased cost for the articles that they themselves employed. He thought it must be admitted that the only permanent source of increased prices was increased demand. That intelligence and scientific application should have the effect of enhancing the product of individual labour was perfectly fair, but any attempt by a combination which should put all men on precisely the same basis seemed to him a species of protection which was not likely to be generally acceptable to the country. Further, it must have a tendency to increase the already considerable advantages of the large manufacturers over the smaller ones. The vital question of the hour was no doubt how the artisan population was to be kept fully employed.

Mr. Yates seconded the motion, and in the course of his speech, referring to the trade of Madagascar, said that the French had tried to monopolise the entire trade of the island. In former years British interests nearly covered two-thirds of the trade of the island, but if the French were going to introduce their goods free of duty, and to put the same duty on British goods that they had to pay on entering the French markets, it meant destroying a great part of the trade.

A meeting of the council was subsequently held, at which it was resolved: "That Her Majesty's Government be respectfully requested to make the most urgent representations to the French Government, with a view to prevent the imposition of any commercial tariff in Madagascar which would hamper English trade with that island, England having for many years past had the same advantages there as every other country."

Blackburn.—A meeting of the Blackburn Chamber of Commerce was held on March 30 last, Mr. Henry Harrison presiding.

A memorial was presented from the Anglo-American Arbitration Board in favour of the settlement of disputes between this country

and America by arbitration. The president signed the memorial on behalf of the Chamber.

The president stated that he was not in a position to report upon the position of affairs between the Chamber and the railway companies. The one point in dispute between the companies and the cotton manufacturers had not been definitely settled. At present the companies' terms were not so favourable as they might be, and they hoped by a little modification to adjust matters satisfactorily before the next meeting.—Mr. Whittaker pointed out that under the readjustment not only those traders who had claimed rebate of charges would be entitled to relief, but also those who had "paid up" in full the amounts demanded.

The president said with regard to the Commercial Mission to China, he had been in communication with the leading authorities in London as to the appointment of agents to the Mission. They had consulted leaders both commercial and political. The great difficulty had been the securing of a suitable consul to take charge of the Mission, but they hoped to obtain a consul who would serve the Mission admirably. The leaders of the China Association in London had promised every support. As to the contributions, it was gratifying to learn that at present the fund amounted to 2,600*l.*, and one of the latest donations was that of the tape-sizers of Blackburn, who had generously contributed 25*l.* a year for three years. Mr. Whittaker urged that the Mission should be despatched without further delay. Personally, he failed to see the need of a special consul being engaged. The president remarked that the mismanagement of other missions was an object lesson to the Chamber not to send out their Mission insufficiently equipped. A sub-committee was appointed to consider the applications received for the post of experts.

Bolton.—At the last monthly meeting of the Bolton Chamber of Commerce, Mr. Alderman Nicholson presiding, correspondence was read by the secretary on the following subjects:—The metric system; prepaid postage in cash; craneage charges; the Anglo-American arbitration movement; a national trade mark to be universally registered; rating of machinery; exhibitions at Paris and Kiel; yarn contracts; railway communication between Burmah and China; telegraphic communication with lighthouses; and other matters of local interest only.

Bristol.—At the ordinary monthly meeting for April of the Bristol Chamber (Mr. George H. Perrin in the chair), the president stated that the directors of the Great Western Railway Company had been pleased to grant the Chamber's requests with reference to the issuing of special cloak-room tickets for commercial travellers at certain stations, and the addition of dining cars to several of their express trains; a letter also was read from the General Manager's office of the Great Western Railway at Paddington with reference to the revision of the first and second-class fares throughout their system.

Dundee.—The annual general meeting of the Dundee Chamber of Commerce was held on March 25 last, Mr. J. C. Buist in the chair.

The directors' report was submitted to the Chamber, and the president, in moving the adoption of the same, referred at length to the stent tax (an antiquated impost, dating as far back as 1597), and to classification for assessment purposes. A protracted discussion took place as to the desirability of appointing permanent firemen in connection with the Dundee Fire Brigade, and new directors and officials were elected for the forthcoming year.

The monthly meeting of the Directors of the Dundee Chamber was held on April 2 last, Mr. John Ogilvy, the president, in the chair. The question of a permanent fire brigade for the city, which was remitted from the annual meeting, was under consideration, and after discussion the secretary was instructed to write to Sir Thomas Thornton, the town clerk, to call the attention of the council to the subject. The proposed changes by the parish council in the classification of property for taxation purposes were also considered. The feeling of the meeting was against the proposed changes, and the secretary was instructed to point this out to the clerk of the parish council.

Liverpool.—A meeting of the committee of the African Trade Section of the Liverpool Chamber of Commerce was held on March 20 last, Mr. John Holt in the chair.

In reply to a representation made a short time back direct to the Governor of Lagos, on the subject of the recent change in the drawback and transit duty system at Lagos, Sir Gilbert Carter had replied, under date of February 20, that the letter had reached him when he was in the interior, whence he had then only recently returned. Sir Gilbert added: "It is too soon to express any opinion as to the effect which the abolition of the transit duty will have upon the general revenue, but at present I do not think the matter will be of sufficient importance to justify me in recommending any immediate change of policy so far as the spirit trade is concerned, but I should be quite prepared to recommend the free transport of British manufactured goods to Porto Novo shipped to Lagos for sale in the French colony." It was resolved that a further communication on the subject should be addressed without delay to the Governor.

Owing to the numerous deaths reported from the coast—the Gold Coast Colony in particular—it was resolved that the attention of the Secretary of the Colonies should be directed again to the paramount need for a good water supply to the unhealthy colonies.

The attention of the committee was directed by members of this section to reports that have been current to the effect that proposals have been made to the Government for the opening out of the Ashantee country by means of a chartered company. It was stated that the whole of the European firms engaged in business at the Gold Coast, together with the numerous native traders, are strongly opposed to any such mode of government.

It was resolved that a letter be written to the Foreign Office, asking whether any such proposal has been made.

Two of the Blue Books of Sierra Leone for 1894 have been received from the Colonial Secretary, Freetown, and also a copy of the Customs Duties Amendment Ordinance 1895. By this ordinance, which came into operation on January 1, 1896, the allowance of duty for under-proof has been abolished, and the duty upon ordinary gin and rum imported into the colony has been raised from 1s. 6d. to 3s. per gallon. It was stated that the result will be that stronger spirits than formerly will be imported, and that they will subsequently be reduced by water to the old strength, in order that they may be retailed at about previous prices. Some of those in the trade also complain that the purport of the ordinance was made known four days before it came into force, and that thereby merchants at Freetown were enabled to withdraw much spirits from bond at the old rate of duty. This is now being sold in competition with spirits paying the higher duty, and trade has suffered disorganisation in consequence.

A meeting of the council of the Chamber as a General Purposes Committee was held in the board-room on March 20 last, under the chairmanship of the president, Mr. Charles McArthur. Letters from the Liverpool Steamship Owners' Association and the cattle importers of Liverpool, together with special communications from the Board of Agriculture on the subject of Diseases of Animals Bill, 1896, were considered; also a letter from the Steamship Owners' Association on the subject of the Foreign Goods (Prevention of Fraud) Bills. Resolutions on the subject of the Bills were adopted, and a letter was despatched to all the members of Parliament who are honorary members of the council of the chamber, 22 in all.

London.—The fourteenth annual meeting of the London Chamber of Commerce was held on the 30th March last, Sir A. K. Rollit, M.P., in the chair.

Speaking of foreign affairs generally, the president said that the cotton and tin-plate trades had been in a depressed condition owing to the uncertain state of affairs in America. He said that matters in general were improving very rapidly, and he mentioned the interesting fact that Luton has been enabled, since they adopted a system of technical education in that town, to get back all the old trade in straw hats, for which the place had been famous. Company law was also considered by the Committee of the Chamber on Law. Companies had been of great advantage by the facilities for investing money, and had served to attract a great deal of money from abroad, especially from France. The Departmental Committee had tried to provide means for the public to protect themselves by ensuring publicity, thorough audits, and by making the statutory meeting a reality.

They believed that light railways would be of much benefit to agriculture, and he hoped that the rates for traffic on all the railways would be reduced in the interests of the railway companies themselves.

With regard to the docks, he said the Chamber felt that the dock companies would be wise to do nothing which would tend to further hinder the trade with the London docks. The tendency of trade, owing to the Suez Canal, was to create emporiums in Middle Europe, and they should in London see that the trade with the Thames should be as free as possible.

The progress realised under the Chamber's commercial education scheme, although not as rapid as might perhaps have been expected, continued to be satisfactory, and there were indications of possible considerable developments in the early future. During the year the London Labour Conciliation and Arbitration Board (established by the Chamber, with the co-operation of the leading trade unions, in 1890) continued its operations in regard to the amicable settlement of labour disputes, its services having been offered in several cases.

Manchester.—An ordinary meeting of the Board of Directors of this Chamber was held on March 11 last, Mr. W. H. Holland in the chair.

From the minutes of the Correspondence and Finance Committee it appeared that representations to the Chamber from British merchants at Rio de Janeiro had been under the consideration of the committee, with regard to the state of business there, as affected by the diplomatic questions arising out of recent proceedings in the island of Trinidad, and a letter was written to the Foreign Office on the subject.

A letter from a correspondent at St. Louis was laid before the board, stating that he had perfected a plan by which great economy could be secured from ginning cotton, by an improved system, at the mill rather than at the plantation. Cotton, with the adhering seed, would be sent to the mill at an increased expense for freight estimated by the correspondent at 70 per cent. On the other hand, he claimed to have demonstrated that the fibre would be improved in value to the extent of from $\frac{1}{2}d.$ to $1\frac{1}{2}d.$ per lb., according to the original length of the staple. By shipping the cotton in bulk a further saving would be effected of the cost of compressing, packing, and packing materials, together with the freight on the latter. The net resultant economy appeared to be, according to the data supplied by the correspondent, from $\frac{3}{4}d.$ to $1\frac{3}{4}d.$ per lb. The president reported that a copy of the letter had been forwarded to the Oldham Chamber of Commerce, requesting its observations upon the proposed change. It was resolved that further information should be sought from the correspondent, and that samples of cotton ginned by the new process and the existing one should be asked for, in order that an opinion might be formed as to the alleged improvement in value.

At the monthly meeting of the Board of Directors held in December last, a copy of a memorial to the Secretary of State for India from a number of Turkey-red dyers in the West of Scotland was under consideration. The memorial had been sent to this Chamber with a request for its support. The memorialists complained of the illicit competition of certain foreign exporters

to India of dyed yarn. By a notification four years ago of the Governor-General of India in Council, which under the above-named Act he was empowered to promulgate, the "count" or "number" of cotton yarn was thenceforward to be held to be an indication of length. The regulation was, however, being evaded by certain non-British dyers, who gave short lengths of hanks and marked the total number of yards on a ticket outside the bundle in an inconspicuous manner. For example, a bundle of 40's ought to give 327,600 yards to a bundle. But, although placing the mark "245,000 yards genuine number 40" outside the bundle, the exporter gave 82,600 yards short of the total standard length, to the great prejudice of honest exporters. The memorialists' concluding prayer was that the attention of the Custom-house authorities in India might be drawn to the matter, and that, if need be, new or amended regulations might be issued which would prevent all possibility of the continuance of the deception.

At the request of the yarn sectional committee, a sample bundle of the yarn had been forwarded to Manchester for examination. The report of the testing-house thereupon showed that the average length of 12 hanks was $619\frac{3}{4}$ yards per hank. The yarn sectional committee, whilst supporting the contention of the memorialists that the proper length of a hank of dyed yarns is 819 yards—which is, indeed, officially affirmed in regulation 4 of the 13th November 1891, of the Indian Merchandise Marks Act,—did not advise the Board to take action at present, in view of the fact that it did not appear that the assistance of the Glasgow Chamber of Commerce had been invoked in the matter. The Board adopted this course, and resolved that the memorialists should be informed of the decision.

A letter from the Colonial Office was submitted, transmitting a memorandum of all the most recent information in the possession of that office upon the spirit traffic in West Africa, which has formed the subject of previous correspondence between the Chamber and the Secretary of State for the Colonies. Other documents were forwarded, including statistics of the spirit trade in British West Africa from 1879 to 1894. Mr. Chamberlain desired to receive information and advice from the Chamber on the whole question. The papers were referred to the African sectional committee for consideration and report.

North Shields.—The annual meeting of this Chamber was held on 3rd March last, Mr. Leonard Macarthy in the chair. The chairman, in moving the adoption of the report, reviewed the trade of the Tyne, including 1893–5, and also the general trade of the country. During the discussion of the report, reference was made to the absence of State aid to the harbour, seeing that it is so important as a refuge, and that over two millions have been spent on the piers and necessary dredging to make it available at all times and in any weather. It should have been a national work. In case naval operations should ever be necessary in the North Sea, ships of the British Fleet could have any repairs, refitting, revictualing, and even re-arming,

within the ports of the Tyne, wherein really exists a naval arsenal for the north-east coast.

North Staffordshire.—A monthly meeting of this Chamber was held on March 18 last, Mr. R. C. Clive in the chair. The meeting was chiefly occupied with discussing the decision of the Board of Trade with reference to the classification of loose earthenware, and as to the excess of the London rate of the North Staffordshire Railway in North Staffordshire over competitive districts, a communication which had been received from Mr. W. D. Phillips, general manager of the above railway, not furnishing any reply as to that particular point.

The Board of Trade have also received copies of the annual reports of the Birmingham, Blackburn and District, and Belfast Chambers of Commerce, and of the Manchester Monthly Record.

XX.—RECENT TRADE BLUE BOOKS.

1. *Coal Production (Number of Persons employed). Statement showing the Production and Consumption of Coal, and the Number of Persons employed in Coal Production, in the Principal Countries of the World, in each Year from 1883 to 1894, as far as the particulars can be stated.* 462. Sess. 2. Price 5d.

This is the second issue of a Return made to an Order of the House of Commons by the Board of Trade. It shows the quantity and value of Coal produced in the principal countries of Europe, in Japan and the United States of America; the quantity and value of Coal produced in the British colonies and possessions; the total quantity of Coal remaining for home consumption, and the consumption per head of the population in the principal countries of Europe, in Japan and in the United States of America; and the total quantity of Coal remaining for home consumption, and consumption per head of the population in the British colonies and possessions; as well as detailed statements for each country, showing the percentages of Coal consumed, distinguishing between the home produce, the Coal of British production, and the Coal the produce of other countries, in each year from 1883 to 1894 inclusive.

It is stated in the general report or introduction which is prefixed to the present return, that the statistics given in the first issue of these tables have been continued in the present number to the latest date for which it has been practicable to obtain the necessary information. In the first issue of these tables it was stated that the Board of Trade hoped to be in a position in future issues to add Tables showing the amount of coal of home production consumed in each country, the amount of British coal consumed by each country, and the amount of coal the produce of other foreign countries consumed in each; and also tables

showing the production of coal in the colonial and other possessions of the United Kingdom. Both these desired objects the Board have been enabled to realise, and in the present issue there have been included statements showing the amount of British coal consumed by each country, as well as details with regard to the coal production and consumption in the principal British colonies and in British East India, which although in some cases incomplete, are still of much interest when compared with the production of Coal in foreign countries.

It is also stated that some of the information which the Board of Trade has been able to obtain with regard to the coal-mining industry in foreign countries and in the colonies has been for many years past published in the "Statistical Abstract for Foreign Countries," as well as in the most recent issue of the "Abstract of Statistics for the Colonies," but that the present is the first attempt to give in a comprehensive form the coal industry of the more important countries of the world.

2. *Agricultural Statistics, Ireland. Tables showing the extent in statute acres and the Produce of the Crops for the Year 1895, with observations of the District Inspectors of the Royal Irish Constabulary and of the Sergeants of the Metropolitan Police who acted as Superintendents of the Agricultural Statistics, together with Tables showing the Average Yearly Extent under the Principal Crops, and the Average Produce thereof in the Ten Years 1885-94; also Bee-keeping Statistics for the Season 1894. (C.—7956.) Price 3½d.*

This is the annual report of the Registrar-General on the produce of the crops in Ireland. The present issue is for the year 1895. In submitting his Report the Registrar-General states that, comparing the extents under the several cereal crops in 1895 with those for 1894, he finds a decrease of 12,806, or 26·0 per cent., in the number of acres under wheat; a decrease of 38,436 acres, or 3·1 per cent., in oats; a decrease from 176 to 139 acres in bere; and a decrease of 406 acres, or 3·4 per cent., under rye; with an increase of 7,055 acres, or 4·3 per cent., in barley. In green crops, potatoes decreased by 6,604 acres, or 0·9 per cent.; turnips increased by 1,971 acres, or 0·6 per cent.; and mangel-wurzel and beetroot increased by 988 acres, or 1·9 per cent. In other crops, flax decreased by 5,878 acres, or 5·8 per cent.; meadow on clover, sainfoin, and grasses under rotation decreased by 5,472 acres, or 0·9 per cent.; while meadow on permanent pasture or grass not broken up in rotation increased by 17,350 acres, or 1·1 per cent. Compared with the average acreage for the ten years 1885-94, turnips increased by 13,399 acres, or 4·5 per cent.; mangel-wurzel and beetroot by 7,533 acres, or 16·6 per cent.; and hay by 61,642 acres, or 2·9 per cent. Wheat shows a decrease of 38,419 acres, or 51·3 per cent., compared with the average; in oats there is a decrease of 48,754 acres, or 3·9 per cent.; in barley a decrease of 3,169 acres, or 1·8 per cent.; the area under bere declined from 326 acres to 139 acres; rye decreased 1,080

acres,⁷ or 8·6 per cent.; and flax decreased 5,233 acres, or 5·2 per cent. Potatoes have decreased by 59,600 acres, or 7·7 per cent.

The average yield per acre of cereal crops in 1895, compared with 1894, exhibits a decrease in wheat of 0·3 cwt., in oats of 0·4 cwt., and in barley of 0·5 cwt., while there is an increase in bere of 0·8 cwt., and in rye of 0·7 cwt. In other crops, potatoes show an increase of 2·3 tons, turnips of 0·6 ton, mangel-wurzel and beet of 1·0 ton, while flax shows a decrease of 13·5 stones. Hay on clover, sainfoin, and grasses under rotation shows a decrease of 0·5 ton, and hay on permanent pasture or grass not broken up in rotation, a decrease of 0·3 ton.

3. *Mines and Minerals. First Annual General Report upon the Mineral Industry of the United Kingdom of Great Britain and Ireland, for the Year 1894. By C. Le Neve Foster, D.Sc., F.R.S., one of Her Majesty's Inspectors of Mines. (C.—7953.) Price 3s. 4d.*

This is the first issue of an annual general Report upon the Mining industry of the United Kingdom, and is prepared by the Home Office. It is stated in the Report that the present return has been compiled with the object of carrying out one of the recommendations of the Royal Commission upon Mining Royalties, which was indorsed by the Departmental Committee upon Mining and Mineral Statistics; part of the Committee's Report being as follows:—

“One of the principal defects of the present system, under which the statistical returns and reports of the inspectors appear, is that there is no general statement with respect to the mining industry as a whole. In other departments, such as those which deal with factories, education, and local government, a general report is presented annually to Parliament; but no comprehensive idea of the condition of the mining industry can be obtained without searching through the thirteen separate reports of the inspectors and the different volumes of the statistical returns.”

“The Committee are therefore of opinion that a general report upon the mining industry of Great Britain and Ireland, as suggested by the Royal Commission on Mining Royalties, should be prepared annually, and that it should be based upon the separate reports of the inspectors of mines and upon the mineral statistics.”

“It should contain tabulated statements which will enable comparisons to be made between the different mining fields, counties, or districts, as regards persons employed, output, accidents, death rates from accidents, royalties, wayleaves, hours of labour, wages, consumption, and distribution. These comparisons should be illustrated as far as possible by diagrams. The mode of occurrence of the different minerals should be briefly described, and other matters of general interest relating to mining might be added.”

"The report should also institute a comparison between the mining industry of the United Kingdom and that of foreign countries, as suggested by the Royal Commission."

The present return is therefore made with the object of fulfilling those requirements. The return itself shows the number of persons employed in the mining industry, the quantity of minerals raised, accidents which have occurred, prosecutions, and comparison with foreign countries. In an appendix are given tables showing the number of persons employed in and about coal mines during 1894 arranged according to age, sex, and counties; information relating to persons employed, accidents, and deaths, from 1851 to 1894; production of coal mines during 1894 arranged according to counties; production of iron ore during 1894 arranged according to counties; production of other mines during 1894 arranged according to counties; list of official publications relating to mining and minerals; index to the 13 separate reports of Her Majesty's inspectors of mines; glossary of technical terms used in the reports of the inspectors of mines; lists of persons holding certificates of competency as managers and under-managers of mines, and certificates of service, granted in 1894; list of official inquiries by the Board of Trade with respect to boiler explosions in mines in 1894; and list of formal investigations by the Board of Trade into causes of spontaneous combustion or explosions of coal gas on board ships in 1894.

4. *Reports by Her Majesty's Representatives Abroad on Bounties other than those on Shipping and Navigation paid by the State in the Countries in which they Reside. Commercial, No. 2 (1896). (C.—7974.) Price 2d.*

The present issue is stated to be in continuation of Parliamentary Paper, Commercial, No. 7 (1895), (C.—7897). That publication contained the replies of Her Majesty's Representatives abroad to the following circular addressed by the Earl of Kimberley:—

"Foreign Office,

"Sir,

"February 1895.

"With reference to the circular of the 2nd May 1889 relative to bounties, I enclose a copy of a letter from the Board of Trade, and in accordance with the request made therein I have to request you to furnish me as soon as convenient with a report on bounties other than those on shipping and navigation.

"I am, &c.,

"(Signed) KIMBERLEY.

Amongst the replies contained in the paper mentioned above (C.—7897) is one from Her Majesty's Representative in the United States, and the present issue contains a despatch from Her Majesty's Ambassador at Washington, correcting an error and supplementing information contained in Parliamentary Paper, "Commercial, No. 7 (1895)," p. 70.

5. *Inspectors of Meat.* 74. Price 1½d.

The present statement, which is prepared by the Home Office in answer to an address of the House of Commons, dated the 27th August 1895, contains a return showing—(1) the number of officials employed as meat inspectors in London, Liverpool, Manchester, Birmingham, Bolton, Birkenhead, Bradford, Blackpool, Hull, Nottingham, Derby, Portsmouth, Glasgow, Edinburgh, Dundee, Paisley, Perth, and Greenock; (2) date of their appointment; (3) qualifications for appointment; (4) vocation prior to receiving appointment; and (5) whether such officials act as meat inspectors only, or hold any office of emolument in a department other than that supervised by the medical officer of health.

The return is dated 24th February 1896.

6. *Royal Commission on Agriculture. Second Report of Her Majesty's Commissioners appointed to inquire into the subject of Agricultural Depression.* (C.—7981.) Price 6d.

This is the second report of the Royal Commission appointed in September 1893 to inquire into the agricultural depression prevailing in the United Kingdom, and whether it can be alleviated by legislation or other measures. The present Report is divided into two parts. Part I., being the Report signed by the majority, contains observations on the burdens on land, the land tax, imperial taxation generally, local taxation, loans for agricultural improvements. Reservations by Lord Cobham and by Mr. Clay are also attached to this report, as well as a Memorandum by Captain Owen Thomas and Mr. Everett. Part II., being the Report signed by the minority, contains observations on the land tax, on tithe, on local rates, and imperial taxation. To this report is attached a supplementary Memorandum by the Chairman (The Right Hon. G. J. Shaw-Lefevre) on the subject of local rates on agricultural land in England and Wales, also observations by Sir R. Giffen on the incidence of imperial taxation on property, together with reports by Mr. F. A. Channing, M.P., and by Mr. G. Lambert, M.P.;

7. *Foreign Merchant Navies (Educational Standard of Officers) Return.* 328. Price 1s. 1d.

This is a return showing the educational standard required from candidates for positions as masters, mates, and engineers, in the merchant navies of France, Germany, Russia, Belgium, Holland, Sweden and Norway, the United States, and the United Kingdom. The present issue is Part I., and refers entirely to the educational standard required in the United Kingdom. That required for positions as masters, mates, and engineers in foreign merchant navies will be found in Parliamentary Paper C.—7711. See Board of Trade Journal, July 1895.

XXI.—QUARANTINE NOTICES.

The following information with regard to quarantine, received from Her Majesty's representatives abroad, has been communicated to the Board of Trade, through the Secretary of State for Foreign Affairs :—

NORWAY.—Galacia and the Russian Governments of Kiew and Volhynia have been removed from the list of cholera-smitten places.

PORTUGAL.—Buenos Ayres is reported infected with yellow fever, and all other ports of the same State in River Plate suspected from February 25.

Bahia has been declared infected with yellow fever, and all other ports of the Province suspected since February 1.

SPAIN.—Arrivals from Bangkok and ports within 165 kiloms. of that place will be granted free pratique if they have a clean bill of health issued by the Spanish Consul, or in default, by a Consul of some other country, the health on board being satisfactory, without any suspicious case.

GREECE.—A medical visit has been substituted at Athens for quarantine against Egyptian ports.

BULGARIA.—Medical inspection is imposed from the 17th instant, on vessels from Odessa and other Russian and Turkish Black Sea ports which have not been inspected at Sulina. If their bilge water and drinking water have not been changed at Sulina they will be changed at the first Bulgarian port called at.

TURKEY.—Quarantine on ships carrying pilgrims from Egyptian ports on the Red Sea has been reduced to five days, and quarantine on arrivals from Alexandria has been suppressed and medical visit substituted.

Medical visit on arrivals from Egyptian Mediterranean ports has been abolished.

Arrivals from Bangkok are admitted to free pratique.

BRAZIL.—There is serious yellow fever at Rio Janeiro which is likely to last. In consequence of the non-completion of the promised hospital of isolation the only available quarantine station is now used as a yellow fever hospital for the crew of an Italian ship-of-war which has, out of 280, had 225 cases, and 84 fatal.

FINES IN BRAZILIAN PORTS IN CONNECTION WITH QUARANTINE REGULATIONS.

A despatch, dated April 1 last, has been received at the Foreign Office from Mr. Phipps, Her Majesty's Minister at Rio Janeiro, transmitting the following list of fines which may be incurred by shipping in Brazilian ports in connection with quarantine regulations:—

	Dols.
Giving incorrect information on arrival respecting occurrences on board - - -	200
Concealing sick persons on board whatever the nature of sickness; sending them to hospital without previous permission of sanitary authority; or taking a medical man on board without the same permission - -	200
And, if the sickness is pestilent, for each sick person -	500
N.B.—Any medical man going on board to examine and treat patients without previous permission of sanitary authority is fined 200 dols.	
Failing to carry out the disinfecting and sanitary measures ordered by sanitary authority within the time marked; or failing to effect the change of anchorage determined - - -	100
N.B.—This fine is doubled for recurrences.	
Allowing entrance to or withdrawal from ship in quarantine of persons other than those in the sanitary service -	200
To be repeated on each occurrence.	
Changing anchorage of a ship in quarantine without previous permission of the sanitary authority -	200
Omitting to bring bill of health from port of departure or ports of call - - - -	200
N.B.—All ports where there is no Brazilian Consul captains should obtain bills of health from local authorities or consuls of other countries.	
Infringing the terms of any permit granted by sanitary authority - - - -	200
Any infraction of sanitary regulations for which no special fine is awarded will be punished with a fine of 20 dols. to 50 dols., to be doubled on recurrence.	

NOTE.—Where fines are collected by the Director of the Lazaretto and not by the Custom House, the loading or discharging work will be stopped until the fine is paid. If such work is already finished the Doctor of the Lazaretto will not endorse the bill of health or grant pratique until the fine is paid.

Should the vessel leave without paying the fine no other vessel belonging to the same owners or commanded by the same captain will be allowed to load or discharge in the port so long as the fine is left unsettled.

The following information has been received at the Board of Trade through the Secretary of State for the colonies :—

GIBRALTAR.—Ships from Alexandria 12 days out obtain free pratique if good health on board.

MALTA.—Government Notice, No. 51.—His Excellency the Governor, having heard the opinion of the Board of Health, has been pleased to revoke Government Notice No. 152 of 27th July 1895, and to direct that the following regulations be observed, viz. :—

1. Any shipment of cattle, any part of which is found to be suffering, or to be suspected of suffering, from an infectious or contagious disease at the time of arrival, or after a period of inspection is to be detained in quarantine.

2. Should the existence of cattle-plague be verified before landing, the cargo will not be allowed to land.

3. The importation of cattle of any kind from Egypt, the Province of Naples, Calabria, Sicily, Syria (including Alexandretta), and Salonica is prohibited, and the introduction into these islands of wool, hides, skins, horns, bones, and hoofs from the above places and from countries where epidemic diseases are prevailing, is likewise prohibited.

4. Cattle intended for importation into these islands must be accompanied by a certificate stating the place of their origin and the fact that cattle disease has not prevailed in that place (with, as far as can be ascertained, the route followed from any inland station to the port of embarkation) during the three months previous to the date of embarkation. Such certificate shall be given by the British Consular Authority at the places of origin and embarkation, or if there be no such British Consular Authority, by the local Government Authority of the places of origin and embarkation.

5. Vessels arriving in this island, having on board more than 100 head of cattle (horses not being included) are to go into the quarantine harbour, and the cattle are to be inspected in the cattle enclosures of the Lazaretto.

6. The importation of cattle from the Black Sea, Sea of Azov, Roumelia, and Thessaly, not otherwise prohibited, may be permitted subject to the undermentioned conditions; provided that, in the opinion of the chief government medical officer, such cattle are not affected with disease so that they may be admitted in the Lazaretto without detriment to cattle already in the island ;

(a.) Such cattle shall only be admitted to the enclosures which may be assigned or approved for that purpose at the Lazaretto, and they shall be subjected to isolation and observation for three months.

(b.) Application must be made for leave to import, and permission is to be obtained in writing from Government in each case before shipment, stating the approximate number of the animals intended to be shipped. No cattle shall be allowed to be landed unless the consignee has obtained such leave from Government and agreed to abide by any regulations which may be established.

(c.) The application referred to in the preceding Article shall imply that the applicant has consented to pay the usual fees for entrance into the Lazaretto, and for guards and extraordinary expenses on each head of cattle, besides 2s. 6d. on each head of cattle per month, or part thereof, in cases in which it is found necessary to keep cattle in the Lazaretto for a longer period than 15 days.

(d.) That such cattle, if slaughtered at the Lazaretto, will be subject to the regulations which may from time to time be made by the port authorities.

(e.) The owners of cattle in the Lazaretto shall be subjected to such regulations as to slaughtering of cattle, disposal of offal, blood, &c., as may from time to time be made by the port authorities.

The importation, however, of horses, mules, and asses from Odessa is prohibited.

7. Swine arriving from Albania and Greece may be imported subject to 20 days' quarantine at the Lazaretto, or in private pens, and to any restrictions which the Chief Government Medical Officer may consider necessary.

8. The importation of dogs is subjected to three months quarantine.

Palace Valletta, March 21, 1896.

XXII.—POST OFFICE NOTICES.

EXTENSION OF MONEY ORDER SYSTEM TO SERVIA.

On and after the 1st April 1896, money orders payable at any place in Servia will be obtainable at any money order office in the United Kingdom.

The orders should not be forwarded to the payees, but should be retained by the remitters as receipts.

The commission chargeable in this country will be the same as for money orders payable at other places abroad, namely:—

			s.	d.
For sums not exceeding 2 <i>l.</i>	-	-	0	6
„ exceeding 2 <i>l.</i> and not exceeding 5 <i>l.</i>	-	-	1	0
„ „ 5 <i>l.</i> „ „ 7 <i>l.</i>	-	-	1	6
„ „ 7 <i>l.</i> „ „ 10 <i>l.</i>	-	-	2	0

A further commission of 1 per cent. on the amount of each order will be deducted by the Swiss Post Office, through which the exchange of money orders with Servia will be carried on.

No single money order will be issued for a larger amount than 10*l.*

Money orders will also be issued in Servia payable in the United Kingdom, subject to a deduction of 1 per cent. by the Swiss Post Office.

MAILS FOR ST. HELENA AND ASCENSION.

The working of the Mail Service to and from the Islands of St. Helena and Ascension during the next few months will be as follows:—

OUTWARD.		HOMEWARD.		
Leave London.	Arrive at St. Helena about	Leave St. Helena about	Arrive at Ascension (leaving same day) about	Arrive in London about
—	—	17 April -	20 April -	4 May.
Morning 25 April -	11 May.	20 May -	22 May -	5 June.
Morning, 23 May -	8 June.	17 June -	19 June -	3 July.
Morning, 20 June -	6 July.	15 July -	17 July -	31 July.
Morning, 18 July -	3 August.	12 August -	14 August -	28 August.
Morning, 15 Aug. -	31 August.	9 September	11 September	25 Sept.
Morning, 12 Sept. -	28 September.	7 October -	9 October -	23 October.

In the homeward direction the actual dates of sailing and arrival cannot be absolutely guaranteed, but the dates given in the foregoing table will be adhered to as closely as possible.

Parcel Mails will be made up on the afternoon of the 23rd of April, the 21st of May, the 18th of June, the 16th of July, the 13th of August, and the 10th of September.

XXIII.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month and Three Months ended 31st March 1896, compared with the corresponding periods of the year 1895.

MONTH ENDED 31ST MARCH.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 31st March		Increase.	Decrease.
	1896.	1895.		
I. Animals, living (for food) - - -	£ 946,257	£ 620,971	£ 325,286	£ —
II. (A.) Articles of food and drink, duty free - - -	12,132,204	10,691,199	1,441,005	—
(B.) Articles of food and drink, dutiable - - -	2,188,624	2,450,617	—	261,993
Tobacco, dutiable - - -	351,105	229,747	121,358	—
III. Metals - - -	1,580,956	1,379,370	201,586	—
IV. Chemicals, dyestuffs, and tanning substances - - -	953,968	885,527	68,441	—
V. Oils - - -	581,593	680,297	—	98,704
VI. Raw materials for textile manufactures - - -	7,094,103	8,049,763	—	955,660
VII. Raw materials for sundry industries and manufactures - - -	3,330,295	3,131,237	199,058	—
VIII. Manufactured articles - - -	7,868,203	6,508,004	1,360,199	—
IX. (A.) Miscellaneous articles - - -	1,226,724	1,251,305	—	24,581
(B.) Parcel post - - -	90,718	81,853	8,865	—
Total Value - - -	38,344,750	35,959,890	2,384,860	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - -	54,918	42,313	12,605	—
II. Articles of food and drink - - -	802,682	815,028	—	12,346
III. Raw materials - - -	1,315,365	1,418,960	—	103,595
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - - -	9,343,265	8,505,620	837,645	—
(B.) Metals and articles manufactured therefrom (except machinery) - - -	2,684,456	2,229,136	455,320	—
(C.) Machinery and millwork - - -	1,341,020	1,121,680	219,340	—
(D.) Apparel and articles of personal use - - -	937,875	777,516	160,329	—
(E.) Chemicals and chemical and medicinal preparations - - -	867,574	739,269	128,305	—
(F.) All other articles, either manufactured or partly manufactured - - -	2,931,006	2,767,391	163,615	—
(G.) Parcel post - - -	144,258	106,087	38,171	—
Total Value - - -	20,422,419	18,523,030	1,899,389	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	4,623,326	4,846,599	—	223,273

I.—FOREIGN TRADE OF THE UNITED KINGDOM—*cont.*

THREE MONTHS ENDED 31ST MARCH.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Three Months ended 31st March		Increase.	Decrease.
	1896.	1895.		
	£	£	£	£
I. Animals, living (for food) - -	2,563,611	1,588,504	975,107	—
II. (A.) Articles of food and drink, duty free - -	36,045,935	31,228,709	4,817,226	—
(B.) Articles of food and drink, dutiable - -	5,830,556	6,029,676	—	199,120
Tobacco, dutiable - -	968,179	640,934	327,245	—
III. Metals - -	4,932,256	4,282,785	649,471	—
IV. Chemicals, dyestuffs, and tanning substances - -	2,687,687	2,106,503	581,184	—
V. Oils - -	2,181,841	1,761,260	420,581	—
VI. Raw materials for textile manufactures - -	23,413,080	24,426,165	—	1,013,085
VII. Raw materials for sundry industries and manufactures - -	9,266,435	7,699,762	1,566,673	—
VIII. Manufactured articles - -	20,488,191	17,568,313	2,919,878	—
IX. (A.) Miscellaneous articles - -	3,610,754	3,229,353	381,401	—
(B.) Parcel post - -	306,817	275,896	30,921	—
Total Value - -	112,295,342	100,837,860	11,457,482	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURES.

	£	£	£	£
I. Animals, living - -	160,396	117,850	42,546	—
II. Articles of food and drink - -	2,409,709	2,144,836	264,873	—
III. Raw materials - -	4,076,142	3,817,991	258,151	—
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - -	28,890,646	25,806,119	3,084,497	—
(B.) Metals and articles manufactured therefrom (except machinery) - -	7,833,262	6,074,707	1,758,555	—
(C.) Machinery and millwork - -	3,889,459	3,204,062	685,397	—
(D.) Apparel and articles of personal use - -	2,847,701	2,270,233	577,468	—
(E.) Chemicals and chemical and medicinal preparations - -	2,316,820	2,015,412	331,408	—
(F.) All other articles, either manufactured or partly manufactured - -	8,404,315	6,993,909	1,410,406	—
(G.) Parcel post - -	374,593	275,212	99,381	—
Total Value - -	61,233,043	52,720,561	8,512,682	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - -	15,087,265	13,141,681	1,945,584	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 7th March 1896 -	32,974 7	51,274 0	20,879 7
" 14th " " -	36,004 6	44,655 7	18,524 2
" 21st " " -	34,404 7	38,111 2	16,996 5
" 28th " " -	30,156 5	36,163 2	12,291 4
March 1896 - - -	133,541 1	170,204 3	68,692 2
Corresponding month in 1895 -	267,583 5	241,253 6	102,917 4
" " 1894 -	208,720 7	117,845 6	63,503 6
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 7th March 1896 -	25 4	21 5	13 8
" 14th " " -	25 5	21 3	13 10
" 21th " " -	25 1	21 1	13 9
" 28th " " -	24 10	21 4	13 4
March 1896 - - -	25 2	21 3	13 7
February 1896 - - -	26 2	22 2	13 11
January " - - -	25 7	23 10	13 10
December 1895 - - -	24 11	23 11	13 11
November " - - -	25 9	25 1	14 2
October " - - -	24 6	25 5	13 5
September " - - -	22 10	24 0	13 7
August " - - -	24 2	20 3	15 7
July " - - -	24 9	19 8	16 1
June " - - -	25 9	19 7	16 0
May " - - -	23 5	20 5	15 2
April " - - -	20 5	21 0	14 2
March 1895 - - -	19 11	21 3	13 10
" 1894 - - -	24 3	27 2	18 2
" 1893 - - -	24 10	25 2	18 2
" 1892 - - -	32 11	27 8	20 4

III.—EMIGRATION.—MARCH 1896.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out of EUROPE during the Month ended 31st March 1896, and the Three Months ended 31st March 1896, compared with the corresponding Periods of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.	Total for corresponding Month, 1895.
Month ended 31st March.							
English - -	3,036	1,054	527	1,555	722	6,894	7,808
Scotch - -	525	19	57	228	83	912	1,245
Irish - -	2,894	59	27	55	36	3,071	4,326
Total of British origin - }	4,455	1,132	611	1,838	841	10,877	13,379
Foreigners - -	5,329	538	30	571	179	6,647	5,935
Nationalities not distinguished - }	7	12	3	—	195	217	272
Total - -	11,791	1,682	644	2,409	1,215	17,741	19,586
Total for corresponding Month 1895 - }	14,464	1,608	653	1,740	1,122	19,586	—
Three Months ended 31st March.							
English - -	7,866	1,854	1,759	4,603	2,731	18,813	18,626
Scotch - -	1,393	82	142	481	291	2,389	2,262
Irish - -	4,451	128	83	97	100	4,859	6,203
Total of British origin - }	13,710	2,064	1,984	5,181	3,122	26,061	27,090
Foreigners - -	10,285	954	40	1,728	412	13,419	11,420
Nationalities not distinguished - }	10	12	3	—	719	744	698
Total - -	24,005	3,030	2,027	6,909	4,253	40,224	39,208
Total for corresponding Months 1895 - }	26,910	2,646	2,130	4,230	3,292	39,208	—

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION—MARCH 1896.

RETURN of the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM, during the Month and Three Months ended 31st March 1896, compared with the corresponding Periods of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.
Month ended 31st March.												
Aliens not stated to be en route to America: *												
Arrived at London -	699	537	218	237	116	96	—	—	120	97	1,153	967
" Grimsby -	111	80	29	35	—	11	—	2	32	11	172	139
" Hull -	87	44	80	174	60	30	—	—	198	45	425	293
" Tyne Ports -	82	36	23	10	236	146	—	—	236	105	577	297
" Leith -	31	12	83	36	—	—	—	—	21	9	135	57
" Newhaven -	—	—	—	—	—	—	835	762	—	—	835	762
" Other Ports -	59	69	56	31	136	136	—	—	263	208	514	444
Total -	1,069	778	489	523	548	419	835	764	870	475	13,811	12,959
Aliens en route to America:												
Arrived at Grimsby -	56	38	103	229	1,075	620	—	—	186	274	1,420	1,161
" Hartlepool, } " West - } " Hull -	111	92	—	—	—	—	—	—	—	—	111	92
" Leith -	92	81	1	28	1,633	1,260	—	—	1,680	599	3,406	1,968
" Other Ports -	26	133	—	5	—	—	—	—	56	1	82	139
"	16	8	2	23	7	26	—	—	696	683	721	735
Total -	301	347	106	285	2,715	1,906	—	—	2,618	1,557	5,740	4,095
Total of Aliens en route and of Aliens not stated to be en route to America * }	1,370	1,125	595	808	3,263	2,323	835	764	3,488	2,032	9,551	7,054
Three Months ended 31st March.												
Aliens not stated to be en route to America: *												
Total No. -	3,021	1,894	1,194	1,035	1,219	1,033	1,619	1,408	1,923	1,163	13,976	16,333
Aliens en route to America: Total No. -	913	756	197	552	3,778	2,817	—	—	4,008	2,913	8,896	7,033
Total of Aliens en route and of Aliens not stated to be en route to America * }	3,934	2,650	1,391	1,587	4,997	3,850	1,619	1,408	5,931	4,076	17,872	23,366

* The distinction made in this Return between "Aliens not stated to be en route to America," and "Aliens en route to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be en route to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 213, Sess. 1895, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Goole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers, and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the Aliens who arrived at ports in the United Kingdom not en route to America in the month of March 1896 was 1,064 and 746 in the same month of 1895, in the three months ended March of 1896 the number was 2,397, and in the same months of 1895 the number was 1,896.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER of RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	March		Three Months ended March	
	1896.	1895.	1896.	1895.
Total gazetted - - - -	No. 436	No. 455	No. 1,157	No. 1,181
Number gazetted in principal trades and occupations:—				
Grocers, &c. - - - -	32	37	93	85
Publicans and hotel keepers, &c. -	38	33	90	99
Farmers - - - -	26	35	70	88
Builders - - - -	20	15	68	61
Butchers - - - -	10	16	33	34
Decorators, painters, plumbers, &c. -	14	6	28	20
Greengrocers, fruiterers, &c. -	9	11	26	28
Boot and shoe manufacturers and dealers - - - -	13	13	26	30
Drapers, haberdashers, &c. - -	13	9	25	20
Bakers - - - -	12	10	23	31
Tailors, &c. - - - -	5	11	18	26
Confectioners - - - -	4	4	17	7
Clerks, commercial and general -	6	6	16	15
Auctioneers - - - -	3	5	15	13
Solicitors - - - -	2	5	14	9
Agents, commission and general -	4	5	13	18
Corn, flour, seed, hay, and straw merchants and dealers - -	7	3	13	12
Jewellers, watchmakers, importers, silversmiths, &c. - - -	3	4	12	8
Carpenters and joiners - - -	6	4	12	11
Cabinet makers and upholsterers -	7	3	12	5
Schoolmasters and schoolmistresses -	4	2	11	6
Clothiers, outfitters, &c. - -	5	7	11	11
Merchants - - - -	5	5	11	9
Fishmongers, poulterers, &c. - -	6	3	11	12
Coal and coke merchants and dealers -	3	7	10	18
Saddlers and harness makers - -	4	5	10	14
Travellers, commercial, &c. - -	1	3	9	10
Restaurant, coffee, and eating-house keepers - - - -	3	4	9	7
Provision merchants, &c. - - -	4	4	9	10
Millers - - - -	4	3	9	8
Printers and publishers - - -	3	5	8	8
Furniture dealers and makers - -	4	2	8	5
Tobacconists, &c. - - - -	6	3	8	8
Carriers, carmen, lightermen, and hauliers - - - -	2	2	7	8
Lodging-house keepers - - - -	2	1	7	7
Gardeners, florists, &c. - - -	2	3	7	8
Dairymen, cowkeepers, &c. - - -	4	1	7	3
Brokers, stock and share - - -	4	—	7	2
Wine and spirit merchants, &c. - -	2	5	6	11
Milliners, dressmakers, &c. - -	2	1	6	3
Cattle and pig dealers - - -	3	—	6	3
General dealers - - - -	5	4	6	11
Ironmongers - - - -	1	4	5	9

	March		Three Months ended March	
	1896.	1895.	1896.	1895.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.	No.	No.
China, glass, and earthenware, &c. dealers - - -	1	2	5	7
Engineers and founders, &c. - - -	2	2	5	8
Clerks in holy orders - - -	2	—	5	6
Contractors - - -	2	1	5	3
Officers in Army - - -	2	2	5	3
Timber merchants and wood dealers -	2	—	5	2
Blacksmiths, farriers, &c. - - -	1	2	4	4
Chemists, druggists, and chemical manufacturers - - -	2	6	4	8
Hairdressers - - -	2	3	4	6
Stationers - - -	—	—	3	4
Carriage, &c. builders - - -	—	2	3	5
Architects and surveyors - - -	—	1	3	3
Hosiery, gloves, &c. - - -	1	3	3	5
Wheelwrights - - -	—	3	2	7
Curriers, tanners, and leathermerchants	—	3	2	4
Cab, coach, and omnibus proprietors -	—	—	2	1
Fishing net, smack owners, and masters	1	2	2	6
Stone, marble, and monument masons -	1	—	2	3
Directors and promoters of public companies - - -	—	3	1	8

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 31st March 1896, and during the first Three Months of the Year 1896, compared with the corresponding Periods of the Year 1895.

Animals.	March		Three Months ended March	
	1896.	1895.	1896.	1895.
	No.	No.	No.	No.
Cattle - - - - -	41,108	55,277	124,571	126,301
Sheep - - - - -	7,185	5,462	28,514	28,026
Swine - - - - -	64,443	71,649	208,859	215,042
Goats - - - - -	112	233	127	255
Horses - - - - -	2,648	1,907	7,455	3,777
Mules or Jennets - - - - -	2	1	6	1
Asses - - - - -	52	42	75	46
Total - - - - -	115,550	134,571	369,607	373,448

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month and Three Months ended 31st March 1896, compared with the corresponding Periods of the Year 1895.

	March		Three Months ended March	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Brill	1,472	1,616	4,930	4,725
Soles	5,920	7,934	21,872	22,553
Turbot	5,066	6,323	17,608	17,457
Prime fish, not separately distinguished	1,260	2,427	5,550	6,041
Total prime fish	13,718	18,300	50,960	50,841
Cod	61,121	68,444	159,602	154,390
Haddock	212,501	223,240	645,810	600,654
Hake	2,693	3,634	5,347	7,993
Halibut	6,566	6,811	12,111	11,614
Herrings	2,101	3,373	24,107	8,766
Ling	11,588	16,216	30,813	34,517
Mackerel	5,123	2,214	6,618	2,436
Pilchards	482	—	6,101	279
Plaice	52,817	55,743	160,630	154,948
Sprats	792	16,003	49,204	42,453
Fish not separately distinguished, except shell fish	81,718	95,224	267,628	257,089
Total	451,220	509,806	1,418,501	1,326,022
Shell fish:—				
	No.	No.	No.	No.
Crabs	465,085	19,300	886,152	64,797
Lobsters	40,592	14,655	75,762	33,019
Oysters	1,165,000	1,043,000	5,000,000	2,675,000
Other shell fish	Cwts.	Cwts.	Cwts.	Cwts.
	44,922	52,252	137,958	112,935
VALUE.				
	£	£	£	£
Brill	4,438	4,657	14,256	13,850
Soles	43,028	51,385	141,050	148,705
Turbot	25,066	28,154	76,893	77,226
Prime fish, not separately distinguished	4,904	7,559	19,409	19,849
Total prime fish	78,036	91,755	251,608	259,660
Cod	38,165	42,365	103,802	101,148
Haddock	132,024	135,927	383,211	349,930
Hake	2,889	4,063	6,398	8,716
Halibut	15,051	15,548	28,627	27,239
Herrings	814	866	5,174	2,666
Ling	7,693	9,193	21,511	21,631
Mackerel	8,105	3,988	10,207	4,479
Pilchards	110	—	1,763	65
Plaice	66,454	65,701	203,971	191,623
Sprats	144	655	7,445	3,642
Fish not separately distinguished, except shell fish	60,051	63,430	183,236	169,933
Total	410,136	433,701	1,206,953	1,140,802
Shell fish:—				
	£	£	£	£
Crabs	4,492	453	8,206	1,171
Lobsters	1,767	679	3,277	1,629
Oysters	4,554	2,866	18,475	8,054
Other shell fish	11,848	12,435	34,725	25,360
Total	22,661	16,513	64,683	36,114
Total value of fish landed	432,797	450,214	1,271,636	1,177,216

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month and Three Months ended 31st March 1896, compared with the corresponding Periods of the Year 1895.

	March		Three Months ended March	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles) - - - -	1,493	992	5,410	2,700
Turbot - - - - -	422	249	920	613
Cod - - - - -	67,279	41,531	173,536	122,375
Eel - - - - -	2,508	1,860	6,156	6,393
Flounder, Plaice, Brill - - -	6,394	6,645	31,801	18,705
Haddock - - - - -	103,835	95,745	365,749	308,110
Halibut - - - - -	2,855	2,989	4,261	4,390
Herrings - - - - -	24,679	12,805	114,233	97,680
Ling - - - - -	12,357	20,312	24,701	47,091
Mackerel - - - - -	173	—	228	61
Saith (Coal Fish) - - - - -	5,514	4,240	15,222	19,060
Skate - - - - -	10,306	12,332	21,404	24,923
Sparling - - - - -	11	15	83	27
Sprats - - - - -	—	94	29,929	1,130
Torsk (Tusk) - - - - -	501	715	747	951
Whiting - - - - -	4,118	3,306	12,991	8,246
Fish not separately distinguished, } except shell fish - - - - - }	4,783	3,191	13,142	11,768
Total - - - - -	247,228	207,023	818,513	680,723
Shell Fish:—	No.	No.	No.	No.
Crabs - - - - -	294,721	15,870	670,679	112,863
Lobsters - - - - -	37,577	29,983	125,902	84,486
Oysters - - - - -	16,604	20,500	87,862	111,000
	Cwts.	Cwts.	Cwts.	Cwts.
Clams - - - - -	2,896	2,191	8,505	7,184
Mussels - - - - -	28,084	16,482	87,158	61,661
Other shell fish - - - - -	7,404	6,791	18,546	14,693
VALUE.				
	£	£	£	£
Soles (Lemon Soles) - - - -	3,602	2,259	8,119	6,349
Turbot - - - - -	1,528	716	3,152	2,039
Cod - - - - -	24,073	15,777	63,486	53,593
Eel - - - - -	1,498	1,180	3,260	3,714
Flounder, Plaice, Brill - - -	7,226	5,606	33,197	18,864
Haddock - - - - -	44,358	39,706	132,988	126,354
Halibut - - - - -	4,842	4,344	7,176	6,582
Herrings - - - - -	4,454	4,345	22,362	26,267
Ling - - - - -	4,266	5,759	8,591	13,774
Mackerel - - - - -	157	—	191	24
Saith (Coal Fish) - - - - -	721	545	2,093	2,464
Skate - - - - -	3,343	3,217	6,608	6,249
Sparling - - - - -	65	52	245	74
Sprats - - - - -	—	5	1,755	85
Torsk (Tusk) - - - - -	102	135	145	171
Whiting - - - - -	1,704	1,675	5,149	4,065
Fish not separately distinguished, } except shell fish - - - - - }	913	773	2,972	2,787
Total - - - - -	102,832	85,984	301,489	273,555
Shell Fish:—				
Crabs - - - - -	1,467	112	2,878	518
Lobsters - - - - -	1,832	1,424	5,661	3,991
Oysters - - - - -	65	93	338	542
Clams - - - - -	389	306	1,168	1,004
Mussels - - - - -	1,874	987	6,176	3,603
Other shell fish - - - - -	1,435	1,339	3,845	2,986
Total - - - - -	7,062	4,266	20,086	12,649
Total value of fish landed	109,914	90,250	321,575	286,204

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month and Three Months ended 31st March 1896, compared with the corresponding Periods of the Year 1895.

	March		Three Months ended March	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles - - - - -	117	260	280	474
Turbot - - - - -	65	114	190	233
Total prime fish - - -	182	374	470	712
Cod - - - - -	5,651	10,028	18,554	21,444
Haddock - - - - -	2,125	3,360	9,700	9,462
Hake - - - - -	153	348	480	1,043
Herrings - - - - -	252	303	1,788	4,057
Ling - - - - -	2,032	5,423	7,015	11,835
Mackerel - - - - -	221	39	596	8,220
Sprats - - - - -	-	-	-	-
Whiting - - - - -	1,106	1,693	4,012	3,697
Fish not separately distinguished, except shell fish - - - - -	2,678	3,296	8,114	7,663
Total - - - - -	14,400	24,364	50,729	68,433
Shell fish:—	No.	No.	No.	No.
Crabs - - - - -	226	3,212	3,140	6,854
Lobsters - - - - -	2,341	4,661	10,926	8,993
Oysters - - - - -	132,850	26,400	268,200	103,950
Other shell fish - - - - -	Cwts.	Cwts.	Cwts.	Cwts.
	1,559	1,775	6,391	4,202
VALUE.				
	£	£	£	£
Soles - - - - -	526	1,177	1,268	2,979
Turbot - - - - -	239	334	749	833
Total prime fish - - -	765	1,561	2,017	2,912
Cod - - - - -	2,616	3,292	8,092	8,211
Haddock - - - - -	1,352	1,936	5,896	5,389
Hake - - - - -	96	195	279	558
Herrings - - - - -	62	37	332	734
Ling - - - - -	758	1,747	2,572	3,353
Mackerel - - - - -	131	31	297	3,744
Sprats - - - - -	-	-	-	-
Whiting - - - - -	721	880	2,230	2,022
Fish not separately distinguished, except shell fish - - - - -	1,462	1,515	4,330	3,349
Total - - - - -	7,963	11,164	26,045	31,252
Shell fish:—				
Crabs - - - - -	2	21	22	45
Lobsters - - - - -	87	153	388	316
Oysters - - - - -	281	50	553	174
Other shell fish - - - - -	264	237	1,036	612
Total - - - - -	634	461	1,999	1,147
Total value of fish landed - - - - -	8,597	11,625	28,044	32,399

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.--COTTON RETURNS.—MARCH 1896.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month and Three Months ended March 1896, compared with the corresponding Months of the Year 1895.

	Month of March		Three Months ended March	
	1896.	1895.	1896.	1895.
IMPORTS.				
	No.	No.	No.	No.
American - - -	299,041	296,125	895,785	1,474,943
Brazilian - - -	9,280	2,147	21,146	2,309
East Indian - - -	14,222	3,262	38,708	5,267
Egyptian - - -	32,765	34,521	139,675	132,176
Miscellaneous - - -	3,192	3,284	10,463	14,305
Total - - -	358,500	339,339	1,105,777	1,629,000
EXPORTS.				
American - - -	16,716	13,120	44,317	53,801
Brazilian - - -	800	4,854	2,350	5,858
East Indian - - -	3,983	3,558	13,491	7,634
Egyptian - - -	6,986	8,623	35,136	29,085
Miscellaneous - - -	2,640	2,338	8,032	7,260
Total - - -	31,125	32,493	103,326	103,638
FORWARDED from PORTS to INLAND TOWNS.				
American - - -	233,467	265,021	691,880	738,769
Brazilian - - -	5,431	933	18,307	2,821
East Indian - - -	4,297	1,410	10,439	4,970
Egyptian - - -	24,306	32,815	95,488	91,101
Miscellaneous - - -	9,014	9,815	*29,521	35,391
Total - - -	276,515	309,994	845,635	873,052
FORWARDED from INLAND TOWNS to PORTS.				
American - - -	180	667	613	937
Brazilian - - -	—	—	7	—
East Indian - - -	—	—	4	—
Egyptian - - -	—	—	49	10
Miscellaneous - - -	48	68	188	196
Total - - -	228	735	861	1,143

* Corrected figures.

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note. — Rouble = 2s.; Franc = $9\frac{6}{10}d.$; Milreis = 4s. 6d.; Lira = $9\frac{6}{10}d.$; Gulden = 1s. 8d.; Drachma = $9\frac{6}{10}d.$; Lew = $9\frac{6}{10}d.$; L. Egyptian = 1l. 0s. 6 $\frac{1}{4}d.$; Dollar = 4s. 2d.

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe	Nov.	Roubles 39,498,000	47,464,000 (1895)	453,536,000 (1896)	472,549,000 (1895)
France	Feb.	Frs. 332,627,000	291,790,000 (1894)	658,463,000 (1895)	558,479,000 (1894)
Portugal	Aug.	Milreis 3,271,000	2,508,000 (1895)	26,572,000 (1896)	24,989,000 (1895)
Italy	Jan. - Feb.	Lire 87,513,000 85,608,000	71,724,000 78,336,000	173,121,000	159,407,000
Austria-Hungary	Feb.	Gulden 61,917,000	53,526,000 (1895)	118,661,000 (1895)	110,797,000 (1894)
Greece	Dec.	Drchms. 9,444,000	11,771,000	106,822,000	109,959,000
Bulgaria	Dec.	Lew 5,900,000	9,340,000	69,020,000	99,229,000
Egypt	Dec.	L. Egypt. 923,000	755,000 (1895)	8,246,000 (1896)	9,119,000 (1895)
United States	Jan.	Dollars 64,603,000	67,518,000 (1894)	— (1895)	— (1894)
Mexico*	Nov.	„ 3,683,000	3,723,000 (1895)	15,514,000 (1896)	13,887,000 (1895)
British India†	Jan.	Rupees 6,23,06,917	6,90,11,310	56,88,07,872	58,99,04,106

II.—EXPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe	Nov.	Roubles 55,114,000	59,845,000 (1895)	641,144,000 (1896)	610,793,000 (1895)
France	Feb.	Frs. 270,339,000	233,929,000 (1894)	532,364,000 (1895)	482,498,000 (1894)
Portugal	Aug.	Milreis 2,536,000	1,826,000 (1895)	17,760,000 (1896)	15,358,000 (1895)
Italy	Jan. - Feb.	Lire 71,358,000 87,559,000	59,544,000 70,749,000	158,917,000	130,299,000
Austria-Hungary	Feb.	Gulden 55,037,000	53,496,000 (1895)	107,470,000 (1895)	108,333,000 (1894)
Greece	Dec.	Drchms. 6,258,000	8,868,000	71,156,000	74,291,000
Bulgaria	Dec.	Lew 5,197,900	7,364,000	77,686,000	72,551,000
Egypt	Dec.	L. Egypt. 2,156,000	1,374,000 (1895)	12,674,000 (1896)	11,884,000 (1895)
United States	Jan.	Dollars 87,108,000	87,330,000 (1894)	— (1895)	— (1894)
Mexico*	Nov.	„ 9,613,000	5,712,000 (1895)	41,078,000 (1896)	32,497,000 (1895)
British India†	Jan.	Rupees 19,76,24,924	10,45,72,949	91,35,78,554	88,83,02,677

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the "special" imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are "general." "Special" means, in the case of imports, imports for home consumption; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Ten Months ended 31st January		Ten Months ended 31st January	
	1895-6.	1894-5.	1895-6.	1894-5.
	R.	R.	R.	R.
I.—Animals—living - - -	3,35,841	23,42,877	12,73,933	12,61,764
II.—Articles of food and drink -	7,20,60,749	6,85,34,044	23,31,04,117	22,33,53,442
III.—Metals and manufactures of—				
A.—Hardware and cutlery (including plated-ware)	1,17,62,189	1,07,55,782	1,49,409	96,652
B.—Metals - - -	5,28,91,113	4,06,53,668	7,60,523	6,17,902
C.—Machinery and mill-work	2,65,00,487	2,02,04,239	815	2,555
D.—Railway plant and rolling-stock (other than Government stores) -	1,30,34,742	1,33,68,045	—	—
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	1,78,01,885	1,55,41,820	12,74,89,585	12,72,87,077
V.—Oils - - -	2,79,16,467	1,65,93,659	61,38,093	60,04,893
VI.—Raw materials and unmanufactured articles - -	4,13,10,436	3,70,53,595	33,52,39,277	33,91,58,721
VII.—Articles manufactured and partly manufactured—				
A.—Yarns and textile fabrics	23,60,95,580	30,63,42,420	11,30,53,057	9,89,60,764
B.—Apparel - - -	1,34,35,084	1,21,83,450	17,32,678	13,91,578
C.—Other articles manufactured - - -	5,29,63,299	4,56,20,510	5,67,42,316	4,84,87,965
TOTAL -	56,88,07,872	58,99,04,106	87,56,83,808	84,64,23,313

* Exports of Indian Produce and Manufacture only.

XXIV.—LIST OF DIPLOMATIC AND CONSULAR REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1896, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.
1657	Réunion - - - - -	1 <i>d.</i>
1658	Tokio - - - - -	1½ <i>d.</i>
1659	Maranhã - - - - -	1 <i>d.</i>
1660	Copenhagen - - - - -	1 <i>d.</i>
1661	Berlin - - - - -	1½ <i>d.</i>
1662	Tehran - - - - -	2½ <i>d.</i>
1663	Salonica - - - - -	1½ <i>d.</i>
1664	Manila - - - - -	½ <i>d.</i>
1665	Florence - - - - -	5½ <i>d.</i>
1666	Dakar - - - - -	½ <i>d.</i>
1667	Havre - - - - -	2 <i>d.</i>
1668	Rouen - - - - -	2 <i>d.</i>
1669	Corfu - - - - -	½ <i>d.</i>
1670	Calais - - - - -	1 <i>d.</i>
1671	Tehran - - - - -	1½ <i>d.</i>
1672	Barcelona - - - - -	2 <i>d.</i>
1673	Amsterdam - - - - -	1 <i>d.</i>
1674	Bordeaux - - - - -	2½ <i>d.</i>

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources:—

No.	—	Price.
387	<i>Portugal</i> .—Report on the East Coast Route to Mashonaland -	1 <i>d.</i>
388	<i>Austria Hungary</i> .—Report on the Iron Industry in Styria and the Betler Ironworks in Hungary - - -	5 <i>d.</i>

XXV.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
47	British Bechuanaland	1890-92	106	Gambia	1893
48	Malta	1890 and 1891	107	Hong Kong	"
49	Gibraltar	"	108	St. Vincent	"
50	Falkland Islands	"	109	Gibraltar	"
51	Leeward Islands	"	110	Bahamas	"
52	Grenada	1890 and 1891	111	Turks and Caicos Islands	"
53	St. Lucia	"	112	Leeward Islands	"
54	Labuan	"	113	Barbados	"
55	British Guiana	"	114	Malta	"
56	Zululand	"	115	Zululand	"
57	Mauritius	"	116	British Honduras	"
58	Lagos	"	117	Trinidad and Tobago	"
59	Straits Settlements	"	118	Falkland Islands	"
60	Ceylon	"	119	Rodrigues	"
61	Natal	1891-92	120	Mauritius	"
62	Basutoland	"	121	Straits Settlements	"
63	St. Helena	1891	122	Labuan	"
64	Sierra Leone	"	123	Basutoland	1893-94
65	Jamaica	1891-92	124	St. Lucia	1893
66	Trinidad and Tobago	1891	125	St. Helena	"
67	Newfoundland	"	126	Ceylon	"
68	British New Guinea	1891-92	127	Fiji	"
69	Victoria	1891	128	Grenada	"
70	Rodrigues	"	129	Sierra Leone	"
71	Bermuda	1892	130	British Bechuanaland	1893-94
72	Fiji	1891	131	British New Guinea	1892-93 and 1893-94
73	British Honduras	"	132	Lagos	1893
74	Turks and Caicos Islands	1890	133	British Guiana	1893-94
75	Gibraltar	1892	134	Jamaica	"
76	Leeward Islands	"	135	Newfoundland	1893
77	Bahamas	"	136	Gold Coast	"
78	Trinidad and Tobago	"	137	Zululand	1894
79	Malta	"	138	Bermuda	"
80	Gambia	"	139	Bahamas	"
81	Straits Settlements	"	140	Barbados	"
82	Grenada	"	141	Turks and Caicos Islands	"
83	Barbados	"	142	Malta	"
84	St. Vincent	"	143	Gambia	"
85	Hong Kong	"	144	Windward Islands	"
86	St. Lucia	"	145	Trinidad and Tobago	"
87	Falkland Islands	"	146	Gibraltar	"
88	Gold Coast	"	147	Falkland Islands	"
89	Basutoland	1892-93	148	Hong Kong	"
90	Ceylon	1892	149	Straits Settlements	"
91	Zululand	"	150	Lagos	"
92	Mauritius	"	151	Seychelles	"
93	Labuan	"	152	Basutoland	1894-95
94	British Honduras	"	153	Fiji	1894
95	Lagos	"	154	St. Helena	"
96	Sierra Leone	"	155	Ceylon	"
97	Fiji	"	156	Mauritius	"
98	Rodrigues	"	157	Labuan	"
99	St. Helena	"	158	Gold Coast	"
100	British Bechuanaland	1892-93	159	British Guiana	1894-95
101	British Guiana	"	160	Sierra Leone	1894
102	Newfoundland	1892	161	Jamaica	1894-95
103	Jamaica	1892-93	162	British Honduras	1894
104	Victoria	1892	163	British Bechuanaland	1894-95
105	Bermuda	1893			

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast	Economic Agriculture.
2	Zululand	Forests.
3	Sierra Leone	Geology and Botany.
4	Canada	Advantages to Emigrants.

THE
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AND
MISCELLANEOUS COMMERCIAL INFORMATION.

Vol. XX.]

May 1896.

[No. 118.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

During April the general state of employment continued to improve, and at its close the proportion of unemployed in trade unions making returns was lower than at any time since June 1891.

In the 109 trade unions, with an aggregate membership of 416,741 making returns, 13,480 (or 3·2 per cent.) are reported as unemployed at the end of April, compared with 3·5 per cent. in March, and with 6·5 per cent. in the 84 unions, with a membership of 386,627, from which returns were received for April 1895.

Coal Mining.—Employment in this industry was better than a year ago, and fully equal to the previous month, after allowing for the Easter holidays. The average number of days worked by 1,141 pits, employing 327,047 workpeople, was 4·69 per week, as compared with 4·81 in March and 4·46 in April 1895. The proportion of unemployed miners in trade unions in Northumberland and Durham fell from 2·0 per cent. at the end of March to 1·9 per cent. at the end of April. In April 1895 the percentage was 5·6.

Iron Mining.—Employment in this industry continues better than a year ago. At the mines included in the returns an average of 5·64 days per week was worked during April, as compared with 5·39 in April 1895. The total number employed at the mines was 16,319, or about 650 more than a year ago.

In the *Pig-iron* industry the state of employment in April last was more satisfactory than in March, and shows a marked improvement compared with April 1895. The 107 ironmasters who made returns had 343 furnaces in blast at the end of April last, as compared with 339 in March, and 301 in April 1895.

The number of workpeople employed at these furnaces increased by 72 compared with March, and by 1,697 as compared with a year ago.

Employment at *Steelworks* is still considerably better than a year ago. The number of workpeople employed at the 111 works included in the returns was 33,516 at the end of April, or 15·8 per cent. more than at the end of April 1895.

At the 88 *Puddling furnaces and rolling mills* making returns, fewer workpeople were employed than at the end of March, but more than a year ago; at the end of April the number employed being 17,593, as compared with 17,698 at the end of March, and 17,321 at the end of April 1895.

The *Tinplate* trade is very unsettled and shows but little improvement as compared with March. At the 89 works included in returns received, 288 mills were working at the end of April, as compared with 286 at the end of March. Of the 89 works, 42 only were giving full employment.

A further improvement has taken place in the *Engineering* and kindred trades, and the percentage of unemployed union members has fallen from 2·9 in March to 2·3 at the end of April, compared with 7·2 in April 1895.

The *Shipbuilding* trades are still well employed in the more important centres. The percentage of unemployed union members at the close of April was 7·4, compared with 7·2 in March and 12·1 in April of last year.

Employment in the *Building* trades has further improved, the percentage of unemployed in unions making returns being 1·5, compared with 2·6 in March and 3·3 in April 1895.

The *Furnishing* trades remain busy, the percentage of unemployed union members having fallen to 0·9, compared with 1·0 in March and 3·1 per cent. in April 1895.

Employment in the *Printing* and *Bookbinding* trades has improved on the whole, though the latter are scarcely so well employed. The percentage of unemployed union members was 4·4, compared with 4·6 in March and 5·4 per cent. in April 1895.

The *Paper* trade has also improved, compared with last month, the percentage of unemployed union members at the end of April being 4·2, compared with 4·7 in March. The percentage in April 1895 was only 2·9.

In the *Glass* trade 14·4 per cent. of union members are unemployed, compared with 14·0 per cent. in March.

In the *Leather* trades employment has continued to improve, and the percentage of unemployed union members has fallen from 5·9 in March to 4·1 in April, compared with 5·9 in April 1895.

Employment in the wholesale *Boot* and *Shoe* trade has slightly improved and is good in the hand-sewn branch.

Employment in both the ready-made and bespoke branches of the *Tailoring* trade continues good.

Employment in the *Cotton* trade shows little change as regards the spinning branch; there has, however, been a slight improvement among the *Weavers*. Employment in the *Woollen* trade

continues good. In the *Worsted* trade there is a slight falling off. Employment in the *Hosiery* and *Silk* trades, except in a few branches, is fairly good.

As regards the employment of women in the *Textile* trades, information respecting 501 mills, employing about 91,000 women and girls, shows an improvement, 92 per cent. being in mills giving full employment during the month, compared with 86 per cent. among those for whom returns were received for March. The improvement took place in the cotton trade.

Trade Disputes.—The number of fresh disputes occurring in April was 84, as compared with 95 in March and 83 in April 1895. 18 disputes took place in the building trade, 18 in the engineering and shipbuilding trades, 13 in the mining and quarrying industries, 11 in the textile trades, 10 in the metal trades, 8 in the clothing trades, and 6 in the miscellaneous group of industries. In 73 of the disputes 8,572 workpeople were involved. Of the 70 disputes, old and new, involving about 7,000 persons, of which the settlement has been reported, 39, involving about 3,100 persons, resulted in favour of the workpeople; 11, involving 902, in favour of the employers; and 17, involving 2,600, in a compromise; the results of the remaining 3, involving 424 persons, being indefinite.

Changes in Rates of Wages and Hours of Labour.—About 116,000 workpeople were affected by changes in rates of wages, about 81,000 receiving increases and 35,000 sustaining decreases. The effect of all the changes was an average advance estimated at 11*d.* per week upon the wages of the total number affected. The increases were mainly in the building and engineering and shipbuilding industries, the numbers of workpeople receiving advances in these trades up to the end of April being 28,000 and 45,000 respectively. About 4,000 workpeople in the wood-working and furnishing trades have also secured increased wages. The decreases reported affected 31,500 miners in Northumberland and 3,850 tinplate workers in South Wales.

The 12 cases of reductions in hours of labour affected 2,676 workpeople, all of whom had their hours of labour shortened. Of this number 381 had their working hours reduced to 48 per week.

BRITISH TRADE SECRETS IN FOREIGN COUNTRIES.

Information has been received at the Board of Trade tending to show that British workmen, tempted by offers of high remuneration, are sometimes found ready to impart to foreign competitors the trade secrets they have acquired in their own country.

It may be of value to merchants and manufacturers to be placed on their guard against the risk of having their trade secrets divulged.

OFFICIAL INQUIRIES UNDER THE BOILER EXPLOSIONS ACT.

Boiler Explosion at Southwark.

A formal investigation was held at Southwark on the 15th, 16th, and 17th April into the circumstances attending the explosion of a boiler at the Bankside works of the City of London Electric Lighting Company on the 25th February last, whereby loss of life ensued.

The boiler, one of the water-tube type, with 21 others, was used for supplying steam to engines used for driving dynamos and other machinery. It was erected in 1893, and it was fed with water containing a certain amount of solid matter. Some of this was deposited in the vicinity of the orifices of the down-take tubes, and from time to time considerable quantities of deposit were found in the tubes of this and of some of the other boilers. Various efforts were made by the engineers of the company to avoid the formation of this deposit in the vicinity of these tubes, and from experience with this and other boilers fed with the same water, it was considered sufficient to clean them partially after 1,000 hours' steaming and thoroughly after 2,000 hours' steaming. In practice, however, with one exception, the boilers were cleaned at less intervals. This boiler and another were thoroughly cleaned and set to work on the 6th October last. The other was thoroughly cleaned on the 22nd February after steaming for 1,032 hours, and there was nothing in its condition to afford ground for apprehension as to the condition of this boiler. On the 25th February, after it had been steaming for 881½ hours, eight headers fractured and steam and water escaped into the stoke-hole, severely scalding one of the men, who was engaged in removing cinders, &c., drawn from the furnace, and he died from shock.

The Court found that the explosion was due to the down-take tubes having become blocked with deposit, thus preventing the circulation of water in the lower horizontal tubes. These became overheated and produced stresses which the headers were unable to resist, and they consequently fractured.

The Court were of opinion that the engineers in the employment of the company had made reasonable efforts with a view to ensure that the boiler was worked under safe conditions, and they were exonerated from blame.

The Court made no order as to costs.

Boiler Explosion at Barrington Colliery Brickworks.

A formal investigation was held at Newcastle-on-Tyne on the 21st, 22nd, and 23rd April into the circumstances attending the explosion of a boiler at the Barrington Colliery Brickworks on the 12th March last, whereby one man was killed and six persons were injured. The boiler was one of the cylindrical egg-ended type, with longitudinal seams in line, and it appears to have been constructed about 50 years ago. It was extensively

repaired in 1868 and tested by hydraulic pressure, and from that time it was used at the brickworks. At a later period it was entrusted to the care of persons appointed under special rules made under the Coal Mines Regulation Act, by whom it was examined and repaired from time to time.

The boiler was not insured, but it was periodically inspected by the inspectors of an insurance company, and reports as to its condition were made by the insurance company to the owners.

Extensive repairs were effected in August 1895, but the owners did not advise the insurance company that those repairs were being executed, nor was the boiler tested by hydraulic pressure thereafter.

The boiler was examined by one of the insurance company's inspectors in December last, but no defects were discovered, and it was not then subjected to hydraulic pressure. It was again examined by one of the owners' servants on the 1st March, but he did not examine it in the flue behind the bridge.

On the 12th March the boiler exploded, and was hurled a considerable distance from its seat, the engine-man, who was on the top of the boiler at the time, being blown a distance of about 140 yards. The shed in which the boiler stood and the engine were destroyed.

The Court found that the explosion was due to seam-rip in one of the longitudinal seams set up by the manner in which repairs had been effected, and by the strains to which the boiler was subjected in working; that notice should have been given to the insurance company of the repairs effected in August 1895; that the boiler should then have been tested by hydraulic pressure, and that it was not properly examined in March 1896; that the insurance company's inspector did not make a proper inspection of the boiler in December 1895; that the inspectors of the insurance company knew that the boiler had not been tested by hydraulic pressure; and that the company should have advised the owners to adopt this precaution.

The Court ordered the owners of the boiler and the insurance company each to pay the sum of 40*l.* towards the costs and expenses of the investigation.

Boiler Explosion at the Fallowfield Mine, Acomb, near Hexham.

A formal investigation was held at Newcastle-on-Tyne on the 23rd and 24th instant into the circumstances attending the explosion of a boiler at the Fallowfield Mine, Acomb, near Hexham, on the 18th March last, whereby two boys were injured.

The boiler was one of the cylindrical egg-ended type with longitudinal seams in line. It was set up in brickwork, the products of combustion passing from the furnace along the bottom of the boiler to the chimney.

About three years ago extensive corrosion was discovered beneath the brickwork on the left-hand side and the boiler was repaired. It was examined from time to time by the engineer,

but he never removed the brickwork on the right-hand side to examine the plates beneath it.

The Court found that the explosion was owing to the plates on the right-hand side of the boiler having become so reduced by external corrosion as to be unable to withstand the working pressure to which it was subjected; the plates for a length of 5 feet had become reduced from $\frac{3}{8}$ inch to $\frac{1}{8}$ and $\frac{1}{64}$ inch, or to a knife edge. They considered that, having regard to the information the owners obtained as to the qualifications of the engineer, they were justified in employing him, but the Court were of opinion that he was not competent to examine a boiler, and they strongly condemned his conduct in neglecting to remove the brickwork on the right-hand side, knowing the extensive corrosion which had previously been discovered on the left-hand side.

The Court held the owners responsible for the neglect of the engineer, and ordered them to pay 45% towards the costs and expenses of the investigation.

GOVERNMENT CONTRACTS.

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply (1) spans, (2) iron telegraph posts.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that office by *two* o'clock p.m. on Tuesday the 19th May 1896, after which time no tender will be received.

TENDERS FOR COAL FOR NORWAY.

The Secretary of State for Foreign Affairs has received a despatch from Her Majesty's Consul-General at Christiania, stating that the gasworks of Christiania have invited tenders for the supply of 25,000 tons of Old Pelton, New Pelton, Londonderry, Pelard Marin, Benwell, or Holmside gas-coal for delivery during the next autumn and winter in Tyne Dock, Newcastle, at the following dates: in August, 1,500 tons; in September and October, 4,500 tons; in November, 6,000 tons; in December, 1896, and January, 1897, 7,000 tons; in February to April, 1897, 6,000 tons. Payment will be made per ton loaded, and will be effected in cash at the completed termination of each cargo. The conditions for delivery may be seen at the Gasworks Office, where tenders in sealed letters, marked "Anbud paa Kul" can be delivered before the 18th of May next.

A further despatch has been received from Her Majesty's Consul-General at Christiania, stating that the Røros Copper-

works have advertised for tenders for about 4,000 tons best Mickley coke and about 1,000 tons coal (Beamish Smalls or Stanley Smalls), to be delivered between the 1st of July next and the 30th of June 1897. Further conditions may be obtained on application to the chief office of the Copperworks at Trondjem.

TENDERS FOR PORT WORKS AT HAVRE.

The Secretary of State for Foreign Affairs has received a despatch from Her Majesty's Consul-General at Havre stating that an adjudication in respect of certain works which are to be executed at the port of Havre, at an estimated cost of about 385,000*l.*, will take place at the Prefecture at Rouen on the 28th instant. There will be no impediment to foreigners sending in tenders provided they comply with the conditions laid down by the authorities.

The conditions of tender may be viewed in the Commercial Department of the Foreign Office any day between the hours of 11 and 6.

TENDER FOR PLANT FOR NEW WATERWORKS AT PORTO RICO.

The Secretary of State for Foreign Affairs has received a despatch from Her Majesty's Consul at Porto Rico, stating that in the "Official Gazette" of Porto Rico of the 11th April last tenders were invited by the municipality of that port for furnishing the machinery, pipes, &c., necessary for the proposed water-works of the city; and that the adjudication would take place on the 2nd of June. The conditions of tender may be viewed at the Commercial Department of the Foreign Office at any time between the hours of 11 and 6.

INSPECTORS OF WEIGHTS AND MEASURES.

The following inspectors of weights and measures have during the month of May 1896 passed the examination provided for in the 11th section of the Weights and Measures Act, 1889 :—

Butler, George Henry	-	-	-	Rotherham.
Crabtree, Ernest	-	-	-	Isle of Wight.
Gordon, William	-	-	-	Mid-Lothian.
Morgan, Thomas Herbert	-	-	-	Kent.
Payne, Walter Granville	-	-	-	Dewsbury.
Wells, William Alfred	-	-	-	Eastbourne.

II.—THE FOREIGN TRADE OF THE UNITED KINGDOM FOR APRIL 1896.*

The total value of the imports into the United Kingdom from foreign countries and British Possessions during the month ended 30th April 1896 was 35,808,800*l.*, as compared with 34,341,358*l.* for April 1895, or an increase of 1,467,442*l.*, equal to 4·27 per cent.; and the exports of British and Irish produce and manufactures were valued at 18,426,699*l.*, as compared with 17,252,311*l.*, an increase of 1,174,388*l.*, or 6·8 per cent.

The exports of foreign and colonial merchandise amounted in value to 5,411,590*l.*, as compared with 6,608,115*l.* for April 1895, a decrease of 1,196,525*l.*, or 18·11 per cent. This is principally due to the large decrease in wool and, in a lesser degree, to decreases in indigo, tallow, and hides.

The classes of goods imported which show a higher value in April 1896, as compared with April 1895, are raw materials for textile manufactures, which show an improvement of 818,964*l.*; manufactured articles, 702,579*l.*; metals, 331,418*l.*; raw materials for sundry industries and manufactures, 318,784*l.*; animals, living (for food), 292,375*l.*; miscellaneous articles, 50,552*l.*; tobacco (dutiable), 48,688*l.*; and articles of food and drink dutiable, 14,600*l.* Decreases are seen in the class of articles of food and drink, duty free, of 1,008,664*l.*; chemicals, dyestuffs, and tanning substances, 43,410*l.*; oils, 35,734*l.*; and parcel post, 22,710*l.*

In the class of raw materials for textile manufactures the chief increase is seen in cotton, raw, and flax, the values of which were 1,244,458*l.* and 73,647*l.* respectively more than in the corresponding period of 1895.

In this class the chief decreases are found to be in sheep and lambs' wool, 403,020*l.*; jute, 91,971*l.*; and mohair and other sorts of goats' wool, 46,345*l.*

Under the heading of manufactured articles improvements are manifested in the imports of silk manufactures, 426,566*l.*, and manufactures of leather, 147,615*l.*

In Class III., metals, there is an increase in the imports of copper (regulus, unwrought, and part wrought), and in iron (ore and bar).

There is also an increase of 429,336*l.* in the value of wood set down as imported for sundry industries and manufactures.

With regard to the class of articles of food and drink duty free which for March showed an improvement of 1,441,005*l.* as compared with March 1895, there is a total decrease of 1,008,664*l.* for April 1896, as compared with a corresponding period of the

* See also Statistical Tables, pp. 626–627.

previous year, to be seen chiefly in sugar unrefined 306,847*l.*; bacon, 163,838*l.*; and potatoes, 138,329*l.* In animals, living for food, there is an increase in the value of oxen and bulls imported amounting to 303,177*l.* With regard to the exports of British and Irish produce and manufactures, improvements are to be found in every class except that of raw materials which shows an unimportant decrease of 37,886*l.* Metals and articles manufactured therefrom (except machinery) have improved 484,452*l.*; yarns and textile fabrics, 394,474*l.*; apparel and articles of personal use, 105,043*l.*; machinery and millwork, 97,541*l.*; chemicals and chemical and medicinal preparations 39,698*l.*, and each of the remaining classes, viz., animals living, articles of food and drink, all other articles either manufactured or partly manufactured, and parcel post shows an increase of amounts varying between 20,000*l.* and 27,000*l.*

The only important decrease in the export figures under notice is in the value of worsted tissues, which are 106,223*l.* less in value than for April 1895; this, however, is more than compensated for by the increase of 404,371*l.* in cotton yarns and manufactures and some slight increases in other articles, so that yarn and textile fabrics as a whole show a net increase of 394,474*l.*

With regard to the trade of the first four months of 1896, as a whole, there is an increase of 12,949,312*l.* in the imports from foreign countries and British possessions, as compared with 1895, of which, articles of food and drink (duty free) represent 3,805,950*l.*; manufactured articles, 3,622,457*l.*; raw materials for sundry industries and manufactures, 1,885,457*l.*; and animals living (for food), 1,267,482*l.*

The exports of British and Irish produce and manufactures, as a whole, show an improvement of 9,687,070*l.* for the four months ending 30th April 1896, in comparison with the figures for the corresponding period of 1895, which improvement is seen in all classes of merchandise. There is an increase of 3,478,971*l.* in yarn and textile fabrics; 2,243,007*l.* in metals and articles manufactured therefrom (except machinery); 1,430,775*l.* in all other articles, either manufactured or partly manufactured; and 782,938*l.* in machinery and mill work.

The exports of foreign and colonial merchandise for the four months ending 30th April 1896 amounted to 20,498,855*l.* as against 19,749,796*l.* in 1895, or an increase of 749,059*l.*

III.—THE SPRAT FISHERY OF THE THAMES ESTUARY.

The attention of the Board of Trade having been drawn to the great diversity in the price of sprats sold in the vicinity of the mouth of the Thames, a statement has been drawn up by that department, showing the total quantity, value, and average price of sprats returned as landed at certain fishing ports on the east and south coasts of England, in the neighbourhood of the estuary of the Thames, during the "spratting" season 1895-1896—that is, from November 1895 to March 1896 inclusive, distinguishing, at the same time, the quantity, value, and average price of such sprats as were sold for manure. The ports included in the above statement are Brightlingsea, Burnham, Southend, Leigh, London, Queenborough, East Swale, Whitstable, and Margate on the east coast, and Broadstairs, Ramsgate, Deal, Walmer, Kingsdown, and Dover on the south coast.

By far the largest quantity of sprats which were landed at the above stations, were brought ashore at Brightlingsea and at East Swale, 16,054 cwt. having been landed at the former and 15,600 cwt. at the latter of these ports during the period of five months above referred to. Billingsgate market comes next with 9,963 cwt.; Deal, 6,498 cwt.; Margate, 5,629 cwt.; Kingsdown, 3,477 cwt.; Leigh, 2,727 cwt.; Burnham (Essex), 2,640 cwt.; Whitstable, 2,500 cwt.; and Walmer, 1,895 cwt.; 1,000 cwt. or less having been landed at each of the remaining ports.

With regard, however, to the value of the hauls, a considerable difference is observable in the order of importance of the various ports. The average value of sprats landed on the Kentish coast for the same period of five months was:—

				Per Cwt.	
				s.	d.
Walmer	-	-	-	7	10½
Deal	-	-	-	7	0¼
Kingsdown	-	-	-	6	3½
Dover	-	-	-	4	11¾
Ramsgate	-	-	-	4	3

At Broadstairs, at which port the smallest amount (45 cwt.) was landed in the period referred to, the highest value was realised, viz., 8s. per cwt.; and at Billingsgate market, 5s. 1d. per cwt. was the average price of a total quantity of 9,963 cwt. returned as landed at that place.

None of the sprats landed at the above places were used for manure. On the Essex coast, on the other hand, at Brightlingsea (the most important port as regards quantity

landed), Burnham, and Leigh, the sprats landed were all sold for manure at an average price of 1s. per cwt. Indeed, with the exception of 234 cwt. landed at Southend, which realised 2s. 10 $\frac{3}{4}$ d. per cwt. and were not sold as manure, all the sprats landed on the Essex coast appear to be of use for manure alone, and to be sold at the above low prices. On the Kentish coast, at East Swale, which is the second port in point of importance as regards the quantity of sprats landed, out of a total of 15,600 cwt. landed during the period under review, 14,200 cwt. were sold as manure at an average price of 1s. 1 $\frac{3}{4}$ d. per cwt., and 3,830 cwt. out of the total of 5,629 cwt. returned as landed at Margate were also disposed of for manure at the rate of 1s. 0 $\frac{1}{2}$ d. per cwt.

The difference between the lowest price of sprats when not sold for manure, viz., 2s. 10 $\frac{3}{4}$ d. at Southend, and the value realised in the Deal and Walmer district (7s. to 8s. per cwt.), is worthy of attention, and is no doubt to be accounted for by the greater demand for the fish for preserving purposes at the last-named ports.

IV.—THE SAXON-THURINGIAN EXPORT ASSOCIATION.

A despatch, dated April 15 last, has been received at the Foreign Office from Mr. George Strachey, Her Majesty's Minister at Dresden, reporting on the working of the Saxon-Thuringian Association. In this report—called for at the instance of the Association of Chambers of Commerce of the United Kingdom—Mr. Strachey states that the Association is one of several German institutions founded about 10 years ago for the promotion of the export business of Germany, and, unlike most of the rival societies, its operations appear to have been attended by a certain amount of success. The institution is lodged, free of charge, in a capacious hall and annex lent by the Government, and Prince Frederic Augustus of Saxony is honorary president.

The museum, or bazaar, contains samples, supplied by members, of Saxon and Thuringian industrial products. Textiles predominate, but the metallic and chemical branches of manufacture are also represented. The foreign division, which includes 700 specimens, is to be augmented by a South African collection, and it is also the desire of the management, if sufficient funds can be raised, to arrange for pattern exhibitions in Russia and South America.

At the suggestion of the Society, and with their co-operation, a so-called German Trade Museum has been opened in Sofia.

The Association sent agents for the furtherance of Saxon and Thuringian trade objects to the American continents. Of expeditions of this character, the expenses of which are mainly met by the firms interested, there have been nine. One tour included India, China, and Japan, while the South African countries have been twice visited. Should the means be forthcoming, another expedition will be despatched to China and Japan.

The Society is active as an advertising agency. It sends abroad export catalogues and lists, in German and English, as well as illustrated albums, some of these works being in various languages. In preparation is an extensive polyglot "Guide to the Export Industries of Saxony and Thuringia," which will be a complete dictionary of reference to the industrial establishments of the countries concerned. This book, to be distributed gratis, is to reach foreign traders through the German Consulates abroad and by the hands of the agents of the Association.

According to the figures last published, the Society is represented in 46 European and 53 distant trade centres; it has, besides, numerous sub-agents and correspondents in foreign parts.

The correspondence office supplies members with useful notices, and forwards circulars on important incidents and changes relative to commercial treaties and legislation, tariff reforms, Custom-house rules, bankruptcy laws, trade marks, and similar trade matters. Advice is furnished on the pecuniary position of

dealers abroad, help is given in the collection of debts, and in the settlement of disputes concerning foreign transactions in which members may be involved with authorities or dealers. Besides assuming these quasi-Consular functions, the office circulates warnings against foreign swindling firms, and against industrial "spies."

The facts and figures above recited may perhaps be thought to indicate that the Association must be securing considerable advantages to the German producer in the markets of the world. Experts, however, with peculiar opportunities for forming a correct judgment, deny that the Society's operations have any special bearings on British interests. A most competent industrial observer states that he hardly ever hears the name of the Association mentioned in his town; that he does not believe that, apart from its work as an advertising agency, it has done much to bring Saxon goods into competition with English; and that of any particular results in that direction no instance has ever come to his knowledge. This view is fully endorsed in other quarters, with different interests and surroundings from those of the authority just quoted: to which may be added that the Association would naturally withhold from publication in Saxony the details of individual cases of successful pushing against foreign competitors, while such facts might probably be ascertained in the distant markets concerned.

In the notice by the United States Consul at Chemnitz, published in the March number of the "Board of Trade Journal," the business instigated, so to speak, by the Sample Museum during the 10 years of its existence is conjecturally estimated at a figure which would give an annual turnover of about 180,000*l*. This is a trifling amount for Saxony and Thuringia, and a mere vanishing fraction of the whole German export, which now reaches a yearly average value of 150,000,000*l*. According to a statement by the Society, the Sample Museum is chiefly beneficial to the textile branch of industry.

The expenditure of the Society is about 1,000*l*. per annum. The receipts may reach 600*l*. a year in subscriptions. The difference is provided by the members interested in the special expeditions, albums, &c.

In conclusion, it may be observed that at last month's (annual) meeting of the Association, Herr Kaehlig, who had lately returned from his second South African expedition, argued at length that German industry ought to make itself independent of English traders. Nearly all the wholesale houses in South Africa are English, or dependent on English capital, and thus, in the nature of things, are principally supplied with English goods. The German exporter should therefore endeavour to transact business with the local retainers, and thus open up direct markets for German goods: on which ground, said Herr Kaehlig, the trades interested should establish a permanent depôt in South Africa. The management of the Association undertook to bear this suggestion specially in mind.

V.—TRADE OF THE FRENCH COLONIES.

The "Bulletin de Statistique" for April last, issued by the French Ministry of Finance, contains statistics of the trade of the French Colonial Possessions.

The following tables show the value of the imports into and the exports from the various colonies, distinguishing in each case the trade of the mother country and other French Colonies and that of foreign countries, and giving the year for which the latest returns are obtainable:—

A.—Imports.

Colony.	Year.	From France and French Colonies.	From other Countries.	Total.
		Francs.	Francs.	Francs.
Africa :				
Senegal and Dependencies - -	1893	9,022,000	4,844,000	13,866,000
French Guinea - - -	1894	897,000	3,996,000	4,893,000
Ivory Coast - - -	"	411,000	2,713,000	3,124,000
Dahomey and Dependencies -	"	3,545,000	7,227,000	10,772,000
French Congo - - -	1893	1,440,000	1,726,000	3,166,000
Mayotte and Comoro Protectorate	1891	209,000	452,000	661,000
Diego Suarez - - -	"	*	*	3,351,000
Nossi Bé - - -	"	462,000	2,171,000	2,633,000
Réunion - - -	1893	9,579,000	10,092,000	19,671,000
Asia :				
French Hindostan - - -	1893	532,000	2,745,000	3,277,000
Cochin China - - -	1894	15,014,000	26,622,000	41,636,000
Cambodia - - -				
Annam - - -	"	128,000	4,404,000	4,532,000
Tonkin - - -	"	10,920,000	19,856,000	30,776,000
America :				
St. Pierre and Miquelon - -	1894	3,054,000	3,696,000	6,750,000
Martinique - - -	"	13,850,000	15,267,000	29,117,000
Guadeloupe - - -	"	12,890,000	9,746,000	22,636,000
French Guiana - - -	1892	7,816,000	3,621,000	11,437,000
Oceania :				
New Caledonia - - -	1894	5,275,000	3,386,000	8,661,000
French Establishments in Oceania	"	370,000	2,215,000	2,585,000

* Not distinguished.

B.—Exports.

Colony.	Year.	To France and French Colonies.	To other Countries.	Total.
		Francs.	Francs.	Francs.
<i>Africa :</i>				
Senegal and Dependencies - -	1893	13,883,000	4,102,000	17,985,000
French Guinea - - -	1894	1,498,000	3,724,000	5,222,000
Ivory Coast - - -	"	1,128,000	2,942,000	4,070,000
Dahomey and Dependencies -	"	2,734,000	7,240,000	9,974,000
French Congo - - -	1893	454,000	1,891,000	2,345,000
Mayotte and Comoro Protectorate	1891	908,000	123,000	1,031,000
Diego Suarez - - -	"	*	*	341,000
Nossi Bé - - -	"	748,000	1,961,000	2,709,000
Réunion - - -	1893	15,869,000	1,217,000	17,086,000
<i>Asia :</i>				
French Hindostan - - -	1893	12,709,000	7,420,000	20,129,000
Cochin China - - -	1894	12,134,000	76,692,000	88,826,000
Cambodia - - -				
Annam - - -	"	151,000	2,398,000	2,549,000
Tonkin - - -	"	358,000	12,953,000	13,311,000
<i>America :</i>				
St. Pierre and Miquelon - -	1894	7,091,000	2,310,000	9,401,000
Martinique - - -	"	21,190,000	1,321,000	22,511,000
Guadaloupe - - -	"	20,914,000	146,000	21,060,000
French Guiana - - -	1892	4,680,000	304,000	4,984,000
<i>Oceania :</i>				
New Caledonia - - -	1894	1,424,000	4,946,000	6,370,000
French Establishments in Oceania	"	127,000	3,014,000	3,141,000

NOTE.—No information obtainable with regard to the trade of Obok, or the French Soudan.

* Not distinguished.

VI.—THE ITALIAN WOOLLEN INDUSTRY.

The following particulars concerning the woollen industry of the Italian Peninsula are derived from a recent official publication of the Italian Ministry of Agriculture, Industry, and Commerce (*Annali di Statistica, Statistica Industriale, Fascicolo LIX*).

According to this publication, sheep breeding in Italy was once much more widely extended than at present. The decrease in the number of the flocks is partly traceable to the substitution of cultivated for pasture lands, to the neglect of the remaining land left in pasture as well as to the cutting down and uprooting of the trees, and finally to the freeing of pasture rights in many places. More than all these, however, the cause may be found in the competition of other countries which produce better and cheaper wool; in hasty and irrational selection; and in the absence of cleanliness and hygiene in the ways and methods of the Italian shepherds.

Sheep-breeding, gradually confined to mountainous and uncultivated parts, has become a part of the agricultural industry, except in the Maremma and in Lazio, and in parts of Southern and Insular Italy, where large extents of pasture land are still to be found.

The most important pasturing is migratory, according to season. During the winter, not finding sufficient food on the mountains, the flocks remain in the plains where they can graze in the meadows and permanent pasture; in the summer they migrate to the feeding grounds of the Alpine and Apennine heights. This "monticazione" (ascent to the mountain pastures) generally takes place at the end of May, and the return from about the second half of September to the end of October, according to the locality and the state of the season.

This movement of flocks is not always looked upon favourably by the farmers, especially in North Italy, who fear the damage that may be done to the crops by the passing sheep. In many districts, such as the Abruzzi and Apulia, where the keeping of flocks was in former days the chief industry, special routes were established along which the flocks might freely pass and even encamp without crossing private property.

For some time past considerable system has been introduced into the industry of sheep-breeding, so that by proper cross-breeding and a greater regard to hygienic laws, there has been an increase in the production of wool fit for soft white yarns, which by their elasticity and strength are specially adapted for certain articles such as military uniforms, hosiery, &c., but, though a fair export trade is now carried on in such yarns, Italy is at present far from producing the delicate wools furnished by the German and Hungarian breeds.

A census of sheep and lambs was carried out in Italy in 1881, and from the statistics furnished it appears that at that time there were 8,596,108 head of sheep and lambs in the Kingdom, the greater number being found on the pasture lands of the southern parts of the peninsula.

No census of ovine animals in Italy has been obtained in Italy since 1881, but it is a known fact that after that year there was a notable diminution in the number, especially in Apulia, for the reason that the numerous flocks coming down from the highlands of the Abruzzi and the Basilicata, to winter in the plains of Apulia, could not find sufficient pasturage on account of the increased acreage devoted to crops. This proportion may have been exaggerated, but according to the Italian Year Book (*Annuario Statistico Italiano*), and taking account of the slight increases verified in some parts, the number of sheep and lambs may be estimated approximately at about 6,900,000. The sheep are sheared once a year, in April or May, according to the province. Some farmers shear again in September; but in that case the whole amount of wool obtained in the year is inferior both as regards quality and quantity. The shearing generally takes place after a washing of the animals in running water (an operation known as the *salto*), and which has for its object the cleansing of the fleece from dust and from a great part of the grease which it contains. This first washing of the fleece on the animal's back reduces it in weight from 20 to 40 per cent. Washed wool is better adapted to preserve its whiteness, a quality much prized by manufacturers of hosiery.

All Italian wool is not submitted to this process, the Venetian kind for example. The greatest loss in weight occasioned by the *salto* is found in Apulian wool; the average loss may be set down at from 20 to 25 per cent. To this may be added a further loss, the effect of other washings, which may be reckoned at from 30 to 35 per cent. for the finest and dirtiest wool of Apulia and 25 per cent. for other kinds; whence the total loss in weight of the natural wool amounts to from 40 to 45 per cent. for Apulian wool, and 25 per cent. for other kinds, and the general average from 25 to 30 per cent., taking the various kinds and qualities into account. It is calculated that, on an average, each head of sheep in Italy, after the "*salto*," will yield rather more than 1 kilog. of raw wool. With regard to total production, the following are the figures given by the Italian Board of Agriculture for the years 1891-94:—

Years.	Kilogs.
1891	9,622,846
1892	9,957,719
1893	9,730,092
1894	9,608,718

The total production of the year 1894 was valued at 16,683,042 lire (667,322*l.*), of which sum Lazio contributed 3,870,899 lire (154,836*l.*); Southern Adriatic Provinces, 3,796,003 lire (151,840*l.*); Southern Mediterranean Provinces, 2,284,879 lire (91,395*l.*); Tuscany, 1,446,017 lire (57,841*l.*); Umbria and the Marches, 1,381,492 lire (55,260*l.*); and the island of Sicily, 1,336,640 lire (53,466*l.*); the share of the remaining districts was less than 1,000,000 lire (40,000*l.*) each.

Among Italian wools the product of the flocks which pasture in the fields of Lazio occupies a foremost position. These flocks are known as the *sopravissana* breed, being a cross between the *merino* and the *vissana* stock. The latter, which take their name from the district of Visso (Macerata), where they are principally found, yield a quantity of wool and are also common in Umbria and the Marches. In Tuscany there are likewise the two chief races with the *merina*, those of the plains being esteemed the best, the mountain breeds yielding a shorter and more ordinary wool.

As the production of wool in Italy is far inferior to its needs, the national industry is obliged to buy largely of foreign producers, and, for many years past, the imports of this article have much exceeded the exports.

The following table shows the amount of wool of all kinds, including wool raw and washed, artificial and dyed, carded, or combed wool, and wool waste, imported into and exported from Italy in each of the five years of the period 1890-94, together with the excess of importation over exportation :—

Years.	Wool of all kinds.		
	Imports.	Exports.	Excess of Imports over Exports.
	Kilogs.	Kilogs.	Kilogs.
1890	8,223,000	1,330,500	6,892,500
1891	8,991,900	1,279,500	7,712,400
1892	9,782,800	2,275,100	7,507,700
1893	8,898,300	1,790,600	7,107,700
1894	9,401,700	3,085,400	6,316,300

The excess of imports over exports had gradually risen, with few fluctuations, from 4,362,100 kilogs. in 1870 to 10,352,400 kilogs. in 1885, since which year, save for a slight rise in 1891, it has steadily declined.

The following table deals with raw wool only, and shows, in the last column, the amount of wool remaining for consumption in Italy in each of the years 1891-94.

Years.	Raw Wool.			
	Production.	Imports.	Exports.	Amount remaining for Consumption.
	Kilogs.	Kilogs.	Kilogs.	Kilogs.
1891	9,622,846	5,912,400	1,047,000	14,488,246
1892	9,957,719	6,062,000	1,898,000	14,121,719
1893	9,730,092	4,435,800	1,468,800	12,697,092
1894	9,608,718	4,501,300	2,652,000	11,458,018

In the above table the figures in the production column generally refer to wool gathered after the *salto*, and which has therefore lost not less than 20 per cent. of its original weight. In the imports and exports columns, however, the wool referred to is for the most part natural, *i.e.*, unwashed, especially when it is imported from South America.

According to recently compiled statistics, the washing of raw wool and spinning and weaving operations, exclusive of the manufacture of hosiery and trimmings, and of the domestic industry, gave employment, in 1894, to 30,625 workpeople, of whom more than half were women and children. The steam-power employed was represented by 5,322 horse-power, and hydraulic motors of 10,540 horse-power.

In 1876 the total number of workpeople had been 24,930, and the steam and hydraulic motor power employed of 1,080 and 6,550 horse-power respectively.

From the above figures, and others given in the report, a decided progress in the woollen industry in the last 18 years is made evident. The number of workpeople are increased by one-fifth, the steam-power is quintupled and the hydraulic-power increased by 60 per cent. The number of establishments have decreased from 564 in 1876 to 489 in 1894, owing to a greater concentration of the industry in large establishments.

Another proof of the progress of the woollen industry is the increase in the number of spindles and looms. The number of spindles has increased by about 13 per cent. (from 305,386 to 345,550), and the machine looms have increased 150 per cent. (2,571 to 6,507), compensating for the decrease in the number of hand looms (from 5,989 to 3,760).

The principal centres of the spinning and weaving industry is found in Piedmont and Venice, especially in the provinces of Novara and Vicenza. Then come in order of importance the provinces of Florence, Brescia, and Caserta, Genoa, Umbria, the Marches, and Rome.

The wool-growing industry is still carried on in Emilia, Calabria, the Basilicata, and the Abruzzi, where it was once much more flourishing than at present; in other districts it is more especially of a domestic character.

The manufacture of "mechanical" wool or shoddy is carried on especially in the wool factories of Biella, Turin, Prato, Brescia, &c.

By "mechanical" wool is understood that which does not come direct from the animal, but is obtained by pulling the woven wool to bits in such a manner as to destroy all trace of it as a piece and render the threads independent of each other. In the case of pure wool, mechanical unravellers are used, and the material obtained is known in the trade as *shoddy* or *mungo*.

The first machine for carding wool adopted in Italy was introduced in 1830, the first mule-jenny in 1840, and the self-acting very much later.

The *number* of the Italian carded yarns is generally a low one, the highest being from 14,000 to 18,000 metres per kilog.

With regard to combed yarn, the average number is between 30,000 and 36,000, but reaches sometimes 70,000 for woven materials and 90,000 for hosiery. The highest numbers are, however, principally obtained from abroad.

The looms employed in the industry are two-thirds machine and one-third hand looms, the latter being used for coarser and closer tissues, such as are made up into military uniforms.

The industry has progressed of late years, especially with regard to carded tissues, the importation of which in 1894 was reduced to a total value of little more than 3,000,000 lire (120,000*l.*), with an exportation of nearly 1,500,000 lire (60,000*l.*).

The woollen tissues produced consist of "fantasie" for men's clothing, military cloth, cloth for women's apparel, flannels, *ponchos*, shawls, scarves, rugs, carpets. The above-named "fantasie," introduced in 1845, were imitations at first of French and Belgian goods, but English articles are now more generally copied.

Hosiery constitutes a special branch of the industry, no imports from abroad competing with it.

Some years ago the manufacture of mixed tissues of wool and cotton was introduced, and also of tissues similar to English meltons, unions, &c., and lately a factory has been set up at Florence for the manufacture of tissues formed of wool and ramie mixed.

This output of mixed tissues is not unimportant, and many hundreds of bales of cotton, imported into Italy by the cotton goods manufacturers, are passed on to the second-class woollen manufacturers to make up into cheap stuffs for the working classes.

The number of looms in Italy worked by the weavers in their own homes is 18,484, of which by far the largest number (4,388) are found in the province of Cagliari (Sardinia).

The length of an average working day in the Italian woollen industry is from 10 to 11 hours.

VII.—BRITISH TRADE WITH EGYPT.*

Egyptian trade with England is carried on more and more year by year through native and foreign houses in Egypt. If the volume of British trade has not diminished, and has rather increased, the British middleman tends to play an ever less important part in Egypt, and is cut out by the native, or by the agents of other European nationalities who are content with smaller profits, and also, perhaps, are more active in creating business.

In illustration of this tendency it would not be possible to quote a more striking fact than that which is afforded by the tenders for the supply of jute sacks sent in to the salt department. These sacks are all made at Dundee or in British India, and yet in the years 1893, 1894, and 1895 there was no single tender made by British agents for the supply of this exclusively British manufacture, the only offers being from German or local Levantine firms.

A certain want of enterprise is indeed noticeable as regards the attitude of British trade and capital towards Egypt. In spite, for instance, of the considerable profit and ready openings for agricultural undertakings in this country, where the sugar industry is annually assuming a more important development, hardly any British capital seems to find its way to Egypt, though Englishmen are readily found to engage in far more speculative operations in countries affording less guarantees of security. Egypt, as Sir A. Milner has said, is "a country which cries aloud for the application of capital to develop its latent wealth"; and this sentence is as true of private as of public enterprise. It is true that manufacturing enterprise in the interior of Egypt is to some extent handicapped by the heavy inland freights on coal. Moreover, the 8 per cent. duty on imports, which is not excessive on manufactured articles, is a very heavy burden on coal and raw material. In spite, however, of these disadvantages, the sugar-crushing industry appears to flourish and increase.

In doing business with Egypt generally, and especially in tendering for Government contracts, British houses, perhaps, fail to discriminate sufficiently between the circumstances of this and other oriental countries; their experiences in other parts of the East leading them to suppose that it is impossible to deal with the Government direct, that all negotiations must be carried on through a middleman, and that even then it is difficult to get their money in.

It is true that warnings have been issued at times by the Consulate-General at Alexandria recommending British houses

* From a Report of Mr. Rennell Rodd, Her Majesty's Secretary of Legation at Cairo.—(*Foreign Office Miscellaneous Series*, 391.)

not to supply goods on commission to local Alexandrian firms without making searching enquiries as to their credit and position, but these warnings have only a local application, and at any rate in the case of Government orders in Egypt payment is assured and immediate. Yet the number of British firms which compete directly for Government adjudications is far fewer than those of other nationalities.

Letters from firms at home complain of the high rate of commission charged by English agents of good standing in Egypt, and state, on the other hand, that they do not feel sufficient confidence in those agents who are prepared to act for them on low commission.

In confirmation of this it is said by persons in a position to give important orders here, that when they apply to firms in England for the supply of articles at the prices quoted for the home market, these firms often refer them to their agents in Egypt, where the prices quoted are on a very different scale.

Moreover, a calculation has been made in one of the Government departments, from which it results that, in the case of certain goods which by special sanction they are able to purchase direct, and which they accordingly buy through their agent in England, there is a saving to the department of 17 per cent. on the price they would have had to pay in Egypt, even after allowing for $2\frac{1}{2}$ per cent. commission to the agent at home.

It would seem that local English firms do, indeed, remain to some extent under the glamour of the ancient order of things in Egypt, when large profits and large risks were inseparably connected. While much was precarious and competition accordingly less strained, there was no doubt every justification for the high commission which their knowledge of the East and their indispensable position as middlemen commanded. But of late all this has been very considerably modified. The keen competition of the Continent, and especially of the German as middleman or broker, has disclosed that safe business can be done on very low commission; the security of the market has developed with order and good government, and, for all practical purposes in commerce, Egypt must now be looked upon rather as an European than an oriental country. Both local traders and Government departments will not only buy in the cheapest market but also through the cheapest channel.

Only the other day an appeal was made to one of the State Administrations by an important firm of English coal merchants to accept the delivery of their coal c.i.f. instead of packed in railway trucks at Alexandria, owing, it was alleged, to the very high rate of commission which was asked by agents of standing there for superintending the transfer.

In studying the record of competitions for public tenders, it is sometimes seen that English firms have been beaten by so small an excess that it would seem as though just this very matter of the commission rates might occasionally turn the scale and lose an important order to British industry.

At the same time, the necessity for reliable and active English agents is especially great in such a country as Egypt, where a large international community exists and commercial patriotism is developed to a high degree. An English merchant who has lately been endeavouring to extend the market for British paints in Egypt has recorded his experience that he could not even obtain an interview to submit proposals and samples with the directors of French houses here, nor had he any better success at the offices of the Suez Canal Company. In another case where a travelling representative approached the German manager of a local firm which has recently obtained a large contract to execute, he was told that all orders were to be placed in Germany, and that no English need apply. Such instances, which might easily be multiplied, indicate how necessary it is for Englishmen to enter by all available means into direct communication with the consumer, who is indifferent to the nationality of his furnisher.

The foreign or native middleman who is now the chief distributor of British produce in Egypt, will order from England such goods as can only be procured there, or as are most cheaply manufactured there; but it cannot be expected that he will show any zeal in promoting English business, nor is the foreigner likely to give orders in England for goods which can also be supplied, even if less satisfactorily, from his own country.

This national spirit or commercial patriotism would seem to be less prevalent among the British mercantile community, for it happened the other day that an English contractor who, wishing to secure the best material, had placed a considerable order for iron pillars in England, found to his surprise that the house which had undertaken the order passed it on to a Belgian firm to execute.

At present, owing to the absence of adequate legislation for the protection of industrial property, Egypt is full of counterfeit goods. "Sheffield cutlery," for instance, comes from Austria; "English pale ale," from Germany and Belgium; hosiery, with English trade marks, is offered by German travellers; English whisky bearing labels of known brands, which are made in the country by enterprising Levantines, and affixed to the worst class of spirits; while "English" patent medicines, closed with forged British Government labels, are imported from Italy. Egypt is the happy hunting-ground for this form of commercial brigandage, from which Great Britain and France are the chief sufferers; and since the distribution of the produce imported into Egypt is so largely in the hands of the foreign middleman, there is every facility for its further development to the detriment of British manufacturers. The native who, under the impression that he is buying a genuine article, is supplied with a reel of cotton holding about a tenth part of the number of metres declared on the label, will not unnaturally attribute the fraud to the supposed country of origin, and avoid purchasing British yarns in future.

It is a matter for serious consideration whether the fact that British trade in Egypt is conducted so largely through foreign

agents, on the one hand, and that English houses in Egypt, on the other hand, are letting trade pass out of their hands by their unwillingness to do business on a scale of profit with which other nations are content, may not be, for the reasons above discussed, the prelude to a considerable falling off in the total British import to the country. Such a symptom has not yet manifested itself to any considerable extent, but the fact that British metal imports have not increased in proportion to the great increased demand is, perhaps, a significant warning of danger.

It would appear that English firms of high standing are not in the habit of pushing for orders as foreign houses do. They are content to wait till orders come to them, and expect purchasers to accommodate themselves to their terms.

Again, a point which is of importance to the native trader is that Continental firms are very much easier about payments than English houses, allowing at least six months' credit, and affording ready facilities for extension, so that time is given for a disposal of part of the consignment before payment; whereas English houses only give two or, at most, three months' credit and frequently insist upon payment on delivery. It is said to be greatly by these credit facilities that German importers have of late so largely increased their custom. On the other hand, the German mercantile community in Egypt have recently addressed a memorial to their Consul-General, drawing attention to the large number of fraudulent bankruptcies which take place in the country, and the insufficiency of the legal methods in force for dealing with them, from which it would appear that the long credit system is not without its dangers.

It has become almost a commonplace in trade reports to allude to the insufficiency and want of resource of the traveller class representing British trade abroad. In no country more than Egypt do the wants of the people require special study. The native will purchase a certain class of foreign article so long as he finds no other; but if his wants are consulted, and that article is offered in a form more to his liking by a rival manufacturer, he will go to the latter in preference. This is precisely what foreign manufacturers lay themselves out to do, and English manufacturers seem to neglect. The English traveller is, as a rule, not well equipped with the arms of his profession, and has perforce to place himself in the hands of some foreign agent in Egypt who knows the language and the country, and who deals with him much as he pleases, turning the trade secrets and experiences he thus learns to any account which may suit his own purposes thereafter.

Gentlemen of experience and enterprise who have visited Egypt on behalf of important British firms, in order to study the requirements of commerce with their own eyes, have expressed themselves as extremely satisfied with the new knowledge they had gained in doing so, and very hopeful of its beneficial results to their own business.

In connection with what has been said as to some of the restrictions and impediments to British trade in Egypt, it may be

mentioned that it has been settled that a British Chamber of Commerce shall now be established at Alexandria. The French, Italians, and Austrians have already such institutions or bodies analogous to them. One of the first duties of the new Chamber of Commerce will doubtless be to lay down, as the Bombay and Calcutta Chambers have done, a table of the rates of commission which they approve for import and export business. The subject in hand would not be complete without some examination into the question of Government contracts, in competing for which English firms do not appear to have been altogether successful, especially of late years.

Under the financial law of Egypt all purchases of material or contract orders by the State Departments must be put up to public adjudication when the outlay involved exceeds 30%. The department is not necessarily obliged to accept the lowest tender, but in practice it is difficult to do otherwise; a refusal to do so has to be justified by adequate explanations, which for obvious reasons it is not always easy to make public, and would readily lay the department open to charges of partiality.

The result of this is that tenders are cut down so low that the departments are in danger of having to accept what they know must inevitably be an inferior article.

Even when goods are found not to be up to sample the Government is at a disadvantage in refusing them, as so many loopholes for litigation are open to the alien, and the Egyptian Government, which has so often been a victim to the letter of the law in defiance of the spirit, has a not unnatural dread of litigation. Or, again, it may be that when the goods are manifestly shown, as not unfrequently occurs, to be of inferior quality, and the contractor, admitting the deficiency, offers to rebate a portion of the price agreed on, the Government, being pressed for time and unable to wait, is forced to accept these terms, and is consequently saddled with an unsatisfactory article, which will before long have to be renewed. These are, however, the inherent disadvantages of the adjudication system, and it is difficult to see how they can be remedied so long as it remains in force.

The experience of the Government departments tends to show, in the first place, that English firms compete far less for Government contracts than those of other nationalities. Allusion has already been made to the tenders for jute sacking invited by the salt department, for which no direct offer was made by any British agent. In the same department, in the case of a small locomotive for light lines put up for adjudication last year, out of seven competitors only one was British, and his offer was 10 per cent. higher than that of the German firm which obtained the contract. The experience of the Daira and Domains Administration is similar.

In the second place they maintain that English firms competing for Government contracts are constantly inexact in complying with the specifications. At times this appears to be due to a want of attention, at other times it is deliberate, and accompanied

by suggestions that such and such methods or measurements are better than what is asked for, or a declaration that they are not in the habit of working exactly in the lines laid down. Consequently their offers are put out of court at once.

To quote a concrete instance of such a difficulty, it may be mentioned that recently the Domains Administration invited tenders for four portable engines of 12 horse-power effective, with a guarantee of their coal consumption per horse-power. Among the tenders received were many for 12 horse-power nominal, which is about equivalent to some 30 horse-power effective, and consequently their prices were far higher than those of firms which complied precisely with the conditions.

Others, again, declined to give the required guarantee—which means in reality a trial of the engine on delivery—as being contrary to English custom. The department naturally accepts in preference the firm that consents to the guarantee. Instances of this nature might be multiplied to any extent.

In the third place, they state that the terms of English tenders are usually far higher than those of other nationalities. Certain specific instances in illustration of this have already been alluded to in the course of this report, and it would, perhaps, be superfluous to multiply them, as the fact is generally uncontested that where English manufactures come into competition with those of the Continent, the latter are for the most part offered at a cheaper rate. The English manufacturer maintains, and with justice, the superior quality of his article, but the Egyptian Government under the adjudication system is not at liberty, except in very special cases, to accept the dearer and more lasting commodity. English manufacturers are, perhaps, ultra-conservative in their views as to the standard and quality of machinery, and might be a little more yielding in meeting the views of the foreign market. They should bear in mind that the Egyptian demand is not always for the very best article—for an article that is, according to English canons, first-rate,—but for one sufficiently good for practical purposes. It is useless under the adjudication system to try and persuade the customer to accommodate his requirements to our system of trade; if he cannot get what he wants from us he will go elsewhere.

On the other hand, from the point of view of the English trader competing for Government orders in Egypt, one real difficulty lies in the fact that they have no knowledge up to what standard to work. For engines and machinery, for instance, English constructors know the Board of Trade test and they are familiar with the chief foreign tests; but they represent that in Egypt specifications are somewhat vaguely drawn, each department having its own ideas, and that there is no definite standard to guide them. They naturally tend to assimilate their work to the English standard, and thus often fail in competition because it is too high and costly.

Again, the fact that the *cahier de charge* is made out in French constitutes for them a real difficulty, inasmuch as it often

is very hard even for expert translators to arrive at the exact equivalent for French technical terms, consequently, they maintain, a direct advantage accrues to the French and Belgian market. Here, again, it is difficult to see how a remedy can be found. The difficulty is one which affects the relations of England to the Continent as a whole, for *vis-à-vis* of other nations England stands practically alone: our tests, our standards, our weights, our measures, our co-efficients are all different to theirs, and peculiar to ourselves; our views on such important engineering questions as the use of continuous girders are in opposition to those of the majority. The fact is that the competition of foreign countries is forcing upon us a special study of their methods; and unless we conform our practice to theirs we cannot hope always to succeed in international competition. Some improvements might, perhaps, be suggested in the manner in which specifications are made out in Egypt. The *cahier de charge* is drawn up by the *contentieux*, or body of law officers, and this body consists for the most part of French lawyers, who are not, as a rule, specialists in commercial law. It is, of course, their object to safeguard themselves and the Egyptian Government in every possible way. As a result it is alleged that the specifications are often overloaded with involved conditions, which are difficult to understand and sometimes frighten serious competitors, who are not sufficiently well up in the Egyptian systems to realise that many of them are far more formidable in appearance than in reality. Those who are well up in the ways of the country know exactly how much of them they can afford to risk disregarding, and are not put off by the difficulty of the conditions. It would, no doubt, inspire greater confidence if a properly qualified commercial lawyer could be added to the *contentieux*.

The terms fixed in the public notification, limiting the time within which tenders must be received, are often so short that it is scarcely possible for firms in Europe to study the conditions and compete, and consequently the Government is thrown into the hands of local agents. It is difficult always to secure long notice, especially towards the close of the financial year. Departments which find themselves with some surplus in hand a month or so before the close are anxious to dispose of their surplus in order to supply outstanding requirements, as, if it remains in hand when accounts are made up, the surplus must be returned to the Treasury. They are, therefore, obliged to invite tenders at short notice.

Generally speaking, however, it would seem that there should be no great difficulty in preparing before the end of each year a list of the principal heads under which tenders will be invited for the next year, and fixing terms considerably longer than has hitherto been the custom, so as to give competitors in Europe more time for correspondence and investigation.

After all, the orders given by the Government departments still show a balance very favourable to British manufactures.

Out of a total of 15,550*l.*E. expended last year by the Domains Administration, some 10,000*l.*E. was spent in England or in British possessions, though chiefly through foreign and native agents in Egypt. The experience of the Daira Sanieh is similar. The chief expending department is always the Railways Administration. Its outlay necessarily varies considerably from year to year; at one time rails are a principal item, at another sleepers, at another rolling stock, and so on.

Purchases of coal for the railway are made exclusively in England, who also supplies all the steel rails.

Mr. Rennell Rodd, in conclusion, states that it is obvious that the foreign trade in Egypt is in a preponderating degree in British hands, and that the volume of this trade is so considerable that we cannot afford to neglect our interest in it. If British houses will only devote to the maintenance of their present position an energy and enterprise similar to that which is displayed by other nations who are now trying to secure a footing in the Egyptian market, there need be no cause for anxiety.

VIII.—THE AMERICAN SARDINE INDUSTRY.*

The sardine industry of the United States has increased considerably in the last 20 years, affording direct employment to more than 7,000, indirect employment to 2,000, and the means of livelihood to nearly 30,000 persons.

It is on the coast of Maine that sardine preserving is, owing to the progress made in the last 20 years, at present carried on with the greatest success.

The centre of the industry is Eastport, in Passamaquoddy Bay. Lubec, on the southern shore of the same bay, also carries on the industry on a larger scale, and there are manufactories likewise at Jonesport, and along the coast of New Brunswick.

What is called the sardine in America is, rather, a small herring, having a length of about 9 centimetres when its head has been removed. There is, therefore, an appreciable difference between the American and the French fish, though not as regards quality, as connoisseurs aver that the sardine canned in the United States is fully equal in that respect to the sardine of the French coasts.

The American manufacturer, however, in his haste for quick returns, has not always been sufficiently careful in the preparation of the fish, and for some years the sales were flat. Greater attention has lately been given to the subject, and a recent law of the State of Maine will certainly tend to improve the quality of the article.

* Report of the French Consul General at Portland (Maine) published in "Le Journal des Tarifs et Traités de Commerce."

While the sardine industry in France dates back to the year 1850, it is hardly 30 years since the first experiments were made at Eastport, U.S.A., with the little herrings.

One of the directors of an American firm had studied in France the methods employed by those engaged in the tinned sardine industry, and on his return to the States in 1867 the first experiments were made, which, however, were not attended with much success, owing to the impossibility of extracting from the herring the oil, which gave the preserved product such a disagreeable taste. Trials were subsequently made with a kind of herring peculiar to the American coasts, known as the "Menhaden," which were tinned in the same way as sardines, and better, but still only temporary results were obtained.

In 1872 a New York firm sent a representative to Eastport, to experiment with Russian sardines, and these were a complete success; in this case, however, it was a question of an article entirely different from the French sardine. The Russian fish is, in fact, a herring, too large to be preserved in oil or mustard. It is packed in barrels, dozens at a time, and strongly spiced. This kind has been exported to Europe in large quantities, competing successfully with the same article as prepared in Germany.

Towards the end of 1875, the same firm finally succeeded in producing the real sardine in oil. 200 cases, containing 20,000 boxes, were at once produced, and the success induced a new factory to be established in the same year. From that time the industry has developed rapidly and now from 200 to 500 cases are turned out daily.

The invention of an oven which has replaced the former system of drying in the sun has given an exceptional impetus to the American industry.

In the beginning a case of 100 boxes of sardines in oil, cost 12 dols.; the average price last year for the same was only 3 dols.; and even at this low figure the American manufacturers can still compete advantageously with the French product, owing to the protective duties of the United States.

It was in the period 1880-85 that the greatest progress was made in the sardine industry of the United States. Towards the end of 1885 there were 20 manufactories at Eastport, 6 at Lubec (since increased to 15), and 2 at Robinson, and other establishments were soon set up along the coast of Maine, at Jonesport, Bar Harbour, Brookline, Southwest Harbour, Lamoine, Millbridge, &c. At present there are 60 manufactories on the Maine coast and 2 in New Brunswick.

On account of the enormous development of the fishery and the consequent wholesale destruction of the fish, the Legislature of Maine State passed, in 1883, a law ordering the manufactories to suspend work from the 15th December to the 15th April. Notwithstanding this restriction the methods of manufacture have reached such a pitch of perfection that the production has recently almost doubled. The method employed is almost identical

with that adopted in France, and the American manufacturers use almost invariably boxes marked with French labels.

About one-third of the products are prepared with mustard, and their price is slightly higher than those preserved in oil.

A small portion of the whole (about 40,000 boxes), consists of large herrings preserved with spices and mustard sauce. These bear all sorts of labels, such as "river trout," "river eel," &c.

In 1880 the total production (for the most part sardines in oil), amounted to 40,000 cases. In 1885 this amount was increased to 250,000 cases, one-third of which were fish prepared in mustard. In 1894 the output was 625,000 cases, of which Eastport turned out 300,000, Lubec 200,000, and other places 125,000. In an average season Eastport can turn out 50,000 cases daily.

At first only olive-oil was used, but since cotton-seed oil has become so common in the market, it has been almost entirely substituted for olive-oil. The amount of cotton-seed oil used in the preparation of American sardines in one year is about 400,000 gallons, at an average cost of 22 cents (11*d.*) per gallon.

The tin used for the boxes is almost entirely made up at Eastport, in establishments fitted up for that object. During the year 1894 about 70,000 lbs. of tin were consumed by the manufactories of Eastport and Lubec, and nearly half as much again by other places. The number of persons directly employed in the production, in a good season, reaches to about 7,000, half of whom are grown men, one-fourth boys, and one-fourth women and girls.

The men who solder the tins are the best paid, receiving on an average 18 dols. (3*l.* 15*s.*) per week. The makers of the tins are paid on an average 15 dols. (3*l.* 2*s.* 6*d.*), and those who pack the fish in the tins (chiefly young girls) receive about 9 dols. (1*l.* 17*s.* 6*d.*) per week.

The number of persons indirectly employed amounts to about 2,000, composed of sailors, fishermen, and providers of the articles necessary to the preparation of the sardine. The fishermen receive generally a high wage, which last season amounted to 2 dols. (8*s.* 4*d.*) for every barrel of herrings. At the beginning of the industry, however, this figure had been as high as 10, 22, and even 50 dols. per barrel.

All fish taken cannot be transferred into sardines, and the surplus is used up in the manufacture of oil and pomade, both of which articles find a ready sale.

Notwithstanding certain drawbacks, the American sardine industry is at present a most flourishing one and progressing from year to year.

IX.—FOREIGN COMPETITION WITH ENGLISH INTERESTS IN JAPAN.

Mr. Gerald Lowther, Her Majesty's Secretary of Legation at Tokio, in a report forwarded by Sir E. Satow to the Foreign Office (*Foreign Office Miscellaneous Series, 390*), calls attention to the vital necessity of manufacturers of railway material in Great Britain now more than ever taking every possible measure to advance their own interests. Hitherto, says Mr. Lowther, Great Britain has practically had a monopoly in furnishing rails, locomotives, rolling-stock, &c. Whether these manufacturers may still enjoy this privilege must depend in a great measure on themselves and the exertions they may make. The railways of Japan have been built by foreign advisers. For this term English advisers might almost be substituted. This being the case, it was evidently convenient that the materials required for the carrying out of designs made by English engineers should be ordered in England. The influence of the English engineers could not fail to make itself felt in this direction, but the tendency of the Japanese Government is to dispense with all foreign advice, and the time is possibly not remote when all Japanese lines, Government and private, will be built and managed without any assistance from the foreigner in Japan, and the material only will be purchased abroad. In Great Britain the leading firms order from designs made by their own staff of engineers, and manufacturers have nothing to do with the preparation of designs. In America, on the contrary, there exists a system under which the companies first state, as regards bridge work, &c., their requirements as to strength, &c., and the manufacturers then send in competitive projects based on their own designs.

The English engineer who has been the designer of railway bridges in Japan since 1882 is leaving the country. On his departure, if the Japanese engineers have to design any structure with which they are unable to deal, it will be the easiest plan for them, many of their number having been educated in the United States, to fall back on this American system, obtaining both design and bridge from the manufacturers, and thus transferring the business to Americans; and the leading firms of that country are careful to be represented in Japan. Not so with our English firms, only some of the most important of which have agents. Manufacturers of railway material would, therefore, find it advantageous to be represented here in the same manner that the large shipbuilding firms of Great Britain have successfully been represented, and as there are evident signs of a "boom" in railway enterprise it is important that this recommendation be acted upon without delay. It would, without doubt, be easier

to maintain Japanese purchasers in the way of buying their material in England, where they have always obtained a satisfactory return for their outlay, than to bring them back to that market once they had adopted another, and it need hardly be pointed out that American manufacturers put their wares before the Japanese purchaser, be he Government official or other, in a very attractive form. That this is worth their while is proved by the fact already mentioned that one-third of all the money spent on railway construction goes abroad.

The opinions of Mr. Wada Tsunashiro, ex-Director of the Japanese Mining Bureau, expressed in an article in a recent number of the "Taiyo," and reproduced in the "Japan Weekly Mail," with reference to the demand and supply of iron in Japan is worth noting.

Mr. Wada states that the demand for iron in Japan is chiefly for purposes of shipbuilding, the manufacture of arms and projectiles, and the laying of railways. According to the average of the last three years, the total quantity of iron used in the various dock-yards under control of the Admiralty was 2,373 tons, costing about 174,700 yen. Of that quantity steel constituted more than one-half, namely, 1,240 tons, costing 84.91 yen per ton, the remainder being made up of cast and wrought iron nearly in equal proportion, the average cost of the former being 31.43 yen, and of the latter 86 yen per ton. Further, in the naval arsenals there was used, during the same period, an average of about 530 tons yearly, in which cast and wrought iron predominated. It is true that men-of-war are generally ordered abroad, but it is also true that not a few have been built at home with imported materials. Thus of 19 vessels added to the navy since 1885, 12 were built in Japan, the iron employed for the purpose aggregating 28,575 tons and costing about 4,370,000 yen. Again, 1,162 tons were required for building 22 torpedo boats, the cost of that quantity of iron being 180,000 yen. In the casting of large-arm projectiles and so forth, 4,697 tons of iron were employed, costing 1,260,000 yen. Putting the last three of these items together, the total iron used since 1885 becomes 34,434 tons, and its cost 5,810,000 yen.

In the army the quantity of iron used is comparatively small, the average for the last three years in the two arsenals of Tokyo and Osaka being only 2,950 tons of cast iron, 450 tons of wrought iron, and 697 tons of steel, the total cost being 366,000 yen. The Tokyo Arsenal is chiefly employed for manufacturing arms and that of Osaka for casting projectiles. By far the largest quantity of iron is used in the construction of railways. The total mileage of lines already laid down, and lines for which charters had been granted up to the end of last year, was 2,637 miles approximately, and the quantity of iron used for rails, bridges, locomotives, and so forth aggregated more than 430,000 tons, and cost over 30,600,000 yen. Even excluding from the account railways to be hereafter laid—which will certainly represent a large figure—and confining the estimate to lines already constructed, the quantity

of iron required annually for re-laying the rails is about 17,220 tons, and the cost 1,220,000 yen, on the generally accepted basis that a railway requires re-laying every 25 years.

Shipbuilding also consumes no small quantity of iron in Japan. The number of ships built during the 5 years ending in 1894, according to the register preserved in the Shipping Control Bureau of the Department of Communications, was 147, an average of 29.4 per year, and the total displacement of these vessels was 3,023 tons. Assuming that in the case of an iron ship, 79.5 per cent. of the displacement is of iron, it is concluded that the iron used for shipbuilding purposes during the above period of 6 years was 5,335 tons, or an average of 1,067 tons per annum, a quantity that will be greatly increased hereafter owing to the development of Japanese maritime enterprise. There are other forms in which iron is largely imported into Japan, namely, in pigs, the average under that heading during the past five years being 84,987 tons, and the cost 1,620,700 yen; in machinery, average cost 3,633,675 yen; and in ships, vehicles, &c., which yearly make a bill of 1,629,870 yen. Altogether it is stated that the average quantity of iron annually imported into Japan may roughly be put at 100,000 tons. He thinks that, in view of the contemplated expansion of the Navy and the Army, and also of the rapid development of industrial and other enterprises, the total iron hereafter needed in the country may safely be estimated at 150,000 tons.

Turning to the question of the home supply of iron, Mr. Wada does not pretend to speak accurately, for he believes that the necessary surveys have not extended to more than one-tenth of the iron bearing strata of the whole country. Nevertheless very encouraging results have been obtained. The best mine thus far discovered is that of Kamaishi in Iwate Prefecture, which, according to Prof. Noro and another expert, is capable of yielding about 29 million tons. The Sennin Mine, also in the same Prefecture, is supposed to contain half a million tons, and the Akaiwa Mine in Niigata Prefecture is put down for 1,360,000 tons. Many other mines have also been discovered in Iwaki, Shinano, Kyushu, and so forth. In short, Japan may be considered as one of the richest iron yielding countries in the world. Moreover, the ore found here is not inferior in quality to the imported metal. Already the Osaka Arsenal has substituted home made cast iron for imported.

The Arsenal has further found that Japanese iron can be made into steel of quality already so excellent that it is expected by and by to excel the imported article. The results of experiments conducted at the Yokosuka Dock-yard confirm those obtained in the Arsenal. By substituting Japanese iron for imported there would be a great saving of expense even in the matter of transportation, for the freight of cast iron represents 75 per cent. of its cost, that of wrought iron 27 per cent., and that of steel about 40 per cent.

The last point discussed by Mr. Wada is whether there is a hope of conducting the foundry business with profit in Japan. The failure that the Government made many years ago at Kamaishi is ascribed by him to the prematurity of the enterprise on a scale such as that officially projected, and also to the disturbance in the price of materials and the rate of wages inseparable from the initiation of a big undertaking in such a remote district. That, by proper management, the business could be successfully carried on is amply proved by the present proprietor of the Kamaishi Foundry, who commenced the business on a small scale at first and has gradually extended it to the present scope.

As to the method how the State Foundry, to be initiated very soon, should be carried on, Mr. Wada is of opinion that foreign experts should be made to take it in charge during the first four or five years, and that the work should be continued afterwards by Japanese alone.

The growing anxiety with which manufacturers on both sides of the Atlantic have regarded the beginnings of an industrial development in the East along lines distinctly competitive with Occidental production, has naturally attracted the attention of those on the lookout for profitable investments on a large scale, and led to a good deal of quiet investigation since the close of the Japanese-Chinese war. The "Iron Age" of New York states that it is informed that a syndicate, represented chiefly by bankers of Amsterdam and the Hague, has had its agents in Japan and China for some months, making arrangements to establish manufactories of staple articles suited to the requirements of markets hitherto supplied with manufactured goods chiefly from England and Germany. The plan proposed by the syndicate is a new one, and if carried out on the scale now under consideration will seriously disturb the world's trade.

To points in Japan and China which have been selected as offering most advantages, first class machinery for the lines of manufacturing to be established is to be sent, under the charge of men of skill and experience, assisted by qualified mechanics to serve as superintendents and foremen. The works thus established are to be under syndicate control, and are simply to produce as cheaply as possible what they are directed to make. The product is to be taken by representatives of the syndicate and shipped to its agents in countries where it is to be sold. Materials are to be purchased by the syndicate's representatives, and those in charge of its mills and factories are relieved of all commercial responsibilities and financial anxieties. They will be furnished, with patterns of the articles they are to make, with full instructions prepared by selling agents intimately acquainted with the requirements of the markets for which the goods are intended. To imitate these patterns at the least possible cost is all that is expected of those conducting the syndicate factories. The distribution of the product is effected by the commercial machinery of the syndicate, which, as far as

possible, will work independent of bankers, shipping merchants, exporters, and traders with established European connections. When it is necessary to do so, or promises an advantage in larger profit, the representatives of the syndicate in the markets invaded will sell direct to consumers. With the command of an immense capital, it will be prepared to meet the conditions existing wherever it does business, and will exchange its manufactured goods for any kind of salable merchandise offered.

For the present Japan is regarded as offering greater advantages than China for manufacturing industries. The people are more generally intelligent, and more readily adapt themselves to the conditions of industrial organisation. They learn quickly, and become expert in the use of tools and machinery with surprising rapidity. Considering their industry and docility, they are the cheapest workmen in the world. Wages equivalent to from $7\frac{1}{2}d.$ to $10d.$ for a day of 10 to 11 hours will command mechanics comparing favourably in all respects with the best class of English and American workmen in charge of machine tools. In all operations requiring hand work they are quick and clever, and can imitate anything they see. The Chinese have these qualities in some degree, but as the country is still in an unsettled condition, it offers fewer attractions than Japan at present to manufacturers looking for cheap labour. Two or three plants are to be established in Chinese ports as an experiment, but the initial operations of the syndicate will be undertaken chiefly in Japan, where substantial inducements have been offered under satisfactory governmental guarantees.

Concerning details of the syndicate's plan but little information is obtainable. It is said that a great deal of machinery has been purchased or ordered, but it has not transpired what kind of machinery it is, except that one considerable order is for shoe machinery to be employed on goods suitable for the South American trade. If the information is correct the operations of the syndicate will not be confined to any one line, but will include every kind of manufacturing which can be profitably established in the East under existing conditions. The scheme is pregnant with possibilities, and if carried out on the lines indicated, the results may be far more important than can now be estimated. The risks and difficulties of establishing industries in Japan and China have been great enough to discourage private enterprise in that direction, but the plan of the Dutch syndicate appears to be quite practical. The magnitude and variety of its operations, and the extent of its resources will make it formidable as a competitor for the control of any branch of business in which it may engage. Manufacturing operations will undoubtedly be restricted to articles which can be as well made in one place as another, and in which the cheapness and efficiency of Oriental labour, supplemented by the best machinery procurable, will not be offset by advantages peculiar to the present circles of production in the lines selected. Should the expectations of the syndicate be realised, the results of the venture will be of incalculable

consequence to the countries now depending upon their exports of manufactures for the maintenance of their industries. It has been said that fear is entertained by the Australians that Japan's cheap labour and cheap manufactures bode nothing but ill to the Australian colonies, and, according to the "Japan Weekly Mail," a native newspaper, the Jiji Shimpō has of late repeatedly referred to the existence of the same feeling in America; but Mr. Gerald Lowther states in his Report referred to above, that "foreigners are too prone to be alarmed by the cry of cheap labour. Cheap labour, or, in other words, the absence of requirements on the part of the labourer or the artisan of this country, will very possibly be corrected to a certain extent by the successes of the late war, and should an improved state of existence be the result, so infallibly will come an advance in requirements, and the present cheap labour of Japan may not have the terrors to commercial competitors that it now appears to present. That low wages means a low condition of energy may be taken for granted. In attempting to conclude a bargain, and on my pointing out to the seller that he would have a fair profit if he accepted my offer, I have been more than once met with the answer, 'If I give you the article for your price it will necessitate my doing another day's work this week.'

"Skilled labour is very limited in all branches of trade, and consequently cheap labour hardly applies to this portion of the working community. The price of labour, moreover, is daily increasing. The daily wage of a coolie is now set down at 33 sen, or about $8\frac{1}{2}d.$, and this is said to be about 100 per cent. higher than it was 10 years ago. The same may be said of many articles of consumption which are rapidly rising in value."

X.—COFFEE PLANTING IN BRITISH CENTRAL AFRICA.

A despatch has been received at the Foreign Office from Sir H. H. Johnston, Her Majesty's Commissioner and Consul-General in British Central Africa, with regard to coffee planting in those parts.

Sir H. H. Johnston states that coffee flourishes in almost any part of the British Central Africa Protectorate. At one time it was thought that its cultivation must be confined to the highlands above 2,000 ft. in altitude, but it has since been found that coffee grows equally well in quite low-lying parts. The Liberian coffee, of course, actually grows better in the tropical plains, but as this kind does not fetch such high prices as the Mocha variety, which is the kind chiefly planted in British Central Africa, it is rather

the facilities for growing the last-named variety which must be taken first into consideration. It is not that the Mocha coffee will grow quite as well in the plains not many feet above the level of the sea as it will in the hills, but it may be stated positively that it grows as well at an altitude of 900 ft. above the sea as at 3,000 ft. Above 3,500 ft. the coffee is liable to be nipped by frost in the cool season.

Throughout British Central Africa the soil is nearly everywhere very fertile—too fertile in some places—as it is absolutely virgin soil, and this excessive richness is doubtless the cause of much of the malarial fever which prevails. Not only is the soil rich, but it is everywhere well watered by perennial streams which render irrigation during the dry season practicable.

The average rainfall in British Central Africa is 50 ins. per annum, but in most of the districts it is about 45. The least rainfall in any district is probably 36 ins. The highest rainfall is in parts of the Mlanje district, where it attains in some years over 100 inches. The rain falls principally in the months of December, January, February, March, and April. There are heavy rains at the latter end of November, and occasional showers in May and June. It often occurs also that rain falls in the hills during August. In the hill-country—and most of British Central Africa may be described as such—it may be said that no month passes without a shower of rain. Nevertheless, it should be broadly stated that five months of the year are very wet and seven months of the year are very dry. This want of a more general distribution of the rainfall is not such a favourable feature as would be the more regularly distributed rain which occurs in Ceylon.

The climate in most parts of the Protectorate is agreeable, but it cannot be described as healthy. Neither can it be described as very unhealthy. The chief condition of health is comfort. Europeans who make themselves thoroughly comfortable and do not expose themselves too much to the sun, and who lead a thoroughly temperate life, seldom have occasion to complain of their health. Almost the only disease which causes any anxiety is malarial fever. There is very little dysentery, and it is of a mild type. Dysentery is hardly ever met with except in the plains. The climate is on the whole beneficial to persons of a consumptive or asthmatic tendency—very beneficial in fact to the latter, who often maintain better health in British Central Africa than in other parts of the world.

The whole of the Protectorate is now absolutely safe for European settlers, the entirety of it being under the direct control of the Administration. The natives are very well disposed towards the Europeans. There is a native population of about 1,200,000, but this is rapidly increasing by immigration from the countries outside the Protectorate into the more settled districts. Labour is very cheap and fairly abundant. It is the great cheapness of labour, however, which is the principal attraction that British Central Africa can offer to coffee planters, and which atones for

its inferiority of climate and rainfall as compared to Ceylon. Adult labourers receive wages to the extent of about 3*s.* a month. At some periods of the year food is given in addition, making the cost of adult labour about 4*s.* 6*d.* a month. The labour of women and children is paid for at lesser rates.

The price of land ranges at present from 5*s.* down to 2*s.* 6*d.* an acre, but, owing to the considerable demand for estates, it is possible this up-set price may increase. Land in Blantyre or in the vicinity of that township fetches rather high prices—from 100*l.* to 120*l.* an acre. Except within the settled and civilised Shiré districts, the price of land is hardly likely to exceed 2*s.* 6*d.* an acre for some time to come. As regards the sale of Government land, however, no estates exceeding 4,000 acres in one spot are permitted to be sold, and, as a rule, 1,000 to 2,000 acres is the area chosen. About one-eighth of the land in the Protectorate belongs to the British South Africa Company, the African Lakes Corporation, and some 200 European settlers. About three-eighths of the land is permanently secured to the natives, and about half the total land area of the Protectorate is now at the disposal of the Government, though, with a view to native expansion, it is not intended to alienate more than about a quarter of the total land area.

Although the bulk of the settlers are of British nationality, there are other nationalities represented, viz., Dutch, Austrians, Germans, French, and Italians. The nationalities are given in the order of their majority as regards number of representatives.

A constantly extending transport service is now carried on throughout the Protectorate by the African Lakes Corporation, Messrs. Sharrer, Zambesi Traffic Company, the African International Flotilla Company, and the Oceana Company. At present the cost of transporting goods from British Central Africa to London may be roughly estimated at an average of 15*l.* a ton—ranging from 20*l.* a ton from the north end of Lake Nyasa to 4*l.* a ton from the Lower Shiré. From the Shiré province, which is the most settled district, the average cost of transport is perhaps 10*l.* or 11*l.* a ton (7*l.* from Blantyre). The cost of transport from England to British Central Africa is about 1*l.* a ton more than the figures given, the difference arising from the greater facilities for sending goods down stream compared to the hauling of them up stream.

XI.—THE CONDITION OF BASUTOLAND.

In a recent report to the Colonial Office the Resident Commissioner of Basutoland states that in that country the land question is the great and fertile source of many troubles, which require continuous attention, and may be expected to recur annually. The country is circumscribed, the population is growing, the land suitable for cultivation is all allotted and taken up, the rising generation of men are no longer able to support themselves upon it, and the mountains, formerly common pasture, are being largely occupied and cultivated and contain practically no unallotted spheres.

The industry of the people is proverbial; some 25,384 have gone abroad to labour during the year 1894. They learn the discipline of labour and appreciate the corresponding value of money, acquiring at the same time probably such vices as are readily picked up in their travels, and losing some of the qualities which the primitive native is acknowledged to possess.

A demand has lately sprung up for industrial teaching. On the request of the Paramount Chief for the building of a school of industry in Central Basutoland he was informed that it would be too big an undertaking for Government, but if the people really cherished the idea, they should come forward and provide the means for such an institution, in which case Government would surely direct their efforts and control it for them.

They are able to bear the burden of their own advancement in such ways, and, if willing, should be encouraged to help themselves. It is necessary, nevertheless, to bear in mind, that ordinary labour is itself a great industry, desirable of cultivation, that South Africa requires ordinary labour largely, and that a preponderance of natives skilled in technical trades might not be beneficial to themselves or their surroundings.

The liquor traffic, though less than formerly, is by no means extinguished. The police, who patrolled 32,168 miles, have done good work towards suppression, and neighbouring officials co-operated most cordially in putting down border canteens and deprecating the traffic. But so long as a class of persons disposed to pursue covertly an illicit and profitable trade exist on one side of a border, a native race on the other side is easily tempted to gratify its vicious tastes, regardless of consequences. Though the Chiefs readily support the law when offenders are caught, some of them are the worst offenders, debauching heavily if they can procure spirits.

Trade generally during the year has been very bad, owing in a great measure to almost entire failure of the mealie crop, which is a primary purchasing power. The amended railway rates, however, afforded facilities for clearance of previously accumulated stocks, mortgaged to Colonial and Free State merchants who feed Basutoland with merchandise.

As railways close in, the business of riding transport is becoming more and more limited, and the traffic to and from Basutoland offers a useful field for this enterprise to a large class of Free State burghers, who pursue it with lucrative results.

The crops of cereals is most prolific, Kaffir corn especially being abundant, so much so that it is feared the manufacture of "beer" may yield an abundant crop of disputes and broken heads, as the outcome of festive gatherings; these prolific seasons are also conducive to laziness.

A remarkable development of the year may be indicated in the success of agricultural shows. These have been held at most of the magisterial stations. Their institution and success are due to the inspiration and labour of assistant commissioners, aided materially by committees of established traders, who have cordially supported the movement, to which also the Basutoland Chamber of Commerce has lent an impetus. The results have been to show the natives what kind of grain to grow with profit, to enlighten them upon the subject of agriculture, and the advantage of improving class of cereals and quality of stock. The manufacture of pottery, carosses, marketable wire-work, the rearing of poultry, and the commercial value of better and cleaner wool have, amongst other things, formed part of what may be termed this educational scheme, which may prove of great advantage.

These shows have received every practicable encouragement from the Government, and the people are being taught to support them by contributions towards the expenses, hitherto borne largely and voluntarily by the white community; it is hoped by their aid to bring home to the Basuto the necessity, if they wish to compete in the foreign markets, of producing cereals of a higher quality, both by planting better seed as well as by adopting a more enlightened system of cultivation. Some of them are already showing an advancement in their methods, and several steam threshing-machines have been brought into the country and used to a considerable extent. A mill has been erected near Maseru, and is said to be freely used by the natives. The overstocking of cattle presents a serious problem in regard to the provision of pasturage; cattle, being the native standard of wealth, are jealously accumulated, and it is impossible to form an estimate of the live-stock now in Basutoland.

The volume of export in grain, in spite of the mealie failure, is in excess of last year, though the total value has declined. This is due to accumulated stocks. Imports for the calendar year ended 31st December 1894 show a heavy falling off; but in the subsequent half-year there was considerable improvement.

The general revenue of the territory in 1894-95 has been well maintained and is slightly in excess of the previous year. Increases are shown under the heads, hut tax, post office, and miscellaneous. The total receipts amounted to 44,627*l.*, made up as follows:—Colonial contribution, 18,000*l.*; hut tax, 21,905*l.*; post office, 1,072*l.*; licenses, 1,392*l.*; fines, 381*l.*; ferry tolls, 522*l.*; fees, 49*l.*; miscellaneous receipts, 1,306*l.*

Arrear tax received amounted to 1,109*l.*, as against 1,882*l.* last year. Tax was paid on 44,677 huts, compared with 40,629 in 1893-94.

The expenditure for the above period totalled 43,064*l.*, being 1,072*l.* less than estimated.

With regard to the trade of Basutoland, the dutiable goods imported during the year ended 31st December 1894 amounted to a gross value of 68,674*l.*, being 29,326*l.* less than in 1893.

The serious decline in the volume of imports compared with the previous year is no doubt due to the severe depression in trade from which the country has been suffering, and the consequent fall in the prices of all agricultural produce.

The principal articles of import are woollen and cotton goods, haberdashery, saddlery, tinware, agricultural implements, and groceries. The three first items represent respectively 45, 8·50, and 9·50 per cent. of the whole imports. The bulk of the merchandise is supplied by the United Kingdom.

Exports for the year 1894, while exceeding those of 1893 to a considerable extent in volume, show, however, a large falling off in the declared value.

The chief articles of export consisting, as they do, of agricultural products (food-stuffs) subject to the fluctuations of uncertain markets, falls in prices realised must necessarily affect, to a considerable extent, the purchasing power of the people whose medium of exchange it is.

The total value of exports in 1894 amounted to 83,408*l.*, as against 103,608*l.* in 1893, thus showing a decline in value of 20,200*l.*

The principal articles exported in 1894 were wheat, to the value of 44,426*l.* (63,417*l.* in 1893); mealies, 16,265*l.* (17,452*l.* in 1893); and wool, 12,411*l.* (14,356*l.* in 1893).

Population is increasing steadily, both by natural increase and immigration. According to the Census of 1891, there was a total of 211,324 persons; it is now estimated at 250,000.

The general temper of the Basuto may be described as progressive. The characteristics of the Kaffir race predominate, and it would be unwise to attempt vigorous eradication or to graft prematurely upon them European ideas and institutions neither suitable nor sympathetic.

The Basutos are naturally suspicious of all innovations and cling affectionately to their country. So long as their customs are not generally repugnant to civilised thought and they are amenable to such laws as are necessary to govern them and their relations with civilised neighbours, there is motive for the exercise of forbearance during their efforts towards development. Though some superstitions may influence them, it is hard to trace in the present day any relics of a national religion. The ethics of Christianity have intervened, and may be said to supply their doctrines of morality. It would be quite incorrect, however, to suppose that the nation was moral in practice.

XII.—ARGENTINE REPUBLIC CUSTOMS LAW.

The following is a translation of the Argentine Customs Law for 1896 :—

Art. 1. Foreign merchandise on importation into the country, and products of the Argentine Republic, other than those allowed to be exported free of duty, shall pay respectively the import and export duties stated below during the year 1896.

I.—IMPORT DUTIES.

Art. 2. The following articles shall be taxed *ad valorem* and pay the per-centage duties specified :—

(a.) 25 per cent. *ad valorem*.—All merchandise not subjected by this law to a special duty and not exempted from duty.

(b.) 50 per cent. *ad valorem*.—Arms, accessories, and appurtenances thereof; harness and harness makers' wares in general, complete or not; articles of any kind of tissue, made up wholly or in part; sword-sticks; portmanteaus and trunks; leather bags or pouches of more than 25 centimetres; boots and shoes in general, finished or in pieces; vehicles, complete or in parts, with bodies and shafts in the rough, for the same; cartridges for firearms; rockets and fireworks; mosaics; furniture, complete or in parts; ammunition; perfumery in general: gunpowder; ready-made clothing, hats or caps not chargeable with a specific duty.

(c.) 45 per cent. *ad valorem*.—Stockings and socks of all kinds.

(d.) 40 per cent. *ad valorem*.—Bags and sacks of linen, &c., of cotton, sack-cloth, or canvas; borax; iron safes in general; empty match-boxes; hides and skins, tanned; fine lace of silk or of silk mixed with other materials; fine thread lace; buckles or clasps in general, for dresses or harness; trimmings and cords, of silk or of silk mixed with other materials, with or without metallic thread; tissues and all articles of silk or of silk mixed with other materials, including those of waste silk; lace tissues; screws, bolts, and nuts; blankets or rugs of wool or of wool with cotton warp, hemmed or fringed.

(e.) 20 per cent. *ad valorem*.—Unbleached cotton cloth.

(f.) 15 per cent. *ad valorem*.—Oak, spruce and white pine, and candle-wood, unplanned; silk bolting cloth.

(g.) 10 per cent. *ad valorem*.—White lead; anhydrous ammonia, in cylinders, for use in the preserved meat industry; metallic antimony; sand and stones used in vessels for ballast; powdered baryta or impure sulphate of baryta; coal tar or pitch; mineral pitch; cocoa in the bean; carts for the transport of cereals; iron chairs and sleepers for railways and tramways; small vessels or boats of any description; tin in bars or ingots; degreas grease; candle-wicks, plaited or not; aloe fibre, jute, or hemp,

spun, for plaiting; lead in sheets; sulphate of lime; so-called Brazil or Paraguay coco-nuts; malt; machines of all kinds of a value of 100 dollars and above, and spare parts for the same; motors and locomobiles; silk for sewing or embroidery.

(h.) 5 per cent. *ad valorem*.—Cork, in squares or roughly prepared; jewels; wire on reels for agriculturists; wire of iron or steel, galvanised or not, up to No. 14 inclusive; barbed iron or steel wire for fencing; needles for sewing machines; Fontaine-bleau sand; flowers of sulphur; cocoa and cocoa husks; twine for reaping machines; iron, in ingots, for foundries; malleable iron in bars, hoops, or plates, unwrought; old iron of all kinds; tinplate, unwrought, cut or uncut; fireproof bricks; fireclay; lead in ingots and bars; sewing machines and spare parts thereof; heavy tar oils; sackcloth or canvas of unbleached aloe fibre; ploughs; saffron; quicksilver; rosin; aloe fibre, jute, and hemp, raw, not carded, combed, nor spun; machines and materials for the installation of public lighting by electricity or gas, with the exception of service fixtures; machines and materials for the installation of public water supply and sewage, with the exception of service fixtures; shearing machines, with or without motors, and spare pieces for same; nitrate of potash, crude, for industrial purposes; compressing machines for baling forage; curry-combs; watches of gold or silver; articles of gold or silver; machines and motors in general for agricultural purposes, and spare pieces for the same; mats, of fibre; kaolin; spun wool and worsted yarn for weaving; printed books of all kinds, including maps; harrows and ploughshares of cast or wrought iron; carbonate of soda, soda ash, industrial silicate, nitrate and impure sulphate of soda, and caustic soda; impure sulphate of aluminum; zinc in ingots or bars.

(i.) $2\frac{1}{2}$ per cent. *ad valorem*.—Cotton, raw, with or without seeds, and cotton yarn for weaving; Trinidad asphalt; precious stones; sheet zinc, up to No. 4, cut for boxes, &c.; yarn for wax matches; caoutchouc in natural state; Guayaquil Corozo; fibre or pulp of wood for the manufacture of paper; hops; paper, natural white, in discs of from 1 to 2 centimetres in width; rabbit-fur; "Quillay" bark; gelatine for use in the preserved meat industry.

Art. 3. The under-mentioned articles shall pay the specific duties stated:—

Comestibles.

Articles.	Per Kilo.	Articles.	Per Kilo.
	Dols. cts.		Dols. cts.
Vegetable oils, of all kinds -	0·10	"Aji" (dwarf pepper), raw -	0·05
Cocoa-nut and palm oil -	0·04	Garlic, of all kinds -	0·01
Olives preserved in oil, stuffed or not, including the weight of the package -	0·08	Capers, in glass jars -	0·08
Olives, preserved in brine -	0·05	Do., in wooden barrels -	0·06
Do., pressed or not -	0·03	Almonds, shelled -	0·10
		Do. unshelled -	0·05
		Starch, of all kinds -	0·08

Articles.	Per Kilo.	Articles.	Per Kilo.
	Dols. cts.		Dols. cts.
Alpiste - - -	0.01	Biscuit, common - -	0.02
Lupines, in the grain - -	0.01	Chick-peas ("Garbanzos") -	0.04
Do., peeled - - -	0.02	Beans, dried - - -	0.01
Aniseed, in grain - - -	0.06	Flour, edible, of all kinds, in	
Vetches, in the grain - -	0.01	packets or tins, with the	
Herrings, smoked, in barrels	0.05	exception of wheat and	
Do., do., in boxes - - -	0.08	maize flour - - -	0.05
Rice - - -	0.02	Flour, in boxes, bags, or any	
Do., in the husk - - -	0.00 $\frac{1}{2}$	other receptacles - - -	0.04
Hazel-nuts, unshelled - -	0.03	Figs, dried, in packages	
Do., shelled - - -	0.05	weighing up to 2 kilos. -	0.05
Oats, in grain - - -	0.01 $\frac{1}{2}$	Do., in larger packages - -	0.03
Sugar, refined - - -	0.09	Eggs of all kinds - - -	0.02
Do., unrefined - - -	0.07	Hams - - -	0.25
Cod, and other similar fish	0.04	Milk, condensed - - -	0.07
Do., do., cut up - - -	0.06	Codfish tongues - - -	0.10
Biscuits, fine, of all kinds	0.15	Earthnuts ("Mani") - -	0.01 $\frac{1}{4}$
Coffee, in the berry - -	0.03	Butter - - -	0.10
Do., ground - - -	0.06	Lard - - -	0.08
Chicory, in packets - - -	0.03	Apples, pears or cherries,	
Do., loose - - -	0.02 $\frac{1}{2}$	dried, including the weight	
Shrimps, dried - - -	0.12 $\frac{1}{2}$	of the receptacles - - -	0.05
Cinnamon, unground - -	0.05	Ginger - - -	0.50
Meat, salted, in casks - -	0.02 $\frac{1}{2}$	"Mazacote" (a preserve	
Chestnuts, shelled - - -	0.02 $\frac{1}{2}$	made of brown sugar) -	0.02
Do., unshelled - - -	0.00 $\frac{1}{2}$	Honey - - -	0.03
Caviar - - -	0.40	Millet - - -	0.03
Barley, husked - - -	0.02 $\frac{1}{2}$	Mustard, English or French	0.10
Do., in the husk - - -	0.00 $\frac{3}{4}$	Lentils - - -	0.01
Onions - - -	0.01	Walnuts - - -	0.03
Rye, of all kinds - - -	0.01	Oysters - - -	0.04
Chocolate, in paste or powder	0.30	Potatoes - - -	0.01
Peas, in the pod - - -	0.01	Raisins, in packages weighing	
Do., shelled - - -	0.02	up to 2 kilos. - - -	0.15
Cloves - - -	0.06	Do., in larger packages -	0.10
Prunes - - -	0.08	Currants - - -	0.05
Chilian cocoa-nuts - - -	0.03	Anchovy paste - - -	0.30
Cumin seed - - -	0.05	Tomato paste - - -	0.05
Comfits, bon-bons and pas-		Stockfish, in bundles -	0.02
tilles, loose - - -	0.25	Fish, in brine or pressed -	0.04
Vegetables, preserved, in jars		Pepper, red - - -	0.03
or bottles - - -	0.15	Pine seeds - - -	0.06
Preserves of fish, shell-fish		Pepper, in grain - - -	0.04
and mushrooms, of all		Pistachio nuts - - -	0.10
kinds, with the exception		"Porotos" - - -	0.01 $\frac{1}{4}$
of sardines - - -	0.20	Cheese of all kinds - -	0.20
Meat preserves, except salted		Salt, common, - per Hectol	0.20
meat - - -	0.20	Do., fine, in kegs or bags,	
Dates, in packages up to 2		per Kilo. - - -	0.01
kilos. - - -	0.10	Do. in flasks - - -	0.02
Do., in larger packages -	0.06	Sauce, English - - -	0.15
Peaches, dried and stoned	0.05	Sardines, in oil or sauce -	0.07
Jams and nougat - - -	0.25	Semolina - - -	0.02
Sausages - - -	0.30	Soups, prepared - - -	0.10
Pickles, in bottles - -	0.10	Tea of all kinds - - -	0.20
Do., in other receptacles -	0.07	"Ticholos" - - -	0.15
Spices of all kinds, ground	0.12	Bacou - - -	0.20
Farina - - -	0.05	Truffles, in natural state	0.75
Vermicelli and macaroni -	0.07	Yerba, prepared - - -	0.04
Fruits, preserved in their		Do. Chilian - - -	0.03
juice or in syrup, and com-		Do. unprepared - - -	0.02
pote of fruits - - -	0.27		
Fruits in their natural state or			
preserved in water or spirit	0.15		

Beverages.

Articles.		Articles.	
	Dols. cts.		Dols. cts.
Brandy, not exceeding 79°, in casks or demijohns, Litre	0·06	Punch, in bottles - Bottle	0·10
Do. do., in bottles, from 501 millilitres up to 1 litre, Bottle	0·25	"Refrescoes con soda," in bottles - Doz. bottles	0·50
Absinthe, not exceeding 68°, in casks or demi- johns - - -Litre	0·29	Soda water - "	0·40
Anis, arrack, cognac, kirsch, rum, and other similar spirits, not exceeding 50°, in casks or demijohns, Litre	0·28	Ginger ale, in bottles Litre	0·50
Absinthe, bottled, not exceed- ing 68°, from 501 milli- litres to 1 litre - Bottle	0·34	Wines in general, bottled Bottle	0·25
Anis, arrack, cognac, kirsch, rum, and other similar spirits, bottled, not exceed- ing 50°, from 501 millilitres to 1 litre - Bottle	0·33	Wines, Oporto, Jerez, Madeira, Rhine, Chateaux Margaux, Chateaux Lafitte, Chateaux Iquen, Burgundy, and other fine wines, in casks or demi- johns - - -Litre	0·25
Byrrh, in casks or demijohns, Litre	0·30	Do., Garnacha, Moscato, Mar- sala, Nebiolo, Sauterne, Moselle, and similar sweet wines and other ordinary wines - Litre	0·12
Do. in bottles - Bottle	0·25	Do., Carlon, Priorato, Seco, common Burdeos, Barbary, and other common wines, in casks or demijohns, of an alcoholic strength of not more than 17°, and not con- taining more than 50 per cent. of dry extract deter- mined by evaporation at a temperature of 100 centesi- mal degrees, including the sugar - - -Litre	0·08
Bitters, Angostura, up to 68°, in bottles, from 501 milli- litres to 1 litre - Bottle	0·54	(When the dry extract ex- ceeds the above limit, the wine will pay 1 centavo for each 5 grms. or fraction thereof in excess, and per litre, additional.)	
Do., do., in half bottles -, - Do. of other kinds, in bottles Bottle	0·27	<i>Note.</i> —When wines and other beverages are of an alcoholic strength exceed- ing the limits above specified they will pay an additional duty of 1 centavo for each degree or fraction thereof in excess, and per litre.	
Do. in casks or demijohns, up to 68° - - -Litre	0·29	Must, alcoholised or concen- trated, and "mistelas" Litre	1·00
"Caña," bottled - -Bottle	0·25	Vinegar, in casks or demi- johns - - -Litre	0·01½
Do. in casks or demijohns Litre	0·20	Do., in bottles - - Bottle	0·02
Beer and ale in casks - Litre	0·09	Vermouth, in casks or demi- johns - - -Litre	0·15
Do. bottled - - -Bottle	0·12	Do., in bottles - - Bottle	0·16
Wine, "Chacoli," in bottles Bottle	0·08	Whisky, in casks or demijohns, not exceeding 50 centesimal degrees - - -Litre	0·30
Do. do. in casks - Litre	0·04	Do., in bottles - - Bottle	0·30
"Chicha," bottled - - Bottle	0·10		
Do., in casks - - -Litre	0·08		
Cider, in casks - - "	0·10		
Do., in bottles - - Bottle	0·15		
Geneva, aromatic "Old Tom," or "Schnapps," in bottles of one litre, not exceeding 50° - - -Bottle	0·33		
Geneva and Schnapps, in casks or demijohns, not exceed- ing 50° - - -Litre	0·23		
"Grappa," in bottles Botle	0·25		
Do., in casks or demijohns Litre	0·20		
Syrups, in bottles - - Bottle	0·15		
Liqueurs, in bottles of one litre, not exceeding 50° Bottle	0·33		
Do., in casks or demijohns, not exceeding 50° Litre	0·29		

Miscellaneous Articles.

Articles.		Articles.	
	Dols. cts.		Dols. cts.
Collars, of cotton, linen, or of cotton and linen mixed, for men and boys - - Doz.	1.25	Matches, of wax, stearine, or similar substances, in boxes containing not less than six dozen - - Kilo.	0.80
Cuffs, of cotton, linen, or of cotton and linen mixed Doz. pairs	2.00	Stearine - - - "	0.08
Felts, adhesive, for men's hats - - - Each	0.30	Kerosene - - - Litre	0.01½
Felt hat shapes, for men's and women's hats - Each	0.60	Playing cards of all kinds Gross	10.00
Felt in pieces, for the manufacture of hats - Kilo.	3.00	Paper, coloured, of all kinds, brown paper, blotting paper, straw paper, packing paper, &c. - Kilo.	0.12
Hats of wool felt, of all kinds, for men and boys - Each	0.40	Paper, white, for newspapers, in rolls or in reams, paper for books, and writing paper, of any kind or quality - - Kilo.	0.03
Do. do., of otter, beaver, vicuña, or rabbit hair Each	1.00	Wire nails - - - "	0.03
Tall hats, varnished, for coachmen - - Each	0.70	" <i>Pichua</i> " - - - "	0.20
Tall hats of other kinds "	2.00	Candles, of stearine, paraffin, or of stearine and paraffin mixed - - Kilo.	0.10
Matches, of wax, stearine, or similar substances, loose Kilo.	1.60		
Do., wooden - - - "	0.40		

Tobacco.

Cigars, Havanna, in wooden boxes - - - Kilo.	1.50	Cigarettes in general Kilo.	1.00
Do., do., loose, or in packets, or in cardboard boxes Kilo.	2.25	Tobacco stems - - - "	0.15
Do., of ordinary tobacco (other than Havanna), in wooden boxes - Kilo.	0.60	Snuff - - - - - "	0.40
Do., do., do., loose, or in packets, or in cardboard boxes - - - Kilo.	0.75	Tobacco, in the leaf or cut (Havanna) - - - Kilo.	0.70
		Do., in the leaf or cut, of other kinds, except Paraguayan - - Kilo.	0.25
		Do., Paraguayan, in the leaf or cut - - - Kilo.	0.15

II.—Export Duties.

Art. 4. The products and manufactures of the country enumerated below shall, on exportation, pay the duties specified:—

(a.) 2 per cent, *ad valorem*.—Meat, salted or jerked.

(b.) 4 per cent. *ad valorem*.—Animal oil; horns and horn shavings of all kinds; bone ash; horse hair; hides and skins; hoofs of cattle and sheep; animal fats; bones of all kinds; sheep's wool, in the grease or washed; hoofs, other than of cattle or sheep; ostrich feathers; tallow or fats, melted or pressed.

Old iron and steel shall pay a duty of 10 dols. per 1,000 kilos.

III.—Articles Free of Duty.

Art. 5. The following articles shall be free of duty on importation :—Cork in the bark or in strips ; sand and stones used in vessels for ballast, consigned to municipalities ; animals in general (after medical examination) ; ships, complete or incomplete ; sugar cane ; coal and charcoal for fuel ; casks of wood or iron, put together or not ; coke ; fish plates, transoms, &c., of iron or steel for railways and steam tramways ; staves for casks ; receptacles, special bags or sacks, cases (put together or not), and tin plate cut for receptacles, imported directly by establishments for meat preserving and destined for packing preserved meats ; specifics in general for curing sheep scab, &c. ; fresh fruit and vegetables with the exception of grapes ; wheat and maize flour ; fire-wood of all kinds ; locomotives, and detached parts of the same ; maize in the ear or in the grain ; machines for mowing, binding, or reaping, with or without motors, platforms, covers, or tarpaulins, and detached parts for the same ; steam thrashing machines with or without motors, covers, or tarpaulins, and detached parts for the same ; steam hulling or decorticating machines with or without motors, covers, or tarpaulins, and detached parts for the same ; furniture and implements belonging to immigrants and forming part of their baggage ; metallic money ; naphtha or impure petroleum ; articles exclusively destined for religious purposes, imported on the request of ecclesiastics ; gold in nuggets, bars, or powder ; water filters (Pasteur or similar systems) ; fish, fresh ; silver, in bars or ingots ; plants, living, subject to the law No. 2,384 ; blasting powder for mines ; reviews, newspapers, and periodicals, with or without illustrations, and diagrams, &c., for use in schools ; seeds for sowing, of all kinds, with the exception of those otherwise mentioned in this law ; wheat ; utensils for schools, imported on the request of provincial Governments or of the Council of National Education.

Art. 6. The products of the country, not specially mentioned in this law, may be exported free of duty.

XIII.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

RUSSIA,

Classification of Articles in Customs Tariff.

Note.—Poud = 36 lbs. Avoirdupois. Gold Rouble = 3s. 2d.

A despatch, dated April 1 last, has been received at the Foreign Office from Mr. W. E. Goschen, Her Majesty's Secretary of Embassy at St. Petersburg, enclosing translation of a notification, published in the "Official Messenger," relative to the classification of the following articles under the Russian Customs tariff:—

1. Gelatine in thin rounds, in shape of spangles, to be cleared under section 43, point 1. Duty, 6 roubles gold per poud.

2. Pictures, drawings, and similar articles, not liable to duty, placed in undetachable frames, glazed, with unpolished sheet glass, to be cleared in accordance with observation 2 to section 61 of the Tariff, and dutiable under point 6 of section 77. Duty, from R. 1.50 to R. 10 gold per poud, the weight to include that of the frames, and, in addition, 30 copecks gold for each arshine (28 inches) of length of the frame.

3. Mirrors in undetached painted china frames to be cleared under point 3 of section 76. Duty, R. 21 gold per poud.

4. Ground stone (dolomite), impregnated with coal tar, according to the quality of the material and the mode of preparation resembling asphalt in broken pieces, to be cleared under point 2 of section 83. Duty, 15 copecks gold per poud.

Tariff Modifications.

A despatch, dated April 21 last, has been received at the Foreign Office from Sir Nicholas R. O'Connor, Her Majesty's Ambassador at St. Petersburg, transmitting translation of an extract from the Official Gazette, which states that, according to a decision of the Council of the Empire, confirmed by His Imperial Majesty the Emperor on the $\frac{1}{2}$ March 1896, section two of the Russian Customs Tariff is to be amplified as follows:—

Broken rice without husk, containing not more than 5 per cent. of whole rice and destined for the manufacture of starch, shall henceforward on importation from foreign countries pay 10 copecks gold per poud, under rules established by the Minister of Finances.

SWEDEN.

Tariff Modifications.

A despatch, dated April 24 last, has been received at the Foreign Office from Mr. G. E. Welby, Her Majesty's Secretary of Legation at Stockholm, reporting the following changes in the Swedish Tariff, which have lately been sanctioned by the Riksdag.

Yeast. The present duty of 25 öre to be reduced to 20 öre per kilogram.

Pig and Ballast Iron. (Tackjärn och barlastjärn.) To be admitted free of duty: the present duty of 80 öre per 100 kilograms to be abolished.

Explosives. (Sprängämnen.) The following new duties to be imposed:—

Guncotton (bomullskrut) 30 öre per kilogram.

Smokeless powder (börksvagt), 50 öre per kilogram.

Other explosives (andra olag), 25 öre per kilogram.

Common powder (krut) to remain as before with the duty of 12 öre per kilogram.

These new duties will come into force on the 1st of January, 1897, unless the Government should in the meanwhile decide otherwise.

GERMANY.

Re-Importation of Articles Exported for further Manufacture Abroad.

A despatch, dated April 20 last, has been received at the Foreign Office from Sir F. Lascelles, Her Majesty's Ambassador at Berlin, containing the following translation of a notice concerning the re-importation of articles exported from Germany for further manufacture abroad.

The free entry of articles exported from Germany for further manufacture abroad is regulated by section 115, paragraph 2, of the German Customs Law of the 1st July 1869 (Gesetz Blatt of the North German Confederation, 1869, No. 30, p. 317). Immunity from duty is thereby accorded in special cases, where articles are exported to foreign countries for working up, finishing, or repair, to be returned in a perfected state. In No. 26 of the Order of the German Bundesrath (Federal Council) for the carrying into effect of the Federal Tariff Law of July 5, 1888 (Centralblatt for the German Empire, 1888, supplement to No. 31, p. 489), the free entry is accorded not only to articles of domestic production but also to articles of foreign origin which have already paid duty. The execution of the regulation in question is entrusted to the finance officials of the several States, and is to be in conformity with the principles recited in the Order

above-named (Article 24), which have been laid down partly in the earlier general conferences of the North German Confederation, and partly in resolutions of the Federal Council, to ensue uniformity of procedure.

The general effect of these principles is as follows:—The manufacturers must apply to the competent territorial finance authorities for their permission for the intended re-import when worked up before the export of the articles. This permission is only to be accorded where such means of identification are practicable as to ensure against the risk of fraud, and only in the case of such processes as could not be effected in Germany at all, or, if at all, only at a disproportionate cost and with great difficulty. When the requisite permission is granted the goods to be worked up are despatched for export, a fixed period being laid down for re-import, and particulars of the net weight, number of pieces, and means of identification being attached to the register. The identification is effected by the official marking of each piece with seals, lead, or stamps. If the articles exported are re-imported in improved state within the period fixed, and there arises no doubt as to their identity, they are exempted from the payment of Customs dues, even when the process of improvement abroad has resulted in a slight increase of weight, or when the articles are repaired or renewed in unessential parts. If, on the other hand, essential parts have been renewed, or if the wares have received additions abroad, the renewals in question or the additions are subject to duty, the duty to be assessed on the basis of the material of which the new parts or additions are composed.

Free Importation of German Sea-Fishery Products.

A despatch, dated April 20 last, has been received at the Foreign Office from Sir F. Lascelles, enclosing the following translation of the regulations respecting the Customs free import of the products of the German sea-fisheries.

I. Salted fish, fish oil, smoked fish (also seal oil and smoked seal), and spermaceti may be admitted free of duty as joint enterprise, other productions of sea animals, dried fish, mussels and other shell fish as individual enterprise; if the fish or sea animals have been caught by the crews themselves of German ships on the sea and if the following regulations have been observed:—

1. Vessels fitted out for the capture of fish and sea animals will only be allowed to sail from those ports in Germany where there is either a chief Custom-house or a first-class subsidiary Custom-house.

Vessels must as a rule return with their catch only to the ports originally left.

Exceptions require the sanction of the highest territorial finance authorities.

2. Vessels must be completely fitted out for the kind of fishery for which they are destined.

3. The masters of the vessels must, in good time before sailing, hand in to the Custom-house of the port a written declaration, mentioning the vessel, the mode of fishery, the part of the sea where the fishing is to take place and the estimated duration of absence, together with a list of the mates and the hands of the vessels and of their fishing gear.

4. The Custom-house in question must inspect the vessel and fishing gear. The barrels taken on board for the purposes of herring fishery must be stamped by the Custom-house.

5. The masters of the vessels must keep an accurate daily journal as to the course and the catch.

6. The catch may be prepared on board for the purposes of extracting oil, &c.

Further, with the sanction of the finance authorities, in accordance with the subjoined regulations, such preparation may take place in private dépôts erected abroad.

(a) The master of the vessel must, as soon as he lands at the dépôt, enter in the journals of the daily catch a declaration on oath that the contents are accurate, and must have his signature certified by the nearest German Consul.

(b) In transmitting the fishery products prepared at the dépôt with a demand for free entry, the superintendent of the dépôt must declare on oath to the master of the transporting vessel, stating the amount of the products of fishery on board, to the catch of which ship they belong, that they were caught by the crew themselves of that ship, and also that the transport vessels belong to German shipping and sail under the German flag, and must have this declaration attested by the nearest Imperial German Consul.

7. If the products of the catch are not despatched to German ports on board the ships fitted out for fishery, but on other transport vessels, the master of the vessel must trans-ship in a port where there is a German Consul resident, he must immediately on arrival inform the Consul of the fact, and not later than 24 hours afterwards present the daily journal, a written declaration of the products of the catch brought in, and also a statement on oath to him for protocol that the products of fishery were all caught by the crew of the vessel themselves, and neither entirely nor partly acquired by purchase, exchange, &c.

The Consul certifies that the journal and the declaration tally with the cargo, and then gives permission to trans-ship. The master of the transport ship receives the protocol and the consular certificate that he has taken on board products of fishery and of what kind they are.

Ships sailing to the herring fisheries may send the first results of their catch to land by means of so-called express-vessels, without any further obligation than that the cargo shall be certified by a declaration made under the prescribed assurance (8) by the master of the vessel which despatches the express-vessel.

8. The arrival of the vessel must be notified to the Custom-house in which the revision is to take place, and at latest 24 hours

afterwards the journal and remaining official papers (6 (b) and 7), and a written declaration of the products of fishery brought in must be also handed in.

At the same time the master and, if required, the mate next in seniority to him must certify on oath :

(a) That the products of fishery brought in with demand for free entry are all the individual catch of the crew, or, in the case mentioned in 6, that these products were all shipped at the dépôt specially appointed for their preparation ; and

(b) That these products have neither been entirely nor in part bought, exchanged, &c., during the transport.

In the event of trans-shipment (7) having occurred, the master of the vessel must declare on oath that the products of fishery consist entirely of the catch of the ship in question, handed over to him in accordance with the consular certificate.

9. The Custom-house may, during the inspection of the ships and their cargo, convince themselves of the correctness of the declaration by taking the deposition of the crew, or by other means.

10. If the import of fishery products by one and the same trader takes place in different ports, he may be required to hand in periodical reports of all the Customs passes for the fishery products brought in to the different ports ; he may also be requested to show the settlements (Abschlüsse) on his books.

11. Whoever shall introduce and declare foreign fishery products to be of his own catching shall be punished for defrauding the Customs, and also be excluded from the benefits of Customs for sea-fishery.

Non-observance of controlling regulations will be punished with a fine of 3—30 marks.

II.—The above regulations do not apply to the fishing industry off the German sea coasts, which is not to be liable to duty. The respective authorised officials may, however, issue controlling regulations according to local circumstances in order to prevent the importation of mussels and shell fish which have not been dredged off German coasts.

New Index to the German Customs Tariff.

The Board of Trade have received, through the Foreign Office, a copy of the revised Index to the German Customs Tariff, which came into use in the German Custom-houses on the 1st January last. A comparison of this Index with that formerly in use shows that several alterations have been made in the classification of goods for tariff purposes, and consequently in the rates of import duties payable, and the following may be quoted as among the more important of the modifications :—

Ferro-aluminium, and alloys of iron with aluminium containing up to 40 per cent. of the latter metal, formerly charged with duty as aluminium (under the head of copper, &c.), now allowed to be entered under the head of iron according to stage of manufacture.

Sheet iron. The tariff classification for sheet iron has been altered as follows :—

Former Classification.	New Classification.
<p>Sheet iron:</p> <ol style="list-style-type: none"> 1. Rough, also ground, as well as only smoothed (dressed), 100 kilogs. gross 3 marks. 2. Polished, varnished, lacquered, coated with copper, nickel, zinc (galvanised), tin (tinplate), or lead, 100 kilogs. gross 5 marks. <p>Wares of sheet iron, rough, also in combination with wood:</p> <ol style="list-style-type: none"> 1. Neither ground, varnished, coated with copper, zinc, tin, or lead, nor enamelled, 100 kilogs. gross 6 marks. 2. Ground, varnished, coated with copper, zinc, tin, or lead, glazed, or enamelled, 100 kilogs. 10 marks. 	<p>Sheet iron,* not cut or cut rectangularly:</p> <ol style="list-style-type: none"> 1. Rough, <i>i.e.</i>, only roughly rolled, still covered with scale; and the same straightened (stretched), 100 kilogs. gross 3 marks. 2. Further worked: wholly or partly freed from scale by acids or otherwise, ground, polished, varnished, lacquered, coated with copper, brass, nickel, zinc (galvanised), tin (tinplate), or lead, 100 kilogs. gross 5 marks. <p>Unaltered.</p>

* The effect of this alteration appears to be that sheet iron cut into shapes, or otherwise than rectangularly, is now to be classed with, and to pay the duties applicable to, wares of sheet iron, according to whether ground, varnished, &c., or not.

Spirits. Arrack, French brandy, cognac, geneva, rum, &c., imported in bottles or jars to the weight of at least 50 kilogs. gross, formerly allowed to be cleared on payment of duty as if imported in casks (125 mks. per 100 kilogs.), now to be charged as spirits, &c., imported in bottles or jars, at the rate of 180 mks. per 100 kilogs.

Wine, cider, &c. The permission to import wines, cider, &c., in bottles or jars to the weight of at least 50 kilogs. gross at the rate of duty applicable to imports in casks hitherto accorded has been withdrawn, and such imports will for the future pay the duties applicable to imports in bottles or jars, *i.e.*, 80 mks. or 48 mks. per 100 kilogs., according to whether sparkling or still.

Vinegar, olive oil, cotton-seed oil, linseed oil, castor oil. Similar alterations to those mentioned above have been made in respect of these articles when imported in bottles or jars to the weight of at least 50 kilogs. gross. Instead of paying the duties applicable to imports in casks as heretofore, the articles named will for the future pay the duties applicable to imports in bottles or jars, as follows :—

Vinegar, 48 mks. per 100 kilogs. (instead of 8 mks.).

Olive oil, 10 mks. per 100 kilogs., instead of 3 mks. per 100 kilogs. (both rates being applicable only to imports from countries having commercial treaties with Germany).

Cotton-seed oil, 20 mks. per 100 kilogs. (instead of 10 mks.).

Linseed oil, 20 mks. per 100 kilogs. (instead of 4 mks.).

Castor oil. Imports of this article under the conditions specified will now pay 20 mks. per 100 kilogs. instead of 9 mks. (This does not affect the privilege accorded to imports from countries having commercial treaties with Germany in regard to imports of castor oil in casks, or in tins of at least 15 kilogs. gross weight, which will still pay 2 mks. per 100 kilogs. gross.)

Scientific instruments. Several articles formerly classed under the heading of "instruments, astronomical, surgical, optical, mathematical, chemical, physical," and consequently admitted free of duty, are for the future to be charged with the duties applicable to the materials of which they are made. The

list of such articles includes barometers, thermometers, cameras, compasses, electrical machines, galvanic elements, globes, levelling instruments, manometers, microscopes, saccharometers, air pumps (for other than scientific purposes), magnifying glasses ("lupen"), optical glasses, hydrometers, photographic apparatus, &c.

Gelatine leaves and wafers and empty gelatine capsules, formerly classed as gelatine, paying 3 mks. per 100 kilos., gross weight, now classed with wares of unenumerated animal or vegetable materials, paying 30 mks. per 100 kilos.

Tubes of malleable cast iron, formerly charged with duty as very coarse cast-iron wares (paying 2 mks. 50 pf. per 100 kilos.), now to be classed as coarse iron wares, unenumerated, paying 6 mks. per 100 kilos.

Some alteration appears to be involved by the definite classification of hosiery piece-goods ("Trikotstoffe"). The classification now is:—

Hosiery piece goods ("Trikotstoffe") :—

1. Woollen, to be charged as woollen tissues.
2. Other kinds, to be charged as hosiery according to material.

Rice meal or flour. These products when coming from countries having commercial treaties with Germany, were formerly permitted to be imported at the

conventional rate of duty applicable to "Millers' products of grain and pulse, "i.e., hulled or pearled grain, pearled "barley, groats, and meal, and ordinary "bakers' wares," paying 7 mks. 30 pf. per 100 kilos. The new index withdraws this privilege, so that rice meal and rice flour will, for the future, pay the general tariff rate under the heading quoted, viz., 10 mks. 50 pf. per 100 kilos.

Dog biscuits. These, which for tariff purposes are classed with prepared meats, have been similarly treated. Not being expressly provided for by treaty stipulations, the application of the conventional rate of 17 mks. per 100 kilos. to these goods has been withdrawn, and the general tariff rate on prepared meats—20 mks. per 100 kilos.—substituted.

Shoe uppers made of tissues, with elastic insertions, have also been similarly dealt with. Instead of paying the conventional rate of 65 mks. per 100 kilos., applicable to fine shoemakers' wares, they will now pay the general tariff rate for the same category, viz., 70 mks. per 100 kilos.

Sewing machines without stands, and parts of such machines (with the exception of needles), formerly classed with machines—paying 5 mks. per 100 kilos., gross weight—are now classed as fine iron wares, and consequently subject to duty at the rate of 24 mks. per 100 kilos.

BELGIUM.

New Alcohol Law.

A despatch, dated March 28 last, has been received at the Foreign Office from Sir F. Plunkett, Her Majesty's Minister at Brussels, enclosing a copy of the Alcohol Law which was passed on the 26th February last.

The most important articles in the Bill are those numbered 6 to 12, by which it is intended to establish agricultural distilleries throughout the country. Practically the whole of the discussion on the Bill has turned on these articles, the effect of which, it is generally thought, will be to flood the country with spirit, and thus enormously increase the temptation and opportunities to drink.

The opponents of the Bill proposed that a state monopoly for distilling and rectifying spirits should be tried, on somewhat the same plan as the Swiss Alcohol Monopoly, which was said to have had a good effect in reducing the consumption of spirits, but not to go quite far enough, and Messieurs Picard and La Fontaine prepared and submitted to the Senate a short Bill to this effect,

but the Finance Minister objected that such a monopoly would have many disadvantages, and would in his opinion not diminish the consumption of spirits more effectively than the measure which he had promised to introduce at an early date.

It was, however, very justly pointed out by M. Janson and others that the establishment of two or three thousand new distilleries must increase the production of alcohol, and presumably also the consumption, and that as the setting-up of these distilleries in order to help the agriculturists was the avowed object of the Bill, it was difficult to see what real gain could accrue to agriculture if the Government intend to at once introduce a Bill tending to restrict the consumption of spirits in Belgium.

FRANCE.

Restricted Importation of Animals of the Bovine Species.

The "Moniteur Officiel du Commerce" states that by a decree of the 9th April last the importation into France from other countries of animals of the bovine species, for other than slaughter purposes, may only be carried on viâ the following Customs-houses :—

Dunkirk, Bailleul, Wervicq-
sud, Tourcoing, Blanc-
Misseron, Jeumont Anor.
Givet, Mogues.
Longwy, Batilly, Avricourt.
Petit-Croix, Delle.
Morteau, Pontarlier.
Bellegarde.
Modane.
Fontan, Vintimille.
Marseilles.
Cerbère.

Hendaye, Behobie.
Bordeaux.
La Rochelle-Pallice.
Nantes, St. Nazaire.
Brest.
Le Légué.
St. Malo.
Granville, Cherbourg.
Honfleur.
Le Havre, Dieppe, Rouen.
Boulogne, Calais.

The tariff of sanitary dues fixed by the decree of 23rd November 1887, as far as concerns animals of the bovine species from other countries, is modified as follows :—

1. Animals of the bovine species declared for the purpose of slaughter :—

Bulls, oxen, cows, 75 centimes each.

Bullocks, young bulls, heifers, and calves, 50 centimes each.

2. Animals of the bovine species other than those declared for slaughter :—

Sole tax, 1.50 francs each.

Classification of Asiatic Sheep and Mufflon Skins.

The "Moniteur Officiel du Commerce" states that hitherto "skins, prepared, of Asiatic sheep and mufflons," have been classified as "skins, prepared, not denominated" (No. 493, 2nd

paragraph), but that this classification having given rise to protests from the fur trade, and it having been found that part of the skins so described were in reality skins of a particular kind of Mongolian or Thibetan goat, the committee of the Customs has decided that the denomination in the 1st paragraph of No. 493 of the Customs tariff ("skins of sheep and mufflons of the Caucasus") should be considered as applying to a whole class of products and not indicating origin only. It has, consequently, been proposed to include under this denomination not only the skins of the said animals of Caucasian origin, but also similar skins of Asiatic origin, *i.e.*, skins of sheep and mufflons coming from Thibet, Mongolia, India, and China. These proposals were ratified by a Ministerial decision of the 25th February last.

Tonnage Dues at Dieppe.

A despatch, dated April 14 last, has been received at the Foreign Office from the Marquis of Dufferin and Ava, Her Majesty's Ambassador at Paris, transmitting extract from the "Journal Officiel," containing a Presidential Decree reducing the tonnage and other dues levied on vessels calling at the port of Dieppe:—

By 60 per cent. when the amount of cargo shipped or landed, expressed in freight tons, does not exceed the fourth part of the net official tonnage.

By 40 per cent. when the said amount shall exceed the fourth part but not the half.

By 20 per cent. when it shall exceed the half but amount to less than three-fourths.

Vessels from the colonies and foreign ports having put in at any other French port and there paid a local tax shall continue in all cases to profit by the reduction established by the decree of 27th July 1888, when the application of the present order does not insure a more favoured treatment.

Increased Port Dues at Nantes.

A despatch, dated May 1 last, has been received at the Foreign Office from the Marquis of Dufferin and Ava, enclosing extract from the "Journal Officiel," containing copy of a Presidential decree of May 1 last, imposing on all vessels using the port of Nantes for commercial purposes a maximum due of 10 centimes per metric ton of goods shipped or landed.

The following are exempted from the above dues:—

1. Vessels belonging to Government or employed on its service, and all goods belonging to Government or for its use by regular contracts.
2. Pilot, towing, or pleasure boats.
3. Boats of the coast fishery, or for internal navigation.
4. Vessels putting in for a rest, if they do not undertake any commercial operation.

Port Dues at Gravelines.

A despatch, dated April 29 last, has been received at the Foreign Office from the Marquis of Dufferin and Ava, Her Majesty's Ambassador at Paris, enclosing extract from the "Journal Officiel" of 29th April last, containing copy of a Presidential decree by which the commune of Gravelines is authorised to levy a due of 70 centimes per ton gauge on all vessels with cargo entering the port.

Government vessels and fishing boats are exempt from this due.

The above due is levied to provide the sum of 56,000 francs for new works at the port.

FRANCE—ALGERIA.

Shipping Dues at Algiers.

With reference to the Tariff note on p. 179 of the February number of the "Board of Trade Journal" respecting the *droit de statistique* levied by a recent law at Algiers on merchandise arriving from or proceeding to foreign countries, a communication has been received at the Foreign Office from Sir R. L. Playfair, Her Majesty's Consul-General at Algiers, reporting that the Governor-General has received a telegram from Paris to the effect that coal taken from the entrepôt at Algiers and embarked on board vessels calling to replenish their bunkers will only be charged a single *droit de statistique* of 10 centimes per ton instead of two, as provided by the *Loi de Finance* of the 28th December last.

PORTUGAL.

Manufacture Tax on Concrete Vegetable Oils and Candle-lights.

A despatch, dated April 17 last, has been received at the Foreign Office from Sir H. G. MacDonell, Her Majesty's Minister at Lisbon, enclosing copy and translation of a Law, dated 16th April last, which runs as follows:—

Art. 1. The undermentioned rates, equivalent to the tax on the manufacture of the various articles of merchandise herein specified, as laid down in the Bills relating to this matter, shall be levied on the clearance of the same on and after the date of publication of this Law in the Official Journal—the "Diario do Governo":—

Oils, concrete vegetable, with the exception of palm oil, reis 8 (0·42*d.*) per kilog.

Candle-lights of any kind or shape for lighting purposes, stearine in paste and analogous products, capable of being at once applied to the manufacture of candles for lighting purposes, 30 reis (1·60*d.*) per kilog.

Art. 2. These rates shall be levied and deposited until the Cortes shall have given their definitive decision as regards the Bill referred to.

Art. 3. Any laws contrary thereto are hereby revoked.

Commercial Agreements.

A despatch, dated March 28 last, has been received at the Foreign Office from Sir H. G. MacDonnell, Her Majesty's Minister at Lisbon, stating that the Russian-Portuguese Commercial Convention of July 9th, 1895, the Norwegian-Portuguese Treaty of Commerce of 31st December 1895, as well as the declaration between Portugal and the Netherlands of July 5th, 1895, were all approved on 27th March last by the Chamber of Peers, with a view to their ratification by the Portuguese Government.

PORTUGAL—EAST AFRICA.

Tariff Changes.

According to a report of Mr. A. Carnegie Ross, Her Majesty's Consul at Beira, for the year 1895 (*Foreign Office Annual Series*, 1678), the alteration in Customs dues during that year consisted (1) in reducing the rate on Portuguese wines from 20 to 10 reis a decalitre; (2) increasing from 40 to 70 reis per litre the duty on beer and other fermented liquors; and (3) increasing by 50 per cent. the duty on all spirits.

The alteration in licences has consisted in demanding an extra annual licence of 25% for the sale of patent medicines.

SPAIN.

Increased Duty on Foreign-grown Wheat.

A despatch, dated 20th April last, has been received at the Foreign Office from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, enclosing a memorandum of Sir G. Bonham, Her Majesty's Secretary of Embassy, showing the views held as to the results of increased protection on the agricultural and financial condition of Spain. The memorandum states that the question of duties on wheat is of considerable importance, and has been discussed at some length in a semi-official newspaper, the "*Eco de las Aduanas*," from which the following is taken:—

The matter is regarded under three aspects—

1. That of the agricultural interest.
2. That of the millowners' interest.
3. The interests of the National Treasury.

A year having now elapsed since the imposition of additional duties on wheat, flour, and bran, the duties now standing at 10 pesetas, 50 centimos for wheat and 17 pesetas 50 centimos for flour per 100 kilos., a sufficiently exact judgment may be formed of the results of that measure, but it should be borne in mind that, in addition to the advantages conferred by increased protection, agriculture has during the last year been favoured by purely accidental circumstances, such as the state of the exchange and the termination of the commercial treaty with the United States.

Thus the diminution of the imports of wheat has been accompanied by a corresponding increase in the exports of flour, and this increased export of flour is not due to foreign wheat as is shown by the fact that of the 37,000,000 kilos. exported during 1895 more than 25,000,000 were sent from Santander, a port which is stated to be exclusively devoted to the export of home-grown wheat.

Without considering the returns for January and February of the present year, during which the contrast has been the more marked, it is shown that the decreased imports of wheat (222,000,000 kilos.) taken in connection with the increased exports of flour (24,000,000 kilos.), amounting together to 246,000,000 kilos. are stated to represent approximately a sum of 49,000,000 pesetas (1,900,000*l.*) to be placed to the credit side of Spanish agriculture, as they denote a larger sale (of Spanish wheat and flour) in Spanish colonial markets.

The continued importation of foreign wheat at the Mediterranean ports is stated to be due to the convenient arrangements for storage, the facilities afforded for payment, the cheapness of transport, and the fact that in Spain certain descriptions of wheat are not grown.

In considering the question whether increased protection is advisable, it is calculated that the amount of wheat required for home consumption, for seed, and for exportation is about 1,000,000,000 kilos., and it is a very debatable point whether the whole can be raised in Spain without unduly increasing the price.

From the millowners' point of view the question assumes a different aspect. However great the benefit would be to the agricultural interest if native-grown wheat were used exclusively in the production of flour, there is considerable doubt if this would be possible.

The difficulties already alluded to in connection with the exclusive use of Spanish wheat are greater in the case of flour production. The taste of the consumer renders necessary wheat which is at present grown only in Russia; the number of small producers renders any system of storage on a large scale difficult, and the system of ready-money payments required by the small farmer is less convenient than payment at three months or six months date as is usual in dealings with the foreign producer.

These objections, combined with the difficulty of railway transport, would, according to the millowners, effectually put an end to an industry in which a large amount of labour is employed, and in which considerable capital has been sunk.

One inevitable result of the protection afforded to native-grown wheat has been the diminution of Customs receipts, which is severely felt in the critical stage through which the country is now passing.

Wheat, which has hitherto been the principal source of revenue, compensating to a great extent for the diminution of receipts, has been gradually disappearing from Spanish ports since the law of 9th February 1895 was put in force, and it may be said that

the benefits on agriculture have been conferred at the expense of the public treasury, in the receipts of which, there has been a considerable diminution.

Fines on British Shipping.

In a recent report to the Foreign Office, Sir George Bonham, Her Majesty's Secretary of Embassy at Madrid, states that the fines imposed on British shipping for errors occurring in ships' manifests and similar documents have been a fruitful source of cases of hardship, giving rise to representations between the two Governments. At one time these difficulties were not infrequently caused by errors on the part of Spanish Consuls in affixing their visas. Stringent regulations were, however, issued in the early part of 1895 which will at all events have deprived them of the excuse of ignorance, and it may be presumed that fines incurred on this account will cease.

Too much stress cannot, however, be laid on the absolute necessity of minute accuracy in such matters, as however obvious it may be that the error was not made with intent to defraud, the law is in the first instance interpreted literally, and restitution can only be obtained, if obtained at all, after a delay extending sometimes over months and years, more especially in cases requiring reference to be made to Cuba or one of the Spanish colonial possessions. It is true that a margin of 4 per cent. between the declared and actual weight is in some cases allowed for miscalculations, but the result has been that in order to save time, weights are sometimes given approximately on the chance that they may turn out to be within the margin allowed, or that the error may be corrected by telegraph after the vessel's departure. The following instance may perhaps be quoted as a warning, though numerous others will occur to those connected with the shipping trade:—

A vessel from New York with a cargo of wheat arrived in a Spanish port, having touched early in 1893 at an intermediate port, where a part of the cargo was discharged, and the manifest for the remainder of the cargo was translated into Spanish.

The number of bushels of wheat was correctly given in the ship's manifest, but owing to an omission on the part of the clerk at New York, the weight in kilos. was incorrectly given, with the result that a very heavy fine was imposed, which was confirmed by the Central Authorities in Madrid.—(*Foreign Office Annual Series*, No. 1684.)

Certificates of Origin.

In a report to the Foreign Office, dated February 13 last, Sir George Bonham points out that a result of the commercial estrangement between Germany and Spain, involving on the one hand the imposition of the higher scale of duties on German goods in Spain, and the penalisation of Spanish goods in Germany, has led to a considerable clandestine importation of German goods into Spain with French or Belgian certificates of origin. This

induced a very careful scrutiny of such documents, and the more strict enforcement of Custom-house regulation with respect to them. English trade has, in consequence, suffered considerable inconvenience, as a form of certificate previously accepted has been held to be inadmissible. The Spanish Government has, however, so far recognised the justice of the complaints and representations, to which the strict application of the Custom-house regulations has given rise, as to direct that goods shipped under certificates of origin previous to a certain date should be treated with as much consideration as circumstances will permit.

The form of certificate which is for the present acceptable has been communicated to the English chambers of commerce. It will be sufficient here to note that if issued by a chamber of commerce, mayor, or local authority, the certificates must state the name of the manufacturer as well as the country of origin. If issued by the collector of Customs the name of the manufacturer is not necessary, the formal declaration of the exporter before that official being considered sufficient. (*Foreign Office Annual Series, No. 1684.*)

ITALY.

Importation of Salted Pork.

According to a Ministerial Decree, dated 11th March last and published in the Official Bulletin of the Italian Ministry of Finance, the certificates of origin necessary for the importation into Italy of pork, salted, smoked, or otherwise prepared for keeping, shall be valid, as far as Europe is concerned, if drawn up by competent local authorities, without need of a consular visa.

Tariff Classification.

Cement, made of sulphur mixed with sand and ashes, is assimilated to bituminous cement and classified as "solid bitumens" (No. 264 of the Customs Tariff), according to a Ministerial Decree of 1st April last published in the Bulletin of the Ministry of Finance.

ROUMANIA.

Import Duties on Printed Cotton Tissues.

The Board of Trade have received, through the Foreign Office, a despatch from Sir G. H. Wyndham, Her Majesty's Minister at Bucharest, stating that a decision has been arrived at by the Roumanian Customs Department in the matter of the import duties chargeable under the Roumanian Tariff on printed cotton goods. Under para. No. 278 of the Roumanian Tariff, printed cotton tissues are chargeable with duty at the rate of 60 lei per 100 kilogs.,

while under para. No. 280 *light* cotton tissues, printed, pay 160 lei per 100 kilogs. Difficulties have apparently been experienced by the Customs authorities in determining what classes of printed cotton goods were to fall under the category of "light" tissues, and it has accordingly been decided to establish the limit of 75 grammes per square metre as the line of demarcation. Tissues weighing less than 75 grammes per square metre are, therefore, for the future, to be considered as "light," and to pay the rate of 160 lei per 100 kilogs.; tissues weighing above this limit will continue to be subject to the duty of 60 lei only.

The Development of the Port of Constantza.

A despatch, dated April 9 last, has been received at the Foreign Office, from Sir G. H. Wyndham, Her Majesty's Minister at Bucharest, stating that a sum of 12,000,000 frs. (480,000*l.*) has been voted by the Chambers for the enlargement of the port of Constantza.

Inclosed in the despatch is a notice, taken from the "*Courrier Financier*" of the 5th instant, to the effect that: 1st, with a view of encouraging the export of grain from the above-mentioned port, the railway tariff has been reduced from 3 centimes to 2½ centimes per kilometric ton for distances over 300 kilometres from the port;

2nd, in order to prevent stagnation of the grain trade during the winter months, a winter season railway tariff has been established for the transport of grain which may happen to be stored in the Danube ports, commencing from the 1st of December up to the 15th of March, new style, according to which not less than 10,000 kilogs. per truck and "*lettre de voiture*" will be transported to Constantza for 2 centimes per kilometric ton for no matter what distance.

UNITED STATES.

Sugar Bounties.

A despatch, dated 11th May last, has been received at the Foreign Office from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, enclosing a communication from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, who reports that he is informed that a Bill providing for a bounty on sugar beets was presented in the last session of the Colorado legislature, but that the Bill failed to become law. It is added that, so far as the State of Colorado is concerned, the cultivation of sugar beets is only in the experimental stage.

Sir Julian Pauncefote, in his despatch, adds that, from statements in the American press, it appears that Chief Justice Maxwell, of Nebraska, has written an opinion declaring that the sugar bounty law of his State is unconstitutional.

Customs Decisions.

The following decisions relating to the classification of articles in the Customs Tariff and the application of the Customs laws of the United States, have recently been given by the Customs authorities in that country:—

“Tam O’Shanters,” or Scotch caps, are assessed for duty at 50 per cent. *ad valorem* under paragraph 285 of the Tariff Act of 1894, as “knit wearing apparel” made of wool or worsted.

Asphalt “epurée” is entitled to free entry under paragraph 390.

Rough opals are dutiable at 10 per cent. *ad valorem* under the provision of paragraph 338 for precious stones uncut.

Strips of silk spot net or netting, about 6 ins. in width, with selvages at both edges, are dutiable at 50 per cent. *ad valorem* under paragraph 301, Act of August 28, 1894.

Cotton “furniture” or “border” lace is assessed for duty at 50 per cent. *ad valorem* under the provisions of paragraph 276 of the same Act.

Astrakhan and mohair plush trimmings are to pay duty at the rate of 60 cents per pound and 60 per cent. *ad valorem* under the provision of paragraph 398 of the Act of October 1, 1890.

Graphoscopes are dutiable at 40 per cent. *ad valorem* under paragraph 98, as optical instruments.

Unfinished pipe bowls are assessed as “articles composed of
“earthen or mineral substance * * * not specially
“provided for in this Act,” and not decorated, under paragraph 86 at 30 per cent. *ad valorem*.

Agate and crystal scale bearings to pay duty at 30 per cent. *ad valorem*, as articles composed of mineral substances, under paragraph 86.

Nickel rods, sheets, and wires are dutiable at 35 per cent. under paragraph 177.

Feathers, stripped and bunched, not dressed, coloured or manufactured, are entitled to free entry under paragraph 477, Act of August 28, 1894.

Bronze casts are dutiable at 45 per cent. *ad valorem* under the provisions of paragraph 215, Act of October 1, 1890.

Squirrel tails with the tail bones removed and strings substituted, and inserted so as to run from one end of the articles to the other, in a prepared condition, suitable to be used to ornament boas and capes, are assessed under paragraph 329 as “furs, dressed on the skin, but not made up into articles, 20 per cent. *ad valorem*.”

“Du Barry’s Revalenta Food” is dutiable at 20 per cent. under section 3, as a non-enumerated manufactured article.

Drumheads, circular sheets of leather or parchment, about 18 inches in diameter, are to be assessed for duty under paragraph 326½ at 25 per cent. as parts of musical instruments.

Formaldehyd, or beer preservative, is to pay duty at the rate of 25 per cent. *ad valorem* under paragraph 60 of the Act of August 28, 1894.

Millet-seed is exempt from duty under paragraph 611 of the same Act.

Manicure scissors are assessed for duty at 45 per cent. under paragraph 140, and nail files or cleaners at 35 per cent. under paragraph 177, Act of August 1894.

Lithographed paper trays, consisting of circular trays made of strawboard, the bottom or back being covered with surface-coated paper, and the front with paper on which a lithographic picture has been printed, are dutiable at 20 per cent. under paragraph 313.

Hemstitched initial handkerchiefs are liable to duty at 50 per cent. *ad valorem* under paragraph 349, schedule "J," Act of October 1, 1890.

Reorganisation of the Customs District of Alaska.

A despatch, dated April 24 last, has been received from the Foreign Office, enclosing copy of a circular issued from the Treasury Department of the United States on March 20 last, quoting an Act for the reorganisation of the Customs collection district of Alaska, by which it is enacted:—

Sec. 1. That the Customs collection district of Alaska be, and the same is hereby, reorganised and established to comprise the territory of Alaska, in which Sitka shall be the port of entry.

Sec. 2. That such other places as may be designated by the Secretary of the Treasury, as the interests of commerce may require, shall be sub-ports of entry or delivery, or both; and Customs officers shall be stationed at such sub-ports, with authority to enter and clear vessels, receive duties, fees, and other monies, and perform such other services and receive such compensation as, in the judgment of the Secretary of the Treasury, the exigencies of commerce may require.

Sec. 3. That all Acts or parts of Acts in conflict with the provisions of this Act are hereby repealed.

NICARAGUA.

New Law as to Consular Certificates.

A communication, dated 24th March last, has been received at the Foreign Office from Mr. Walter J. Chambers, Her Majesty's Acting Consul at Nicaragua, reporting that a new law has lately been decreed by the Nicaraguan Government, exacting consular certificate for all goods entering Nicaragua. The object of the Government is evidently to avoid Custom-house frauds as much as possible, but this new law is likely to be very inconvenient to importers of foreign goods, and the greatest care will have to be taken in declaring merchandise, as the slightest mistake is liable to be looked upon as an attempt to pass one article for another, and the package will then be confiscated.

MEXICO.

Customs Rules and Regulations.

Her Majesty's Consul at Vera Cruz, in a recent report to the Foreign Office, states that, in view of the Customs rules and regulations in force, foreign shippers are greatly to blame for errors and omissions occasioned through carelessness and which give rise to many questions.

Bills of lading and consular invoices should be sent to consignees at the ports and not to persons in the interior, who are mostly inexperienced in maritime practices, and lack that despatch so necessary in business matters.

Consignments to order should be avoided as much as possible as business methods in Mexico are not adapted to such transactions, and many complaints are occasioned by the shippers of such cargoes not forwarding the proper advices and documents, and, again, by forwarding the bill of lading to some person in the interior, who, by not acting promptly, occasions delay.

All charter parties should contain an arbitration clause for the settlement of questions by arbitration in Europe, and masters of vessels should be allowed greater latitude for arranging minor questions according to circumstances—(*Foreign Office Annual Series, No. 1681.*)

BAHAMAS.

Amendment of Customs Tariff.

The Colonial Office have forwarded to the Board of Trade copy of the Bahamas Tariff Amendment Act of 1895, by which it is ordained:—

I. This Act may be cited as the "Tariff Amendment Act, 1895," and shall continue in force for and during the continuing in force of the "Tariff Act, 1895," which Act is hereinafter called the principal Act.

II. On the passing of this Act there shall be imposed, levied, collected, and paid to Her Majesty, her heirs and successors, in support of the Government of these islands, upon the articles hereinafter mentioned, in addition to the duties imposed by the principal Act, the duties hereinafter set forth, that is to say:—

Gin and whisky, for each degree over proof according			
to Sykes' hydrometer, per gallon	-	-	One halfpenny
On all other spirits not enumerated in the table of duties			
in the principal Act, per gallon	-	-	5s.
Do., for each degree over proof according to Sykes'			
hydrometer, per gallon	-	-	One halfpenny

III. In reading hereafter the table of duties in the principal Act, the word "reputed" shall be inserted and read therein before the word "quarts" wherever such last-named word appears in such table.

XIV.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

CRISIS IN THE BATOUM NAPHTHA TRADE.

A communication, dated 15th April last, has been received at the Foreign Office from Mr. P. Stevens, Her Majesty's Consul at Batoum, reporting that, although traffic on the Trans-Caucasian Railway was resumed in January last, frequent interruptions, varying for periods of from two days to a fortnight and more, have ever since been prevalent, and a large number of British vessels that had been chartered after the 1st January 1896, to load mineral oils, both in bulk and cases, at Batoum, during the months of March and April, after waiting for their cargo for many days, were obliged to sail thence in ballast, to other Black Sea ports to seek for homeward freights in consequence of the exhaustion of the stocks of kerosene at Batoum and the non-arrival of fresh supplies of illuminating oils from Baku.

The critical situation in the naphtha export trade of Batoum which existed during the last two months of the year 1895, after the floods on the railway had occurred, has prevailed ever since; and under present conditions, although reduced traffic has again been recently resumed, it is difficult to say when the present crisis will cease; since the works hitherto carried out on the line do not appear to be of a sufficiently permanent nature to allow of full traffic being re-established.

Besides this, a great deal of snow still remains on the mountains and uplands of the Trans-Caucasus, and considerable fear is entertained that as soon as the thaw sets in, further damage to the line will be the natural result of the inundations and landslips which it is fully expected will occur.

In view of the foregoing, it is stated that, in principle, it has been decided to lay a pipe-line from a point on the eastern side of the Suram Pass to Batoum, and that the contract for the work has been taken by a Moscow firm of merchants, to whom, it is reported, a concession has been granted for the exploitation of the line during a period of ten years.

STRIKE OF WHARF LABOURERS AT STETTIN.

A telegram, under date of May 7 last, has been received at the Foreign Office from Mr. Wilfrid Powell, Her Majesty's Consul at Stettin, stating that, in consequence of the strike of wharf labourers at that place, ships had better discharge cargo at Swinemünde, pending settlement of the strike. (*See* p. 598 of *General Trade Notes*.)

GERMAN COMPETITION WITH ENGLISH TRADE.

A despatch, dated 21st April last, has been received at the Foreign Office from Mr. F. R. St. John, Her Majesty's Minister at Berne, enclosing tabular statement published by the "*Fenille Fédérale Suisse*," showing the paucity of British commercial travellers who have visited Switzerland during the past three years as compared with those from Germany. From this statement it would appear that the number of British commercial travellers, or rather travellers representing British firms, visiting Switzerland in 1893 was 105, which decreased to 69 in 1894, and further to 58 in 1895. The number of travellers representing German firms was 3,791 in 1893, 3,310 in 1894, and 3,246 in 1895; French travellers were 673 in 1893, 653 in 1894, and 794 in 1895.

It cannot be too often repeated that, though a small country, Switzerland contains throughout the year a very large and wealthy foreign community quite willing to purchase better commodities than those now supplied almost exclusively by Germany.

The remarkable diminution in the yearly visits shown in these tables is an indication of the economic impetus given in Switzerland to home manufactures by her recent commercial conflict with France, a circumstance which, however, should in no wise be looked upon as mitigating the advantages offered by Switzerland as a market for the sale of the very best class of British goods.

THE ROYAL WEST INDIAN MAIL COMPANY OF THE NETHERLANDS.

A despatch, dated 30th March last, has been received from Sir H. Rumbold, Her Majesty's Minister at the Hague, transmitting copy and translation of a new contract between the Government of the Netherlands and the Royal West Indian Mail Service, sanctioned by a law of the 20th March last, in which the Company binds itself to make a passage with steamers, under the Netherlands flag, once every three weeks from Amsterdam to Paramaribo and Curaçoa, and from Curaçoa to Paramaribo and Amsterdam, and to convey in those vessels the letters and parcels, which for that end shall be handed over to them by or in the name of the Postal Administration of the Netherlands, of Surinam, of Curaçoa, or of other countries, at Amsterdam, Paramaribo, Curaçoa, or in any other ports which the steamers touch at. The contract further goes into detail with regard to the steamers to be used, the days of departure, the ports to be touched at, the maximum duration allowed for each trip, the fines and penalties inflicted in case of contravention of any of the articles of the contract, the bounty to be paid to the Company for carrying the mails, &c., &c.

The full translation of the contract may be seen by those interested in the matter at the Commercial Department, Board of Trade, 7, Whitehall Gardens, between the hours of 11.0 and 5.0.

PROTECTION TO INVENTIONS AT THE BRUSSELS EXHIBITION.

A despatch, dated April 25 last, has been received at the Foreign Office from Sir F. R. Plunkett, Her Majesty's Minister at Brussels, enclosing copy of a Royal decree which appeared in the "Moniteur Belge," applying Article XI. of the Convention concluded in Paris the 20th March 1883, for the protection of industrial property, to the goods, &c., which may be sent to the Brussels International Exhibition arranged for next year.

 THE DROUGHT IN SPAIN.

A despatch, dated 4th May, has been received at the Foreign Office from Sir George Bonham, Her Majesty's Secretary of Embassy at Madrid, enclosing copy of statistics from the "Official Gazette," showing the rainfall in Madrid during the first four months of each year from 1860 to 1896 inclusive.

Of the 37 years dealt with, the present is that in which, during the period specified, the least quantity of rain has fallen, namely, on 12 days only. The largest rainfall was in 1881, in which there was rain on 73 days, the next being 1895, with rain on 57 days.

The driest of the last 20 years were 1878, 1882, 1887, 1893, and the present.

Previously to 1878 the amount of wheat imported exceeded 60,000,000 kilogs. only in three years, viz., 1869, 1870, and 1871, and during these, rain fell during the first four months on 20, 28, and 22 days respectively.

Since 1882 the average imports in years when there was a good or fairly good harvest, or those in which rain fell during the first four months on more than 40 days, are estimated at 150,000,000 kilogs.

This increase is attributed to the fact that considerable tracts previously cultivated with cereals were then devoted to the cultivation of the vine, and now that the wine trade is no longer in a prosperous condition the attention of farmers is directed to the advantage of returning to the practice of corn-growing, as even in good years the imports of wheat amount to 140,000,000 kilogs., representing a loss to the country of 25,000,000 pesetas (1,000,000*l.*).

It should be stated that the harvest in Spain takes place in June, so that the continued drought will be most disastrous to the corn crops.

 PROSPECTS OF TRADE WITH CUBA.

In a report to the Foreign Office, dated 2nd March last, Mr. A. Gollan, Her Majesty's Consul-General at Havana, states that at the present juncture of affairs the exercise of the greatest caution should be observed in extending trade relations with Cuba. The inducements are few and the risks many. When

order is restored, the extraordinary fertility of the soil of Cuba may possibly bring matters round to a point advantageous for the investment of foreign capital, but that time is not the present. Those who have already money at stake here may find it necessary to go on, so as to try to recoup themselves for recent losses; but where this is not the case there must be many more profitable fields where business enterprise can be prosecuted with much greater satisfaction and security.—(*Foreign Office Annual Series, No. 1676.*)

REGULATIONS AS TO TENDERS FOR BULGARIAN GOVERNMENT CONTRACTS.

A despatch, dated 15th April last, has been received from Mr. F. Elliot, Her Majesty's Agent and Consul-General at Sofia, enclosing copy of a note received from the Bulgarian Ministry for Foreign Affairs, which states that the certificates of respectability which must be presented by all persons, whether natives or foreigners, who send tenders for competition in public adjudications in Bulgaria, must bear a date not more than three months previous to the day on which they are presented.

STEAMSHIP SERVICE BETWEEN THE BLACK SEA AND BULGARIA.

A communication, dated 30th April last, has been received from Colonel C. E. Stewart, Her Majesty's Consul-General at Odessa, reporting that, by a recent Ukaz of His Imperial Majesty, the steamship service maintained some years ago by the steamers of the Russian Navigation and Trade Company between the Black Sea and Bulgaria, and formerly known as the Roumelian line of steamers, is to be re-established. Steamers of the above company will accordingly ply fortnightly between Odessa and Constantinople, calling at Varna, Burgas, and other places, left to the discretion of the Minister of Finance, who is authorised to advance to the company in question a sum of not exceeding 30,000 roubles (about 3,000*l.*) as mileage for the current year.

AGRICULTURAL SHOWS IN BULGARIA.

A despatch, dated 13th April last, has been received at the Foreign Office from Mr. F. Elliot, Her Majesty's Agent and Consul-General at Sofia, transmitting extract from "*La Bulgarie*," containing a programme of agricultural shows to be held at the following places in Bulgaria during 1896:—Vratza, 29th and 30th June; Viddin, 23rd to 25th August; Haskovo, 23rd to 25th August; Lovetch, 25th to 27th August; Bella-Slatina, 1st to 3rd September.

At these shows foreign exhibitors may compete in the classes of agricultural machinery and tools. A representative or agent of the firm must be present at the show.

British exhibitors should bear in mind that only machinery of the simplest and cheapest description has any chance of finding favour in Bulgaria.

BRITISH AND GERMAN TRADE WITH SERVIA.

In a report to the Foreign Office, dated 12th February last, Mr. Ranald D. G. Macdonald, Her Majesty's Consul at Belgrade, states that during the last 10 years the relative position of the European States from whom Servia purchases her manufactured goods has remained much the same. Austria-Hungary is easily first in almost all commodities, Great Britain second, owing to her cotton yarns and piece-goods, and then come Germany and Turkey, both of which countries have succeeded in largely increasing their sales to Servia. Turkey furnishes nearly all the tobacco imported, and has profited by the extension of the Servian vineyards to push a trade in Macedonian wines. Purchases from Germany consist of woollen and cotton goods, metals, leather, clothing, and fancy articles of a cheap and inferior description, suited to the primitive taste and slender means of the bulk of the people. A part of her success is doubtless to be attributed to cheap railway tariffs, but the swarm of Hebrew agents who work between Belgrade, Hamburg, and other German towns (either direct, or through Vienna) do valuable service in pushing through innumerable petty channels a trade which comes in the aggregate to assume respectable proportions. The smaller Servian traders who have no capital to work upon, find their convenience in certain German houses who supply goods in almost retail quantities, and whose complaisance extends to a credit of nine months or a year, unhampered by interest. British terms are, as a rule, six months' credit with 5 per cent. interest from the date of the account—a condition which, while keeping down the figures of importation, reduces, perhaps, the chances of loss by confining dealings to the more solid and reliable firms in the country.

More frequent visits might, with advantage to British trade, be paid by German-speaking commercial travellers, provided with a sufficient supply of samples. A special department of the Ministry of Commerce and Agriculture undertakes to answer inquiries as to the standing of Servian merchants.

It may appear remarkable that British trade should be almost entirely unrepresented in such matters as tools and cutlery, of which the total importation in 1894 appears to have been somewhat under 5,000*l.*, but the fact is that the Servian purchaser is resolutely conservative in adhering to traditions of shape and pattern, for which, indeed, he cares more than he does for quality ;

the kitchen knife, for instance, is an important article with which the Serb is accustomed to chop bones and meat; the blade must therefore be especially broad at its insertion into the handle, in order that the holder's knuckles may escape abrasion. The German manufacturer yields to this whim, and gives the blade the required shape—a concession which British manufacturers have hitherto refused. The result is the triumph of a German instrument, of which the blade turns on bone, over an English knife of about equal price but far superior temper. It is due to the German, however, to remark that he spares no trouble to give a proper finish to the article by carrying the metal of the haft quite through the bone or ivory handle, to which he rivets it, whilst the English haft goes in a part of the way only, and works quickly loose. The question of pattern is equally decisive as regards other tools.—(*Foreign Office Annual Series, No. 1685.*)

THE SALONICA-CONSTANTINOPLE RAILWAY.

A report, dated 6th April last, has been received at the Foreign Office from Mr. J. E. Blunt, Her Majesty's Consul-General at Salonica, stating that the works of the Salonica-Constantinople Junction Railway have been completed, and the whole line, which is 508 kilometres in length, and the construction of which was commenced on 22nd June 1893, was opened for traffic on the 1st April last.

The time now occupied in the journey from Salonica to Dédéaghatch is about 13 hours. The direct through service from Salonica to Constantinople via Adrianople will perform the journey in 24 hours, including stoppages.

MODIFICATION OF PILOT SYSTEM AT NEW YORK.

A report, dated 31st March last, has been received at the Foreign Office from Mr. Percy Sanderson, Her Majesty's Consul-General at New York, stating that the system of pilotage at New York was modified on the 1st of December 1895, and that although the new system is still in the experimental stage, it does not appear likely that there will be a reversion to that formerly in vogue.

For very many years pilot boats, the property of separate owners, used to proceed independently to sea, often as far as Sable Island to the north, and the Capes of the Chesapeake to the south, for the purpose of intercepting vessels and piloting them into port.

This system was found to be both expensive and dangerous. It had the additional disadvantage that when a vessel was spoken far out at sea the master would often decline to avail himself of the services of a pilot at such a distance from the port.

The pilots, with the approval of the New York Pilot Commission, under whose control they are, formed themselves into an association, called "The United New York Sandy Hook Pilot Association."

They decided that no change should be made in the number of the pilots, but that the number of boats should be reduced, and that none of them should proceed beyond 100 miles from the bar. In lieu of the 29 sailing vessels formerly employed in this business, there are now eight sailing vessels and a steamer. Four of the boats are stationed on a line from Fire Island to Barnegat, two others on a line from Asbury Park to Jones's Inlet, and the other two remain in harbour off Staten Island. The steamer is used as a station boat at the bar.

As previously stated, the new system is still looked upon as experimental, but the Pilot Commissioners state that they have received no complaints.

It is probable that in the course of a short time the sailing vessels will be replaced by steamers.

OPENINGS FOR BRITISH TRADE IN THE WESTERN UNITED STATES.

A memorandum, dated April 13 last, has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, which calls attention to some articles of commerce which should, in the interests of British trade, be watched by British merchants. The memorandum states that there is a good market in the western parts of the United States for earthenware, crockery, china, glass-ware, and enamelled ware. Although the imports from Great Britain are steadily increasing, still they do not increase as fast as the country develops, and cheap American products are being used where British-made goods, if the same were made known, might be utilised.

Many large firms in Kansas City, Leavenworth, Topeka, Atchinson, St. Joseph, Lincoln, Omaha, Sioux City, and Denver would probably buy if an opportunity were given.

This would appear to be a favourable moment for pushing the above class of goods forward in the western country.

In 1894 Kansas City received from Great Britain 6,000*l.* value of the above-named goods; in 1895 the value had increased to 15,871*l.* In 1894 Omaha received 422 crates, and in 1895 as much as 612 crates. The orders are increasing, but there appears to be room for larger ones.

It should also be pointed out that British merchants should observe more closely the market in the following articles, viz.:—Linens, carpets, linoleum, salt, saltpetre, cotton goods, woollen goods, knit goods, caustic soda, plate-glass, quicksilver, and fuller's earth.

Mr. Burrough, Her Majesty's Vice-Consul at Kansas City, is of opinion that the trade in the above goods, in his district at least, could be increased materially.

The importation of tin from the United Kingdom to Kansas City and Omaha is falling off rapidly this year so far, and American tin is coming in and taking the place of the British article.

The following table shows the amount of tin used annually in the different cities :—

	Lbs.
Kansas City - - - -	14,000,000
Omaha - - - -	8,000,000
St. Joseph - - - -	4,000,000
Denver - - - -	3,000,000

One firm in Kansas City uses about 7,000,000 lbs. per year, and there are some 10 firms using tin in large quantities in the West.

The British merchant should be able to increase his business, for tin will probably increase as fast and in the same ratio as the packing house business does.

The following amounts were imported from Great Britain to the cities as named :—

Kansas City, in 1894, 4,825,919 lbs.; in 1895, 6,774,851 lbs.

Omaha, in 1894, 4,500,000 lbs.; in 1895, 4,980,000 lbs.

St. Joseph, in 1895, 2,000,000 lbs.

Denver, in 1895, 3,102,000 lbs.

Another trade which should be watched is the binding-twine business. In Kansas City there is a very large sale of all kinds of ropes. Of late some very heavy orders have been placed in Ontario, Canada, and it looks as if more would follow.

To give an idea of the importance of the binding twine sales, figures are here given showing the average annual sale in the following four cities :—

	Lbs.
Kansas City - - - -	15,000,000
Omaha - - - -	8,000,000
St. Joseph - - - -	2,000,000
Denver - - - -	3,900,000

As regards Portland cement, complaints are made from the above four cities that the British article is too much mixed, and that its diminution is due to that cause. On the other hand, German and Belgian Portland cements are giving universal satisfaction, and their trade is increasing.

Of imported cements used in the Western cities, 70 per cent. is German, 20 per cent. Belgian, and only 10 per cent. British.

More cement is being used every year.

THE AMERICAN TIN-PLATE TRADE.

A communication, dated April 16 last, has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul

at Chicago, enclosing extract from a local newspaper which gives the views of the tin-platers of the United States, and expresses their alarm at what will result if the Steel Trust Companies should advance prices.

Should this occur, it is generally expected that it will ruin the American tin-plate makers, and will allow British plate to come in again in large quantities.

There is no doubt that the steel billet manufacturers of the United States have recently succeeded in raising the price of their commodity to a point that threatens to close the tin-plate mills, and throw the tin-plate trade into the hands of the British concerns. The manufacturers assert that they cannot possibly continue competition with the product of foreign mills that employ cheaper labour, and enjoy the privilege of exporting to America at a mere nominal import duty. Whether this is true, or whether the complaint is made for the purpose of resisting the advance in the price of steel billets, does not appear to be definitely known. If the marked advance of the steel products is the result of a combination, the tin-plate men may succeed in compelling a reduction that will enable them to keep their mills open. The men who are operating the steel mills, it is thought, will doubtless see their own interest in stimulating a demand for their products instead of driving their patrons out of business.

If, on the other hand, the advance in steel billets is due to the increased cost of production, it is thought no permanent relief can be looked for until Congress restores the plate duty to a point that will enable American mills to compete with those of England and Wales.

It is argued that the manufacture of tin plates in the United States has passed the experimental period; and further, that an industry that comprises some 137 mills, and gives employment to 30,000 men, and has sustained itself in the face of severe competition with European wares, and in a time of unprecedented business depression, must have a substantial foothold: that, if a tariff were adjusted on a rational basis that would counteract the advantage of cheap labour, which the English tin-plate manufacturers enjoy, the industry would be sure to expand until the whole American demand for tin-plate would be supplied by the home products.

IMPORTATION OF AMMONIA INTO UNITED STATES.

A communication, dated April 16 last, has been received at the Foreign Office from Mr. A. G. Vansittart, Her Majesty's Consul at Chicago, stating that a considerable amount of ammonia is being imported by the packing houses established in Kansas City and throughout the West for freezing and cooling purposes.

It appears the demand exceeds the American supply. As these houses must keep a large stock in hand, one important firm in Kansas City has contracted for a whole year's import of ammonia to come from Berlin.

[CENTENNIAL EXHIBITION OF TENNESSEE (U.S.A.).

A despatch, dated 23rd April last, has been received at the Foreign Office from Mr. T. F. Bayard, United States Ambassador in London, transmitting a communication, addressed by the Executive Officers of the Tennessee Centennial Exhibition to Her Majesty the Queen, extending a cordial invitation to Her Majesty's subjects to participate in an exhibition which is to be opened on May 1st, 1897, at Nashville, Tennessee, for the purpose of celebrating the 100th anniversary of the admission of the State of Tennessee into the Federal Union.

THE BRITISH TEXTILE TRADE IN MEXICO.

In a report to the Foreign Office, dated 27th February last, Mr. Arthur Chapman, Her Majesty's Consul at Vera Cruz, states that British textiles have more than held their own, in the face of heavy protection, and the exports of British cottons to the Republic in 1895 have nearly doubled, as compared with the preceding year.

This in itself is evidence of the artificial existence of the Mexican factories, which cannot compete even when favoured by a low value of silver, and are bound to collapse when protection is removed.

Under present circumstances everything depends on the price at which British textiles can be placed on the market, and, without tampering with the quality of the article, every economy should be practised in the first cost and in merchants' profits. Competition with native factories would be facilitated by the establishment of large retail centres where, like the home-made article, British textiles could be sold at a percentage on account of the manufacturer.—(*Foreign Office Annual Series*, No. 1681.)

TRADE MARKS IN MEXICO.

In a recent report to the Foreign Office, Mr. Arthur Chapman, advises that manufacturers should register their names and trade marks in Mexico and vigorously defend them. The illicit trade in fraudulent marks flourishes under many opportunities in Mexico, and although the law when applied in this respect may be severe, it is necessary that the defrauded party should look after his own interests, and there is no surer road to the confidence of the public, nor better means of advertising, than would be obtained by energetic measures in such cases.

These remarks apply not only to hardware, but to many branches of trade, especially to wines and liquors.

Manufacturers should send competent agents to Mexico, who should take sufficient time to study the market, register their trade marks, take action in cases of fraud, and establish their business.

There are no doubt agents to be found in the Republic conversant with Spanish, the business required, and business methods, but it should be remembered that such persons are few, and only to be successfully selected by good business representatives themselves.

Business methods in Mexico differ in many points from those in other parts of the world, and British merchants should, with every regard to a high standard of commercial honesty that invariably attains the advantage in the end, be more ready to adapt themselves to local methods in practice.

Many of the conditions which induced British houses of business to withdraw from Spanish America, and caused them to be supplanted by houses of other nationalities, have undergone modifications in Mexico, and there are not wanting indications that the establishment of agencies representing substantial British houses would be favourably received.

Vera Cruz is principally a port of transit, but yet it may be taken as a fair sample of one of the centres from which a certain amount of business is transacted, and as the harbour improvements advance towards completion the resources of the country are developed and the coasting trade improved, the place will assume more and more importance as a centre on which other districts will depend, and opportunities for business will increase.—(*Foreign Office Annual Series*, No. 1681.)

RAILWAY CONCESSIONS IN GUATEMALA.

A despatch, dated 13th March last, has been received at the Foreign Office from Mr. J. Roberts, Her Majesty's Chargé d'Affaires at Guatemala, transmitting copy and translation of a concession granted to Mr. Adolfo Stahl for the construction of a railway to connect the Occidental Railway (which now runs from the Port of Champerico to Retalhuleu) with Mazatenango, a town lying due east and distant about 15 miles from the present terminus, as the crow flies; and to build branches to traverse the principle coffee belts of the vicinity.

The term of the concession is 25 years from the opening of the line, at the end of which period the railway becomes the property of the nation.

The Government grants a subsidy of 18,000 dols. per mile constructed, to be raised by a toll charge on dutiable articles imported at Champerico, to be collected by the company and deducted by the Government on payment of the Customs duties by the merchants.

All implements, machinery, cars, provisions, clothing, &c. required for the railway or its employés to enter duty free.

The road to be laid three feet between rails and bridges to be sufficiently wide for a broad gauge to be laid down is required. Rails to be not less than 30 lbs. per lineal yard. The line to be completed in three years' time, and a guarantee of 30,000 dols. in Occidental Railway shares or State securities to be deposited with the Government for due fulfilment of contract.

In the event of any contemplated sale of the railway the Government to have the preference as purchaser, and the line may not be sold or mortgaged to a foreign Government.

In a further despatch to the Foreign Office, dated 13th March last, Mr. J. Roberts transmits copy and translation of a concession granted to Luis de Ojeda by the Guatemalan Government for the construction of a railway from a central point in Costa Cuca, Department of Quezaltenango, to Caballe Blanco, there to connect with the Occidental Railway. The latter station is about 20 miles north of the Port of Champerico, and the projected line will act as a feeder traversing the rich coffee country in the neighbourhood.

The concession is for a term of 75 years from the date of opening the line, after which the road becomes the property of the Nation, who grants a subsidy of 15,000 dols. per mile, payable on the completion of each 20 miles or fraction thereof should the length of the line be less. Interest at the rate of 8 per cent. to be paid by the Government on unpaid subsidy, for the payment of which latter, 5 per cent. of the total amount of all the import dues of the Republic is assigned.

All machinery, cars, tools, materials, food, clothing, &c. required for the railway or its employés to enter free. The rights and privileges of the concession to be transferable; but if to foreigners, without recourse to diplomatic intervention.

The line to be completed within three years from the commencement of work, and a forfeit of 30,000 dols. in cash or State bonds to be deposited with the Government for due fulfilment of the contract.

The maximum grade to be $3\frac{1}{2}$ per cent., bridges to be of iron, or steel and wood, with sufficient width for 4 ft. $8\frac{1}{2}$ gauge if necessary, width between rails 3 ft., and rails to be not less than 36 lbs. per lineal metre.

The line may be prolonged or other branches built, if the construction of same be commenced within five years of approval of contract, and La Charrera may be substituted as a terminus for Caballe Blanco at Concessionnaires option.

The Government to have the preference, in case of contemplated sale, at a price to be fixed by two experts, one chosen by each side, who in the event of disagreement shall select a third whose decision should be final.

GUATEMALA EXHIBITION OF 1897.

A despatch, dated 21st March last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Chargé d'Affaires at Guatemala, enclosing copy of the following circular from the General Committee of the Exhibition, inviting all foreign powers to take part in it:—

"The Central Committee, whilst taking into consideration that the exhibition which is to be held here in 1897 shall be purely of a Central American character, have arranged that in view of the country's principles of universal brotherhood, all friendly nations, even though not American, shall be permitted to exhibit.

With this object the general regulations make arrangements for a foreign section, in which all exhibits coming from friendly nations are to be placed.

It has also been agreed to ask the Minister of Interior to direct a note to all foreign Governments, inviting them to take part in the exhibition, and stating that the nation of Guatemala will take pleasure in seeing their products of science, agriculture, industry, and commerce exhibited in the foreign section of the exhibition."

ARRANGEMENTS BETWEEN THE "PACIFIC MAIL STEAMSHIP COMPANY" AND THE "COLOMBIAN STEAMSHIP LINE."

With reference to notices on page 564 of the November 1895, and page 75 of the January, 1896, numbers of the "Board of Trade Journal," a despatch, dated 26th March last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Chargé d'Affaires at Guatemala, stating that the arrangement between the "Pacific Mail Steamship Company" and the "Colombian Steamship Line" has now been definitely and finally carried out.

By this arrangement the "Pacific Mail Company" cease running steamers from Colon to New York, and the "Colombian Line" will no longer run their boats on the Pacific between Panama and San Francisco.

THE BAR AND HARBOUR OF RIO GRANDE DO NORTE.

A report, dated 11th March last, has been received at the Foreign Office from Mr. A. L. G. Williams, Her Majesty's Vice-Consul at Pernambuco, on the bar and harbour of Rio Grande do Norte. In the current sailing directions which have been compiled for this coast, chiefly from surveys made by Baron Roussin and by Monsieur Mouchez, of the French Navy, Rio Grande do Norte is spoken of as being frequented by vessels of less than 150 tons burthen.

To-day, however, steamers of 1,200 and 1,300 tons register can load at Natal (Rio Grande do Nord), down to a draft of 15 feet, with perfect safety, carrying as much as 1,500 to 1,600 tons dead weight of sugar on this draft of water.

The bar has altered by the sand silting up in one direction, and thus deepening the channel in another, so that, from soundings recently taken, on entering the port nothing shallower than four fathoms of water at high spring tides was found. This may not always be relied on, but it goes to prove that there is much more water on the bar than is generally known by British shipowners and underwriters.

The steamers of Messrs. T. and J. Harrison call regularly at Rio Grande, coming from and returning to Liverpool, *via* Pernambuco.

The river, at its mouth, is about 180 to 200 yards wide, facing north, between two points of the coral reef which runs along the coast; much wider, of course, where it flows over the reef.

There is a fort on the southern side, built on the reef, which is visible from two to three miles off.

The Federal Government have some harbour improvement scheme in view, but so far the works appear to have been limited to planting bushes and trees in the sand hills, to prevent the sand being swept by the wind into the channel at the bar.

The town of Natal lies within half a mile of the mouth of the river, and steamers can load and discharge alongside the warehouses, the water being of ample depth, and quite still, at all tides.

The principal danger of the bar lies in its S-like shape, which necessitates steaming at full speed, so that the ship may turn with rapidity and perfect certainty. Steamers of greater length than 300 feet run a risk of going aground in the channel, just where the sea is breaking over the shallow part of the bar, otherwise this bar is far better than those of Penedo Aracaju and Estancia.

THE PORT WORKS OF MONTEVIDEO.

A despatch, dated 27th March last, has been received at the Foreign Office from Mr. W. Baring, Her Majesty's Minister at Montevideo, enclosing copies of the report drawn up by the engineers, Messrs. Guérard and Kummer, on the subject of the Port of Montevideo.

The copy of this report, which is too voluminous for publication in the "Board of Trade Journal," can be seen on application at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, S.W., between the hours of 11 and 5.

THE "DEUTSCHE BANK" IN CALCUTTA.

A communication, dated 9th April last, has been received at the Foreign Office from the Hon. C. Dundas, Her Majesty's Consul-General at Hamburg, stating that the Deutsche Bank, which has its headquarters in Berlin, is going to establish at once a branch bank in Calcutta.

THE EAST COAST ROUTE TO MASHONALAND.

With reference to an article on pp. 207-8 of the "Board of Trade Journal" for February 1896, respecting the East Coast route to Mashonaland, a communication has been received at the Board of Trade from the Agent-General of the Cape of Good Hope enclosing copy of a communication from the general manager of the Cape Government railways at Cape Town, in which it is affirmed that the Cape route to Mashonaland does not pass through a Foreign State as stated in the above article.

XV.—GENERAL TRADE NOTES.

SHIPPING THROUGH THE SUEZ CANAL.

The "Canal de Suez Bulletin," in its issue for the 12th April last, gives detailed statistics of the shipping passing through the Canal in the year 1895, as compared with the two preceding years.

From these figures it appears that the total number of British ships passing through the Canal in the year 1895 was 2,318, of a gross tonnage of 8,382,075 tons, as compared with 2,386 (8,326,826 tons) in 1894 and 2,405 (7,977,728 tons) in 1893.

The percentage of British ships in the total gross tonnage of vessels passing through the Canal, which was 74.2 in 1893, fell to 73.8 in 1894 and to 70.8 in 1895.

Of vessels under the German flag, which, numerically speaking, rank next in importance, 314, of a gross tonnage of 977,029 tons, passed through the Canal in 1895, as compared with 296 (887,363 tons) in 1894 and 272 (798,929 tons) in 1893.

The percentage of German tonnage passing through the Canal has thus risen from 7.4 per cent. in 1893 to 7.9 per cent. in 1894 and 8.3 per cent. in 1895.

278 French vessels (1,005,051 tons) passed through the Canal in 1895, as compared with 185 (710,990 tons) in 1894 and 190 (702,634 tons) in 1893. The percentage of French vessels, which fell from 6.6 in 1893 to 6.3 in 1894, rose again to 8.5 in 1895.

Netherlands ranks next in importance in these statistics, 192 vessels (497,903 tons) having passed through the Canal in 1895, as compared with 191 (484,570 tons) in 1894 and 178 (443,148 tons) in 1893.

The other countries which used the Canal in 1895 in the order of their importance are—Italy, Austria-Hungary, Norway, Russia, Turkey, Spain, United States, Portugal, Egypt, Japan, Sweden, Denmark, and China.

According to the issue of the "Bulletin" for 2nd May last, the number of ships passing through the canal during the month of February last was 276, of a gross tonnage of 970,066 tons, 258 of which showed a tonnage of 868,714 tons. Of these vessels, 171 were English (637,658 tons), 28 Italian (82,391 tons), 24 German (82,100 tons), 18 French (68,677 tons), 17 Dutch (43,103 tons), 7 Austro-Hungarian (23,411 tons), 5 Spanish (20,740 tons), 2 Russian (8,148 tons), 3 Ottoman (2,614 tons), and 1 Norwegian (1,224 tons).

THE BELFAST LINEN TRADE.

According to the report of the Belfast Linen Trade Board, as published in the "Irish Textile Journal," the condition of the

spinning trade in March remained unsatisfactory, values being slightly lower on the month.

The demand for flax was of a very quiet character, and for yarns it was entirely confined to the purchase of small sorting-up lots as required from day to day. The demand for cloth was well sustained, both power and hand-loom manufacturers being well engaged.

Trade with France and Germany shows further development, the shipments for March being considerably in excess of same period last year; on French account, more than double the quantity was taken.

Orders for the United States have been coming forward more slowly, and the improvement in the earlier part of the year has not been followed up. The quantities for the three months of the year were 17 per cent. less than for the same period of last year.

Foreign West Indian trade is considerably less than at same period of 1895, the unsettled condition of the country being a great drawback. The Australian trade continues to increase, and the Canadian demand for the quarter shows an improvement on last year. Mexico, Columbia, and the Argentine are all looking better. Except with the United States, West Indies, Spain, and Brazil, all other markets appear as larger consumers.

For the three months ended March 31 the total quantity of linen piece-goods exported from the United Kingdom was 5.6 per cent. less than in 1895, but values were 3.6 per cent. more.

THE FOREIGN TRADE OF EUROPEAN RUSSIA IN 1895.

According to the Austrian "Handels Museum" for April 23, 1896, the foreign trade of European Russia amounted in the year 1895 to a total value of 1,180,395,000 roubles, the imports amounting to 489,365,000 roubles, and the exports to 691,030,000 roubles, as against 515,236,000 roubles and 664,247,000 roubles respectively in 1894, and 421,956,000 roubles and 594,668,000 roubles respectively in 1893. The import of provisions last year was valued at 67,651,000 roubles, as against 66,197,000 in 1894; that of raw and half-manufactured goods 282,345,000 roubles, as against 314,464,000; and cattle 2,883,000 roubles, as against 2,734,000. Taken by weight the import of coal and peat amounted to 117,664,000 pouds (-2,500,000), coke 6,605,000 (-2,500,000), iron 13,720,000 (-1,850,000), steel 1,909,000 (-301,000), steel rails 1,137,000 (+584,000), machinery, with the exception of agricultural implements, 3,152,000 (+165,000), agricultural machinery 935,000 (-62,000), parts of machinery 1,238,000 (+300,000). The figures above in brackets show the difference between the years 1895 and 1894, increase (+) decrease (-).

The diminution of the importation of coal is due to the development of the Russian industry in this product; the

decreased importation, too, of iron and steel is due to the progress made by the industrial trades of Russia. A considerable increase is met with in the import of steel rails, in consequence of the construction of so many railways in Russia. The import of agricultural machinery suffers from the high duties, for the lessening of which there has long been a pressing demand made in Russia.

There were exported 237,093,000 pounds of wheat (+ 33,000,000), 90,588,000 pounds of rye (+ 10,000,000), 66,438,000 pounds of oats (- 30,000,000), 107,876,000 pounds of barley (- 45,000,000), 25,564,000 pounds of maize (- 2,300,000), 3,122,000 pounds of flour (- 400,000), raw sugar 2,839,000 pounds (+ 143,000), refined sugar 149,000 pounds (- 6,000), spirits 177,132,000 pounds (- 53,290,000), flax 13,779,000 (+ 5,000,000), hemp 13,361,000 (- 11,400,000), eggs 1,401,291,000 pounds (+ 454,000,000), petroleum 50,178,000 pounds (+ 8,000,000), boards, laths, &c. 25,922,000 pounds (- 400,000).

The diminution in the export of oats and maize is due to the competition with other countries, which was more severe than in previous years. The export of sugar rose as the result of better yields from the beet. The export of spirits fell off in consequence of unfavourable conditions in the foreign markets. The export of petroleum finally rose as the result of a successful rivalry with America. In all, the excess of the exports over the imports was 201,665,000 roubles, as against 149,011,000 roubles in 1894.

NEW PATENT LAW IN RUSSIA.

A recent issue of *Kuhlow's* states that, according to "*Wieck's Gewerbe-Zeitung*," the Russian Government has just completed a patent bill, which requires only the sanction of the Czar to become a law. This bill fixes the life of a patent at 12 years subject to the payment of an annual tax, which increases every year, amounting to 20, 30, 40, 60, 80, 100, 150, 200, 300, 400, 500, and 600 roubles for the 12th year. Thirty roubles have to be paid at the time of application, beside the tax for the first year. Of the taxes or application fee nothing is refunded. To indigent applicants a suspension of the tax for the first two years is granted, the application fee, however, must be paid at all events. The taxes have to be paid before the expiration of the two years' respite, otherwise the patent is annulled. A patent is granted to the first applicant. Protests, which have to be filed with the courts, are considered within the first two years only. Notice has to be given to the authorities of transfers of patents. Supplementary patents must be filed within one year after the issue of the original patent. For a supplementary patent 30 roubles have to be paid at the time of application and 20 roubles after its being granted. A patent is annulled (1) after it has been in force for 12 years, (2) if the annual tax is not paid regularly, (3)

if it is not applied for, or the proof of it furnished, at the proper time, and (4) if on protest of third parties the courts declare the owner of the patent as not being entitled to it. After the expiration or annulment of a patent it becomes public property. The intentional imitation of an article protected by a patent constitutes a violation of the patent, even if a few immaterial changes have been made; the sale of such articles without consent of the patent owner constitutes an infringement of patent. Any person guilty of these offences is liable for damages as well as to a criminal prosecution.

EXPORTATION OF TIMBER FROM THE CAUCASUS.

According to a communication of the French Consul at Tiflis published in the "Moniteur Officiel du Commerce," timber is chiefly exported from the Caucasus *via* the ports of Poti and Batoum, and at certain more or less sheltered points along the coast, such as Sukhum, Otchemtchiry, Kodor, Adler, Anaklia and Bzib.

The shipments at one time included boxwood, walnut, and oak, but are now confined more or less to walnut only.

As regards boxwood especially, there is but a very small quantity left in accessible districts belonging to private persons, but the Russian Imperial Government preserves with jealous care all that is found in its domains, and for this reason, in spite of all representations, it has decided to do nothing with the magnificent forest of Bzib, which alone contains, perhaps, over 200,000 tons of boxwood.

The fine oak trees have also almost entirely disappeared and are not to be found at less than 40 or 50 kiloms. in the interior of the forest; and as roads are unknown, the expense of transport is such as to prohibit all idea of exportation.

With regard to walnut, however, from 3,000 to 5,000 tons have been yearly sent to France and England. This movement, which shows a palpable decrease, is for the moment directed towards Germany *via* Hamburg.

The export dues on boxwood and walnut were formerly about 10 kopecks gold per pound (36 lbs.); but in order to arrest the clearing of the forests, these dues have been raised for some years past to 30 kopecks per pound.

THE EXPORT TRADE OF SWEDEN.

The Austrian "Handel's Museum" for April 23 states that during the past year, the export trade of Sweden has made considerable progress over the previous year in several respects. Thus there is an increase recorded of 18.7 million kilogs. in raw

and ballast iron; 23 million kilogs. cast work; 5.5 million kilogs. in iron hoops and bars; 26 million kilogs. in bar-iron; and 1.8 million kilogs. in scrap iron.

The export of copper rose in 1894 from 12,500 kilogs. to 208,000 kilogs., and that of zinc from 24.8 millions to 33.3 million kilogs., while there was no important change in the export of timber, and that of lucifer matches rose from 13.3 million to 15.2 million kilogs. Moreover, the value of the exports of machinery increased from 3.5 million krone in 1893, and 4.1 millions in 1894, to 5,058,000 krone in 1895; the export of cotton yarns from 732,000 and 874,000 kilogs. to 1,037,000 kilogs. The export of woollen yarns is put down as 336,000 kilogs. as against 253,000 kilogs. in the previous year.

BRITISH AND GERMAN TRADE WITH RUSSIA.

A recent number of "Kuhlow's" notices a report prepared by the Dresden Chamber of Commerce from Russian statistics, which deals with the rivalry of British and German trade in Russia. After pointing out that in 1894 the value of the goods imported from European countries into Russia exceeded by 92 million roubles the value of the imports for 1893, the report states that 45 per cent. of this increase is due to the extension of German trade, which has succeeded in ousting England from her former position at the head of the list of countries engaged in trade with Russia. As lately as 1893 the value of English imports exceeded that of the German imports by 11 million roubles, but in 1894 Germany imported goods into Russia for $14\frac{1}{2}$ million roubles more than England. In the same way, the imports into Germany from Russia have increased 11 per cent.

GALICIAN PETROLEUM IN GERMANY

"Kuhlow's" states that Galician oil has now begun to compete with the American product in Germany. For the first time the export of refined petroleum from Austria-Hungary to Germany has been made possible by the decline in the price of the Galician crude oil, caused by the greatly increased production in that country. The price of Galician crude was formerly 4 florins, but since the opening up of the great wells at Schodnica it has fallen below 2 florins. This made it possible to produce refined petroleum at a figure which allowed an export to Germany. In fact, several refineries, especially that at Pardubitz, Bohemia, have commenced to compete with the American petroleum in the German market and large quantities of refined oil have been exported already to Germany, mostly to Bavaria and Saxony. The Austrian railroads are assisting the refiners in their export trade by reducing the freight rates. Since 1st November a

· specially reduced freight tariff for refined petroleum and crude benzine has been in force for the export to Bavaria. The export of crude oil to Germany is impossible at present, because the German import duty is the same on crude as on refined petroleum, namely, 6 marks (6s.) But as in Germany the desire becomes steadily stronger to supply the home demand by home production, that is refining, and to renounce the American oil, it may be possible that the import duty on crude will be reduced so as to allow the export of Austrian (Galician) crude oil to Germany.

STRIKE OF WHARF LABOURERS AT SWINEMÜNDE.

The German "Reichs Anzeiger" of 8th May last states that, at a meeting of the wharf labourers on strike at Stettin, it was reported that the wharf labourers of Swinemünde had declared themselves in union with them, and that cargoes destined for Stettin could no longer be discharged at Swinemünde. (See p. 578 of *Extracts from Diplomatic and Consular Reports*.)

NEW INDUSTRIAL ESTABLISHMENT IN ITALY.

The French Consul-General at Milan reports, according to the "Moniteur Officiel du Commerce," that a new metallurgical establishment has just been set up in the suburbs of Brescia by Messrs. Cremona and Tabladini for the manufacture of:—

1. Iron, steel, copper, bronze, and nickel locksmith's work—such as locks, bolts, hinges, and various articles for the fittings of furniture, doors, and window sashes.

2. Household and domestic articles.

Italy had been hitherto dependent on other countries for these various goods, especially Germany, which has taken the place of France since 1887.

The new establishment appears to have machinery, capital, and a staff which will enable it to gradually reduce the German importations of the above articles and retail them itself to the public at a saving of 25 per cent.

ANTHRACITE IN ROUMANIA.

The Belgian Consul at Craïova writes to the "Bulletin Commercial" stating that recent mining explorations in the district of Gorgin (Roumania) have led to the discovery of an extensive bed of anthracite apparently of good quality. A syndicate, composed of English capitalists, has been formed for the working of

this bed on a large scale. Although the undertaking has only lately been set on foot 30 tons of anthracite are daily extracted from the mine.

THE DRAINING OF LAKE COPAIS (GREECE).

The "Bulletin Commercial" states that the Belgian Legation at Athens has issued an interesting report on the draining of Lake Copais, containing a description of the enormous works undertaken by the French "Company for the Draining and Exploitation of Lake Copais," and completed three years ago by the "Lake Copais Company, Limited."

The total surface drained is about 24,000 hectares (hectare = 2.47 acres), of which 7,000 hectares are already cultivated by the peasants of the surrounding villages; 2,500 hectares are provisionally occupied by proprietors of the neighbourhood, 400 hectares serve for experiments in cultivation and form the basis of a large model farm which the Lake Copais Company is creating, and the rest, over 14,000 hectares, is partly covered with reeds and partly serves as pasture. The report also treats of the conditions of leasing the land, the results of the experimental cultivations, the salubrity of the district, &c.

TRADE OF UNITED STATES WITH CUBA.

The "Baltimore Journal of Commerce" states that the values of the principal articles imported into the United States from Cuba in 1895 were:—Bananas, 582,986 dols.; oranges, 72,394 dols.; sugar, 36,544,252 dols.; tobacco, 9,222,977 dols.; mahogany, 987,307 dols.; total, 51,652,125 dols., against 76,413,131 dols. the previous year. As regards exports to Cuba in 1895 the figures were, agricultural implements, 52,708 dols.; books, &c., 21,940 dols.; corn, 100,843 dols.; wheat flour, 962,944 dols.; carriages, &c., 88,338 dols.; coal, 891,208 dols.; cotton goods, 72,060 dols.; fruits and nuts, 60,321 dols.; hardware, 190,042 dols.; iron and steel, 505,638 dols.; sewing machines, 18,035 dols.; leather, 7,107 dols.; naval stores, 16,738 dols.; turpentine, 21,950 dols.; oils, 502,135 dols.; beef products, 35,205 dols.; hog products, 768,676 dols.; lard, 2,036,337 dols.; lumber and manufactures, 651,480 dols.; small items, such as butter, cheese, seeds, cigarettes, &c.: total, 9,498,654 dols., against 17,186,835 dols. previous year. The United States imported from Spain 3,826,960 dols., and exported to Spain 10,594,474. The effect of the fighting in Cuba can be plainly seen in the reduced business with the island last year.

RAILWAY CONSTRUCTION IN THE UNITED STATES IN 1895.

A recent number of the "Railway Age" gives the details of railway construction in the United States in 1895, from which it

appears that 1,803 miles were added to the mileage of the previous year as against 1,948 added in 1894, and 2,635 added in 1893. Only one of the New England states contributed to the mileage of 1895. The middle Atlantic group added 184 miles on 29 lines, the central northern group built 285 miles on 37 lines, the 10 southern states which are credited to the south Atlantic and gulf and Mississippi valley groups added 497 miles on 54 lines, but were surpassed by the 7 south-western states and territories, which built 524 miles on 28 lines. Only 3 of the 7 north-western states are in the list, giving 55 miles on 10 lines, while in the vast region of the 7 Pacific slope states and territories only 171 miles were added by 12 lines. Texas leads all with 209 miles; Indian territory is second with 151 miles, and Pennsylvania third with 102 miles. No other state shows as much as 100 miles.

As to the prospects for railway building in 1896 it is too early to give a detailed estimate, but it may be said that the outlook is better than it was a year ago. There are between 3,000 and 4,000 miles of proposed roads on which work was in progress in 1895 or for which contracts had been let, and many thousand miles more might be counted representing legitimate undertakings which sooner or later are likely to be carried out. If the general financial situation continues to improve, as now is the common expectation, railway building will receive a decided impulse.

CONSUMPTION OF TEA IN THE UNITED STATES.

According to Calcutta "Capital," since 1872 tea has passed the American customs free. Nevertheless the consumption is stationary. In 1895 it averaged 1.38 lb. per head of the population, considerably less than that reached in 1872.

China still furnishes the bulk of the supply, and, indeed, is increasing her exports. In the year just past she sent nearly 55,000,000 lbs. to the States, Japan 37,000,000 lbs., while India is ignominiously classed with "all other countries in Asia" as furnishing only 867,000 lbs. Some Indian tea, of course, finds its way *via* London, but the imports from that city into America were only 3,622,000 lbs. in 1895; and the figures of the last decade show a decided downward tendency. Prices have fallen steadily during the last 20 years, and are now at a point which appear to defy Indian competition. In 1813 the wholesale price of a pound of average tea imported into the States stood at 37.7 cents. It is now 13.5 cents, 1.6 cents less than in 1894.

TRADE AND NAVIGATION OF CYPRUS IN 1895.

According to official returns published in the "Cyprus Gazette," the value of the merchandise imported into the island

in the 12 months ended 31st December 1895 amounted to 242,068*l.*, the principal articles of import being cotton piece-goods (30,558*l.*), woollen manufactures (16,931*l.*), cotton yarn (15,538*l.*), tobacco, raw and manufactured (17,593*l.*), timber (13,430*l.*), and leather, unmanufactured (11,171*l.*).

With regard to the exports for the same period, the total value amounted to 274,608*l.*, the chief articles being carobs (61,067*l.*), barley (29,600*l.*), wheat (28,653*l.*), wine (23,822*l.*), and live animals (16,132*l.*).

During the same year 1,079 vessels, of a tonnage of 298,830 tons, of which 219 were steam vessels representing 246,853 tons, were entered at the six chief ports of the island, and 1,085 vessels, of a tonnage of 299,455 tons, of which 218 were steam vessels representing 245,475 tons, were cleared at the same ports.

The above are exclusive of vessels engaged in the coasting trade of the island.

CANADIAN WOOD-PULP INDUSTRY.

The "Canadian Gazette" states that the Dominion Government has now under consideration a proposal which has often been urged upon Canada for an export duty of 2 *dols.* per cord upon pulp wood, so as to discourage the export of the raw material from Canada, and encourage its manufacture into pulp in Canadian mills. At present the United States tax on imported pulp is 10 per cent., while pulp wood is free. A Canadian export duty would, it is claimed, induce the United States to remove their duty; this the Canadian Association think would give them control of the trade. At present Canada exports about 500,000 cords of pulp wood yearly, so that the proposed export duty would, it was said, yield a revenue of 1,000,000 *dols.* annually. The exporters of pulp wood, of course, object to such an impost. As it is, they have their hands full, they say, to compete with the pulp wood sellers of Virginia and Maine.

On the face of it, Canada is wise to encourage her own manufacturers. Her exports of the raw material, *i.e.*, pulp wood, practically all of which goes to the United States, have been as follows:—

	Dols.			
1890	-	-	-	108,998
1891	-	-	-	219,458
1892	-	-	-	386,092
1893	-	-	-	392,262
1894	-	-	-	468,359

These figures show a constantly increased export of raw material. The pulp wood exported in 1894 would be capable of producing mechanical pulp to the value of 400,000*l.* sterling, or about five times the value of the pulp wood now exported. If converted into chemical pulp the export value would be no less than 700,000*l.* sterling.

As it is, Canada is doing an increasing export trade in pulp as well as in the wood. The exports of pulp stand thus:—

				Dols.
1890	-	-	-	168,180
1891	-	-	-	280,619
1892	-	-	-	355,303
1893	-	-	-	455,893
1894	-	-	-	547,217
1895	-	-	-	590,874

These figures show also a steady annual increase.

The British demand for wood pulp is largely on the increase. The imports for 1895, for example, were of the value of 1,574,400*l*. (297,098 tons), an increase of 150,000*l*. over 1894.

There is a further stage to which Canadian mills should carry this wood-pulp business, namely, to the stage of paper manufacture. The United Kingdom imported last year unprinted paper to the value of 2,046,106*l*., and strawboards, millboards, and wood-pulp boards to the value of 548,254*l*. The exports of paper from the United States were last year of the value of about 500,000*l*. sterling. Canada, with her forest resources and unlimited water-power, should certainly take her place in the business. At present Great Britain looks chiefly to Germany, Holland, Sweden, and Belgium for her imported unprinted paper.

THE NEWFOUNDLAND FISHERIES.

The "Canadian Gazette" states that the Newfoundland Fisheries Board has adopted certain rules and regulations which it intends to propose for confirmation at the next session of the Legislature. These regulations provide among other things with regard to lobsters, that canners must take out an annual licence; that there be a close season from August to April; that spearing and hooking be prohibited; and for annual returns from the canneries. As to herring, it is provided that none be taken for manure, or on Sunday. Cod traps are allowed, subject to a number of limitations. Fish required for scientific or breeding purposes may be taken at any time and by any method.

MINING IN NOVA SCOTIA.

According to the "Monetary Times" the Nova Scotia Department of Mines has some encouraging things to say respecting certain departments of mineral production in that province. For example, the gypsum, or plaster of Paris industry, is described as showing an increase during 1895, though not yet having attained the high figure of a few years ago. The iron trade, though dull in common with other industries nearly all the year—total production 29,090 tons, principally from Londonderry and Ferrona—became more animated at the close, and the outlook is

now encouraging. The yield of gold was 22,112 ounces from 37 mines; and 58,082 tons of quartz was crushed.

The actual production of coal at the Nova Scotia Mines is not given, but the sales show a decline of 188,000 tons, or about 9 per cent. compared with the previous year. Increased sales to New Brunswick, Prince Edward Island, and the West Indies, are shown, but less was sold at home, and less sent to Quebec and the United States.

THE AGRICULTURAL RESOURCES OF BRITISH COLUMBIA.

According to an article in a recent number of the "Canadian Gazette" the agricultural resources of British Columbia are somewhat limited, yet there are many millions of acres still open for pre-emption. It will be admitted that a country where farming lands are limited, so long as there is enough of them, must be a good country to make a home in. The immense mineral and other natural resources of the province are now attracting men by thousands within its borders. This means a market for all that the agriculturists can produce. The land that is open for pre-emption is not the most convenient to market centres, or just as desirable as it might be in every respect, but for those with small means they are obliged to start in a small way. In every section of the province, however, there are farms of 160 acres, or even from 20 acres upwards, that can be purchased at prices varying from 8 dols. per acre. Scores of people have taken up land, not for a home, but being on the ground in time they saw a chance of making a snug sum by doing the required improvements, and obtaining a Crown grant to the land. There are thousands in British Columbia thus situated, the majority of them single men, who are willing to sell at a low figure.

To those who have some capital there is no better chance anywhere than in British Columbia's mountain valleys. In many cases men who came there without any capital whatever are now well-to-do farmers. Settlers are coming in every day, and going into good positions the next. British Columbia can show the most remarkable yields in grain and field produce generally that can be produced in the world, whilst the size that fruit attains can scarcely be believed unless seen with one's own eyes. It does not cost a great deal to start farming and fruit growing in British Columbia. Good work horses can be purchased from 40 dols. upwards; waggon, about 75 dols.; harness, 30 dols.; plough, 20 dols.; and other agricultural implements in proportion. A little capital is an absolute necessity for anyone thinking of embarking in farming. Less than 500 dols. would not be desirable, nor safe to start in with; farmers who have applied themselves to their avocation are to-day in comfortable circumstances.

THE ROUTE OF THE UGANDA RAILWAY.

The "Times of India," in a notice on the protective force which left India in March for Mombassa in connection with the construction of the Uganda Railway, states that it is believed that there will be a considerable amount of fighting, especially when the railway construction works have been pushed on towards the uplands, where the slave traders may give some trouble. For the first 140 miles of the new track it is not likely that many Arabs will be encountered, and the country is a tolerably easy one. The last 58 or 60 miles of this march are across the waterless desert of Taro, and the next 50 miles, from Tzaro to Kibwesi, is part of the dreaded tsetse flybelt. The people here are known as the Wakamba, and will give no trouble—a fortunate circumstance in view of the fact that all the country is covered with mimosa bush, in which a hostile army could easily ambush. So far and for 40 miles beyond this point the railway will follow the Caravan route. Avoiding the trading station of Machako it will branch slightly southwards across prairie land, and rejoin the Caravan route at a point near Kikuyu, passing in the meantime through the country of the Masais, with whom there will probably be a sharp encounter. The route so far as Kikuyu is a very gradual rise to a height of about 6,000 feet. Then there is a further rise of 600 in 10 miles, and then another 50 miles of easy prairie country till Eldoma is reached, about 470 miles from the coast. Here there is a ravine some 300 feet across, and about the same distance deep. Beyond the ravine the route rises again and traverses the forest of Subuyu, the home of the Wadurobo tribe of elephant-hunting savages. These are a very wild race, and may give trouble, but no serious difficulty is expected. The rest of the country is healthy and for the most part anything but difficult from an engineering point of view; so that excluding 60 miles of desert, very trying in the hot weather, and 50 miles of the tsetse fly country, the expedition has nothing to dread in the way of climatic influences. Messrs. Grindlay, Groom and Company, it may be mentioned, have been appointed by Government as the Agents in India of the new railway.

REVENUE RETURNS OF CAPE COLONY.

A communication, dated April 9 last, has been received from the Agent-General for the Cape of Good Hope, enclosing a copy of the "Cape of Good Hope Government Gazette," containing comparative statements, from which it appears that the revenue received in the month of February last was 557,379*l.*, showing a net increase of 178,397*l.* on the corresponding amount for 1895. For the eight months, July—February 1895–6, it amounted to 4,398,163*l.*, showing a net increase of 971,989*l.* on the amount for the corresponding period of 1894–95.

TRADE OF THE CAPE.

A communication, dated April 9 last, has been received from the Agent-General for the Cape of Good Hope, enclosing extracts from the "Cape of Good Hope Government Gazette," of March 10 last, from which it appears that the value of the imports into the Colony for the two months ended February 29, 1896, including specie, amounted to 3,599,896*l.*, as compared with 2,634,934*l.* for the corresponding period of 1895.

With regard to the exports, the total value for the two months ended February 29, 1896, including specie, was 2,494,291*l.*, as compared with 2,468,121*l.* for a like period in 1895.

RAILWAY TRAFFIC EARNINGS OF CAPE COLONY.

The "Cape of Good Hope Gazette" for the 13th March last publishes a statement from which it appears that the traffic earnings on Cape Government railways for the month of January last were 295,368*l.*, as compared with 231,360*l.* in January 1895.

PROJECTED LIGHT RAILWAY IN BRITISH NORTH BORNEO.

The "London and China Telegraph" states that, according to the "British North Borneo Herald," Mr. Henry Walker, commissioner of land in British North Borneo, was recently instructed to make an estimate of the cost of running a light railway across the company's territory from Sipitong, in Brunei Bay, to Tenom, a distance of 52 miles, whence there is river carriage to Sandakan on the east coast. He has given the cost at a little over 1,000*l.* per mile. The line is to cross the Padas river and run down the Penotal gorge. It is hoped that the North Borneo Company will be able to find the money to carry out the work, which will open up a large district suitable for planting and containing much produce.

TRADE OF AUSTRALASIA IN 1895.

The Melbourne "Journal of Commerce" publishes the following tabular statement concerning the trade of certain Australasian Colonies for the year 1895 as compared with 1894 :—

Colony.	Imports.		Exports.	
	1894.	1895.	1894.	1895.
	£	£	£	£
Victoria - - -	12,470,599	12,472,344	14,026,546	14,547,732
New South Wales - -	15,801,641	15,992,415	20,577,873	21,934,785
Queensland - - -	4,264,635	5,334,273	8,402,646	8,714,701
South Australia - -	6,226,699	5,585,599	7,301,774	7,177,038
Tasmania - - -	979,676	1,094,456	1,489,041	1,373,063
New Zealand - - -	6,788,863	6,399,722	9,239,277	8,556,505
Total - - -	46,532,113	46,878,809	61,037,157	62,303,824

The aggregates show an increase of 346,705*l.* in imports, and an increase of 1,266,667*l.* in exports. It must be borne in mind that the returns include intercolonial trade, which may roughly be said to be close upon 50 per cent. The excess in imports, however, may be regarded with satisfaction.

PRODUCTION OF GOLD IN AUSTRALASIA IN 1895.

The "Australasian Trade Review" states that, according to latest computations, the gold yield of the Australasian colonies last year was 2,350,562 ozs., as against 2,243,634 ozs. in 1894, showing an increase of 106,928 ozs. The increase was shared by five out of the seven colonies, Queensland and Tasmania being the only members of the group which showed decreases. The following table gives the totals as nearly as they can be at present ascertained, with increases and decreases:—

Colony.	1894.	1895.	+ Increase. — Decrease.
	Ozs.	Ozs.	Ozs.
Victoria* - - -	716,955	740,086	+ 23,131
New South Wales - - -	324,787	360,165	+ 35,378
Queensland - - -	679,511	623,000	- 56,511
South Australia† - - -	35,844	47,343	+ 11,499
Tasmania - - -	57,873	54,964	- 2,909
Western Australia - - -	207,131	231,513	+ 24,382
New Zealand - - -	221,533	293,491	+ 71,958
Total - - -	2,243,634	2,350,562	—
Net increase - - -	—	—	106,928

* Mining Department's Estimates.

† Received at Sydney and Melbourne Mints.

RAILROAD CONSTRUCTION IN AUSTRALIA.

The "Engineering and Mining Journal" in an article on railroad construction in Australia states that in West Australia railroad construction has been ingeniously reduced to a very low figure by the contractors. Last year tenders were invited for the construction of a railway from Mullewa to Cue, to open up the Murchison and Yalgoo coal-fields. The railway was to be about 196 miles in length, and in the middle of December the contract was awarded at 84,535*l.* (about 431*l.* per mile), and this is not the first experiment in West Australia at constructing railways at almost incredibly low prices. Some months ago a firm undertook to construct a railway from Southern Cross, the then terminus, to Coolgardie, a distance of 115 miles, and the contract worked out at 560*l.* per mile, including station buildings, goods sheds, and, indeed, everything except rails and the fastenings. This was 310*l.* per mile under the lowest price on record, and now the Cue contract has been placed at 129*l.* per mile less.

How can contractors build railways fit to travel upon at such a price? It is managed in this way. The railway toward Coolgardie was opened in January to Woolgough, roughly speaking, 80 miles beyond Southern Cross and 40 miles from Coolgardie. The contractors will hand over the first half of the line at the end of the month, but they will operate the remaining 60 miles on their own account until next September. This is the whole explanation. Trains are now running to the outskirts of Coolgardie, and the contractors will have nine months' traffic receipts to set off against the absolute loss they sustain on the construction of the line. A correspondent writing from Perth, the capital of West Australia, states that it would have caused no surprise had the lowest tender for the Cue contract been 200,000*l*. The actual tender at 84,535*l*. is obviously based on very large receipts from traffic during construction. At the present time the business on the Cue road is much smaller than on the Coolgardie, though every month it is increasing.

The low cost of construction has rendered it a comparatively simple matter to make the railways of West Australia pay a high amount of interest on the capital invested. It was not always thus, for in 1889, when 188 miles were in operation, the gross earnings were 6,500*l*. less than the working expenses. In succeeding years a gradually increasing balance of revenue was shown, until for the financial year ending June 30th last the return was 5·44 per cent., a much greater return than is yielded by the railways in other parts of Australasia. The average cost of construction of West Australian railways is 3,804*l*. per mile. The cost of the railways of the other colonies are:—Queensland, 6,902*l*.; South Australia, 7,297*l*.; New Zealand, 7,771*l*.; Tasmania, 8,382*l*.; Victoria, 12,570*l*.; New South Wales, 14,335*l*. As regards the percentage of working expenses to receipts, the figures, 61·50, show that there is room for economy. New South Wales, with a percentage of 56·58, stands the best of all the colonies with respect to this. Tasmania makes a most miserable showing as regards her railways. She swallows up 85·02 per cent. of her gross receipts in working expenses, and her return upon railways is 0·61 per cent.

TRADE AND INDUSTRY IN THE NORTHERN TERRITORY (AUSTRALIA).

The "Adelaide Observer," in a recent notice on the trade and industry of the Northern Territory of Australia for 1895 states that, according to the "Northern Territory Times," the total staple exports on the whole show a falling off in value of 38,674*l*., more than two-thirds of which is accounted for by the shrinkage in the cattle trade. Revenue has increased by 424*l*., imports show the insignificant decrease of 3,787*l*., and on total exports there is a decline of 51,315*l*. With regard to population considerations it is shown by the returns that during the year 453

persons arrived and 356 departed against 223 arrivals and 410 departures for 1894.

An industry which has progressed vigorously is that of fishing for pearl shell in the waters adjacent to Port Darwin. Including the nine luggers and schooner of the Pearling and Trading Company, upwards of 20 boats were occupied on the shelling grounds, chiefly raising shell from the known beds. The result was a total take of $172\frac{3}{4}$ tons, valued at 12,985*l.*, as against 111 tons, valued at 8,618*l.* for 1894, the difference averaging about 5 tons per month. Of course the above valuation does not include the pearls found, some of which were of considerable worth. The prospects of this industrial pursuit are in the highest degree cheering.

The principal returns affecting the pastoral industry have reference to the export of cattle, horses, sheep, wool, hides, and horns. These in the aggregate have an export value of 51,038*l.*, of which the trade in live cattle was responsible for 39,190*l.* There was a very marked falling off in the operations of 1895 compared with the previous year, the figures being—for 1894, 28,607 head, valued at 66,559*l.*; for 1895, 13,896 head, valued at 39,190*l.* A monetary deficit of 27,369*l.* is here shown, together with the decrease of 14,711 in the number of cattle sent out of the colony. As to horses, the number exported in 1895 was 1,230, against 1,424 in 1894, the decrease in stated value being 3,047*l.* Wool remains much about the same as in the previous year, and the other items, such as sheep, hides, and horns, compare favourably with the records of 1894.

Turning now to the mining industry, the Customs returns indicate rather a downward trend. The export of gold has unfortunately not been well maintained, but the deficiency may safely be attributed to the suspension of work which was brought about late in the year through the negotiations for the sale of six producing claims. During 1895 the colony exported 29,581 oz. of gold, of a value 102,734*l.*, whilst for the previous year the output was 31,543½ ozs., valued at 109,392*l.*, the comparison show a falling off in quantity of 1,962 oz. and in value of 6,658*l.* The most notable discovery was the finding of the new goldfield, Wandii, which, though promising well, is not sufficiently long established to enable an accurate opinion to be formed as to its probable future. Another important development is visible at Brock's Creek, where an English proprietary has been for some months past laying the foundation for a going concern. In the south the mica fields have produced mica less by a valuation of 2,169*l.* than in 1894, being a decline of nearly half the trade. Copper also shows a heavy drop, but the export of tin improved by a value of 664*l.*

In agricultural matters there is no particularly noticeable sign of progress. The products grown are entirely for local consumption, and are the result of gardeners' operations only.

POPULATION OF VICTORIA IN 1895.

According to the official report on the movement of population in the colony of Victoria, issued from the office of the Government Statist, and published in the "Australian Trade Review," the following statement shows the estimated population of Victoria on 31st December 1895:—

1895.	Males.	Females.	Total.
Births - - - -	17,408	16,311	33,719
Arrivals by sea - - - -	55,481	25,718	81,199
Total increase - - -	72,889	42,029	114,918
Deaths - - - -	8,905	6,726	15,631
Departure by sea - - - -	60,061	28,825	88,886
Unrecorded departures (estimated)	6,007	1,728	7,735
Total decrease - - -	74,973	37,279	112,252
Increase (+) or decrease (-) during the year - - -	- 2,084	+ 4,750	+ 2,666
Estimated population on 31st December 1895 - - - -	605,176	576,593	1,181,769

During the year 1895 the births registered numbered 33,719, or 539 fewer, and the deaths recorded 15,631, or 201 more, than in the previous year. The excess of births over deaths was thus 18,088, viz., 8,503 of males and 9,585 of females, as compared with 18,828 in 1894.

The recorded arrivals by sea numbered 81,199, but, as the recorded departures by sea numbered 88,886, there was an apparent loss by net emigration of 7,687, of which 4,580 was of males, and 3,107 of females. Allowing for unrecorded departures by sea, however, the estimated total loss by emigration is believed to have been 15,422. This is greater than in 1894 by 1,691, than in 1893 by 2,011, or than in 1892 by 3,137; the total loss in the four years having amounted to 54,849.

THE POPULATION OF NEW ZEALAND.

According to the "New Zealand Trade Review," the population of New Zealand on the 31st December 1895, is estimated at 698,706, inclusive of 4,150 Chinese, but exclusive of Maoris. As the births and deaths of the Maoris are not registered, no estimate of their numbers is obtainable except on the taking of a census. The census of 1891 gave their number as 41,993.

XVI.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Blackburn.—The usual monthly meeting of the Chamber was held on April 29 last, Mr. H. Harrison in the chair.

The secretary read a letter from the Foreign Office, in reply to the Chamber's request that all woven fabrics sent by Consuls to the Foreign Office should be forwarded to Blackburn, to be examined by the members of the Chamber and the trade, and in this the Marquis of Salisbury stated that they received very few of such samples, but the authorities hoped that, with the assistance of the Associated Chambers of Commerce, arrangements would be made for the receipt and proper distribution, for exhibition, of more of these samples.

In reporting upon the Commercial Mission to China, the president said that throughout the country it was being regarded with great interest and generally approved. They had decided to send two experts, and were awaiting the arrival in London of a gentleman who, if he would accept the post, would make an admirable head for the mission.

It was also announced that the Darwen Weavers' and Winders' Association had decided to make a grant of 20*l.* a year for three years towards the cost of the mission, and the Bury, Radcliffe and District Weavers' Association 10*l.* a year for three years, for the same object.

Railway rates and the Indian import duties were also referred to, and the president delivered an address on the state of trade.

Bradford.—A meeting of the Council of the Bradford Chamber of Commerce was held on April 29 last, Mr. T. A. Duncan in the chair.

The Secretary made a statement to the Council summarising the effect of correspondence with the Postmaster-General on the subject of parcel-postage rates to foreign countries. He said he was informed that it was found more advantageous to send parcels from England to Germany for the purpose of being sent from Germany to other foreign countries by German post, instead of sending them direct from England to their destination.

The President moved the reference of a letter from the Halifax Chamber of Commerce, enclosing a resolution on the subject of the presence of tar in wool, to the Wool Supply Committee. He also suggested that the question of the presence of lumps of dirt and tags in home-grown wool should be referred to the same committee, remarking that there was great carelessness in this matter on the part of farmers, in some districts especially. He thought many of them must be unaware that it was illegal to wind wool in that condition.

Mr. G. Hoffman stated that, in accordance with a resolution adopted at the last annual meeting of the Associated Chambers of Commerce, a letter had been sent to Lord Salisbury asking him to instruct British Consular officers abroad to send home

samples of goods from places where they were resident, with particulars of interest to British merchants and manufacturers. In reply Lord Salisbury had pointed out that the question of cost had a good deal to do with the possibility or impossibility of carrying it out, and asked for a more specific statement as to what would interest various chambers of commerce. He suggested that each chamber should consider carefully what samples might really be of great interest to it.

The matter was referred to the Tariff Committee.

Other matters discussed were The Education Bill and Half-Time Employment, Horseless Carriages, and The Finances of the Chamber.

Bristol.—The monthly meeting of the Council of the Bristol Chamber of Commerce was held on the 29th April last. The chief business transacted referred to the election of a new President and of the various committees of the Chamber (Docks and Shipping, Railways, Finance, Parliamentary, &c.).

The following resolution was carried with regard to the accommodation for coasters:—"That a letter be addressed to the Docks Committee by the council of this Chamber, urging that greater facilities should be afforded for extending the shed accommodation at this port for the convenience of coasting vessels regularly trading to and from Bristol, in order that the important trades affected may not be diverted to other competing ports."

Dundee.—The monthly meeting of the directors of the Dundee Chamber was held on 7th May last, Mr. John Ogilvy in the chair.

It was intimated that the deferred Congress of Chambers of Commerce for the Empire would hold its sittings in London in the week beginning 8th June. The Dundee Chamber had accepted an invitation to be present, and would be represented by the President and other members. A circular had been received with reference to the subject of freights to South Africa, and also a form of charter and bill of lading for the coal trade. The latter was remitted to the Shipping Committee. It was agreed to support the Manchester Manufacturers' Association in their opposition to the Truck Bill now before Parliament, it being stated that the application of the measure to the textile factories was uncalled for, and would be perfectly unworkable. The Secretary was instructed to communicate with the local members of Parliament, and ask them to oppose the Bill.

The reply of the Indian Government to resolutions forwarded by the Chamber at the beginning of the year with reference to the working hours in India was read.

Halifax.—A monthly meeting of the Halifax Chamber of Commerce was held on the 29th April last, Mr. Sutcliffe presiding.

The General Purposes Committee reported in favour of the Light Railways Bill.—Mr. James Booth, moving the adoption of the report, said, however, that a very large amount had to be expended in order to obtain the line of railway up to the upper part of

Halifax, although it was only $3\frac{1}{2}$ miles in length, and was unopposed, and was, of course, for the benefit of the public. In the memorial they should suggest also that the benefits of some of the provisions of the bill be extended to industrial districts.

In answer to the circular sent out by the secretary of the Halifax Chamber, calling attention to the practice of marking sheep with tar, a communication had been received from the Colonial Office stating that Mr. Chamberlain's attention had been called to the drawbacks occasioned to spinners and manufacturers owing to the use of tar and similar substances in the marking of sheep: and a copy of the letter from the secretary of the Halifax Chamber would be communicated to the Governors of the Australian and South African colonies and the Falkland Islands, and to the Governor-General of Canada and the High Commissioner of Cyprus.

Leeds.—The monthly meeting of the Council of the Leeds Chamber of Commerce was held on April 29 last, Mr. J. Peate in the chair.

At the meeting of the Associated Chambers in London last month a resolution in favour of the metric system was agreed to, on the initiative of the Leeds Chamber, and the Secretary now read a memorial upon the subject which he had prepared, and which would be presented by the Associated Chambers to the Government. The memorial was adopted. Another resolution adopted at the same meeting, on the motion of the Leeds Chamber, had reference to a trade route from Burmah to China. On this subject it is intended that a deputation should wait upon the Government, and the President, ex-President (Mr. Slater), Mr. J. W. Williams, and Mr. Holt Hallett were selected to represent the Chamber on the deputation. Some conversation took place with regard to the relation of British Consuls to British trade in foreign countries, and delegates were appointed to attend the meeting of Chambers of Commerce of the Empire, to be held in London in June.

Liverpool.—A meeting of the African Trade Section of the Liverpool Chamber of Commerce was recently held under the presidency of Mr. Ellis Edwards.

Several letters from the Right Hon. J. Chamberlain, Her Majesty's Secretary for the Colonies, were read, having reference to the transit duties and drawbacks on spirits at Lagos, the affairs of Ashanti, the Lagos Bar, the mortality on the west coast, and the water supply on the Gold Coast and at Lagos.

With reference to the mortality on the Gold Coast, Mr. Chamberlain writes: "I have noticed with great regret that the mortality among the European officials has again been exceptionally heavy during the last three months, and that whilst the deaths last year occurred on the windward coast, the mortality is now greatest on the leeward coast, so that no portion of the colony appears to enjoy immunity in this respect. I fear that there is no possibility of finding a healthier spot than Accra for the headquarters of the Government, even if it were now possible to make another change; and that the persons

" employed on public works (among whom the mortality appears
 " to be highest) must always be exposed to more risk of fever
 " than other persons. But I request that you will consider
 " whether anything can be done to improve the conditions of
 " life on the coast, and that you will furnish me with a report on
 " the question. Ashanti and the Gold Coast are likely to be the
 " scene of extended trade and operations for railway and other
 " works, and I am anxious that nothing should be left undone to
 " lessen the mortality among those engaged."

With regard to Lagos railways, a letter from the Manchester Chamber of Commerce, referring to the recent conference between representatives of the African sections of both Chambers, was considered by the committee, and it was agreed to recommend that diagram 34, in the report of the survey, approved by the conference, should be carried out. This scheme shows a group of lines which will accommodate Abeokuta and Ibadan, with one terminus at Lagos. The lines are from Lagos to Abeokuta, with a branch from Otta to Ilaio, which branch in the opinion of the committee can be dispensed with for the present. Another section of the line diverges from the main line near Lagos, and goes viâ Shagamo to Ibadan, and as Abeokuta has the advantage of direct water communication with Lagos, it is the opinion of the committee that the line viâ Shagamo to Ibadan should be proceeded with as soon as the section to Otta, now in progress, is constructed.

The annual meeting of the Liverpool Chamber of Commerce was held on April 29 last, Mr. Charles McArthur in the chair.

The President in a long address reviewed the work of the Chamber for the previous 12 months, referring especially to the settlement in Ashanti, the opening up of South-West China, the measures for the relief of agricultural distress, and the reduction in the rates and dues levied by the Mersey Docks and Harbour Board.

The proceedings closed with a vote of thanks to Mr. McArthur on the expiration of the fourth year of his term of office as president.

Manchester.—A quarterly meeting of the Manchester Chamber of Commerce was held on May 4 last, at which the president, Mr. W. H. Holland, delivered a long address reviewing the state of trade during the past three months.

Nottingham.—A meeting of the council of the Nottingham Chamber was held on May 4 last, Mr. George Wigley in the chair.

A letter was read from the Self Propelled Traffic Association, enclosing copies of resolutions which had been passed in regard to this matter. On the motion of Mr. Davey, it was decided to use the influence of the Chamber in the direction of getting the maximum weight increased to four tons. A communication was also received from the Walsall Chamber on the same subject.

The quarterly report, to be presented to a quarterly meeting of the members to be held at the close of the meeting of the council of the Chamber, was then read.

The report was merely a recapitulation of the business transacted by the members of the council at their meetings during the past quarter.

Other matters referred to were the Education Bill and the proposal as to a conditioning house for cotton.

Oldham.—The monthly meeting of the Oldham Chamber of Commerce was held on April 9 last, Mr. A. Emmott in the chair.

A letter was read by the Secretary from the Blackburn Chamber of Commerce, on the subject of the commercial mission to China, a matter which has been previously referred to in the Chamber. The Blackburn Chamber said the matter was of great and far-reaching consequence to Lancashire, and there would be no further delay in organising and despatching the mission. Mr. Emmott said the mission was certainly one of great importance, and he had sent his subscription. Mr. Joseph Lees said that gave him an opportunity of recommending the suggestion made by Mr. Ascroft, M.P., at their annual dinner, that their subscription was rather low and might be increased with advantage. This was a good time to suggest by a circular to all the subscribers of the Chamber and to others that it was desirable to support this movement, and that they should give to that mission the same subscription as they gave to that Chamber for three years. That would realise 500*l.*, or 10 per cent. of the total cost, which, in his opinion, would not be too much for Oldham's share in the work. In the course of the discussion which ensued, Mr. E. Mellor said the members of his society (the Operative Spinners') had the question before them, and would probably do something upon it. It transpired that the same communication as the Chamber had received had been sent to the various companies and firms in the district, and it was decided eventually to circularise all the members of the Chamber, and all the owners of mills who were not members, in respect to the mission, expressing the opinion that they should subscribe at least 5*l.* each.

Other questions referred to were the Companies' Bill, the yarn contracts question, and the Congress of the Chambers of Commerce the Empire.

Sheffield.—A meeting of the Sheffield Chamber of Commerce was held on April 14 last, Mr. S. E. Howell presiding.

A letter was read conveying information as to the proposed adoption of the metric system in Russia, and suggesting that the Chamber should receive a report in reference to the position of the matter in that country.

A letter from the Bengal Chamber of Commerce was also read, pointing out that there had been no substantial reduction in the cost of telegrams between Great Britain and India for many years past. The letter contained statistics showing that with regard to the Australasian colonies a reduction of from 9*s.* 4*d.* to 4*s.* 9*d.* had resulted in practically doubling the number of words telegraphed, and raising the value of the traffic in similar proportion. It was also pointed out that in connection with

telegraphic communication with South Africa, a reduction of from 8s. 9d. to 5s. a word had come into force. The matter having been considered, the secretary was instructed to write to the Bengal Chamber, making inquiries as to the authority of the statistics quoted, and inviting suggestions as to how the Sheffield Chamber might best assist the Bengal Chamber in their attempt to reduce the telegraphic rates in question.

The Chamber discussed a number of other matters of interest, including the question of the attitude of the French Government towards commercial treaties between Madagascar and this country, and the United States.

Walsall.—A meeting of the Council of the Walsall and District Chamber of Commerce was held on the 27th April last, Mr. E. J. Shaw, president, in the chair.

The President remarked that there was at present sitting a Select Committee upon the use of petroleum lamps. Witnesses were being heard from all parts of the kingdom, but they were mostly professional men—chemists and captains of fire brigades and others not closely connected with the manufacture of lamps. To much of this evidence manufacturers were taking strong exception as affording example of how a large industry might be very injuriously affected by such testimony. Propositions of the most drastic kind were put forward, and it was even suggested that a Secretary of State should have the power of drawing up a code regulating the manufacture of lamps during any one year from the 1st January, though for the next year he might totally change his pattern, and thus manufacturers would have on their hands all the appliances and plant they had provided. Personally he thought that very much more was made of lamp accidents than should be, and that newspapers boomed every mishap in a mysterious way. He thought it was time that something should be said in favour of lamps. In Walsall there were 10,000 people burning them, and yet there were practically no lamp accidents. He had written on the point to other towns, and found that in many, such as Congleton and Lichfield, there had been absolutely no lamp accidents whatever during the last 12 months. He complained that one official of the London County Council should have gone out of his way to recommend a particular lamp, whereas there were millions of lamps just as cheap and safe, being turned out every day. He hoped the Chamber would take advantage of the *locus standi* so accredited to them before the Select Committee, and assist the manufacturers in giving evidence on a matter affecting the industry of 10,000 or 15,000 people. He added that the evidence of Superintendent Tozer, of Birmingham, was very straightforward, and to the effect that in that city, with a population of 400,000, there were last year only 16 lamp accidents, and many of these were due to drunkenness and violence.

Other subjects discussed were the commercial examinations, the Leicester Trade Union Circular, and the Locomotives on Highways Bill.

XVII.—RECENT TRADE BLUE BOOKS.

1. *Statement of the Trade of British India with British Possessions and Foreign Countries for the Five Years 1890-91 to 1894-95. (C.—7997.) Price 2s. 9d.*

This is an annual statement issued by the India Office showing the Trade, Shipping, Customs Revenue, &c., of the Indian Empire in a comparative form for a period of five years. The subjoined table gives the foreign sea-borne trade of the country in each year from 1890-91 to 1894-95 inclusive, the values being given in “tens of rupees,” a denomination which, as is stated in the introduction to the volume, while it is not subject to the charge of inaccuracy attaching to that of the “£ sterling,” calculated at the nominal exchange of 2s. to the rupee, nevertheless enables comparisons to be made with figures in any past volumes in which that expression of value was employed. The sign “RX” is used to signify tens of rupees.

Total trade of British India with other countries by sea, during the past five years :—

	Tens of Rupees.				
	1890-91.	1891-92.	1892-93.	1893-94.	1894-95.
Private Imports :					
Merchandise - - -	69,034,900	66,587,457	62,605,030	73,956,957	70,167,438
Treasure - - -	21,919,486	14,722,662	17,009,810	18,425,256	9,559,007
	90,954,386	81,310,119	79,614,840	92,382,213	79,726,445
Private Exports :					
Merchandise - - -	100,135,722	108,036,010	106,535,997	106,447,590	108,814,999
Treasure - - -	2,071,906	3,143,186	6,928,994	4,024,737	8,158,017
	102,207,628	111,179,196	113,464,991	110,472,327	116,973,016
Government Transactions :					
Imports { Stores - - -	2,940,470	2,844,926	3,660,247	3,064,475	3,361,555
{ Treasure - - -	15,000	—	—	36,000	22,200
Exports { Stores - - -	91,626	137,582	59,479	55,779	98,779
{ Treasure - - -	51,272	143,500	29,929	75,455	68,055
	3,093,368	3,126,008	3,749,655	3,231,709	3,550,589
Total Trade - - -	196,260,382	193,615,323	196,829,436	206,086,249	200,250,050
Excess Exports of Merchandise (including Government Stores)	28,251,978	38,741,209	40,330,199	29,481,937	35,384,785
Excess Imports of Treasure -	19,811,308	11,435,976	10,050,887	14,361,064	1,355,135
Net Excess Exports of Total Trade - - -	8,440,670	27,305,233	30,279,312	15,120,873	34,029,650

In the general summary it is stated that the import tariff imposed on the 10th March 1894 was extended on the 27th December of the same year by the inclusion in the schedule of taxable articles of cotton goods and yarns, which had been exempted when the tariff was framed early in the year, and certain other articles. Whatever restriction the tariff imposed upon trade was sensibly increased by the low rates of exchange ruling throughout the year, and the constant fluctuation in them. The highest rate quoted for bank bills on demand in Calcutta was 1s. $1\frac{1}{16}d.$, and the lowest 1s. $0\frac{1}{8}\frac{1}{2}d.$, the difference being 10·18 per cent. The restriction of the tariff was, however, not material, for cotton goods and yarns were not affected until the beginning of the last quarter of the year, and for some time before that the imports of these goods had been unusually active in anticipation of their inclusion with the other imports subject to import duty. The decline in other goods was rather the result of the reaction from the extremely and abnormally large imports of the preceding year than of the operation of the tariff.

The features of the import trade are a decline (of about 5 per cent.) in merchandise, and a very much greater decline in gold and silver. The import trade generally was reduced by Rx 12,655,768, or 13·7 per cent. In merchandise the reduction amounted to Rx. 3,789,519, in gold to Rx. 1,390,250, and in silver to Rx. 7,476,000.

The export trade generally increased by Rx. 6,500,689, or about 5·6 per cent., the increase being largest in gold, which left India in quite abnormal quantity; this export accounted for Rx. 4,225,090, being two-thirds of the total increase. In Indian merchandise the increase amounted to Rx. 1,748,849, or 1·7 per cent. There was a large increase in the re-export of imported goods, and in silver there was a small decline.

The trade was about 3 per cent. smaller than in 1893-94, but it was larger than in any of the years preceding that year.

In 1893-94, in consequence of the suspension of the sale of Council bills for some months, the exports exceeded the imports by only about Rx. 18,000,000; in 1894-95 the exports exceeded the imports by the very large sum of Rx. 37,250,000.

2. Railway Accidents. Returns of Accidents and Casualties as reported to the Board of Trade by the several Railway Companies in the United Kingdom, during the Year ending 31st December 1895, in pursuance of the Regulation of Railways Act (1871), 34 & 35 Vict. cap. 78; together with reports of the Inspecting Officers of the Railway Department to the Board of Trade upon certain Accidents which were inquired into, (C.—8007.) Price 1s. 10d.

This is an annual statement issued by the Railway Department of the Board of Trade. It shows that during the year ended the 31st December 1895 accidents to trains, rolling-stock, permanent-way, &c., caused the death of 17 persons and injury to 487 persons, viz. :—

	Total for the Year 1895.		Total for the Year 1894.	
	Killed.	Injured.	Killed.	Injured.
Passengers and others - -	5	399	16	347
Servants of companies - -	12	88	6	62
Total - -	17	487	22	409

During the 12 months there were reported 26 collisions between passenger trains or parts of passenger trains, by which 1 passenger was killed and 123 passengers and 3 servants were injured; 46 collisions between passenger trains and goods or mineral trains, &c., by which 2 servants were killed and 125 passengers and 29 servants were injured; 16 collisions between goods trains or parts of goods trains, by which 2 servants were killed and 28 servants were injured; 49 cases of passenger trains or parts of passenger trains leaving the rails, by which 2 passengers and 2 servants were killed and 42 passengers and 10 servants were injured; 12 cases of goods trains or parts of goods trains leaving the rails, by which 3 servants and a person improperly riding on a mineral waggon were killed and 8 servants were injured; 2 cases of trains or engines travelling in the wrong direction through points, by which 1 passenger was injured; 15 cases of trains running into stations or sidings at too high a speed, by which 91 passengers and 1 servant were injured; 148 cases of trains running over cattle* or other obstructions on the line, by which 1 person was killed and 7 persons were injured; 39 cases of trains running through gates at level crossings, by which 1 servant was injured; 1 case of the bursting of boilers or tubes, &c., of engines, by which 1 servant was injured; 5 cases of the failure of machinery, springs, &c. of engines, by which 2 servants were killed and 1 was injured; 188 failures of axles, by which 2 passengers were injured; 8 failures of couplings, by which 1 servant was killed and 5 passengers and 2 servants were injured; 1 failure of rope used in working inclines, by which 1 servant was injured; 5 failures of tunnels, &c., by which 3 servants were injured; and 5 other accidents, by which 3 passengers were injured.

3. *Railways (Under Bridges).* Copy of Circular from the Board of Trade to Railway Companies of the United Kingdom on the subject of Bridges under their Lines of Railway, and correspondence ensuing thereon. (C.—7961.) Price 1s. 2d.

The present paper contains the replies which have been received from the various railway companies to circulars issued

* During the twelve months, 39 horses, 37 beasts and cows, 7 donkeys, 65 sheep, and 3 pigs were run over and killed, and 1 horse, 4 beasts, and 9 sheep were injured.

by the Board of Trade under date of the 6th of November 1885 and the 14th of July 1891, requiring information to be furnished to them with regard to the condition of railway bridges constructed of iron on each line. The following are the circulars in question.

Board of Trade (Railway Department),
London, S.W., 6th November 1885.

Sir,

I am directed by the Board of Trade to inform you that their attention has been called to the subject of railway bridges constructed of iron.

It appears to them that lapse of time and possible deterioration, changes in the weight and number of trains, increased experience concerning the stresses which the metal will bear with safety, are circumstances which point to the necessity for careful and renewed attention.

Under these circumstances, the Board of Trade would be glad to learn what steps the Company are taking on this subject and whether they have any information or suggestions which they can place at the disposal of this department.

I am, &c.

The Secretary of the (Signed) HENRY G. CALCRAFT.
Railway Company.

Board of Trade (Railway Department),
London, S.W., 14 July 1891.

Sir,

With reference to the circular letter of 6th November 1885, R. 4468, on the subject of railway bridges constructed of iron, and in view of the recent accident due to the giving way of a girder under bridge at Norwood Junction on the London, Brighton, and South Coast Railway, I am directed by the Board of Trade to request that you will move the directors of the company to supply at the earliest date practicable, a return showing the number and nature of the bridges which the Railway Company have renewed or strengthened since the issue of that circular, and also the total number of under-bridges upon their line in which the superstructure is composed of :—

- (a.) Cast iron (distinguishing arched from flat girders).
- (b.) Wrought iron.
- (c.) Steel.
- (d.) Timber.

I am further to request that a return may be furnished on the accompanying form on all such bridges, if any, which, taking into account the increase in the weight of engines during recent years and the existing requirements of the Board of Trade, the company *after having caused special examination to be made*, are advised should be strengthened or renewed.

I am, &c.,

The Secretary of the (Signed) HENRY G. CALCRAFT.
Railway Company.

R. 6061/1891.

Railway Company.

Particulars of Girder Bridges requiring Strengthening or Repairing.

No.	Between what Stations.	Date of Construction.	Span.		Description of Girder.	Date of last Inspection.	General Condition of Bridge.	Remarks.
			Square.	Skew.				

4. *Return. Life Assurance Companies.* 118. Price 3s.

This return contains Statement of Account and of Life Assurance and Annuity Business, and Abstracts of Actuarial Reports deposited with the Board of Trade in pursuance of "The Life Assurance Companies Act, 1870," during the year 1895.

The total number of companies appearing in the summary to the present return is 92, of whom 81 have been classed as ordinary, 7 as industrial, and 4 appear in both classes, the returns of these companies showing the ordinary and industrial business separately.

During the year three names have been removed from the official list of companies, viz., London Amicable, Limited; West of England; and Whittington; their business having been transferred.

5. *Memorandum on the Law relating to Truck, with Appendix of Statutes and Decided Cases, and on the Checkweighing Clauses in the Coal Mines Regulation Acts, with Appendix of Decided Cases.* (C.—8048.) Price 10½d.

In the introduction to the present paper it is stated that the following memorandum upon the Truck Acts and upon the checkweighing clauses of the Coal Mines Acts has been drawn up by the direction of the Secretary of State for the Home Department. That Department state that it gives a fairly correct view of the principal provisions of the Acts and of the more important decisions of the courts of law; but the subjects with which it deals are, it is stated, of considerable difficulty, and it is not to be taken as necessarily representing the settled opinions of the Secretary of State or of his advisers, and it is liable to such modifications as further consideration may render necessary from time to time.

The Acts directed against truck are amongst the oldest which have been passed with respect to the regulation of labour. They date from the 15th century.

Until 1831 such Acts were invariably directed against the part payment of wages in goods.

But in 1831 all prior Acts were repealed, and a new Truck Act passed, which has given rise to no small difficulty as to its construction.

It is stated that, as far as the Department are aware, no detailed report exists as to the discussion which took place in Parliament respecting this Act. It appears to have been introduced by Mr. Lyttleton, in Lord Grey's Ministry, when Lord Melbourne was Home Secretary. Mr. Hume denounced it as being a yielding to "senseless clamour," and Colonel Torrens said that, as the working classes desired it, he would support it, though he thought it opposed to strict principles.

The provisions of the Act as amended by the Act of 1887 may be briefly summarised as follows :—

1. In all contracts for hiring, wages are to be made payable, and the entire wages are to be actually paid, in coin only. No contract is to be made as to how a workman shall lay out or expend his wages (Act of 1831, secs. 1, 2, 3).

2. No workman to be liable in debt for goods bought at any shop in which his employer is interested (Act of 1831, sec. 6 : Act of 1887, secs. 5, 6.)

3. No interest to be charged on advances of wages (Act of 1887, sec. 3).

4. Exemptions :—

- | | |
|--|--|
| (1.) Rent. | |
| (2.) Victuals dressed under employers roof. | |
| (3.) Medicine (subject to audit, sec. 9 of Act of 1887.) | |
| (4.) Fuel. | } by contract in
writing, and
not to exceed
real and true
value of same. |
| (5.) Horse food for use in the
way of trade. | |
| (6.) Materials and tools for use
by miners. | |
| (7.) Advances for benefit societies. | |
| (8.) Allowances (not intoxicants) to farm labourers. | |

5. Penalty on employer who makes illegal payments (Act of 1831, sec. 9).

6. Artificers may recover back wages not paid in coin (Act of 1831, sec. 4).

7. All "workmen" under Employers and Workmen's Act 1875, to be subject to the Act (Act of 1887, sec. 2).

6. *Seventh Annual Report of the Railway and Canal Commission, with Appendix, 1895.* (C.—7998.). Price 2½d.

The present report is issued by the Commissioners in accordance with the Railway and Canal Traffic Acts, 1873, 1888, and 1894. It contains lists of applications made to the Court during the year 1895, and of the cases heard. Lists of the working agreements submitted to the Commission for approval are also given in the Appendix. The report is dated February, 1896.

7. *Report from the Select Committee on Rule of the Road at Sea; with the Proceedings of the Committee.* 140. Price 2d.

This is the report of the Select Committee appointed by the House of Commons to consider the objections which have been raised to the adoption by Order in Council of Rule 15 of the revised International Regulations for preventing collisions at sea dealing with sound signals in fog, as suggested by the Washington Conference, and considered by the Maritime Powers (including any objections which may be taken to that rule, having regard to the sound signals prescribed by Rule 28).

The Committee now report that they have met and have examined numerous witnesses. The Committee state that the evidence taken before them may be divided into three classes; evidence explanatory of the proceedings in England and the United States antecedent to the publication of the new rules; evidence of experts belonging to Her Majesty's Naval service upon the merits of the new rules; and evidence of shipowners and officers or pilots connected with the Mercantile Marine upon the same subject.

The new rules are stated to be the outcome of a conference at Washington of representatives of the Powers, including the United States and all the Maritime Powers of Europe. They were adopted by the Conference after careful and critical examination by a committee appointed by the conference, and consisting of persons skilled in navigation. They were subsequently examined by a committee appointed by the Board of Trade in Great Britain who recommended their acceptance, though some at all events of the members of the British committee would have preferred to simplify them by blending two or more into one and thus reducing their number. That committee considered that in order to obtain international agreement it would be advisable to accept them in their entirety. After a great deal of correspondence the Governments of all the nations possessing any mercantile navy of any importance expressed their willingness to accept the new rules, though two of them indicated their preference for some changes, and the Committee were informed that it is uncertain as to whether one Power (Germany) is still willing to adopt these rules in view of the opposition among British shipowners which has been recently developed, and that if the question is re-opened the Netherlands reserve to themselves the right to open up other objections.

The present Report is dated the 14th of April 1896.

XVIII.—QUARANTINE NOTICES.

The following information with regard to quarantine, received from Her Majesty's representatives abroad, has been communicated to the Board of Trade through the Secretary of State for Foreign affairs :—

DENMARK.—The Government of the Danish Antilles have issued a notice imposing 15 days quarantine on arrivals from Rio de Janeiro, in consequence of yellow fever and other maladies.

BELGIUM.—Vessels arriving in Belgian ports direct from Italian ports are exempted from presenting a bill of health.

FRANCE.—The Governor-General of Algeria has authorised the pilgrimage to the holy places at Mecca this year under the following conditions :—

Vessels which embark at Algerian ports pilgrims for the holy places will be subject to a rigorous medical visit at their departure, in order to ascertain that none of the passengers are suffering from infectious diseases, and that the vessel satisfies hygienic requirements.

These vessels going through the Suez Canal will be liable to five days of observation at Port Said, and they will be obliged, without intermediary stoppage, to make direct for Djeddah, where they will undergo a fresh medical visit.

Similar regulations will be also issued for Tunis as regards the pilgrims from the Regency.

PORTUGAL.—Din has been declared to be infected with cholera, and other ports in Portuguese India suspected of the same disease since April 1. Ceara has been declared infected with yellow fever (April 24), and all other ports of the Province suspected of the same disease.

TURKEY.—Forty - eight hours' observation is imposed at Constantinople on ships arriving from Alexandria with passengers.

UNITED STATES.—From and after sunrise on the 1st day of May 1896, and until the 1st day of December 1896, unless sooner revoked, quarantine shall be enforced against all vessels entering Lower Mobile Bay, as follows :—

1st. Against all vessels, with their ballast, crews, passengers, baggage and freight coming from ports regarded by the Board as infected.

2nd. Against all vessels with their cargoes, ballast, crews, passengers, baggage and freight coming from ports regarded by the Board as suspected.

3rd. Against all vessels and their contents, which are infected, or with a history of infection during the present year, and have not subsequently been subjected to improved maritime sanitation.

4th. Against all vessels and their contents, that, during the quarantine season, have visited ports declared by this Board to be

infected, and subsequently arrive in our bay from foreign or domestic ports where no disinfection by the improved process is practised.

Ports now regarded as infected are all the ports of Cuba, Rio de Janeiro, Para, Bahia, Pernambuco, Santos, Ceara, San Juan (Porto Rico), Puntas Arenas, Guayaquil, and Vera Cruz, and vessels coming from ports hereafter becoming infected will be treated in same manner as those from ports named. Ports regarded as suspected are all other South American and West Indian ports, as well as those of Central America and Mexico, with the exception of the following, to wit: Ruatan, Bonacco, Utilia, Truxillo, Swan Island, Grand Cayman, and Cayman Brac; but vessels from these especially named, with passengers aboard, will be subjected to such detention and disinfection of vessels and contents as the quarantine board or executive officer may prescribe on arrival at quarantine station.

The following information has been received at the Board of Trade through the Secretary of State for the Colonies:—

GIBRALTAR.—Vessels 12 days out from Alexandria obtain pratique if health is good.

MALTA.—The importation of cattle from Thessaly is now allowed.

Arrivals from Alexandria are subjected to seven instead of 11 days' quarantine.

The following modification of Government Notice No. 56, of April 4, 1896, has been issued by Her Majesty's Representative at Malta, under date of April 17 last.

“Quarantine for seven days, to be reckoned from the commencement of the voyage, with permission for handling the cargo in quarantine.

“The following shall be subjected to a period of seven days' quarantine, but shall be permitted, under the directions of the collector of Customs, to discharge goods (which are not susceptible of communicating contagion) by means of the vessel's crew on lighters, or to employ for handling cargo local labourers subject to seven days' quarantine:—

“Vessels from Egypt with passengers arriving from Port Said, excepting vessels that carry a certified medical officer, who shall declare that no suspicious symptoms have occurred on board during the voyage, and excepting vessels of which the master certifies that no passengers or crew have been taken at Port Said.”

CYPRUS.—Twenty-four hours' quarantine on arrivals from Alexandria direct.

XIX.—POST OFFICE NOTICES.

MAILS FOR THE FALKLAND ISLANDS.

The following table shows the dates on which, during the next few months, the mails for the Falkland Islands, both direct from London and *via* Punta Arenas by Pacific Packet, will leave London and Punta Arenas and will be due at Port Stanley :—

Leave London.	Leave Punta Arenas.	Arrive at Stanley.
1896. Evening of	1896.	1896.
20 May - - - - - }	24 June - - - - -	26 June.
23 May (Supplementary) - - - }	- - - - -	17 July.
16 June - - - - -	- - - - -	- - - - -
1 July - - - - - }	5 August - - - - -	7 August.
4 July (Supplementary) - - - }	- - - - -	28 August.
28 July - - - - -	- - - - -	- - - - -
12 August - - - - - }	16 September - - - - -	18 September.
15 August (Supplementary) - - }	- - - - -	9 October.
8 September - - - - -	- - - - -	- - - - -
23 September - - - - - }	28 October - - - - -	30 October.
26 September (Supplementary) - }	- - - - -	20 November.
20 October - - - - -	- - - - -	- - - - -
4 November - - - - - }	9 December - - - - -	11 December.
7 November (Supplementary) - }	- - - - -	- - - - -
1 December - - - - -	- - - - -	1897. 1 January.

Parcel mails will be despatched only on the 16th of June, 28th of July, 8th of September, 20th of October, and 1st of December.

NEWFOUNDLAND.—RESUMPTION OF DIRECT MAIL SERVICE.

The direct mail service between this country and Newfoundland, by means of packets leaving Liverpool once a fortnight for St. John's, has been resumed.

The mails for Newfoundland will be made up in London on alternate Friday evenings. The packets will leave Liverpool on alternate Saturdays counting from the 16th of May, and will proceed direct to St. John's without calling at any intermediate port.

The parcel mails will be made up at Liverpool on the mornings of the dates on which the packets leave that port.

XX.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month and Four Months ended 30th April 1896, compared with the corresponding periods of the year 1895.

MONTH ENDED 30TH APRIL.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 30th April		Increase.	Decrease.
	1896.	1895.		
	£	£	£	£
I. Animals, living (for food) - -	892,153	599,778	292,375	—
II. (A.) Articles of food and drink, duty free - - -	10,377,028	11,385,692	—	1,008,664
(B.) Articles of food and drink, dutiable - - -	1,886,232	1,871,632	14,600	—
Tobacco, dutiable - - -	280,355	231,667	48,688	—
III. Metals - - -	1,683,325	1,351,907	331,418	—
IV. Chemicals, dyestuffs, and tanning substances - - -	528,531	571,941	—	43,410
V. Oils - - -	628,705	664,439	—	35,734
VI. Raw materials for textile manufactures - - -	7,911,840	7,092,876	818,964	—
VII. Raw materials for sundry industries and manufactures - - -	3,324,050	3,005,266	318,784	—
VIII. Manufactured articles - - -	6,930,600	6,228,021	702,579	—
IX. (A.) Miscellaneous articles - - -	1,291,680	1,241,128	50,552	—
(B.) Parcel post - - -	74,301	97,041	—	22,710
Total Value - - -	35,808,800	34,341,358	1,467,442	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - -	75,190	48,694	26,496	—
II. Articles of food and drink - - -	789,457	768,931	20,526	—
III. Raw materials - - -	1,365,733	1,403,619	—	37,886
IV. Articles manufactured and partly manufactured, viz.:—				
(A.) Yarns and textile fabrics - -	7,331,901	7,437,427	394,474	—
(B.) Metals and articles manufactured therefrom (except machinery) - -	2,716,535	2,232,083	484,452	—
(C.) Machinery and millwork - -	1,370,030	1,272,139	97,891	—
(D.) Apparel and articles of personal use - -	717,558	612,515	105,043	—
(E.) Chemicals, and chemical and medicinal preparations - - -	715,256	675,558	39,698	—
(F.) All other articles, either manufactured or partly manufactured - -	2,723,558	2,703,139	20,419	—
(G.) Parcel post - - -	121,481	97,866	23,615	—
Total Value - - -	18,426,699	17,232,311	1,174,388	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	5,411,590	6,603,115	—	1,191,525

I.—FOREIGN TRADE OF THE UNITED KINGDOM—*cont.*

FOUR MONTHS ENDED 30TH APRIL.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Four Months ended 30th April		Increase.	Decrease.
	1896.	1895.		
I. Animals, living (for food) - -	£ 3,455,764	£ 2,183,232	£ 1,267,482	£ —
II. (A.) Articles of food and drink, duty free - -	46,422,963	42,617,013	3,805,950	—
(B.) Articles of food and drink, dutiable - -	7,716,788	7,375,308	—	153,520
Tobacco, dutiable - -	1,248,534	371,501	376,933	—
III. Metals - -	6,615,581	5,634,692	980,889	—
IV. Chemicals, dyestuffs, and tanning substances - -	3,216,218	2,678,444	537,774	—
V. Oils - -	2,810,546	2,423,679	384,847	—
VI. Raw materials for textile manufactures - -	31,324,920	31,519,041	—	194,121
VII. Raw materials for sundry industries and manufactures - -	12,590,485	10,705,028	1,885,457	—
VIII. Manufactured articles - -	27,418,791	23,796,334	3,622,457	—
IX. (A.) Miscellaneous articles - -	4,902,434	4,470,481	431,953	—
(B.) Parcel post - -	381,118	372,907	8,211	—
Total Value - -	148,104,142	135,154,830	12,949,312	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURES.

	£	£	£	£
I. Animals, living - -	235,586	166,544	69,042	—
II. Articles of food and drink - -	3,199,166	2,913,767	285,399	—
III. Raw materials - -	5,441,875	5,224,610	220,265	—
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - -	36,722,547	33,243,576	3,478,971	—
(B.) Metals and articles manufactured therefrom (except machinery) - -	10,549,797	8,306,790	2,243,007	—
(C.) Machinery and millwork - -	5,259,489	4,476,551	782,938	—
(D.) Apparel and articles of personal use - -	3,565,259	2,882,748	682,511	—
(E.) Chemicals, and chemical and medicinal preparations - -	3,062,076	2,690,970	371,106	—
(F.) All other articles, either manufactured or partly manufactured - -	11,127,873	9,697,098	1,430,775	—
(G.) Parcel post - -	496,074	373,018	123,056	—
Total Value - -	79,659,742	69,972,672	9,687,070	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - -	20,498,855	19,749,796	749,059	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 4th April 1896 -	29,442 3	26,336 7	11,157 5
„ 11th „ -	25,350 5	12,827 5	8,659 2
„ 18th „ -	27,564 6	13,463 7	9,458 1
„ 25th „ -	31,367 0	9,011 2	9,922 4
April 1896 - - -	113,724 6	61,639 5	39,197 4
Corresponding month in 1895 -	138,187 1	55,734 1	40,257 0
„ „ 1894 -	164,090 0	31,654 7	30,127 1
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 4th April 1896 -	24 7	21 10	13 3
„ 11th „ -	24 6	21 0	13 1
„ 18th „ -	24 11	23 6	14 0
„ 25th „ -	25 6	21 0	13 11
April 1896 - - -	24 10	21 10	13 6
March 1896 - - -	25 2	21 3	13 7
February „ - - -	26 2	22 2	13 11
January „ - - -	25 7	23 10	13 10
December 1895 - - -	24 11	23 11	13 11
November „ - - -	25 9	25 1	14 2
October „ - - -	24 6	25 5	13 5
September „ - - -	22 10	24 0	13 7
August „ - - -	24 2	20 3	15 7
July „ - - -	24 9	19 8	16 1
June „ - - -	25 9	19 7	16 0
May „ - - -	22 5	20 5	15 2
April 1895 - - -	20 5	21 0	14 2
„ 1894 - - -	24 7	27 3	18 3
„ 1893 - - -	25 1	24 10	18 6
„ 1892 - - -	31 2	26 9	20 6

III.—EMIGRATION.—APRIL 1896.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out on EUROPE during the Month ended 30th April 1896, and the Four Months ended 30th April 1896, compared with the corresponding Periods of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.	Total for corresponding Month, 1895.
Month ended 30th April.							
English - -	4,921	1,726	723	1,616	619	9,605	8,757
Scotch - - -	1,013	337	29	127	73	1,584	1,315
Irish - - -	8,922	91	37	35	11	9,096	10,612
Total of British origin - }	14,856	2,154	789	1,778	708	20,285	20,684
Foreigners - -	6,790	1,101	17	552	120	8,580	6,519
Nationalities not distinguished - }	13	6	—	—	212	231	183
Total - -	21,659	3,261	806	2,330	1,040	29,096	27,386
Total for corresponding Month, 1895 - }	22,167	2,223	644	1,364	933	27,386	—
Four Months ended 30th April.							
English - - -	12,767	3,580	2,482	6,219	3,350	28,418	27,333
Scotch - - -	2,406	419	171	608	369	3,973	3,577
Irish - - -	13,373	219	120	132	111	13,955	16,314
Total of British origin - }	28,566	4,218	2,773	6,959	3,830	46,346	47,774
Foreigners - -	17,075	2,055	57	2,230	532	21,999	17,939
Nationalities not distinguished - }	23	18	3	—	931	975	831
Total - - -	45,664	6,291	2,833	9,239	5,293	69,320	66,594
Total for corresponding Months, 1895 - }	49,077	4,374	2,774	5,594	4,275	66,594	—

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION.—APRIL 1896.

RETURN of the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM during the Month and Four Months ended 30th April 1896, compared with the corresponding Periods of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.
Month ended 30th April.												
Aliens not stated to be en route to America:*												
Arrived at London -	797	367	167	141	164	143	—	—	88	80	1,216	731
Grimsby -	100	62	30	26	—	8	—	1	19	27	149	124
Hull -	85	72	32	97	61	78	—	—	129	67	307	314
Tyne Ports -	17	15	19	30	224	235	—	—	201	196	461	466
Leith -	41	41	109	66	1	2	—	—	15	23	166	182
Newhaven -	—	—	—	—	—	—	943	1,010	—	—	943	1,010
Other ports -	147	135	43	43	140	102	—	—	264	213	594	493
Total -	1,187	692	400	403	590	538	943	1,011	716	606	3,836	3,270
Aliens en route to America:												
Arrived at Grimsby -	57	8	61	137	1,456	437	—	—	250	225	1,874	807
Hartlepool, } West -	65	169	—	—	—	—	—	—	—	—	65	169
Hull -	33	9	7	6	1,849	1,879	—	—	1,878	1,221	3,765	3,418
Leith -	44	105	29	108	12	13	—	—	96	45	181	271
Other ports -	—	—	3	—	36	9	—	—	626	766	665	775
Total -	199	291	120	251	3,353	2,338	—	—	2,378	2,260	6,550	5,440
Total of aliens en route and of aliens not stated to be en route to America* -	1,386	983	520	654	3,943	2,876	943	1,011	3,594	2,866	10,386	8,440
Four Months ended 30th April.												
Aliens not stated to be en route to America:*												
Total No. -	4,208	2,586	1,504	1,438	1,809	1,611	2,562	2,419	2,639	1,769	12,812	19,823
Aliens en route to America: Total No. -	1,112	1,947	317	863	7,131	5,155	—	—	6,883	5,173	15,446	12,478
Total of aliens en route and of aliens not stated to be en route to America* -	5,320	3,633	1,911	2,241	8,940	6,766	2,562	2,419	9,525	6,942	28,258	32,601

* The distinction made in this Return between "Aliens not stated to be en route to America," and "Aliens en route to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be en route to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 130, Sess. 1896, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Gole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the aliens who arrived at ports in the United Kingdom not en route to America in the month of April 1896 was 1,034, and 920 in the same month of 1895; in the four months ended April 1896 the number was 3,431, and in the same months of 1895 the number was 2,816.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER of RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	April		Four Months ended April	
	1896.	1895.	1896.	1895.
Total gazetted	No. 268	No. 378	No. 1,425	No. 1,559
Number gazetted in principal trades and occupations :—				
Publicans and hotel keepers, &c.	23	34	113	133
Grocers, &c.	16	22	109	107
Farmers	19	24	89	112
Builders	14	19	82	80
Butchers	7	10	40	44
Bakers	12	11	35	42
Decorators, painters, plumbers, &c.	3	3	31	23
Boot and shoe manufacturers and dealers	5	12	31	42
Greengrocers, fruiterers, &c.	3	3	29	31
Drapers, haberdashers, &c.	3	8	28	28
Tailors, &c.	6	16	24	42
Clerks, commercial and general	5	4	21	19
Confectioners	2	4	19	11
Auctioneers	2	3	17	16
Solicitors	3	4	17	13
Agents, commission and general	4	8	17	26
Carpenters and joiners	5	—	17	11
Corn, flour, seed, hay, and straw merchants and dealers	3	2	16	14
Jewellers, watchmakers, importers, silversmiths, &c.	4	4	16	12
Cabinet-makers and upholsterers	3	—	15	5
Clothiers, outfitters, &c.	4	7	15	18
Schoolmasters and schoolmistresses	2	—	13	6
Coal and coke merchants and dealers	3	6	13	24
Saddlers and harness makers	3	4	13	18
Travellers, commercial, &c.	4	—	13	10
Merchants	1	3	12	12
Fishmongers, poulterers, &c.	1	3	12	15
Tobacconists, &c.	4	3	12	11
Provision merchants, &c.	2	4	11	14
Furniture dealers and makers	3	3	11	8
Restaurant, coffee, and eating-house keepers	1	3	10	10
Millers	1	—	10	8
Printers and publishers	—	2	8	10
Carriers, carmen, lightermen, and hauliers	1	4	8	12
Dairymen, cowkeepers, &c.	1	4	8	7
Brokers, stock and share	1	—	8	2
Chemists, druggists, and chemical manufacturers	4	1	8	9
Lodging-house keepers	—	1	7	8
Gardeners, florists, &c.	—	1	7	9
Milliners, dressmakers, &c.	1	1	7	4
General dealers	1	3	7	14
Clerks in holy orders	2	3	7	9

	April		Four Months ended April	
	1896.	1895.	1896.	1895.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.	No.	No.
Blacksmiths, farriers, &c. - -	3	1	7	5
Wine and spirit merchants, &c. - -	—	5	6	16
Cattle and pig dealers - -	—	1	6	4
Engineers and founders, &c. - -	1	1	6	9
Officers in Army - -	1	2	6	5
Ironmongers - -	—	2	5	11
China, glass, and earthenware, &c. dealers - -	—	1	5	8
Contractors - -	—	1	5	4
Timber merchants and wood dealers - -	—	—	5	2
Stationers - -	2	2	5	6
Carriage, &c. builders - -	2	1	5	6
Hair-dressers - -	—	1	4	7
Architects and surveyors - -	—	—	3	3
Hosiers, glovers, &c. - -	—	4	3	9
Wheelwrights - -	1	2	3	9
Curriers, tanners, and leather merchants - -	1	3	3	7
Cab, coach, and omnibus proprietors - -	—	1	2	2
Fishing net, smack owners, and masters - -	—	1	2	7
Stone, marble, and monument masons - -	—	—	2	3
Directors and promoters of public companies - -	—	1	1	9

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 30th April 1896, and during the first Four Months of the Year 1896, compared with the corresponding Periods of the Year 1895.

Animals.	April		Four Months ended April	
	1896.	1895.	1896.	1895.
	No.	No.	No.	No.
Cattle - - - - -	52,363	52,753	176,934	179,054
Sheep - - - - -	15,249	6,920	43,763	34,946
Swine - - - - -	48,181	39,935	257,040	254,977
Goats - - - - -	645	705	772	961
Horses - - - - -	3,449	3,064	10,904	6,841
Mules or Jennets - - - - -	2	—	8	1
Asses - - - - -	223	56	298	102
Total - - - - -	120,112	103,434	489,719	476,882

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month and Four Months ended 30th April 1896, compared with the corresponding Periods of the Year 1895.

	April		Four Months ended April	
	1896	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Brill	1,561	1,718	6,491	6,443
Soles	6,129	6,611	23,001	29,199
Turbot	5,611	6,442	23,279	23,929
Prime fish not separately distinguished	2,722	1,504	8,282	7,605
Total prime fish	16,023	16,335	66,053	67,176
Cod	61,197	68,929	220,799	223,319
Haddock	189,537	212,571	835,347	813,225
Hake	13,827	7,918	19,674	15,911
Halibut	11,100	10,906	23,211	22,520
Herrings	7,575	13,999	31,682	27,756
Ling	10,936	13,420	41,749	47,937
Mackerel	105,786	54,920	112,404	57,356
Pilchards	—	—	6,101	270
Plaice	43,164	54,783	208,794	209,731
Sprats	8	639	49,212	43,134
Fish not separately distinguished, except shell fish	92,851	89,046	360,479	346,135
Total	557,004	548,457	1,975,505	1,874,479
Shell fish:—				
	No.	No.	No.	No.
Crabs	1,089,140	734,169	1,975,292	798,666
Lobsters	95,710	36,588	171,472	69,607
Oysters	1,297,000	1,255,000	6,297,000	3,930,000
	Cwts.	Cwts.	Cwts.	Cwts.
Other shell fish	43,779	55,406	181,737	168,341
VALUE.				
	£	£	£	£
Brill	4,854	4,968	19,110	18,348
Soles	46,841	51,618	187,891	200,323
Turbot	24,437	28,150	101,330	105,406
Prime fish not separately distinguished	10,235	5,018	29,644	24,867
Total prime fish	86,367	89,784	337,975	349,444
Cod	45,218	52,943	149,020	154,091
Haddock	108,428	127,194	491,639	477,144
Hake	12,282	6,800	18,680	15,016
Halibut	24,018	22,723	52,645	49,982
Herrings	2,003	3,694	7,177	6,390
Ling	6,900	7,803	28,411	29,434
Mackerel	82,426	74,140	92,633	78,619
Pilchards	—	—	1,763	65
Plaice	59,863	61,135	263,834	252,758
Sprats	5	38	7,450	3,680
Fish not separately distinguished, except shell fish	70,587	61,983	253,823	231,916
Total	498,097	507,737	1,705,050	1,648,539
Shell fish:—				
	£	£	£	£
Crabs	10,090	6,719	18,296	7,890
Lobsters	4,114	1,670	7,391	3,299
Oysters	4,396	3,790	22,871	11,844
Other shell fish	12,847	16,263	47,572	40,823
Total	31,447	27,442	96,130	63,856
Total value of fish landed	529,544	535,179	1,801,180	1,712,395

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month and Four Months ended 30th April 1896, compared with the corresponding Periods of the Year 1895.

	April		Four Months ended April	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles)	2,294	1,721	5,704	4,421
Turbot	534	332	1,454	945
Cod	60,593	45,395	234,129	174,270
Eel	1,583	903	7,739	7,296
Flounder Plaice, Brill	6,158	8,438	37,959	27,143
Haddock	82,922	82,021	448,671	394,034
Halibut	7,847	6,251	12,108	10,641
Herrings	15,756	14,070	129,989	111,750
Ling	28,337	23,685	52,038	75,776
Mackerel	55	5	283	66
Saith (Coal Fish)	8,852	4,374	24,074	23,434
Skate	9,733	8,537	31,137	33,460
Sparling	9	17	92	44
Sprats	—	—	29,932	1,130
Torsk (Tusk)	2,512	1,537	3,259	2,488
Whiting	3,146	3,310	16,137	11,556
Fish not separately distinguished, except } shell fish	4,707	4,003	17,846	15,771
Total	235,038	210,499	1,053,551	891,222
Shell Fish:—	No.	No.	No.	No.
Crabs	724,347	239,233	1,895,026	352,096
Lobsters	58,861	53,066	184,763	133,452
Oysters	24,600	42,300	112,462	153,300
Clams	1,138	1,759	9,643	8,943
Mussels	22,648	14,330	109,896	76,000
Other shell fish	7,133	6,444	25,679	21,437
VALUE.				
	£	£	£	£
Soles (Lemon Soles)	4,077	3,612	12,196	10,161
Turbot	1,477	1,243	4,629	3,287
Cod	20,044	16,350	83,530	69,945
Eel	799	617	4,059	4,331
Flounder, Plaice, Brill	6,044	7,447	39,241	26,011
Haddock	33,660	36,431	166,648	162,685
Halibut	7,739	6,921	14,915	13,503
Herrings	2,291	3,114	24,653	29,331
Ling	7,569	7,703	16,160	21,482
Mackerel	14	2	205	26
Saith (Coal Fish)	935	526	3,028	2,990
Skate	1,982	1,792	8,590	8,031
Sparling	30	100	275	174
Sprats	—	—	1,755	85
Torsk (Tusk)	349	224	494	395
Whiting	1,235	1,693	6,384	5,758
Fish not separately distinguished, except } shell fish	865	806	3,837	3,593
Total	89,110	88,281	390,599	361,836
Shell Fish:—				
Crabs	3,517	1,521	6,395	2,039
Lobsters	2,757	3,054	8,418	7,045
Oysters	92	211	450	753
Clams	171	237	1,339	1,241
Mussels	1,374	999	7,550	4,607
Other shell fish	1,531	1,556	5,376	4,542
Total	9,442	7,578	29,528	20,227
Total value of fish landed	98,552	95,859	420,127	382,063

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month and Four Months ended 30th April 1896, compared with the corresponding Periods of the Year 1895.

	April		Four Months ended April	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles - - - - -	175	177	455	651
Turbot - - - - -	76	100	266	338
Total prime fish - - -	251	277	721	989
Cod - - - - -	2,728	4,114	21,282	25,558
Haddock - - - - -	1,938	2,031	11,638	11,483
Hake - - - - -	140	128	620	1,171
Herrings - - - - -	29	72	1,817	4,129
Ling - - - - -	2,082	2,394	9,097	14,229
Mackerel - - - - -	35,747	23,776	36,343	32,296
Sprats - - - - -	-	-	-	-
Whiting - - - - -	680	970	4,692	4,667
Fish not separately distinguished, except shell fish }	4,099	2,986	12,213	10,654
Total - - - - -	47,694	56,738	98,423	105,176
Shell Fish:—	No.	No.	No.	No.
Crabs - - - - -	24,979	10,208	28,119	17,062
Lobsters - - - - -	9,806	7,991	20,732	16,984
Oysters - - - - -	127,700	54,100	395,900	158,050
Other shell fish - - -	Cwts.	Cwts.	Cwts.	Cwts.
	1,699	1,568	8,090	6,770
VALUE.				
	£	£	£	£
Soles - - - - -	854	830	2,122	2,909
Turbot - - - - -	309	273	1,058	1,106
Total prime fish - - -	1,163	1,103	3,180	4,015
Cod - - - - -	1,454	1,867	9,546	10,078
Haddock - - - - -	1,201	1,270	7,097	7,159
Hake - - - - -	76	79	355	677
Herrings - - - - -	10	16	342	750
Ling - - - - -	1,031	793	3,603	4,546
Mackerel - - - - -	25,813	16,103	26,110	19,847
Sprats - - - - -	-	-	-	-
Whiting - - - - -	438	533	2,668	2,555
Fish not separately distinguished, except shell fish }	2,064	1,876	6,394	5,225
Total - - - - -	33,250	23,640	59,295	54,892
Shell Fish:—				
Crabs - - - - -	106	40	128	85
Lobsters - - - - -	331	192	719	608
Oysters - - - - -	224	106	777	280
Other shell fish - - -	355	269	1,391	881
Total - - - - -	1,016	707	3,015	1,854
Total value of fish landed -	34,266	24,347	62,310	56,746

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.—COTTON RETURNS.—APRIL 1896.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month and Four Months ended April 1896, compared with the corresponding Months of the Year 1895.

	Month of April		Four Months ended April	
	1896.	1895.	1896.	1895.
IMPORTS.				
	No.	No.	No.	No.
American - -	242,988	222,544	1,138,773	1,697,487
Brazilian - -	3,242	450	24,388	2,759
East Indian - -	26,907	3,488	65,615	8,755
Egyptian - -	21,695	22,422	161,370	154,598
Miscellaneous - -	2,575	4,793	13,038	19,098
Total - -	297,407	253,697	1,403,184	1,882,697
EXPORTS.				
American - -	15,723	22,723	60,040	76,524
Brazilian - -	—	—	2,350	5,858
East Indian - -	4,363	3,193	17,854	10,827
Egyptian - -	7,676	5,795	42,812	34,880
Miscellaneous - -	2,115	2,258	10,147	9,518
Total - -	29,877	33,969	133,203	137,607
FORWARDED from PORTS to INLAND TOWNS.				
American - -	206,478	261,282	898,358	1,000,051
Brazilian - -	2,793	920	21,100	3,741
East Indian - -	6,663	1,175	17,102	6,145
Egyptian - -	20,706	29,145	116,194	120,246
Miscellaneous - -	9,165	8,122	38,686	43,513
Total - -	245,805	300,644	1,091,440	1,173,696
FORWARDED from INLAND TOWNS to PORTS.				
American - -	185	521	798	1,458
Brazilian - -	—	—	7	—
East Indian - -	—	—	4	—
Egyptian - -	—	—	49	10
Miscellaneous - -	52	135	240	331
Total - -	237	656	1,098	1,799

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note.—Rouble = 2s. ; Franc = 9 $\frac{6}{10}$ d. ; Milreis = 4s. 6d. ; Lira = 9 $\frac{6}{10}$ d. ; Gulden = 1s. 8d. ; Drachma = 9 $\frac{6}{10}$ d. ; Lew = 9 $\frac{6}{10}$ d. ; L. Egyptian = 1l. Os. 6 $\frac{1}{4}$ d. ; Dollar = 4s. 2d.

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe	Nov.	Roubles 39,498,000 (1896)	47,464,000 (1895)	453,536,000 (1896)	472,549,000 (1895)
France	Mar.	Frs. 389,024,000 (1895)	343,779,000 (1894)	1,047,487,000 (1895)	902,558,000 (1894)
Portugal	Oct.	Milreis 3,218,000 (1896)	2,743,000 (1895)	32,811,000 (1896)	30,284,000 (1895)
Italy	Mar.	Lire 100,508,000	104,852,000	273,629,000	254,959,000
Austria-Hungary	Mar.	Gulden 69,068,000 (1895)	61,514,000 (1894)	187,729,000 (1895)	172,310,000 (1894)
Greece	Dec.	Drchms. 9,444,000	11,774,000	106,822,000	109,959,000
Bulgaria	Dec.	Lew 5,900,000	9,340,000	69,020,000	99,229,000
Egypt	Dec.	L. Egypt 923,000 (1896)	755,000 (1895)	8,246,000 (1896)	9,119,000 (1895)
United States	Feb.	Dollars 62,484,000	58,310,000	131,137,000	125,804,000
	Mar.	„ 66,384,000 (1895)	69,295,000 (1894)	197,521,000 (1895)	195,159,000 (1894)
Mexico*	Nov.	„ 3,683,000 (1896)	3,728,000 (1895)	15,514,000 (1896)	13,887,000 (1895)
British India†	Feb.	Rupees 5,75,93,000	5,27,13,817	62,64,12,647	64,26,17,923

II.—EXPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1895.	1894.	1895.	1894.
Russia in Europe	Nov.	Roubles 55,114,000 (1896)	58,840,000 (1895)	641,144,000 (1896)	610,793,000 (1895)
France	Mar.	Frs. 301,726,000 (1895)	302,304,000 (1894)	834,090,000 (1895)	784,802,000 (1894)
Portugal	Oct.	Milreis 2,374,000 (1896)	2,328,000 (1895)	22,417,000 (1896)	19,994,000 (1895)
Italy	Mar.	Lire 101,039,000	88,333,000	259,956,000	218,623,000
Austria-Hungary	Mar.	Gulden 67,913,000 (1895)	58,349,000 (1894)	175,382,000 (1895)	167,181,000 (1894)
Greece	Dec.	Drchms. 6,258,000	5,868,000	71,156,000	74,291,000
Bulgaria	Dec.	Lew 5,197,000	7,364,000	77,686,000	72,351,000
Egypt	Dec.	L. Egypt 2,156,000 (1896)	1,374,000 (1895)	12,674,000 (1896)	11,884,000 (1895)
United States	Feb.	Dollars 77,655,000	55,983,000	164,564,000	137,213,000
	Mar.	„ 75,528,000 (1895)	65,162,000 (1894)	240,092,000 (1895)	202,375,000 (1894)
Mexico*	Nov.	„ 9,613,000 (1896)	5,718,000 (1895)	41,078,000 (1896)	32,497,000 (1895)
British India†	Feb.	Rupees 11,50,21,668	10,37,14,388	1,02,55,99,697	99,20,14,565

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the “special” imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are “general.” “Special” means, in the case of imports, imports for home consumption ; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Eleven Months ended 29th February		Eleven Months ended 29th February	
	1895-96.	1894-95.	1895-96.	1894-95.
	R.	R.	R.	R.
I.—Animals—living - -	30,62,048	25,38,631	14,74,908	12,24,774
II.—Articles of food and drink -	8,00,57,598	7,58,05,446	26,73,26,064	25,37,99,891
III.—Metals and manufactures of—				
A.—Hardware and cutlery (including plated-ware)	1,28,99,149	1,17,69,042	10,45,627	8,09,543
B.—Metals - -	5,90,56,026	4,55,31,355		
C.—Machinery and mill-work	2,92,00,563	2,22,51,712		
D.—Railway plant and rolling-stock (other than Government stores) -	1,40,32,811	1,50,29,635		
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	1,96,60,685	1,70,93,802	14,49,46,738	14,59,39,062
V.—Oils - - - -	2,95,17,761	1,89,48,163	65,89,878	67,86,737
VI.—Raw materials and unmanufactured articles - -	4,47,73,285	4,02,25,978	37,69,36,315	37,35,44,377
VII.—Articles manufactured and partly manufactured—				
A.—Cotton yarn - - -	2,65,86,565	2,53,55,622	6,05,96,490	5,19,75,613
B.—Cotton piece goods -	19,95,45,837	27,13,56,140	1,45,43,139	1,31,34,018
C.—Other articles - -	10,80,20,319	9,61,57,347	11,26,91,990	9,84,06,943
TOTAL - - -	62,64,12,647	64,26,17,923	93,61,51,149	94,56,20,973

* Exports of Indian Produce and Manufacture only.

XXI.—LIST OF DIPLOMATIC AND CONSULAR REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1896, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.	No.	Place.	Price.
1657	Réunion - - -	1d.	1681	Vera Cruz - - -	1½d.
1658	Tokio - - -	1½d.	1682	Patras - - -	1d.
1659	Maranham - - -	1d.	1683	La Rochelle - - -	1½d.
1660	Copenhagen - - -	1d.	1684	Madrid - - -	1½d.
1661	Berlin - - -	1½d.	1685	Belgrade - - -	2d.
1662	Tehran - - -	2½d.	1686	Algiers - - -	5d.
1663	Salonica - - -	1½d.	1687	Galveston - - -	2½d.
1664	Manila - - -	½d.	1688	New Orleans - - -	2d.
1665	Florence - - -	5½d.	1689	Suakin - - -	1d.
1666	Dakar - - -	½d.	1690	Pernambuco - - -	1d.
1667	Havre - - -	2d.	1691	Guatemala - - -	1½d.
1668	Rouen - - -	2d.	1692	Guayaquil - - -	1d.
1669	Corfu - - -	½d.	1693	Wenchow - - -	1d.
1670	Calais - - -	1d.	1694	Piræus - - -	3d.
1671	Tehran - - -	1½d.	1695	Tokio - - -	3d.
1672	Barcelona - - -	2d.	1696	Marseilles - - -	1d.
1673	Amsterdam - - -	1d.	1697	Manila - - -	1d.
1674	Bordeaux - - -	2½d.	1698	Jerusalem - - -	1d.
1675	Warsaw - - -	1d.	1699	Cherbourg - - -	2d.
1676	Havana - - -	1½d.	1700	Leghorn - - -	1½d.
1677	Berlin - - -	1d.	1701	Boston - - -	1½d.
1678	Beira - - -	1½d.	1702	Kiungchow - - -	1d.
1679	Saigon - - -	1d.	1703	Naples - - -	2½d.
1680	Trebizond - - -	1d.	1704	Stockholm - - -	2d.

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources :—

No.		Price.
387	<i>Portugal.</i> —Report on the East Coast Route to Mashonaland -	1d.
388	<i>Austria-Hungary.</i> —Report on the Iron Industry in Styria and the Betler Ironworks in Hungary - - -	5d.

XXII.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
47	British Bechuanaland	1890-92	106	Gambia	1893
48	Malta	1890 and 1891	107	Hong Kong	"
49	Gibraltar	"	108	St. Vincent	"
50	Falkland Islands	"	109	Gibraltar	"
51	Leeward Islands	"	110	Bahamas	"
52	Grenada	1890 and 1891	111	Turks and Caicos Islands	"
53	St. Lucia	"	112	Leeward Islands	"
54	Labuan	"	113	Barbados	"
55	British Guiana	"	114	Malta	"
56	Zululand	"	115	Zululand	"
57	Mauritius	"	116	British Honduras	"
58	Lagos	"	117	Trinidad and Tobago	"
59	Straits Settlements	"	118	Falkland Islands	"
60	Ceylon	"	119	Rodrigues	"
61	Natal	1891-92	120	Mauritius	"
62	Basutoland	"	121	Straits Settlements	"
63	St. Helena	1891	122	Labuan	"
64	Sierra Leone	"	123	Basutoland	1893-94
65	Jamaica	1891-92	124	St. Lucia	1893
66	Trinidad and Tobago	1891	125	St. Helena	"
67	Newfoundland	"	126	Ceylon	"
68	British New Guinea	1891-92	127	Fiji	"
69	Victoria	1891	128	Grenada	"
70	Rodrigues	"	129	Sierra Leone	"
71	Bermuda	1892	130	British Bechuanaland	1893-94
72	Fiji	1891	131	British New Guinea	1892-93 and 1893-94
73	British Honduras	"	132	Lagos	1893
74	Turks and Caicos Islands	1890	133	British Guiana	1893-94
75	Gibraltar	1892	134	Jamaica	"
76	Leeward Islands	"	135	Newfoundland	1893
77	Bahamas	"	136	Gold Coast	"
78	Trinidad and Tobago	"	137	Zululand	1894
79	Malta	"	138	Bermuda	"
80	Gambia	"	139	Bahamas	"
81	Straits Settlements	"	140	Barbados	"
82	Grenada	"	141	Turks and Caicos Islands	"
83	Barbados	"	142	Malta	"
84	St. Vincent	"	143	Gambia	"
85	Hong Kong	"	144	Windward Islands	"
86	St. Lucia	"	145	Trinidad and Tobago	"
87	Falkland Islands	"	146	Gibraltar	"
88	Gold Coast	"	147	Falkland Islands	"
89	Basutoland	1892-93	148	Hong Kong	"
90	Ceylon	1892	149	Strait Settlements	"
91	Zululand	"	150	Lagos	"
92	Mauritius	"	151	Seychelles	"
93	Labuan	"	152	Basutoland	1894-95
94	British Honduras	"	153	Fiji	1894
95	Lagos	"	154	St. Helena	"
96	Sierra Leone	"	155	Ceylon	"
97	Fiji	"	156	Mauritius	"
98	Rodrigues	"	157	Labuan	"
99	St. Helena	"	158	Gold Coast	"
100	British Bechuanaland	1892-93	159	British Guiana	1894-95
101	British Guiana	"	160	Sierra Leone	1894
102	Newfoundland	1892	161	Jamaica	1894-95
103	Jamaica	1892-93	162	British Honduras	1894
104	Victoria	1892	163	British Bechuanaland	1894-95
105	Bermuda	1893			

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast	Economic Agriculture.
2	Zululand	Forests.
3	Sierra Leone	Geology and Botany.
4	Canada	Advantages to Emigrants.

THE
Board of Trade Journal
OF
TARIFF AND TRADE NOTICES
AND
MISCELLANEOUS COMMERCIAL INFORMATION.

Vol. XX.]

June 1896.

[No. 119.]

I.—BOARD OF TRADE NOTICES.

STATE OF THE SKILLED LABOUR MARKET.

The following memorandum has been prepared by the Labour Department of the Board of Trade for the "Board of Trade Journal," and also (with additions) for the "Labour Gazette":—

The improvement in the state of employment has been maintained during the month in most industries.

In the 110 trade unions, with an aggregate membership of 420,594, making returns, 13,883 (or 3·3 per cent.) are reported as unemployed at the end of May, compared with 3·2 per cent. in April, and with 6·0 per cent. in the 85 unions, with a membership of 387,411, from which returns were received for May 1895.

Coal Mining.—Employment in this industry was considerably better than a year ago. The average number of days worked by 1,127 pits, employing 324,374 workpeople, was 4·88 per week, as compared with 4·69 in April (when, however, the average was lessened by the Easter holidays in England and Wales), and 4·57 in May 1895. The percentage of unemployed miners in trade unions in Northumberland and Durham was only 1·8 at the end of May, compared with 1·9 at the end of April, and 5·2 in May 1895.

Iron Mining.—Practically full time was worked at iron mines in May, except at some places in the Midlands. At the 129 mines included in the returns, employing 16,454 men, an average of 5·76 days per week was worked during May, as compared with 5·74 days in May 1895. The numbers employed at these mines have increased by about 3 per cent. since May 1895.

In the *Pig Iron* industry the state of employment, though considerably better than last year, has fallen off very slightly as compared with April. The ironmasters making returns had 345 furnaces in blast at the end of May, as compared with 346 in April, and 305 in May 1895. The number of workpeople employed at these furnaces was 30 less than in April, and 1,601 more than in May 1895.

Employment at *Steelworks* continues to show a marked improvement as compared with a year ago. At 113 works included in returns received, the number of workpeople employed was 33,768 at the end of May, being 12·5 per cent. more than at the end of May 1895, and ·3 per cent. more than at the end of April last.

At the 89 *Puddling Furnaces* and *Rolling Mills* from which returns were received, the number employed was less at the end of May than at the end of April, and also less than a year ago, the numbers being: for May, 16,946; for April, 17,695; and for May 1895, 17,196.

The *Tinplate* trade remains very unsettled. At the 89 works covered by the returns received, only 41 were giving full employment. Altogether, 289 mills were reported as working at the end of May, as compared with 288 at the end of April.

The *Engineering* and kindred trades continue well employed in most branches, the percentage of unemployed union members being 2·2 at the end of May, compared with 2·3 in April, and 6·6 in May 1895.

In the *Shipbuilding* trades the percentage of unemployed union members at the end of May is returned as 7·7, compared with 7·4 in April, and 11·4 in May 1895.

With the exception of the plumbers, the *Building* trades continue well employed, the percentage of unemployed in unions making returns being 1·6, compared with 1·5 in April, and 2·5 in May of last year.

The *Furnishing* trades are still busy. The percentage of unemployed union members at the end of May was 1·1, compared with 0·9 in April, and 2·7 in May 1895.

The *Printing* and *Bookbinding* trades are not so well employed on the whole, though the lithographic branch of the former has somewhat improved. The percentage of unemployed union members at the end of May was 4·8, compared with 4·4 in April, and 5·6 in May of last year.

The *Paper* trade has improved compared with last month, but is worse than a year ago, the percentage of unemployed union members at the end of May being 3·9, compared with 4·2 in April, and 1·4 in May 1895.

Employment in the *Glass* trade remains practically stationary, 14·1 per cent. of union members being unemployed, compared with 14·4 in April and 14·0 per cent. in March.

In the *Leather* trades employment has fallen off, and the percentage of unemployed union members has risen from 4·1 in April to 5·3 at the end of May, compared with 3·8 in May 1895.

Employment is reported slack in most of the principal centres of the wholesale *Boot* and *Shoe* trade. In the bespoke trade employment is good.

In the *Tailoring* trade employment is good in the bespoke branch, and the ready-made branch was well employed up to the Whitsuntide holidays.

Employment for *Cotton Spinners* has somewhat improved. *Weavers* are fairly well employed on the whole.

In the *Woollen* and *Worsted* trades employment is still good, although there has been a slight decline in some branches.

In most branches employment in the *Hosiery* trade is good, and in the *Silk* trade fairly good.

As regards the employment of women in the *Textile* trades information respecting 463 mills, employing about 84,000 women and girls, shows little change, 93 per cent. being in mills giving full employment during the month, compared with 92 per cent. among those for whom returns were received for April.

Trade Disputes.—The number of fresh disputes taking place in May was 135, as compared with 84 in April, and 84 in May 1895. 43 disputes occurred in the building trades, 14 in mining and quarrying, 17 in engineering and shipbuilding, 10 in other metal trades, 20 in textile trades, 19 in the furniture trades, 4 in clothing trades, 4 in connection with seafaring and dock labour, and 4 in other miscellaneous industries. In 127 of these new disputes 40,447 workpeople were involved. Of the 97 disputes settled, 63 (involving about 13,000) were successful to the workpeople; 19, involving about 7,500, to the employers; 12, involving 1,277, were compromised; the results of the other three disputes being indefinite.

Changes in Rates of Wages and Hours of Labour.—The changes in rates of wages reported during May affected over 60,000 workpeople, of which number more than 58,000 received increases and less than 2,000 sustained decreases. In the building trades about 34,000 workpeople (including over 23,000 London operatives) received increases, and 8,000 workpeople in the engineering and shipbuilding, and 5,000 in the wood-working and furnishing trades also secured advances. The decreases were practically confined to about 1,700 workpeople in the tinplate trade.

The changes in hours of labour, though affecting 47,000 persons, were in most cases very slight in amount.

WARNING TO BRITISH SHIPPERS

With reference to a notice which appeared on page 573 of the May number of the "Board of Trade Journal" respecting the fines imposed on shipping in Spain for errors occurring in ships' manifests and similar documents, a further despatch has been received at the Foreign Office from Sir George F. Bonham, Her Majesty's Chargé d'Affaires at Madrid, reporting that the very heavy fine (about 3,000*l.*) imposed on a British vessel at Barcelona in 1893 has now been confirmed. This fine was inflicted for an error in the ship's manifest, which, though stating the number of bushels of wheat in her cargo correctly, represented their weight as 900 tons odd, instead of 1,700 tons.

The Board of Trade desire, therefore, again to call attention to the need for the greatest accuracy in regard to the drawing up of ships' manifests and bills of lading, especially in the case of vessels trading with Spain, as the slightest error may be attended with very grave consequences.

TENDERS FOR COAL FOR SURINAM.

The Secretary of State for Foreign Affairs has received a despatch from Her Majesty's Consul at Paramaribo, reporting that the Government of Surinam have issued their annual request for tenders for the supply of 2,000 tons of Cardiff steam navigation coals for the year 1897. A copy of the contract with a marginal translation may be seen on application to the Commercial Department, Foreign Office, between the hours of 11 and 6. Any further inquiries should be addressed to the Beheerder Koloniale Vaartingen, Paramaribo.

TENDERS FOR CONCESSION FOR THE SUPPLY OF DRINKING WATER AT MESSINA.

The Secretary of State for Foreign Affairs has received from Her Majesty's Consulate at Palermo a despatch reporting that tenders have been invited by the municipality of Messina for a concession to be given for the supply of drinking water for the city and suburbs of Messina.

Particulars may be seen on application to the Commercial Department of the Foreign Office between the hours of 11 and 6.

GOVERNMENT CONTRACTS.

India Office.—The Secretary of State for India in Council is prepared to receive tenders from such persons as may be willing to supply locomotives.

The conditions of contract may be obtained on application to the Director-General of Stores, India Office, Whitehall, S.W., and tenders are to be delivered at that office by *Two o'clock p.m.* on Tuesday the 16th June, after which time no tender will be received.

SHIPS FOR SALE.

Tenders will be received until noon [on Wednesday, the 24th June, for the purchase of the following ships as they lie at Portsmouth :—

	Tonnage. B.M.
" Assistance," troopship - - -	2,500
	(displacement load.)
" Bacchante," 2nd-class cruiser - -	2,679

Forms of tender, containing conditions of contract and all particulars, may be obtained on personal application at the Admiralty, or by letter addressed "The Director of Navy Contracts, Admiralty, Whitehall, S.W."

BOARD OF TRADE AWARDS FOR GALLANTRY.

The Board of Trade have made the following awards for gallantry in saving life at sea, and services to shipwrecked sailors :—

Bronze medals to Thomas Verrill, Joseph Crispin, and Robert Longster, crew of the fishing coble "Mary and Jane," of Whitby, in recognition of their services in rescuing the crew of the fishing coble "Phyllis," of Whitby, which capsized off Staithes on the 14th March last.

Bronze medals to George Webster and John Harrison, fishermen, crew of the fishing coble "Sarah," of Whitby, in recognition of their services in rescuing the crew of the fishing coble "Edward Camble," of Whitby, which was dismasted off Staithes, on the 14th March last.

A binocular glass to Mr. Giovanni B. Gavino, master of the Italian S.S. "Manilla," of Genoa, in recognition of his kindness and humanity to an injured apprentice belonging to the sailing ship "Wasdale," of Liverpool, on the 29th February last.

Silver medals to Benito Borrazas, master, and Edward Bartolozzi, chief engineer of the tug "Emperor," of Monte-Video, in recognition of their services to the crew of the barque "Firth of Clyde," of Glasgow, when stranded in the Rio de la Plata on the 16th March 1896.

Bronze medal and a sum of £1. to Cornelius Brossinan, in recognition of his services in rescuing J. Callahan when the small boat of the fishing lugger "Meteor," of Tralee, capsized in Dingle Bay on the 8th May last.

A binocular glass to Mr. Henrique Steenken, master of the Brazilian barque "Anna Elisa," of Rio de Janeiro, in recognition of his services to the shipwrecked crew of the S.S. "Egglestone Abbey," of Cardiff, which was wrecked off Cape St. Thomé, Brazil, on the 13th March last.

II.—THE FOREIGN TRADE OF THE UNITED KINGDOM.*

The total value of the imports into the United Kingdom from foreign countries and British possessions during the month ended 31st May last was 33,349,988*l.*, as compared with 34,752,086*l.* for May 1895, showing a decrease of 1,402,098*l.*, or 4.03 per cent.; and the exports of British and Irish produce and manufactures were valued at 18,812,927*l.*, as compared with 18,344,744*l.*, an increase of 468,183*l.*, or 2.49 per cent. The value of the exports of foreign and colonial merchandise amounted to 4,770,958*l.*, as against 5,215,785*l.* for May 1895, showing a decrease of 444,827*l.*, or 8.53 per cent.

With regard to the imports for the past month, the only classes that show an increase in value as compared with May 1895 are animals, living (for food), 130,722*l.*; tobacco, dutiable, 64,469*l.*; metals, 194,666*l.*; and raw materials for sundry industries and manufactures, 11,406*l.*. All other classes of imported goods have decreased in value as compared with the corresponding period of the previous year, viz., articles of food and drink (duty free), 1,186,476*l.*; articles of food and drink (dutyable), 18,461*l.*; chemicals, dyestuffs, and tanning substances, 91,050*l.*; oils, 123,529*l.*; raw materials for textile manufactures, 89,592*l.*; manufactured articles, 243,594*l.*; miscellaneous articles, 27,367*l.*; and parcel post, 23,292*l.*. The total decrease in value of imports of articles of food and drink (duty free) amounts to 1,769,268*l.*; but an increase in some articles, amounting altogether to 582,792*l.* must be set against this, showing a *net* decrease of 1,186,476*l.*. Of this sum, the articles showing the largest diminution in value continue to be corn of all sorts (except maize), wheat imports having declined 567,315*l.*; wheat meal and flour, 235,717*l.*; barley, 140,687*l.*; and oats, 95,636*l.*. Refined and candy sugar shows a decrease of 159,317*l.*, and unrefined sugar, 135,377*l.*. On the other hand, the imports of butter have increased in value 147,050*l.*, and of Indian corn or maize, 137,012*l.*. In raw materials for textile manufactures, cotton, raw, has decreased 336,640*l.*, and flax, 257,951*l.*, but sheep or lambswool has gone up 538,541*l.*, and jute, 135,584*l.*; the net decrease in this class is only 89,592*l.*

As regards the exports of British and Irish produce and manufactures, improvement is found in articles of food and drink, 35,679*l.*; metals and articles manufactured therefrom (except machinery), 490,831*l.*; machinery and millwork, 122,329*l.*; apparel and articles of personal use, 119,786*l.*; and parcel post, 25,676*l.*. Diminished values are seen in the categories of animals, living, 8,359*l.*; raw materials, 158,597*l.*; yarns and textile fabrics, 32,665*l.*; chemicals, and chemical and medicinal preparations,

* See also pp. 753-54.

100,598*l.*; and all other articles, either manufactured or partly manufactured, 25,899*l.*

Under "yarns and textile fabrics" there is an increase in value of the exports of cotton piece-goods, amounting to 203,884*l.*, but a diminution of 148,511*l.* in worsted tissues and certain decreases under other headings, make a net decrease of 32,665*l.* in this class.

Iron unwrought and wrought shows an improvement to the extent of 357,895*l.* and steam engines of 63,782*l.* Other slight increases in some articles are almost balanced by decreases in other, so that the net increase in the value of exported British and Irish produce and manufactures amounts to 468,183*l.*

Taking now the figures for the five months ending 31st May 1896, as compared with the results for the corresponding period of 1895, it is seen that, as regards imports, all classes show a decided improvement, except articles of food and drink (dutiable), raw materials for textile manufactures, and parcel post.

In spite of the recent fall in the value of the imports of articles of food and drink (duty free), the total value for the five months ended 31st May 1896 is still 2,619,474*l.* in excess of what it was for a corresponding period of 1895; while manufactured articles show a still greater difference in favour of the present year, viz., 3,378,863*l.* and the increase in the value of the imports of animals, living (for food) metals, and raw materials for sundry industries and manufactures in each case exceeds a million pounds.

With regard to the exports for the period of five months referred to, all classes show an advance, notably, yarns and textile fabrics, 3,446,306*l.*; metals and articles manufactured therefrom (except machinery), 2,733,838*l.*; and all other articles, either manufactured or partly manufactured, 1,404,876*l.*

It may be also noted, that though the quantity of cotton yarn and twist exported during the period of five months referred to amounted to 102,572,200 lbs. as compared with 111,280,500 lbs. for the corresponding period of 1895, a decrease of 8,708,300 lbs., yet the value in 1896 was 4,159,690*l.*, as compared with 3,934,253*l.* in 1895, or an increase of 225,437*l.*

In like manner cotton piece-goods, grey or unbleached, decreased in quantity 37,350,300 yards, but increased in value 389,215*l.*, and cotton piece-goods, bleached, decreased in quantity 27,165,600 yards, but increased in value 114,537*l.*, during the two five monthly periods of 1896 and 1895.

III.—THE PRODUCTION OF CIDER IN FRANCE.*

The year 1895 has been a good one for cider manufacturers in France, the amount produced having risen to 25,586,514 hectolitres, or an increase of 11,803,785 hectolitres over the average of the last 10 years, that is, also including the year 1893, which was quite an exceptional one, the production then having attained the extraordinary total of 31,600,000 hectolitres. The most characteristic feature of the year 1895 is, however, not so much the total production as the sale of apples. German agents have been literally emptying Normandy and Brittany of fruit, no longer by vanloads, but by entire trains. Official statistics are silent on this point; but it will be shown later on what use the Germans have made of these millions of apples which they have manufactured into sparkling cider and "champagne." It may not be devoid of interest to glance at the average production of cider according to the various departments. *Ille-et-Vilaine* leads with an average of 2,520,658 hectolitres; in certain years it reached 6 millions: *Calvados* follows with 1,320,890 hectolitres; *La Manche*, 1,170,520 hectolitres; *Cotes du-Nord*, 1,069,274; *Orne*, 1,033,388; *Seine-Inférieure*, 933,388; *Morbihan*, 825,202; and *Eure*, 932,552 hectolitres; and *Mayenne*, *Sarthe*, *Oise*, and *Loire Inférieure* each show an average production of from 339,000 to 539,000 hectolitres.

The country between *Vimontiers* and *Argentan* and the district round within a radius of 20 leagues is where the best kinds of cider are produced; and for the peasants of these districts the apple, when there is an abundant supply, is an important article. In good years, and when attention is paid to the cultivation, the apples alone will pay the rent, and, notwithstanding the irregularity of the harvests, it is rare that the Norman peasant cannot turn out a sufficient quantity of cider for his family and household, who together consume no inconsiderable amount.

It is as the result of long and patient experiment that certain parts of France have become the premier cider-producers of the world. The production of this beverage dates back to ancient history. The Romans, as also, indeed, the Greeks and Egyptians, had their wines made from apples and pears, and the contemporaries of *Pliny* and *Diodorus Siculus* valued highly the juice of the apples from *Gaul*. In the sixth century cider had become a common drink, especially amongst the lower classes, though not exclusively with them, for *St. Radegonde*, Queen of France, held it in high esteem, and had it served daily at her own table. The *Biscayens* also knew cider, and obtained grafts of various kinds of apples, the juice of which was found superior to similar productions of the period.

* Based on an article by *Georges Michel* in the "*Economiste Français*."

Normandy was the first to popularise cider as a drink. Thence it spread to Picardy, Flanders, Germany, Russia, England, and finally to America. Ever since the fourteenth century the Norman juices have been the most celebrated, and even to-day her ciders are considered the finest in the world.

The nature of the soil has doubtless much to do with the excellence of French ciders, but science and art have also lent their aid, and it is only by dint of care and repeated analysis that producers have arrived at this point of perfection. What strikes the attention when this question of cider is studied in its details is the incredible variety of apples concerned in its production. M. Fabius de Champoille, who has just published a complete essay on the cultivation of the apple, and the manufacture of cider ("*Comment s'obtient le bon cidre.*" *Paris. Société d'editions scientifiques*), has drawn up a catalogue which enumerates several hundred varieties of apples. From this treatise it appears that *acid* apples (*pommes acides*) give an abundant, clear, and light juice, and the cider which is made from them is liable to turn black, and it is generally weak and difficult to keep. Sweet apples (*pommes douces*) also give a clear cider which is agreeable to the palate as long as it is new, but as soon as the sugar is absorbed it ferments and goes sour, and contracts a bitter taste which renders it unpleasant as a drink.

Another kind of sweet apple (*pommes sucrées*) give as clear a liquid as the preceding, but the presence of a quantity of alcohol in the sugar renders it stronger and better adapted for keeping. Finally, bitter or sour apples (*pommes amères ou âcres*) give the most generous cider, which is of a fine colour, and keeps well. The cider obtained from a blending of bitter or sour with sweet apples, though it keeps rather less well than the preceding, is lighter and more agreeable to drink. With regard to the time of year at which they ripen, apples are divided into three categories:—The apples of the first season ripening in the middle of September, those of the second season which ripen in October, and those of the third season, called late apples, which are gathered in the last fortnight of October or the first of November. After sugar, tannin is the most important constituent of an apple, and it plays a *rôle* of incontestable utility.

It is very difficult to state exactly what is the value of cider produced in an average year, because prices often vary according to the district and according to the quantity and quality of the production. Speaking generally, the price of cider reaches, in the department of Orne, 15 francs per hectolitre for an ordinary harvest. Certain years when apples are rare it may amount to 20 and 22 francs per hectolitre; but the average price of the whole production may be set at about 10 francs per hectolitre. Taking the output at 20 million hectolitres, the value is made out to be 200 million francs; and this is only an average, since the production for 1895 was over 25 million, and that of 1893 more than 31 million hectolitres. In this figure is not included

the production of apple spirit, which reaches an average of from 60,000 to 70,000 hectolitres.

Here, then, is an important source of public wealth, taking into consideration that the production is concentrated into a very few districts. But it may be asked if French producers know how to get the utmost possible from the resources of the soil, and if, with greater industry and care, they could not materially increase the output of the apple-orchards. The events of last year would seem to show that there is much to be done. Why should Germany, which consumes very little cider, buy up Normandy apples in such large quantities? The reason appears to be that the Germans know well how to profit by French products. They have become the suppliers of England and America, and extracting much cider from French apples by an addition of water, they are able to export annually millions of bottles from Bremen and Hamburg to London and New York. In addition, with the help of certain chemical products, a sort of sparkling sweet beverage is manufactured, which is known as "German champagne."

The manufacture of cider in Germany is not an affair of yesterday. French Consuls have frequently called the attention of the Government to the importation of French apples through the frontier towns. In 1892, M. Henri Belle, French Consul-General at Frankfort, informed the French Government that the city of Frankfort alone had received during the year 1891 1,417 waggon loads of cider apples. During the following year this movement had become so considerable that the same Consul thought fit to draw up a special report on the question, in the course of which he said:—

"Cider factories, which increase every year are now in full activity (1892). The number of workmen employed in this industry is constantly on the increase, and may now be estimated at 1,190 the yearly value of the production of this industry which contributes so much to the town's prosperity being calculated at from 12 to 13 millions of francs. Frankfort, which has five large cider factories, and 45 of less importance, consumes but a small part of its production, the principal outlets abroad for this article being, in the first place, Turkish ports, and then Athens, Damascus, Aden, Calcutta, Sydney, Cape Colony, New York, Baltimore, San Francisco, Buenos Ayres, and even Pekin. The two agencies for Frankfort cider, recently established in London, are doing excellent business."

It may be added that the cider factories of Frankfort had established at the Chicago exhibition, bars where the German product could be sampled.

At the present time France exports on an average barely 13,000 hectolitres of cider, while Germany, who hardly grows any apples at all of her own, exports 10 times as much, and at very high prices. From this point of view, syndicates for production and sale can work with the best results. The Norman producer, left alone, is powerless to create fresh outlets; he

depends upon a forcedly restricted local market, and is incapable of attacking foreign markets, and occupying himself with the sale of his products abroad. The *rôle* of a syndicate is a double one; on the one hand they create fresh outlets by organising a service of correspondents and sales abroad, and on the other hand by guaranteeing with their responsibility the quality of the products given over for consumption. Already certain well administered syndicates have been organised in several districts for the sale of cheese, fruit, wine, and cider, and their efforts have been crowned with success.

The agricultural and horticultural syndicate of Ille-et-Vilaine, for example, founded at Rennes in 1886, now numbers 1,500 members. This syndicate has for its especial object, the acting as intermediary for the purchase and sale of manures, agricultural and horticultural products, and machinery, farm animals, grain, and seeds; for facilitating the sale of farm products, chiefly grain, cider, milk, butter, fruits, seeds, farm animals and all products of cultivation. In 1893 the syndicate did business to the value of 70,000 francs in phosphates, chemical manures, forage, seeds, and agricultural machines. The Calvados Syndicate in May 1892 tried the experiment of direct sale of all agricultural products, and the idea rapidly made its way. This system consists in the despatch of samples of the products. The producer advertises in the bulletins of the various syndicates what products he has for sale, indicating the price, and, at the same time, affording the consumer a chance of himself reckoning the value of the product, by sending him a sample by parcels post. These are, however, but a series of attempts; much yet remains to be done to satisfy all the demands of producer and consumer.

It is not, however, only from the point of view of sale that the agricultural syndicates can render good service; their action may extend to the cultivation and care of the apple trees and the manufacture of cider. To orchard proprietors they could give valuable advice on the choice of species, on the quality of grafts, and on the means of destroying the parasites which, like those of the vine, devour the crops and ruin the trees; and they could indicate to those interested the best kind of apple-press and methods of manufacture, which, it is said, still leave much to be desired. In fact, an almost unlimited field of activity is here opened up to the agricultural syndicates, who have already done such good service and will do more still, if their spirit of initiative be not checked.

IV.—THE ITALIAN MACHINERY TRADE AND INTERNATIONAL COMPETITION.

The question of international competition in foreign trade has of late occupied a prominent position alike in the press, at public meetings, and at the meetings of Chambers of Commerce, and Mr. W. Percy Chapman, Her Majesty's Consul at Leghorn, in a recent report to the Foreign Office, makes the following remarks on this important subject based on observations of the course of commerce at Leghorn and elsewhere.

The most formidable competitors to English trade are everywhere acknowledged to be the Germans, and their success is said in a great measure to be due to the superior knowledge of languages possessed by their travellers. Others state that their Consuls foster and develop the trade of the fatherland to a greater extent than their British colleagues.

In this matter Mr. Consul Chapman says, "I beg to differ, and I speak with some experience, as I was permitted to represent German interests for some years in an important manufacturing district and seaport where I was Consul, and was there able to study their system. I am unaware of any resources in possession of the German Consul which are unknown to the British, and always placed freely at the disposal of his compatriots. As a solace, however, the foreign press invariably holds up the British Consul as an example, so possibly these statements are made with the laudable intention of encouraging zeal in all quarters. To quote an extreme example, the semi-official 'Pesther Lloyd' pointed to the efficiency of the British Consulate system, to which, it said, the predominating influence of English trade is chiefly due."

Another reason of German success which has been overlooked may be found in the more accommodating ways of German firms as regards conditions of payment than those usually practised by British firms. The latter as a rule demand payment upon delivery; and often a substantial advance is a *sine quâ non* upon taking the order.

With regard to machinery, for instance, probably few, if any, British firms would entertain an order for machinery subject to the condition that it shall be erected in Italy, and proved in action to respond to all the conditions of contract before payment of the value becoming due, not even were an advance obtainable on receiving the order. It is obvious that in such case the seller is to a great extent at the mercy of the buyer. A dispute may arise between the two parties as to whether the machinery fulfils all the stipulated conditions; very often, from one cause or another, machinery does not do so; or if this be not the case, a buyer not affected with scruples may try to better the conditions of his

bargain by pointing out some pretended flaw or deficiency in the material, and claiming an allowance on the strength thereof.

Should this happen, it is clear that the seller must try either to come to a compromise with the buyer, or he must resort to legal proceedings, which in Italy, as elsewhere, involve heavy expenditure, great delay, and ultimate uncertainty as to the issue. In fact, unless a very large amount were involved, it would be folly to resort to law. Yet in the past some German and Swiss firms, in their eagerness to obtain orders, have been willing to take this risk, and in some instances it is stated that disputes have arisen between buyer and seller of the nature just indicated.

Down to 1880, agriculture and manufactories in Italy, and especially manufactories of textile goods, were almost entirely dependent upon Great Britain for their machinery and plant. It was about this time that Germany manifested a considerable activity in the production and improvement of its industrial machinery, and began to make a special effort to secure Italian custom. The greater cheapness of the German articles, offered by adroit and persistent travellers thoroughly acquainted with the Italian language, at once secured for them a ready sale in Italy.

There is another point of interest and importance which helps to account, to some extent, for the diffusion of German machinery and plant in Italian factories. At the time that great attention began to be given to schools for technical education in this country, young men were sent to Germany as apprentices so as to pick up the requisite knowledge for managing industrial establishments in Italy. The fact of their being sent to Germany rather than to the principal manufacturing country can only be accounted for by the constant visits to Italy of intelligent German commercial travellers, and even professors, who, speaking Italian, were easily able to instil into all with whom they came in contact the advantages of an industrial education in Germany. It was a far-sighted policy. On the return of these Italians to their native country, many of them became factory managers or developed into factory proprietors, and, being already familiar with the German language and German methods of conducting a manufactory, they naturally turned to Germany for their supplies of industrial machinery. The cheapness of German machinery, and the easy terms of German contracts, have also, of course, something to do with the preponderance that Germany threatens to acquire in this branch of trade, but there is no doubt that had England taken the same part in the education of recent industrial Italy, English machinery and plant would not have been so easily displaced. The superiority of English articles is freely admitted by all, and there are still certain manufacturing goods in which England continues to hold the first place, such as ships' engines, spinning and weaving machinery, and agricultural implements and machines. The latter especially, from their solid and practical character, enjoy, even at present, an easy superiority over competitors.

Agricultural machines, and especially threshing machines, are coming into very general use in Tuscany, so much so that three establishments for their manufacture have at different times been set up in the great Maremma Plains. Two of them have already come to grief, and the third, according to all accounts, is working with poor results. It would seem that neither the native nor foreign article has yet succeeded in replacing agricultural machines of English make.

English exporters should direct their attention to a matter to which an almost exaggerated importance is attached in Italy. Italian manufacturers complain that English machines are not constructed with sufficient regard to economy in coal. There are no data on which to base an opinion as to whether the complaint is well-founded, but as coal is not a native product of the country, and is consequently dear, it is natural that Italian manufacturers are unusually sensitive to the expense of its consumption. If English machine makers, in any of their newer inventions, are able to point to a saving of coal, it will do much to attract Italian custom.

The rivalry of Germany in its relation to English machine makers does not seem as if it would continue a serious factor even in the near future, for the Italians themselves are devoting great attention to the production of machinery and machines, and apparently they attain at least as much success and favour as German-made articles. Steam engines, both locomotive and locomobile, ships' engines, and industrial machinery (the latter at one time almost exclusively supplied by Germany) are now successfully being made in Italy, and Italian machinery is becoming the real rival of the once indispensable English-made product. Great enterprise will be needed by English manufacturers if they wish to keep pace with the industrial development of Italy, but as it is quite possible that a community new to industrial enterprises may have defects and weak points in its system visible only to experts, they should endeavour, by means of their travellers, to discover existing deficiencies, and by showing, if possible, that their machines would obviate such weaknesses as may exist, their trade might gain a great, if only temporary benefit.—(*Foreign Office Annual Series*, No. 1700.)

V.—TRADE WITH SPANISH AMERICA.*

All European commercial houses trading with Spanish America employ clerks who are perfectly familiar with the requirements of the Spanish American tariff, and this accurate knowledge, coupled with the ability to conduct Spanish commercial correspondence, can be stated without exaggeration to be one of the chief causes of the preference given to European goods. While it is permissible to have consular invoices written in the language of the exporting country, and have Spanish translation made at the foreign Custom house of entry, it is rare that invoices of European goods are written in any language but Spanish. Such Spanish invoices, prepared in proper form as to classification for presentation to the Custom house, facilitate business. The buyer in Spanish America could certainly have the English invoice translated into Spanish, but this involves the risk of errors and misunderstanding through lack of technical or business knowledge on the part of the translator, and might also reveal business secrets. If trade with foreign countries is worth having and worth striving for, it is certainly worth proper attention. European firms dealing with Spanish America always correspond with their customers in Spanish, unless they are countrymen of the sellers.

The federal revenue in Latin America is derived from Custom duties, sale of stamps, and occupation taxes, hence the importer and merchant have to pay into the treasury the larger portion of the money needed to meet the expenses of the Federal Government in the shape of high tariffs for revenue. Custom duties, for instance, in Mexico are specific; with few exceptions all imports pay a duty by weight. Weights are divided into three classes: the gross weight, the net weight, and the legal weight. The legal weight means merchandise in the last envelope whereon duties have to be paid. Thus, it can readily be seen how great is the importance of using a packing as light as is consistent with safety in transportation. Nearly all of the larger cities in Mexico have rail facilities, and in all of them large importing houses exist and act as depôts from which the remote towns beyond the railroads are supplied. At these centres any necessary repacking will be done to meet the requirements of mule packing and mountain trails.

The marking and numbering of each packet for Latin America is very important. Old marks and numbers should be entirely erased, because there is a fine on each package having more than one mark or number. The rules for preparing consular invoices and manifests are very strict, and foreign buyers are always most particular when ordering goods to give accurate direction as to how goods should be marked, labelled, and packed. These

* Based on articles in the "American Export Monitor."

instructions often appear trivial to manufacturers, but they should remember that their own business experience may differ widely from long established practices in foreign countries. The practice of giving a part of the fines to the officer who discovers or reports the cause of the fine acts naturally as a strong temptation to ferret out all possible errors in the classification of the entry. Customs officials regard facts only and not shippers' intentions.

The metric system being the legal standard for weights and measures all over the world except in the United States, Great Britain, Turkey, Denmark, Russia, and Japan, considerable trade with other countries is lost through the persistent use of English weights and measures by manufacturers and merchants in their circulars, catalogues, and price lists, these being unintelligible and useless to foreign buyers.

The credit system prevails throughout all the Spanish American Republics. The large importing houses buy on credit; they sell to the wholesale houses on credit, and they in turn sell to the retailer on credit, and the retailer sells to the final buyer, the consumer, on credit. The banker carries all these credits, protected by a blanket mortgage on all their holdings, present and future. Very often settlements are only effected once a year, in the shape of crops coming from the small farmer to the retail house, from him back to the wholesale house, and then to the buying commission house, who consigns the same through the banker whose credit he has enjoyed. They pay an average interest of 8 per cent. per annum. Very few business houses have deposits in banks, and these are very small. It should not be overlooked that the buyer of a bill of goods upon long time must make a large cash outlay in freights, Customs duties, &c., often largely exceeding in the aggregate the first cost of goods. When a merchant finds himself unable to make his payment at maturity, extension of time is, as a rule, easily secured, with a charge of 1 per cent. interest per month. The bankruptcy laws of Spanish America are so severe that failures are more dreaded than in Europe.

VI.—THE TRADE OF DAKAR.

The “*Economiste Français*,” referring to the Consular District of Dakar, in Senegal, which is one of the oldest French Possessions in Africa, states that in 1895 the export trade of the colony considerably increased, particularly so far as regards the transmission by sea of its principal natural products. These are arachides (or *corrazos*), gum, india-rubber, palm-nuts, and palm-oil. The two first-mentioned products are cultivated or simply collected for exportation. On the other hand, there seems to have been a diminution in the exports of skins and horns. But Senegal produces gold, which comes from the district of Galam, where the raw gold has twice as much value as that which comes from the rest of the West Coast. It is proposed to establish two companies to work the productive auriferous beds of Senegal and the neighbouring countries.

At Dakar the goods chiefly imported are tobacco, glass, cutlery, made-up goods, and printed calicos. An American holds what we may call the monopoly of the importation of tobacco into Senegal. He resides at Goree, and brings his tobacco from the United States through Liverpool. The “*Economiste*” says: “Why should this trade not be conducted by a Frenchman, who would bring his tobacco from Algeria? He would thus help to found an intercolonial commerce, the importance of which would not be a whit less than that which exists between the colonies and the metropolis.” The coloured cotton goods are chiefly from Manchester, on account of the extreme cheapness of the Lancashire textiles. It may be added that these foreign cottons pay a customs surtax of 6 centimes the metre or a total duty of $8\frac{1}{2}$ centimes the metre as against $2\frac{1}{2}$ centimes only on cottons made in France, which latter, however, for a cause presently to be mentioned, enter the colony in an inferior condition.

All these branches of trade are on the increase, encouraged by the extension of the French domain as far as Timbuctoo, as well as by the notable improvement made in the means of communication between St. Louis and the Soudan—that immense Hinterland which is at this moment attracting so much attention.

The transportation of goods between Senegal and France, the French colonies, and the adjacent ports of the West African coast is almost entirely carried on by sailing vessels flying the French flag. These are also in the majority as concerns the transit of foreign articles, both entering and leaving; and it is a condition of things very satisfactory to France, for it shows that the importance of her merchant navy is progressing, and that the country is beginning to understand the advantages of securing the communications with her colonies. These advantages are great from several points of view, and, moreover, it is so much

conquered from the English merchant navy, now obliged to content itself with the transit of coal, that is, so far as Dakar is concerned.

Moreover, owing to the forethought of the French colonial authorities in Senegal, the port of Dakar has been enabled to respond to the eventual demands of a very large traffic, to such a degree that at the present moment ships drawing about seven metres can not only touch but actually unload their cargoes at the quay—a very remarkable advantage for a West African port.

The small native industries of this part of Senegal have for the most part dwindled in competition with the superiority of similar European products. This is notably the case in the textile trade, basket-work, and jewellery. The imported cotton, too, has killed the native cotton, which the Yoloofs, the finest negro race in Africa, cultivate no longer. However, they still attend to the breeding of horses, which they tattoo as they do themselves. They also grow millet and maize.

In this part of Senegal there are found at least five different plants furnishing textile fibres, which can be used in the manufacture of tissues, of string, of rope, of netting, &c.; but it would require an expert, at once a botanist and a trader to appreciate exactly what the value of these plants may be with respect to the expenditure of capital in sending them to Europe. However, the example of the alfas is, according to the "*Economiste*," encouraging for anyone who is willing to create a new industry in Senegal.

VII.—JAPAN AS A MARITIME POWER.*

(From a Japanese Point of View.)

Mr. Tsukahara, ex-chief of the Ship Control Bureau, in the course of a lecture recently delivered before the Toho Kyokai (Oriental Association), referred to the history of the development of maritime business in Japan, and stated that at the fall of the Shogunate, between 40 and 50 steamers were owned either by the Regency or by various powerful feudal barons, and about 30 of these were transferred to the Imperial Government. Subsequent increments were due chiefly to domestic or foreign troubles. Thus, the expedition to Formosa in 1874 resulted in the purchase by the Government of 12 steamers of 16,000 tons displacement; the Satsuma rebellion that broke out three years later was the cause of adding seven or eight steamers to the list; while the series of disturbances in Korea between 1882 and 1884 caused Japan to purchase about 13 steamers, of a total displacement of 18,000 tons approximately. In the late Japan-China war more than 70 vessels were purchased. Thus it will be seen that as most of the steamers now owned by Japanese have generally been purchased through stress of State necessities, and not from commercial exigencies, it has not been possible to minutely examine their seaworthiness. As to whether shipbuilding can be successfully carried on, cheapness of labour, abundance of coal, and absence of competitors alike indicate a hopeful future for this industry in Japan. It is satisfactory to see maritime business so far developed as to entirely exclude steamers flying foreign flags from the coasting trade. But that is not sufficient, as indeed was signally proved by the late war, when the requirements of the coasting trade were barely met by chartering over 40 foreign steamers. Japan must develop her maritime business so that she may not be obliged to depend on foreign countries in emergencies, for it will not always be possible to rely on aid from abroad.

Mr. Tsukahara approved, on the whole, of the Navigation Encouragement Bill, but reserved judgment on some details. Thus he regretted to see no distinction made between foreign-built vessels and those constructed in home dockyards, in the matter of subsidy, while the limit of capacity, 1,000 tons, ought to be lowered to ships above 300 tons. Again, he could not understand why provisions were not made as to competition at home, for competition not infrequently led to disastrous consequences. Absence of a fixed period as to the enforcement of the law is another serious defect, for it deters the starting of various honest maritime undertakings. Mr. Tsukahara is of opinion that vessels aggregating more than 10,000 tons will be

* "Japan Weekly Mail."

built as a result of the present law. The training of seamen was also touched upon by the lecturer. He is not satisfied with the existing methods for training officers and sailors. The Tokyo Shosen Gakko, the only place for the former, and the Nippon Kai-in Yekisai-kai, a society for affording aid to sailors, for the other. Both institutions must be considerably enlarged. He is of opinion that the work done by Japanese captains during the war must have considerably elevated their seamanship in the eyes of foreigners. Protection at sea is still defective, there being only about 90 lighthouses along the entire coasts of Japan. Again, Mr. Tsukahara cannot understand the Ship Inspection Law thus far enforced only against sailing vessels and steamers: it should be amended so as to include Japanese junks, for inspection, carried out only once every three years could be of no value. He apprehends that the effect of inspecting junks, which, by a natural tendency, are gradually decreasing, will be followed by an increase in their number. The relief of distressed seamen should be a State affair, as is the case in most Western countries. In conclusion, the lecturer briefly alluded to the shipping business of the East, pointing out that Japan should strive to develop her shipping and, at the same time, gather in her own hands the transport trade of the Orient and the South Seas. The result of the enactment of various navigation laws, some of which have secured the approval of the Diet, and the subsequent expansion of Japan's maritime business, will be to affect the resources of Hong Kong: a gradual transference of the right of coasting in China and Japan will follow, and regular services to the South Seas will afterwards be undertaken by Japan. With regard to the future site of Japan's greatest commercial centre, some people are disposed to regard Kobe as destined to enjoy this influential position, but Mr. Tsukahara is of opinion that Tokyo would be the place, and he cannot but marvel that the citizens are apparently indifferent to the important question of the reconstruction of the harbour of Tokyo.

VIII.—THE IMPORT TRADE OF JAPAN.

In a recent report to the Foreign Office, Sir Ernest Satow, Her Majesty's Minister at Tokio, deals largely with the question of the import trade of the country and its methods and prospects. He states that the whole import business of Japan may be said to divide itself very sharply into two distinct classes. The first is that in ordinary staples of trade, raw cotton, cotton yarn and piece-goods, woollen yarn and piece-goods, sugar, miscellaneous metals, clocks and watches, kerosene oil, drugs both manufacturing and medicinal, dyes, provisions, clothing, wines, stationery, &c. The second is that in the supply of the requirements (*a*) of the Government, and (*b*) of the great railway, shipping, and manufacturing joint stock companies, vessels of war, merchant steamers, war material of every kind, railway iron, metals, locomotives, machinery, &c.

With regard to this latter class of trade, while few British firms in Japan lay themselves out for it, there are still some who do, and there are several great Japanese firms in Tokio of the very highest standing which almost exclusively concern themselves with it. It is by working in association with these firms, whether British or Japanese, that British manufacturers can best hope to obtain extensive orders from either the Government or the great joint stock companies. Should manufacturers in England send out their own purely commercial representatives to Japan, they would find the hand of every merchant already resident there against them, but what English manufacturers can and most emphatically should do, and what they have on previous occasions been repeatedly advised to do, is to keep on the spot experts in the principal classes of machinery whose technical knowledge is sufficient to enable them at any time to furnish merchants with the fullest specifications for all kinds of machinery that may be in demand from time to time.

That this is so will be evident from a brief statement of the methods in which this class of business is conducted in Japan.

All contracts for the supply of Government requirements for any amount exceeding 500 dols. are thrown open to public tender. An exception is, however, made in the case of goods which are well known to be a speciality of one particular manufacturer, provided the latter is represented in Japan by a sole agent, when orders may be given without inviting competition. A further partial exception is made by the arsenals, which limit the right of tendering to a few chosen firms. The reason for the latter exception is that admission to the arsenal is frequently necessary before the tender can be drawn up, and this cannot be allowed indiscriminately. It is only right to add, however, that any respectable and well-known firm can very easily have its title placed on the list by taking the proper steps

in due time. No one is allowed to tender who has not been engaged in the particular business for at least two years and who does not pay at least 200 dols. per annum in taxes to the Japanese Government, and all tenders must be accompanied by a deposit of 5 per cent., and a further deposit of 10 per cent., either in cash or in approved bonds, has to be made by the contractor on the acceptance of his tender.

Notices inviting tenders are published in the Official Gazette, which is issued daily, and they may also frequently be seen, especially in the case of municipal contracts, on the notice boards which are affixed at the public entrances of many Japanese Government departments. Officials make their own estimates as to the approximate cost or value of whatever is required, and specifications cannot be altered when once tenders have been publicly invited, but if the original Government estimate is substantially exceeded by all the tenders that are received, a revision is made of the original estimate and new tenders invited.

The principal joint stock companies do not, as a rule, invite tenders publicly, nor is it their invariable practice to open tenders in the presence of the persons making them, though this is occasionally done. Notice of their requirements is given to persons known to be engaged in the particular trade or to represent well-known manufacturers in Europe, and it is only from them that tenders are received in each particular instance. But any new competitor, properly accredited, appearing on the field, would have no difficulty in having his name placed on the list, while in all cases much may easily be achieved by one who is energetic and watchful for the new openings that are now constantly occurring. The conditions of the tenders are very similar to those imposed by the Government, the only marked distinction being that deposits as security are seldom required. What is said below as to strict adherence to time and the precise descriptions contained in the contracts applies to these as well as to peculiarly Government contracts.

The periods specified as those within which tenders will be received are usually limited to one fortnight from the date of publication of the notice inviting them, though in some special cases, where the requirements are of an exceptionally novel or intricate nature, they are extended to six weeks. It is here that the services and advice of qualified experts on the spot would be invaluable to the tendering merchant. At present, where the period allowed is only the usual fortnight, the latter is obliged to telegraph the full specifications to England at great expense and no inconsiderable risk of misunderstanding and to obtain prices in the same way. Even when the longer period is allowed and full specifications can be sent by post to England, prices must still be obtained only by telegraph. Most of this would be avoided if an expert in Japan could give the prices at once. By being present he might also be able to acquire a much more exact idea of what is required than can be gained by study of the formal specifications, and if attentive and watchful, he would also in

many instances be able even to anticipate the issue of the public notices.

As an instance of the result of this experiment, one case only, that of American locomotives, may be mentioned. Railways owe their original introduction into Japan to English capital. The only foreign assistance that has ever been employed on them has been English, and it is from English instructors that every Japanese engaged in railway construction or management from the Director-in-Chief down to the humblest mechanic, has learnt all he knows. Originally, all railway plant of every description was obtained solely from England. But, during the last five years, fully 100 American locomotives have been introduced into Japan both for Government and private railways, and that this has been so is very largely owing, according to one of the most experienced merchants in Tokio, to the fact that the leading firm of makers in the United States has, for several years past, maintained an energetic expert in Japan, who has throughout worked in association with a mercantile firm in Tokio. He has been always able to acquire knowledge of the prospective requirements of each line, ready with full estimates the moment tenders are called for, on the spot to superintend the fitting together and ultimate delivery of each engine, present at its test trials, which are frequently competitive with those of English make, and careful to see that these are conducted under conditions that are at least not unfavourable. And all this time English builders have been entirely unrepresented in Japan, except, and that in very few cases, by ordinary mercantile agents. They have been ready enough to accept orders when sent, but have not spent a cent or taken the least trouble to seek them, and have left the safeguard of their interests at test trials to the few remaining English employés in the Government Railway Department. Patriotism and pride no doubt induce the latter to do their best to secure fair play for the product of their own country, but their first duty, always faithfully and honourably performed, is to their employers, and their interest is to consult the wishes of the latter.

In the two respects of observance of time and adherence of mathematical strictness to the precise terms of the contract, all Japanese Government departments are exacting to the utmost degree. No excuses for delay are admitted, except strikes at the seat of manufacture and *force majeure*—in either case attested by incontrovertible evidence—and fines varying from one-fifth to one-tenth per cent. per diem are rigorously enforced for any delay in the execution of the contract. And as to adherence to the precise terms of the contracts, every clause of the latter is read by the Government examining experts in its most literal sense. No discretion is exercised by them in the interpretation of their instructions, and a standard of excellence and exactness is insisted upon such as is neither expected nor demanded in any other country of the world. The smallest deviation in measurement or quality, the slightest injury, though it be such as could

in no way possibly impair the efficiency of the article, and though only detected by a very searching examination, is at once made the grounds either for a total rejection or for a substantial abatement on the contract price.

Strong illustrations have very recently been afforded of the excessive strictness of the examining experts on behalf of the purchasers in the case of a large import of cast-iron pipes, both from Belgium and Scotland, which is now taking place for the municipal waterworks for the city of Tokio.

Every individual pipe is carefully examined both inside and outside, and on the observance of the slightest surface hole, even though of no greater size than a large pin head, the pipe is at once marked as unfit for acceptance, unless at a reduced price. So searching is this examination that it is scarcely an exaggeration to say that the exactitude properly expected in a watch-case is demanded in the surface of a large iron casting. "The result," said a suffering Japanese contractor, "of our long training in high art. We have been accustomed for 300 years past to absolute flawlessness in artistic works, and our young tyros, fresh from theoretical training in the University, with little or no practical experience, cannot be made to understand that the perfection naturally looked for in a work of art is impossible in mere utilitarian work." Each pipe is then weighed and measured, lengthwise, both internally and externally, its thickness tested at four places, and finally tested by hydraulic pressure. All this is proper and fair enough if the results were construed with any approach to reason. But the ultimate object of the pipes is entirely overlooked, and infinitesimal deviations from the strictest terms of the specification, to which not a moment's consideration would be given by examiners with more practical experience, are at once fatal. Out of 2,500 tons of these pipes recently imported from Belgium, over 1,000 tons have been set apart either for entire rejection or for acceptance only at a reduction, and out of 600 tons from the most celebrated manufacturers in Scotland, imported by and at the sole risk of the principal Japanese importing merchant in Tokio, over 150 tons have been similarly dealt with on grounds so trifling, considering the ultimate object of the pipes, as to be absolutely frivolous. In the face of the decision of the examiners, the importer is entirely without remedy or resource. Their terms must be accepted, and the pipes finally rejected are left on the importer's hands, perhaps to be sold by him only as old iron.

In another instance, 450 tons out of 700 tons of galvanised iron telegraph wire, imported from Germany, were rejected because they had become very slightly oxidised in places when on board ship, and in another again a further quantity of telegraph wire was rejected because its diameter was found to be $\frac{3}{1000}$ th of an inch too large.

Illustrations such as the above might be quoted in great number. No expert's presence at the time of delivery would entirely prevent their occurrence until experience has taught the

Japanese both to place a more liberal construction on their own instructions, and to construe contracts reasonably, liberally, and in their ordinary sense, but his influence might probably often be sufficient to modify them in some degree, or at least to turn a wavering scale in the merchant's favour.

The expense of maintaining such a representative in Japan need not be very extravagant, and even the partial success which has just been suggested, in one single instance, such as that of the Belgian water pipes, might recoup it for a whole year.—(*Foreign Office Annual Series*, No. 1695.)

IX.—THE INDUSTRIES OF OSAKA AND COMPETITION WITH AUSTRALIAN PRODUCTS.

A correspondent of the "Adelaide Observer" writing in a recent number of that paper from Osaka (Japan) states that that place has become the Manchester of the Far East, and the largest commercial city in Japan, with the fine harbour and port of Kobé for its Liverpool, and a population of over 500,000.

Some idea of the magnitude of the manufacturing industry of Osaka will be formed when it is known that there are scores of factories with a capital of 50,000 yen and under, more than 30 each with a capital of over 100,000 yen, four with more than 1,000,000 yen, and one with 2,000,000 yen. These include silk, wool, cotton, hemp, jute, spinning and weaving, carpets, matches, paper, leather, glass, bricks, cement, cutlery, furniture, umbrellas, tea, sugar, iron, copper, brass, saké, soap, brushes, combs, fancy-ware, &c. It is, in fact, a great hive of activity and enterprise, in which the imitative genius and the unflagging pertinacity of the Japanese have set themselves to equal, and if possible excel, the workers and artisans of the old civilised nations of the West.

As showing how completely the Japanese have mastered cotton-spinning and weaving there are 10 mills running in Osaka, the combined capital of which amounts to 9,000,000 yen; all of them fitted up with some of the latest machinery; all of them entirely under Japanese management; all of them working full time, some of them with double shifts, and completely lit by electricity, and all of them paying handsome dividends, several of them 18 per cent. on the capital invested.

The total quantity of raw cotton imported into Japan during 1894 was 108,000,000 catties, of the value of 19,000,000 yen, of which Kobé and Osaka received 86,000,000 catties, of the value of over 15,000,000 yen. Nearly all of this latter was spun in Osaka.

There is only one woollen mill in Osaka at present, which was established about four years ago, the Mōshi Kabushiki Kaisha. The number of hands employed is about 500, 70 per cent. being girls and women. The capital is 400,000 yen. The article manufactured is a thick flannel woven in coloured patterns. The home demand is greater than the present output, but with increased producing power in this mill from steam machinery, and the establishment of other factories in the near future, Japanese flannels will before long be offered to Australian buyers. The average wages paid throughout the mill was 40 sen a day, 12 hours' work, but for some time they had been working three hours' overtime, for which extra payment was made.

Osaka manufactures a large number of cotton and jute carpets and rugs, the patterns and colours of Axminster, Turkish, and Persian carpets being imitated with wonderful effect. These are exported in large quantities to America, Europe, and Australia. Australians will, however, be interested to learn that the weaving of woollen carpets is about to be commenced, and this will create a still further demand for Australian wool. Japan can never be a producer of wool. The sheep, which is so hardy as to be able to thrive on a great variety of inferior foods, and to accustom itself to apparently hostile climatic conditions, is unable to stand Japan. Several attempts have been made to introduce sheep, but they have all ended in complete failure. The native kaya grass and a dwarf bamboo, which is intermingled with all the indigenous herbage, are both difficult to masticate, and when masticated to the extent that the sheep considers necessary, and swallowed, they irritate the intestines, the sheep fall off in condition, and die. Even if the native herbage could be rooted up and succulent grasses laid down, the available area of country is too small for large flocks. To carry a population of over 42,000,000 all the good land is required for food products. For all the finer manufactures, therefore, for which Chinese and Indian wools are too coarse, Australia is the nearest and cheapest source of supply. And, as in addition to the big woollen mills at present at work, several others will shortly be started, and woollen manufacture will equal and perhaps exceed that of cotton, a large and profitable market for Australian wool will be found in Japan.

The total quantity of cement imported into Japan in 1894 was 5,230,044 catties, of the value of 51,039 yen. But the manufacture of cement is being carried on in several parts of Japan, the limestone and other ingredients for a first-class article being abundant. The "Osaka Cement Kobushiki Kaisha," has a capital of 150,000 yen. The machinery is all of a modern type, and the appliances for testing the briquettes are of the latest scientific accuracy. The testing strain of over 400 lbs. to the square inch is readily obtained after a week's submersion. It is packed in casks of 400 lbs. weight, and is sold at 3 yen 30 sen = 7s. 2d. per cask. The home-made article is gradually monopolising the market. No export from Australia is practicable.

Bricks are largely used for buildings in all parts of Japan, and when faced with stone or cement dressings, or entirely covered, Tokio, Kioto, and Osaka can show edifices which for architectural beauty and strength are not surpassed in any Australian city. Architects and builders have always to reckon with earthwaves, earthquakes, and foundation, superstructure, and materials are selected to comply with the law of the least possible disturbance. Bricks are found to be very stable. Osaka has several large companies who manufacture sound and enduring bricks.

With regard to a recent statement that Japan had practically ousted Scandinavia from the manufacture of matches, while match factories have sprung into existence with wonderful celerity, while the woods of the country have proved admirably adapted for matches, and while much progress has been made since the first tandstickors and safeties were turned out, Japanese manufacturers have some things yet to accomplish before Sweden and Norway will retire, beaten out of the field. For one thing, the boxes must be made more substantial; for another, the process of impregnation must be more thorough; and, for another, the "glow" or continued burning of the match after the flame has gone out must be prevented. The entire process, from the splitting of the wood into the match splinters to the dipping of the matches and the labelling of the boxes, is done by women, girls, and young children, and as in all other countries so in Japan, match manufacture is one of the poorest-paid occupations of the people.

As matters stand under the existing treaties no patent is granted to a foreigner. No foreign inventor by applying through a Japanese citizen can obtain a patent except by fraud. If a patent were obtained by a false representation that the Japanese citizen was the inventor, and it were discovered that he was not, the patent would be at once cancelled. It is precisely the same with trade marks and designs—there is no registration and no protection. As might be expected, the result is that every other ware of British produce and manufacture of which the label is worth copying can be bought all over Japan of Japanese manufacture, and at a quarter to a fractional part of the cost of the original and genuine article. One of the advantages to be derived from the new treaty will be patent rights. Article 17 runs, "The subjects of each of the high contracting parties shall enjoy in the dominions and possessions of the other the same protection as native subjects in regard to patents, trade marks, and designs upon fulfilment of the formalities prescribed by law."

As no problem is too difficult, no process too arduous, no work too exhausting to the Japanese, so no industry is too trivial. Immediately the treaty ports were opened and articles of foreign manufacture came into the country Japanese manufacturers and workmen set themselves to imitate and to produce if possible at a cheaper rate the marketable wares of the foreigner. Boots and shoes, though little worn by themselves, soon attracted their attention, and leather wares generally, bone brush handles and

backs, imitation tortoise-shell, soap, &c. were seized upon as materials for payable handling. As a result Japan imported in 1894 1,653,182 catties of leather, of the value of 880,662 yen; of hides, 2,355,831 catties, of the value of 394,892 yen; of hoofs, 564,090 catties, of the value of 36,827 yen; of lard and tallow, 153,830 catties, of the value of 15,857 yen. All these are Australian products, and in Osaka, in particular, there is a market for these by-products of the slaughter-yard at better prices than can be obtained elsewhere. It is this consideration—taken in connection with the facts that these industries are only in their infancy, that they admit of immense expansion, and that the great flocks and herds of Australia are the nearest, and necessarily the cheapest source of supply, next to China and the United States,—that makes access to the market of Japan of great moment to Australian producers, and the throwing aside of the proffered treaty because of a prejudice against a coloured race, and an utterly unwarranted dread of an embarrassing influx of Japanese, would be a serious blow to Australian export trade. At present a large business is done with the United States, which also supplies Japan with nearly all the 16,000,000 catties of flour, the butter, cheese, biscuits, wines, brandy, canned fruits, jams, and canned meats she consumes. The United States have signed the treaty, which is almost identical with the treaty signed by Great Britain, and if Australia decides to reject the treaty all attempts to compete with the United States and to secure a share of the largest new market for wool, flour, animal products, wines, and fruit products, lead, and silver, that is ever likely to arise, will be in vain.

All the coins used in the empire—gold, silver, nickel, and copper—are produced at the Osaka mint. This mint is said to be one of the three most complete mints in the world. It was started under British auspices, but the last of the British employes left in 1889. The amount of silver coined in 1894 was of the value of 10,000,000 yen. The currency is on a silver basis, and the paper notes, which are issued of the denomination of 20 sen and upwards are redeemable in silver, and stand at par. Japan imported in 1894 21,307,718 oz. of silver, of which the quantity imported from Australia was only 75,310 oz. The importation of lead was 2,726,253 catties, of the value of 177,637 yen, of which Australia supplied 980,517 catties. Vigorous efforts are being made at the present time to secure a larger portion of the silver and lead trade, but the rejection of the treaty must inevitably close the ports and the mint against Australian metals, and so strike a blow at Australian mining.

It is impossible in the limits of an article to give an account of all the industries of Osaka. It may be safely said, however, that whilst they create a most desirable demand for Australian staples, their manufactures will only slightly interfere with the factory workers of Australia. Boots and shoes the Japanese are sure to make, but the small part now done by hand causes this to

be of slight moment. They will also make fancy soaps, but they are buyers of tallow. In slops and shirts some day they may prove formidable competitors to the makers of the Minorities and Bethnal Green districts. But as Australia gives no evidence of being a silk, wool, cotton, hemp, jute spinning, and weaving country, nor a manufacturer of matches, porcelain, pottery, fans, cloisonne, lacquerwares, bronzes, surgical instruments, paper, acids, alkalis, and other things, which are peculiarly Japanese, the apprehension of being flooded with cheap Japanese manufactures to the detriment of Australian workers, is as ill-founded as the dread of an invasion of Japanese coolies and artisans. No two countries by diversity of conditions could give fairer promise of mutual advantage and enrichment by reciprocal commerce.

In conclusion, it may be noted that one thing characterises the Japanese, be he student, merchant, shopkeeper, artisan, or worker, and that is intensity of application. Work, occupation, toil, is not regarded as a hardship, but as the natural condition of life. With the passing away of feudalism and the military class, by whom trade in all its branches and relations was despised, the people "have a mind to work," and to this may be ascribed the fact that there is "no complaining in their streets," no idle class, and no industrial strife.

X.—THE PROGRESS OF BRITISH GUIANA.

In a retrospective view of the progress of British Guiana the Demerara "*Argosy*" points out that the strong vitality the colony has revealed after repeated disastrous experiences, and its sustained progress from decade to decade, although with many intermediate relapses, must be regarded by those closely interested in it, as a promise that it is a colony with a future, possessing latent resources, the development of which will be attended with results that will amply repay all that has been spent upon it.

Since 1848–50, when the total abandonment of the sugar estates was regarded as a rapidly approaching calamity which no available resources could obviate, the march of progress has been more or less steady. In 1850, when a special commission appointed by the Government was sent to enquire into and report upon the state and prospects of the colony, they found whole districts fast relapsing into bush, the most ordinary marks of civilisation disappearing, and communication by land fast becoming impracticable, except on the East Coast, where the present railway had been laid down. The number of sugar estates had fallen from 230 in 1829 to 180, and the coffee and other estates from 174 to 16. The exports had fallen in the 21 years between 1829 and 1849, from 103,898,000 lbs. of sugar, 9,230,000 lbs. of coffee, and 1,596,000 lbs. of cotton to 60,811,000 lbs. of sugar and 91,000 lbs. of coffee; cotton having disappeared altogether as an article of export.

It had become apparent by that time that the labouring classes would not settle down to constant work, either as employés or as farmers, and it was for the Government either to give the sugar planters more encouragement to go on or for the colony to prepare to "shut down." The Governor of the day ranged himself on the side of progress, temporary assistance to the planters was provided, and 10 years later, the sugar cultivation had nearly doubled the figures of 1849 and 1850. The public revenue was now feeling the good effects of increased immigration with increased exports, and the Governors were encouraged to venture upon sanitary schemes for the benefit of the people—such as the regulation of village settlements, the drainage of unhealthy areas, the opening-up of roads, the extension of the steamer service, the improvement of eleemosynary institutions, and so on. Those who see the colony for the first time in these days and find that they can travel along the coast from Essequibo to Corentyne on excellent carriage roads, will find it hard to believe that less than 20 years ago the setting-in of the wet season was the signal for an announcement by the head of the police, who then had charge of the mails, that owing to the impassable state of the roads, the mail waggon had ceased to run and mails would be taken on horseback.

Occasionally a belated mail waggon would get caught in the mud and sink into a hole in the public highway, there to remain until a day or two's dry weather and special assistance could be obtained to extricate it. The coast roads are now restored to good condition, but there are miles of roads which used formerly to connect the river settlements with town still to be re-made; and—of very great importance now—roads to be made into the interior regions of the colony, which six or eight years ago were visited only by the occasional naturalist or the bushman.

In these three or four years past British Guiana has passed through an experience similar to that of 1850, although perhaps not so intense in its distress or so hopeless in its prospects; but if agriculture had stood alone as the colony's source of subsistence, the condition of the two periods would have been pretty much alike. The crisis, however, has been modified by the fact that there exists now a gold industry as a supplementary aid to the sugar industry, and although—as bad fortune will have it—the gold industry is for the moment labouring under a corresponding sinister influence, which will not be removed until the boundary question is settled, it still gives employment to a large section of the people and keeps money in circulation.

It is now becoming evident that the interior of the colony offers advantages far superior to the coast lands for the settlement of industrious villagers, whose efforts on the mud flat are paralysed by the difficulties of drainage, of water supply, and of necessary combination, the neglect of any of which means destruction to crops and loss of labour. In the interior, a labouring man could be placed in the position of a small holder in the mother country. The front lands are suitable for the growth of sugar cane, and rice and cotton; and every encouragement should be given to get the abandoned coast estates restored to cultivation by a coolie proprietary body—the East Indians taking more kindly to labour on the mud flats than the blacks do. The gold industry should be fostered, but always with the ulterior view of getting the masses to settle in the uplands, and to make farm-homesteads for themselves; and the question also arises as to whether the time has not come for placer-holders or miners to provide accommodation for the married section of their labourers, or at all events for a proportion of them. By every legitimate means the resources of the colony should be developed, and British or foreign capital induced to invest, while immigrants suitable for the land's requirements might be introduced in large numbers.

XI.—DEVELOPMENT OF THE NORTHERN TERRITORY OF SOUTH AUSTRALIA.

A writer in a recent number of the "Adelaide Observer" states that something like a genuine attempt is apparently being made to develop the resources of that portion of South Australia lying north of the 26th parallel of latitude.

The northern territory for many years past has been an incubus upon the colony of South Australia. It has been facetiously referred to as the "White Elephant," and only a few months ago the proposal was seriously made in Parliament that it should be got rid of at any price. With development at a standstill, trade paralysed, and the finances going back at an alarming rate, it was quite evident that action was necessary to relieve South Australia proper of the heavy burden she had undertaken. It was not at all a pleasant reflection that South Australia should be compelled to acknowledge that she was unable to manage the tropical portion of the province, admittedly rich in minerals, and offering special inducements for agricultural and pastoral development, and yet that seemed to be the only course open. Dwellers in the temperate zone have never been able to properly appreciate the great possibilities the tropics offer to producers, and at the same time the difficulties under which the latter have to labour. In the southern part of the colony not only has ignorance prevailed respecting the requirements of the territory, but some prejudice has prevented the adoption of any policy calculated to turn the country to good account.

Unfortunately there is no indication at present of these prejudices having been overcome, but there are at least welcome signs of a revival of confidence in the settlement. In the report which the Government geologist drew up after having spent some months inspecting the country, it was made abundantly clear, as indeed it had been by the detailed accounts of other experts, that rich mineral deposits were only awaiting the enterprise of the prospector and the miner, and the impression exists in some quarters that the gold reefs in the northern portion of Western Australia run well into the territory.

Whether this be so or not, an Englishman has come forward who has determined to spend a large sum of money in prospecting and actual development, and has entered into an agreement with the Government by which he undertakes to spend 5,000*l.* per annum in wages alone. It is said that other capitalists in Germany and elsewhere are prepared to invest large amounts under similar conditions, and leases have been taken up by local gentlemen for the purpose of prospecting and mining for coal.

Whilst all this is extremely satisfactory, inasmuch as it encourages the hope that at last a systematic attempt on intelligent lines is to be made to test the mineral resources of the

northern possessions, it should not be overlooked that other departments of production offer equal inducements. In a report by Mr. Langley J. Brackenbury on agricultural and other lands in the northern territory, the writer expresses the opinion that in some parts both soil and climate are favourable to the production of sugar, rice, tobacco, indigo, coffee, tea, and sisal hemp. Mr. Brackenbury visited the districts of the Alligator, South Alligator, Adelaide, Victoria, and Daly rivers, carefully inspecting each, and collecting samples of soil. All that he affirms in regard to the country being suitable for the cultivation of tropical products has been said over and over again. More than that, it has been proved in the most practical manner possible that such commodities as those named can be successfully raised, and yet the land remains idle, and a country offering high rewards to capital and labour is neglected. The reason for this does not require much seeking. Capitalists will not take risks and spend their money when the chances of success under prevailing conditions are considered to be exceedingly limited. Those competent to judge declare that white labour alone, or even to any material extent, cannot be utilized for the growing of tropical products. This is what the Hon. T. Playford, in his "Notes of a visit to India and the Northern Territory for information of Ministers," wrote in 1892 :—"On the question of the kind of labour required for growth of tropical products in the territory, I have come to the following conclusions:—That the territory must have cheap labour if tropical products are to be grown and sold with profit in the markets of the world. This is admitted by all those who have any special knowledge of the subject. Only tropical products can be grown in the territory. European labour is not cheap; therefore if Europeans could stand the climate tropical products could not be produced at a profit by them. It is generally admitted that Europeans cannot stand field work in tropical countries; therefore, first on the ground that European labour is not cheap, and, secondly, on the ground that the labourers cannot stand the climate, it is not possible to employ Europeans at tropical agriculture." There cannot any longer be delay in considering whether the northern territory is to be developed and made to pay its way or allowed to drift idly along, producing only huge deficits, which the taxpayers of South Australia will have to make up. The present promises of development are encouraging so far as they go, but even if the new enterprises are successful South Australia will not be relieved of financial responsibility, neither will the best possible use be made of the country. The time has gone by for the dilly-dallying policy which has been so disastrous to the northern territory.

XII.—COMMERCIAL TREATY BETWEEN GERMANY AND JAPAN.

A despatch has been received at the Foreign Office from Sir Frank Lascelles, Her Majesty's Ambassador at Berlin, enclosing copy of the treaty of commerce and navigation between Germany and Japan, which was signed at Berlin on the 4th April last, and which is based on the Anglo-Japanese treaty of 1894. The following are the more important modifications:—

By Art. III. of the German treaty, the nationals of each of the contracting parties may lease lands for residential, industrial, and commercial purposes. (The word "industrial" does not occur in the British treaty.)

Arts. VIII. and IX. of the German treaty, relating to importation of patterns and the imposition of inland duties are new, and read as follows:—

Art. VIII. Articles liable to duty imported as patterns by merchants, tradesmen, and commercial travellers of one of the contracting parties, who visit the territories of the other contracting party, will mutually be exempt from import and export duty on the understanding that, if not sold within the period determined by the law of the land, they shall be re-exported, subject to the fulfilment of the Customs regulations necessary for the re-exportation or for the return into bond. The re-exportation of patterns must be immediately guaranteed in both countries at the first place of import, by depositing the amount of the necessary Customs duties or by insurance.

Further, pattern cards and patterns in cut pieces, and samples, so long as they are only intended for use as such, will be mutually admitted free of import duty, even though imported in a different manner to that mentioned in the preceding paragraph.

Art. IX. Should an inland tax be raised on the manufacture, production, or consumption of an article within the territories of one of the contracting parties, either in the whole country or in a limited district, whether on account of the State or on account of a commune or corporation, such article may, if imported from the territories of the other contracting party, be subjected in that country or that district only to a similar and no higher or more onerous tax.

No (inland) duties of any kind may be raised if in the country or district in question similar articles are not produced or manufactured, or, if the articles are produced or manufactured there, they are not subjected to the same duty.

An additional clause is inserted in Article XVIII., facilitating the acquisition of proprietary rights by foreigners.

By Article XXI. the treaty is to take effect at the expiration of one year after the Japanese Government shall have notified

their wish to put it in force, but in no case before the 17th July 1899 (thereby bringing it into line with the earliest date for putting the Anglo-Japanese treaty in force), but with the following important modification, viz., that Article XVII., dealing with the protection of trade-marks, &c., shall come into force on the day of the exchange of ratifications, provided no other date shall be fixed beforehand by the contracting parties.

By Article XXII., the ratifications are to be exchanged at Berlin as soon as possible.

The treaty specifies the following tariff of *ad valorem* rates of duty as applicable to the articles therein enumerated, as far as they are of German produce or manufacture, upon importation into Japan.

Item.	Articles.	<i>Ad valorem</i> Rates of Duty.
		Per cent.
	Cottons :	
1	Velvets, velveteens - - - -	10
2	Tissues of all sorts not otherwise specified in the tariff, plain or mixed, with tissues of flax, hemp, or other fibre, including wool, the cotton, however, predominating -	10
	Lead :	
3	Pig, ingot, and slab - - - -	5
	Chemicals and medicines :	
4	Amorphous and phosphorous - - - -	10
5	Subnitrate of bismuth - - - -	10
6	Bromide - - - -	10
7	Quinine - - - -	8
8	Chlorate of potash - - - -	10
9	Dynamite - - - -	10
10	Arsenite of iodine of potassium - - - -	10
11	Nitrate of potash - - - -	5
12	Salicylic acid - - - -	10
	Wire :	
13	Telegraph wire - - - -	5
14	Iron and steel wire, and small rod iron and steel, not exceeding $\frac{1}{4}$ th inch English in diameter -	10
	Iron and steel :	
15	Pig and ingot - - - -	5
16	Rails - - - -	5
	Bar rod plate and sheet :	
17	Iron - - - -	7 $\frac{1}{2}$
18	Steel - - - -	7 $\frac{1}{2}$
19	Galvanized tin, flat and corrugated - - - -	10
20	Soldering tin - - - -	10
21	Tubes - - - -	10
22	Railway passenger carriage, or parts thereof - - - -	5
23	Iron nails and wire tacks - - - -	10
24	Iron screws, bolts, and nuts, and the same galvanised -	10
	Glass, window, ordinary :	
25	Uncoloured and unstained - - - -	8
26	Coloured, stained, or ground - - - -	10
	Paints and painting material :	
27	Aniline dyes - - - -	10
28	Alizarine dyes - - - -	10
29	Extract of logwood - - - -	10
30	Oil paints - - - -	10
	Yarns :	
31	Cotton - - - -	8
32	Flax, hemp, or jute for weaving - - - -	8

Item.	Articles.	<i>Ad Valorem</i> Rates of Duty.
	Wool and worsted :	Per cent.
33	For weaving - - - - -	8
34	For other purposes - - - - -	8
35	Yarns of all kinds not otherwise provided for in this tariff - - - - -	10
36	Half silk satin and silk-faced cotton satins - - - - -	10
37	Hops - - - - -	5
38	Hats, including felt hats - - - - -	10
39	Caoutchouc, manufactures of - - - - -	10
40	Flax tissues - - - - -	10
	Leather :	
41	Sole - - - - -	15
42	Other kinds - - - - -	10
43	Locomotives, and parts thereof - - - - -	5
	Milk :	
44	Condensed or dessicated - - - - -	5
45	Sterilised - - - - -	5
46	Paper, of all kinds - - - - -	10
47	Paraffin oil - - - - -	10
48	Paraffin wax - - - - -	5
49	Portland cement - - - - -	5
50	Clocks (exclusive of watches), and parts thereof - - - - -	10
	Woollen (and worsted) tissues of all kinds, plain or mixed with other materials; the wool, however, predominating :	
51	Coverings - - - - -	10
52	Flannel - - - - -	10
53	Muslin - - - - -	10
54	Cloth - - - - -	10
55	Italian cloth - - - - -	10
56	Other tissues - - - - -	10
	Zinc :—	
57	Block, pig, and slab - - - - -	5
58	Sheet - - - - -	7½
59	Refined sugar - - - - -	10

Import duties payable *ad valorem* under this tariff shall be calculated on the actual cost of the articles at the place of purchase, production, or fabrication, with the additional cost of insurance and transportation from the place of purchase, production, or fabrication, to the port of discharge, as well as commission, if any exists.

In the protocol attached to the treaty it is provided that the *ad valorem* duties established by the above tariff shall, so far as may be deemed practicable, be converted into specific duties, to be reckoned in the existing Japanese silver currency, by a supplementary convention, which shall be concluded between the two Governments as soon as possible; the medium prices, as shown by the Japanese Customs returns during the six calendar months preceding the date of the present protocol, with the addition of the cost of transportation from the place of purchase, production, or fabrication to the port of discharge, as well as commission, if any, shall be taken as the basis of such conversion.

XIII.—NEW CUSTOMS TARIFF OF MAURITIUS.

The following is a statement of the Customs duties now levied on articles imported into Mauritius:—

No.	Description of Goods.	Rate of Duty.
		Rs. c.
1	Ale, beer, porter, cider, and perry :	
	In casks - - - - - Per hectolitre	7 85
	- - - - - Per dozen bottles, each bottle not to exceed 1 litre.	1 20
	In bottles - - - - - Per dozen bottles, each bottle not to exceed 5 decilitres.	0 60
2	Almonds - - - - - Per 100 kilos.	3 85
3	Anchors and grapnels - - - - - „	1 65
4	Animals :	
	Dogs - - - - - Per head	6 05
	Asses, horses, mules - - - - - „	0 60
	Cattle and oxen - - - - - „	0 25
	Other live stock - - - - - „	0 10
5	Animal charcoal - - - - - Per 1,000 kilos.	0 30
6	Arrowroot - - - - - Per 100 kilos.	2 75
7	Asphaltum - - - - - Per 1,000 kilos.	1 40
8	Assafetida - - - - - Per 100 kilos.	7 0
9	Bacon, hams, sausages, and tongues - - - - - „	5 0
10	Bags, pockets, linen (empty) - - - - - Per 100 pockets	0 80
	Do., Vacoa and Madagascar straw (empty) - - - - - Per 100 bags	0 10
	Do., of all other descriptions (empty) - - - - - „	1 20
11	Bark - - - - - Per 1,000 kilos.	11 0
12	Barley - - - - - Per 100 kilos.	1 0
	Do., pearl - - - - - „	2 85
13	Beans - - - - - „	1 0
14	Beef and pork, salted - - - - - „	2 0
	Beer. See Ale.	
15	Biscuits (not sweetened) - - - - - „	1 10
16	Bitumen - - - - - Per 1,000 kilos.	1 40
	Blue, Prussian. See Indigo.	
17	Bran - - - - - Per 100 kilos.	0 80
18	Brassware - - - - - „	12 10
19	Bread - - - - - „	1 0
20	Bricks and tiles - - - - - Per 100 bricks or tiles	0 10
21	Brimstone or sulphur :	
	Common - - - - - Per 100 kilos.	0 70
	Refined - - - - - „	1 30
	Bungs. See Corks.	
22	Bunting - - - - - Per metre	0 15
23	Butter, margarine, or any other substance sold or used as butter - - - - - Per 100 kilos.	4 50
24	Camphor :	
	Crude - - - - - „	9 0
	Refined and in powder - - - - - „	12 50
25	Candles :	
	Paraffin - - - - - „	3 0
	Sperm - - - - - „	3 0
	Wax - - - - - „	10 0
	Composition and all other sorts - - - - - „	3 0
26	Canvas - - - - - „	3 30
27	Caoutchouc (manufactured) - - - - - „	18 0

No.	Description of Goods.	Rate of Duty.
		Rs. c.
28	Caps (percussion) - - - Per 1,000 caps	0 20
29	Capsules (botling) - - - Per 100 capsules	1 10
30	Cardamoms (common) - - - Per 100 kilos.	2 30
	Do. (small) - - - "	33 0
31	Cards, playing - - - <i>Ad valorem</i>	25 per cent.
32	Cartridges :	
	Empty - - - Per 100 cartridges	0 20
	Loaded - - - "	0 25
33	Casks, empty, old or new - - - Per cask	0 55
34	Cement - - - Per 100 kilos.	0 30
	Cider. <i>See</i> Ale.	
35	Cinnamon - - - "	3 0
36	Charcoal - - - "	0 10
37	Cheese - - - "	5 0
38	Chillies - - - "	2 0
	Chocolate. <i>See</i> Cocoa.	
39	Choorah - - - "	0 70
40	Cloves - - - "	5 0
41	Clay, pipe and fire - - - Per 1,000 kilos.	5 0
42	Coals, coke, and patent fuel - - - "	0 55
43	Cocoa and chocolate - - - Per 100 kilos.	13 20
44	Cocoanuts - - - Per 100 cocoanuts	0 05
45	Coffee - - - Per 100 kilos.	5 0
46	Coir fibre - - - "	0 85
	Coke. <i>See</i> Coals.	
47	Copper, old - - - "	3 0
	Do., sheets, bars, bolts, nails, &c. (red) - - - "	8 80
	Do., do., (yellow metal) - - - "	4 40
48	Copperah or pocnac - - - "	0 10
49	Cordage :	
	Coir - - - "	2 0
	Do., oiled - - - "	4 10
	Hemp - - - "	2 20
	Do., oiled - - - "	5 0
50	Corks and bungs - - - Per 1,000	0 55
51	Cornflour - - - Per 100 kilos.	5 0
52	Cotton :	
	Wool - - - "	1 90
	Wick - - - "	2 75
	Waste - - - "	1 95
	Cutch. <i>See</i> Gambier.	
53	Detonators - - - Per 1,000	5 0
54	Dholl - - - Per 100 kilos.	1 10
55	Dye-wood - - - Per 1,000 kilos.	11 0
56	Dynamite - - - Per 100 kilos.	14 0
57	Eggs (fresh or preserved) - - - Per 1,000	2 20
58	Felt sheathing - - - Per 100 sheets	0 60
59	Fibre, jute, raffa, and all other sorts - - - Per 100 kilos.	3 30
60	Firewood - - - Per 2 cubic metres	0 15
61	Fireworks - - - <i>Ad valorem</i>	16 50 per cent.
62	Fish :	
	Dried or salted - - - Per 100 kilos.	1 0
	Pickled - - - "	1 35
63	Fruits :	
	Dried (except cocoanuts) - - - "	2 50
	And vegetables (fresh) - - - { Per package of 50 kilos. }	0 25
64	Gambier or cutch - - - Per 100 kilos.	2 20
65	Gandia - - - Per kilo.	22 0
66	Ghee - - - Per 100 kilos.	12 0
67	Ginger, dry - - - "	1 65

No.	Description of Goods.	Rate of Duty.	
		Rs.	c.
68	Glass :		
	Window - - - - Per 100 metres	2	20
	Bottles, empty - - - - Per 100 bottles	0	10
69	Glue - - - - Per 100 kilos.	3	30
70	Gram - - - - "	1	10
	Grappnels. <i>See</i> Anchors.		
71	Grease, cart - - - - "	1	10
72	Gum :		
	Arabic - - - - "	2	20
	Copal - - - - "	8	80
73	Gunpowder :		
	Sporting - - - - "	6	0
	Blasting - - - - "	1	10
	Hams. <i>See</i> Bacon.		
74	Hay and straw - - - - "	0	15
75	Hemp, undressed - - - - "	2	75
76	Hides :		
	Raw and salted - - - - "	3	30
	Tanned - - - - "	4	95
77	Hogslard - - - - "	4	15
78	Honey - - - - Per hectolitre	3	0
79	Hops - - - - Per 100 kilos.	1	65
80	Horns - - - - Per 1,000 horns	0	85
81	Horsehair - - - - Per 100 kilos.	5	50
82	India-rubber (manufactured) - - - - Per 100 kilos.	17	60
83	Indigo, Prussian blue, ultramarine blue, and any like preparation sold or used for laundry purposes - - - - Per kilo.	1	10
84	Iron, wire netting and galvanised - - - - Per 100 kilos.	3	0
	Do., pig - - - - Per 1,000 kilos.	2	20
	Do., bars, hoops, pipes, wire, nails, galvanised sheets, ridging chains, &c. - - - - "	8	0
85	Jams - - - - Per 100 kilos.	3	50
86	Jellies - - - - "	3	50
87	Lead, sheet and pipes - - - - "	1	40
88	Lead, shot - - - - "	3	0
89	Leather, sole - - - - "	8	0
90	Lemon juice - - - - Per hectolitre	3	30
91	Lentils - - - - Per 100 kilos.	0	65
92	Lime - - - - "	0	20
93	Lime juice - - - - Per hectolitre	3	30
94	Macaroni - - - - Per 100 kilos.	3	30
95	Mace - - - - Per kilo.	0	60
96	Machinery and apparatus for the manufacture and improvement of sugar, rum, or other produce of the Colony - - - - Per 1,000 kilos.	1	10
	Do., when using the crane - - - - "	2	10
97	Maize - - - - Per 100 kilos.	0	80
98	Malt - - - - "	1	65
99	Manure of all sorts; and the following substances when imported for the purpose of being used in the preparation of manures, or of other colonial produce, or as disinfectants - - - -		
	1. Ammoniacal liquor - - - -	Per 1,000 kilos.	0 30
	2. Bones, bonedust, bone oil, and dissolved bones - - - -		
	3. Carbolic acid - - - -		
	4. Carbonate of baryte - - - -		
	5. Chloride of lime and of potassium - - - -		

No.	Description of Goods.	Rate of Duty
	Manure, of all sorts, &c.— <i>cont.</i>	Rs. c.
	6. Chloride of manganese - - - - -	Per 1,000 kilos. 0 30
	7. Chloride of soda, solution of soda - - - - -	
	8. Chloride of zinc - - - - -	
	9. Coal and wood soot - - - - -	
	10. Dried muscular flesh and dried blood - - - - -	
	11. Ether - - - - -	
	12. Fish and other substances damaged and condemned by the Customs sanitary officers as fit for manure only - - - - -	
	13. Lime, carbonate of lime, sulphate of lime or gypsum, phosphate and superphosphate of lime - - - - -	
	14. Nitrates, silicates, and carbonates of potash and soda - - - - -	
	15. Perchloride of iron - - - - -	
	16. Permanganate of potash - - - - -	
	17. Phosphate of soda - - - - -	
	18. Phosphoric acid (solid) - - - - -	
	19. Substances imported by agriculturists and to be used in the destruction of insects or other parasites prejudicial to agriculture - - - - -	
	20. Sulphate of iron - - - - -	
	21. Sulphate and muriate of ammonia and other ammoniacal salts - - - - -	
	22. Sulphate of potash, sulphate of potassium - - - - -	
	23. Sulphate of zinc - - - - -	
	24. Sulphuric acid - - - - -	
	25. Urate and sulphurated urine - - - - -	
	Margarine. <i>See</i> Butter.	
100	Marmalade - - - - -	Per 100 kilos. 3 50
101	Matches - - - - -	Per gross, on boxes containing each not more than 100 matches and a proportional duty on boxes containing more than 100 matches. 1 10
102	Mats and matting - - - - -	Per 100 kilos. 2 75
103	Molasses - - - - -	" 1 10
104	Moss - - - - -	" 10 0
105	Mustard (prepared) - - - - -	" 3 30
	Nuts :	
106	Arca - - - - -	" 2 0
	" (boiled) - - - - -	" 5 0
	Gall - - - - -	" 2 50
	Pistachio - - - - -	" 1 10
107	Walnuts - - - - -	" 3 60
	All other sorts - - - - -	" 2 50
108	Nutmegs - - - - -	" 4 50
109	Oakum - - - - -	" 3 30
110	Oatmeal - - - - -	" 1 65
111	Oats - - - - -	" 1 10

No.	Description of Goods.	Rate of Duty.
112	Oils :	Rs. c.
	Castor - - - - - Per 100 kilos.	3 30
	Gingely, mustard, and pistachio - - - - - "	2 75
	Cocoanut (when not imported from the Oil Islands) - - - - - Per hectolitre	1 65
	Do. (when imported from the Oil Islands) - - - - - "	0 06*
	Olive, in cases - - - - - { Per case not exceeding 12 litres	0 55
	Neatsfoot - - - - - { Per hectolitre	4 0
	Petroleum - - - - - "	3 30
	Colza - - - - - "	2 75
	Linseed - - - - - "	1 95
	All other sorts (except perfumed) - - - - - "	2 75
113	Opium :	
	Crude - - - - - Per kilo.	22 0
	Refined - - - - - "	44 0
114	Paper, cigarette - - - - - { (gross weight)	4 40
	Patent fuel. See Coals.	
115	Peas - - - - - Per 100 kilos.	1 0
116	Pepper :	
	White - - - - - "	16 50
	Black - - - - - "	11 0
	Perry. See Ale.	
117	Pitch - - - - - "	0 55
118	Plaster of Paris - - - - - "	4 10
	Pockets, empty. See Bags.	
119	Pollard - - - - - "	1 0
	Poonac. See Copperah.	
	Pork, salted. See Beef.	
	Porter. See Ale.	
	Prussian blue. See Indigo.	
120	Rabannahs (not exceeding 2 metres each) Per 100 pieces	1 65
121	Rattans - - - - - Per 100 kilos.	0 55
122	Rice - - - - - "	0 60
123	Rope :	
	Coir - - - - - "	2 0
	Do., oiled - - - - - "	4 10
	Hemp - - - - - "	2 20
	Do., oiled - - - - - "	5 0
124	Rosin - - - - - "	0 75
125	Sago - - - - - "	1 0
126	Salt - - - - - "	1 0
127	Sand, moulding - - - - - Per 1,000 kilos.	0 85
	Sausages. See Bacon.	
128	Seeds :	
	Aniseed - - - - - Per 100 kilos.	3 0
	Coriander - - - - - "	3 0
	Gingely, linseed, mustard, metty, millet, poppy, and all others - - - - - "	1 0
129	Shooks, per bundle containing not more than sufficient to make one barrel, cask, or tierce (<i>tierçon</i>), and a proportional duty on bundles containing more than the above quantity - - - - - Per bundle	0 55
130	Skins, sheep and goat (tanned) - - - - - Per 100 kilos.	6 60
131	Slates and stones for building and paving - - - - - Per 100	0 15
132	Soap (ordinary, not including scented) - - - - - Per 100 kilos.	1 40
133	Soda, caustic - - - - - "	0 80

* In addition to a sum of Rs. 4,000 paid under Ordinance No. 41 of 1875 by the proprietors of the Oil Islands.

No.	Description of Goods.	Rate of Duty.
		Rs. c.
134	Solder - - - - Per 100 kilos.	3 85
135	Spirits, plain or compounded, of any strength not exceeding proof according to Sykes' hydrometer, and a further proportional duty for any greater strength - - - - Per litre	1 20
136	Starch - - - - Per 100 kilos.	0 85
137	Steel, unwrought - - - - "	2 20
	Stones for building and paving. <i>See</i> Slates.	
	Straw. <i>See</i> Hay.	
138	Sugar :	
	Raw - - - - Per 100 kilos.	1 65
	Refined and sugar-candy - - - - "	5 25
139	Sulphate :	
	Of iron - - - - "	0 55
	Of copper - - - - "	1 65
	Sulphur. <i>See</i> Brimstone.	
140	Tallow - - - - "	2 20
141	Tamarinds - - - - "	0 75
142	Tapioca - - - - "	1 65
143	Tar - - - - "	0 55
144	Tea - - - - Per kilo.	0 10
	Tiles. <i>See</i> Bricks.	
145	Tin plates - - - - Per 100 kilos.	1 40
146	Tin slabs - - - - "	8 80
147	Tobacco :	
	Manufactured - - - - Per kilo.	2 45
	Unmanufactured - - - - "	1 85
	Unmanufactured, grown and produced in any of the dependencies of Mauritius, except Seychelles - - - - "	0 35
	Cigars and snuff - - - - "	3 30
	Tongues. <i>See</i> Bacon.	
148	Treacle - - - - Per 100 kilos.	1 10
149	Turmeric - - - - "	2 0
150	Turpentine - - - - Per hectolitre	2 20
151	Twines :	
	Hemp - - - - Per 100 kilos.	2 45
	All other sorts - - - - "	1 65
152	Vanilla - - - - Per kilo.	1 65
153	Varnish (all kinds) - - - - Per hectolitre	5 50
	Vegetables, fresh. <i>See</i> Fruits.	
154	Vermicelli - - - - Per 100 kilos.	3 30
155	Vinegar - - - - Per hectolitre	1 40
156	Wax :	
	Bees - - - - Per 100 kilos.	6 60
	Sealing and bottling - - - - "	2 20
157	Wheat - - - - "	0 60
158	Wheat flour - - - - "	0 90
159	Wines, in casks - - - - Per hectolitre	7 65

And a further duty of Rs. 1·20 per degree, or fraction of a degree, of alcohol and per hectolitre above 18°, according to Gay Lussac's alcohometer, contained in *Bordeaux, Provence*, and similar wines. Provided this duty do not affect wines shipped on or before the 15th of December 1895. (*See* Article 7 of the Ordinance.)

No.	Description of Goods.	Rate of Duty.
159	Wines— <i>cont.</i>	Rs. c.
	In bottles - - - { Per dozen bottles, each bottle not to exceed 1 litre	1 50
	- - - { Per dozen half-bottles, each half-bottle not to exceed 5 decilitres - - -	0 75
	Do. Sparkling : Per bottle, not exceeding 1 litre	0 10
	Champagne and others - - - { Per half - bottle, not exceeding 5 decilitres -	0 05
160	All goods, wares, and merchandise not otherwise charged with duty, and not mentioned above, or not specially exempted, shall be liable to an <i>ad valorem</i> duty of - - - - -	10 per cent.

Exemptions.

No.	Description of Goods.
1	Ballast, when the same consists of sand or stone.
2	Glass bottles imported full (except fancy bottles or decanters).
3	Instruments for regimental bands.
4	Ice.
5	School materials for the use of free schools.
6	Articles imported for the use of His Excellency the Governor.
7	Articles of civil, naval, and military uniform, intended for the personal use of the importer.
8	Provisions and stores of every description imported or supplied from bond for the Colonial Government, or, under special authority from the Governor, for the use of ships of war of foreign nations.
9	Wearing apparel, luggage, or any instrument intended for professional use, if it be the property of a person coming to the colony, and if it arrives within <i>three months</i> before or after the arrival of such person.
10	All goods upon which the full amount of duty shall have been paid on their first importation into Mauritius, legally exported hence and afterwards returned: Provided such goods shall be returned within three years from the date of their exportation, and it be proved, to the satisfaction of the collector of Customs, that they are the identical goods exported from Mauritius; and provided the property of such goods continue in the person by whom or on whose account the same were exported.
11	Objects and specimens (animal, mineral, and vegetable) illustrative of natural history, including live plants and vegetable productions connected with the study of botany.
12	Animals and goods (except oil, spirits, and tobacco), the produce of any of the dependencies of Mauritius other than Seychelles.
13	Books and music.
14	Coin and bullion.
15	Leeches.
16	Seeds intended for agricultural and horticultural purposes.
17	Poultry.
18	Goods imported into Mauritius by the proper military authorities for the public use of Her Majesty's land forces (Ordinance 9 of 1887).
19	Goods in transit transhipped direct from vessel to vessel.

Prohibitions.

Vine plants affected with any disease, or brought from a place beyond the limits of the island where any disease of vine plants is existing or is supposed to exist or may hereafter exist (Ordinance 14 of 1882 and Proc. No. 9 of 1888).

Base or counterfeit coin (Ordinance 28 of 1892, Schedule II.).

Articles of foreign manufacture, and any packages of such articles, bearing any names, brands, or marks, being or purporting to be the names, brands, or marks of manufacturers resident in the United Kingdom.

Indecent or obscene prints, paintings, books, cards, lithographic or other engravings, or any other indecent or obscene articles.

Infected cattle, sheep, or other animals.

Cast-off clothes (Ordinance No. 22 of 1883).

Goods referred to in Article 14 of the Merchandise Marks Ordinance, 1888.

Restrictions.

The shell of the *tortue de mer* or "green turtle," known as "*kahouanne*," and turtle oil cannot be landed, removed, or imported into Mauritius from any of its dependencies, except in virtue of a special permit issued by the Collector of Customs in Mauritius (Ordinance No. 21 of 1871).

NOTE.—If any goods are imported or brought into Mauritius contrary to any of the prohibitions or restrictions mentioned in the above tables, the same will be forfeited.

XIV.—TARIFF CHANGES AND CUSTOMS REGULATIONS.

RUSSIA.

Regulations for the Importation of Plants.

A despatch, dated 10th (22nd) May last, has been received at the Foreign Office from M. P. Lessar, the Russian Chargé d'Affaires in London, enclosing copy of a translation of the regulations drawn up by the Russian Government with regard to the importation of plants into Russia for the due prevention of the introduction of phylloxera.

France, Italy, Austria, China, Japan, India, and the isles of the Indian Ocean are added to the list of countries from which plants may be imported into Russia. Such plants must be accompanied by a certificate of the local authority declaring that no instance of phylloxera is known within a radius of a kilometre from where the plants in question originate.

According to the "Journal de St. Petersbourg" the number of Custom-houses for the entry of plants will be increased by the addition of the Baltic Ports and that of Novorossiisk in the Black Sea, and the Customs visit, hitherto only permitted at the frontier, may henceforth be carried out at the destination of the goods, if there be a Customs-house.

Free Importation to the Mouths of the Obi and Yenissei Rivers.

A despatch, dated 11th May last, has been received at the Foreign Office from Sir Nicholas O'Connor, Her Majesty's Ambassador at St. Petersburg, transmitting the substance of an Imperial Ukase which appeared in the "Official Gazette" of the 26th April

8th May 1896, ordaining that for the next three years, goods may be imported, free of duty, to the mouths of the Obi and Yenissei rivers.

Such importation is, however, subject to the following conditions :—

1. That it shall be confined to certain articles and in limited quantities to be determined by the Minister of Finance, in agreement with the Minister of Agriculture and Imperial Domains.

2. That the examination and landing of the goods allowed to be imported free of duty shall be effected at points at the mouths of the Yenissei and Obi to be fixed by the Governor-General of Irkutsk and the Governor of Tobolsk.

Tariff Modifications.

Note.—Poud = 36 lbs. avoirdupois. Gold rouble = 3s. 2d.

The Russian Department of Customs under order, dated the 7th (19th) May 1896, has notified the following classification of goods under the Russian Customs tariff:—

1. Glass powder (sparkling flakes of thin white glass or of glass coloured in the mass) approaching in character to enamel to be cleared under section 68. Duty, 3 roubles gold per poud.

2. Evaporating bowls of copper, lined with tin, used for manufacturing processes, to be cleared as worked copper ware under section 149. Duty, 4 roubles 32 copecks gold per poud.

3. Forks of aluminium, to be cleared under point 1 of section 158. Duty, 13 roubles 60 copecks gold per poud.

4. Small planchettes and inner frames, paper faced with designs glued on them, the designs being reproduced by typographical, lithographical, photographical, and phototypical processes, to be afterwards printed in completely by hand, to be cleared under point 1 of section 178. Duty, free.

5. Cotton chenille worked on weaving frames (not trimming finished) to be cleared under section 189. Duty, 60 copecks gold per Russian pound.

6. Woollen tissues made of variegated yarn to be cleared as unprinted tissues under section 199. Duty, 1 rouble 5 copecks gold per Russian pound.

7. Copper caps, specially intended for mining work and not for military-technical purposes, to be cleared under letter *b* of section 220. Duty, 3 roubles gold per poud gross.

8. Wool waste in the shape of cuttings or clippings of yarn, cords, and tissues, to be cleared under section 3 of the tariff of exportable goods. Duty, 30 copecks gold per poud.

SWEDEN.

Tariff Modifications.

A despatch, dated May 9 last, has been received at the Foreign Office from Mr. G. E. Welby, Her Majesty's Secretary of Legation at Stockholm, reporting the following changes in the Swedish tariff which have just been sanctioned by the Chambers.

Dextrine or Dextrine Gum. (Dextrin och dextrin gummi).—The increase of the present duty of 17 öre per kilog. to 20 öre per kilog.

Pianos.—The present duties of 150 kroner on upright and square pianos, and of 200 kroner on grand pianos to be extended to second-hand pianos.

Cartridges, loaded and unloaded. (Patroner laddade och oladdade).—A uniform duty of 35 öre per kilog.

Umbrella and Parasol Frames. (Poraplyer och parasoll stälningar sammansatta.)—The increase of the present duty of 50 öre per kilog. to 80 öre per kilog.

These new duties will come into force on the 1st January next, unless the Government should in the meanwhile decide otherwise.

A further despatch, dated the 13th May last, notifies that the following changes in the Swedish tariff have been sanctioned by the Chambers:—

Stone for lithographic purposes, grindstones and millstones, hitherto charged an *ad valorem* duty of 10 per cent. under No. 362 of the tariff, to be admitted free.

Platinum.—Certain articles used for technical purposes in which platinum is exclusively employed, hitherto charged an *ad valorem* duty of 10 per cent. under No. 362 of the tariff, to be admitted free under No. 428.

A communication has been received at the Foreign Office to the effect that the above charges will, probably, shortly come into effect.

GERMANY.

The Tariff of the North Sea Canal.

A despatch, dated 20th May last, has been received at the Foreign Office from Sir Frank C. Lascelles, Her Majesty's Ambassador at Berlin, stating that a Bill has been read a third time in the Reichstag, extending to the 30th of September 1899, the period within which the Emperor, in consultation with the Bundesrath, has the right to fix the tariff of the North Sea Canal.

The Minister of the Interior stated in the course of the debate that a conference of experts had been called to consider the question of the tariff. Personally he was not in favour of lowering the dues at present. To a further question he replied that the Government was unwilling to institute any system of differential treatment, and that all nations should be treated alike.

New Sugar Bill.

The new law for the amendment of sugar taxation in Germany came into force on the 1st June. The clauses in it which are of principal interest and novelty are those which deal with export bounties. Clause 77 of Article I. reads:—

In case of the export of sugar, or of its being stored in a public or private warehouse under official bonding in a quantity of at least 500 kilograms an export bounty will be paid, which will be for each 100 kilograms.

(a.) For raw sugar of at least 90 per cent. sugar contents and refined sugar of under 98 per cent., but at least 90 per cent. sugar contents, 2.50 marks.

(b.) For candy and sugar in white, full hard lumps, blocks, bars, rods, or cubes, or in white, hard, transparent crystals of at least 99½ per cent. sugar contents, all these sugars being also, after pulverisation under Custom-house inspection, 3·55 marks.

(c.) For all other sugars of at least 98 per cent. sugar contents, 3·00 marks.

The Bundesrath reserves the right to extend the export bounty to certain goods containing sugar.

Article II. raises the duty on raw sugar intended for home consumption, from 18 marks per 100 kilos., as it was fixed by the law of 31 May 1891, to 20 marks.

Clause 65 of Article I. introduces the following production-tax :—

“ On the sugar produced from a sugar factory for entry into the Custom-house there will be charged an addition to the sugar duty (a production tax), which will be, on each 100 kilos. of raw sugar produced—

	Marks.
Up to 4,000,000 kilos. inclusive	- - 0·10
From over 4,000,000 up to 5,000,000	- - 0·125
From over 5,000,000 up to 6,000,000	- - 0·15

and so on, rising with an increase of 0·025 of a mark for each million kilogrammes.

Customs Regulations at Swinemünde.

A communication, dated May 30 last, has been received from Mr. Wilfrid Powell, Her Majesty's Consul at Stettin, reporting that a new regulation was to come into force on June 1 last, with respect to clearances of foreign-going vessels in Swinemünde. The principal alteration refers particularly to outward clearance.

Paragraph 91 carries the following meaning: Hitherto, ships clearing at Stettin and passing through Swinemünde, or vessels clearing from Swinemünde, either with cargo or in ballast, had to be examined and passed by the Custom-house at Swinemünde before the pilot was permitted to proceed to sea with the vessel.

Ships coming from Stettin were obliged to moor to the quay side or anchor in the river, so that the Customs officer and his assistant could come on board and make an examination of papers and seals before clearance.

From June 1 this year an alteration has been made by order of the Minister of Finance, as follows :—

Ships coming from Stettin and passing Swinemünde are to be enabled to clear from Stettin finally; they will not be compelled to moor at the quay side in Swinemünde or to anchor in the river for the Customs officer's visit.

Ships which have cleared at Stettin and which intend to pass through Swinemünde without stopping shall, when coming in sight of Swinemünde (that is, on this side of the Kaiserfahrt), show—

(a) during day, that is from sunrise to sunset, at their after-mast head a flag of 1·6 metre length and 1 metre breadth, being

diagonally divided into halves, the upper half white, and the lower half black, so that the black shall be below and next to the mast;

(b) during the night, that is, from sunset to sunrise, at the same place as the flag, two lights, the upper white and the lower green. The lights must be constructed and placed so that their light does not show forward, but so that an equal and uninterrupted light is shown towards the stern over an arc of 12 points of the compass, namely, six points on each side. Small vessels are permitted to carry the lights between the after-mast and the shrouds.

When such vessels are close to Swinemünde they shall reduce their speed, and the Customs guard who accompanies the ship from Stettin shall be taken off in a boat, provided that he is able to leave the ship without danger. At the same time the pilot shall be placed on board ship, and after examination of the Stettin clearance, shall take the vessel to sea. Ships which have no Customs guard on board from Stettin, but the hatches of which or some provisions of which are under seal, shall clear at Swinemünde as heretofore.

BELGIUM.

Free Entry of Goods for the Brussels Exhibition.

A despatch, dated 24th May last, has been received from Sir F. Plunkett, Her Majesty's Minister at Brussels, enclosing copy of a Royal Decree which appeared in the "Moniteur Belge" of 24th May last, regulating the formalities which are to be observed in order to obtain freedom from Customs duties for goods sent to Belgium for the Brussels Exhibition of 1897.

The above may be seen on application at the Commercial Department of the Board of Trade, 7, Whitehall Gardens, between the hours of 11 and 5.

Tariff Modifications.

A despatch, dated May 24 last, has been received at the Foreign Office from Sir F. Plunkett, enclosing extract from the "Moniteur Belge," containing copy of a decree which notifies that certain cardboard accessories in the shoemaking trade, formerly classed under the category of "Mercerie" and "Quincaillerie" will henceforth be included under "Produits divers pour l'industrie," and dutiable at 5 per cent. *ad valorem*.

Regulations for the Sale of Honey.

A despatch, dated May 9 last, has been received from Sir F. Plunkett, enclosing copy of extract from the "Moniteur Belge," containing Royal Decree regulating the sale of honey, providing for the marking in a proper manner of artificial or mixed honey, and declaring certain penalties for infractions of these regulations.

Article 1 of the above decree declares that the denomination of honey (*miel*) is to be given only to the actual substance gathered by bees from flowers ; and that honey furnished by bees fed artificially (except in winter) must be distinguished by adding the name of the substance given them for food—as, for instance, sugar honey, glucose honey (*miel de sucre, miel de glucose*) or simply “mixed honey.”

Article 3 forbids the sale of (1) honey containing pollen, wax, or other matters insoluble in water in a greater proportion than 1 per cent.; (2) honey containing more than 0·5 per cent. of mineral matter (ashes); (3) honey containing insect débris; (4) spoilt honey.

Regulations concerning the Sale of Cocoa and Chocolate.

A despatch, dated 30th May last, has been received at the Foreign Office from Sir F. R. Plunkett, enclosing copy of a Royal decree which has appeared in the “*Moniteur Belge*,” modifying the existing regulations for the sale of cocoa and chocolate.

By this decree cocoa can only be sold as “pure” when none of its butter has been extracted.

After the 1st March 1897 products containing less than 35 per cent. of cocoa may not bear a label in which occurs the word “chocolat,” or any derivative of that word.

New Cargo Steamer Service between Tilbury and Ostend.

A despatch, dated 23rd May last, has been received from Sir F. R. Plunkett, enclosing copy of a Ministerial Order published in the “*Moniteur Belge*” of the 23rd May last, approving the Anglo-Belgian tariff for the transport of goods between England and Belgium *viâ* Ostend-Tilbury, which was to come into force on the 1st June 1896.

FRANCE.

Pilotage Dues at Calais.

A despatch, dated 23rd May last, has been received at the Foreign Office from the Marquis of Dufferin and Ava, Her Majesty's Ambassador at Paris, enclosing an extract from the “*Journal Officiel*,” containing the text of a Presidential Decree authorising the Chamber of Commerce at Calais to regulate the pilotage of vessels entering and leaving the port, and to levy dues for that purpose.

ITALY.

Increased Pilotage Dues at Palermo

A communication, dated 19th May last, has been received at the Foreign Office from Mr. E. M. de Garston, Her Majesty's Acting Consul at Palermo, reporting that a new scale of pilotage

dues has come into force at the port of Palermo, by which the amounts formerly payable are considerably increased.

The following is the new scale of dues :—

From	1	to	100 tons	-	-	Lire	30
"	101	"	200	"	-	"	39
"	201	"	300	"	-	"	48
"	301	"	400	"	-	"	57
"	401	"	500	"	-	"	66
"	501	"	750	"	-	"	78
"	751	"	1,000	"	-	"	90
"	1,001	"	1,500	"	-	"	105
"	1,501	"	2,000	"	-	"	120
"	2,001	tons upwards		-	-	"	130

Pilotage, both inwards and outwards, is entirely optional. The same scale applies both to steam and sailing vessels. Should a vessel only require to be piloted into the roads the fee payable is half of the above. Should the vessel subsequently require piloting into harbour the remaining half is payable.

PORTUGAL.

Tariff Modifications.

A despatch, dated 1st May last, has been received at the Foreign Office from Sir H. G. Macdonell, Her Majesty's Minister at Lisbon, transmitting copies and translation of a law, dated 27th April last, which imposes taxes on the manufacture and consumption of raw sugar, oils for food, artificial butter, clarified sugar, refined sugar, vegetable concrete oils, and any kind of candle lights, as follows :—

Art. 1. The "manufacture" and "consumption" taxes are levied on the products as specified in the law, made in the continental part of the Kingdom and in the adjacent islands, and intended for consumption in this country.

Art. 2. Similar products to those taxed in virtue of this law, coming from foreign countries and cleared for consumption in this country, will pay the same taxes on clearance, saving any express provision to the contrary.

Art. 3. The following articles are subject to the taxes on manufacture and consumption, to which reference is made in Article 1 :—

- a. Raw sugar.
- b. Fixed liquid oils for food.
- c. Artificial butter.
- d. Sugar clarified, sugar refined, whether it be amorphous or crystallised.
- e. Vegetable concrete oils.
- f. Candles of any kind or shape for lighting purposes.

Art. 4. The taxes on the manufacture and consumption of raw sugar, artificial butters, and fixed liquid oils for food will continue

to be the same as laid down in the laws of June 23rd 1888, and of 12th April and 10th May 1892, subject to the modifications derived from the provisions set forth in this law.

Sec. 1. For the intents and purposes of the 2nd article, all fixed liquid oils taxed in Article 69 of the General Customs Tariff shall be considered to be similar to the fixed oils used for food, and as regard butters the same rule will apply not only to all butters but also to any imitations.

Sec. 2. The provisions of the decree of 31st December 1895 with respect to the island of Madeira are maintained.

Art. 5. A tax of 15 reis (0·80*d.*) will be levied per kilogramme on sugar, either clarified or refined.

Art. 6. The recovery of the manufacture or consumption taxes referred to in the preceding article will take place either on the clearance of the same or on the withdrawal of the raw sugar from the manufactories.

Art. 7. For the intents and purposes of the provision set forth in Article 2 of this law, sugar, clarified or refined, amorphous, ground or crystallised, glucosis, and any sugars, in any condition, that can be directly used for consumption, are considered to be similar products to those referred to in the preceding article.

Molasses containing more than 75 per cent. of total sugar will pay an addition to the tariff duty at the rate of 10 reis (0·53*d.*) per each 10 per cent. of saccharosis (by polarimetrical determination), without any deduction whatever, but neglecting any fractions of a saccharimetrical degree.

Art. 8. Vegetable concrete oils made in this country for home consumption are subject to a manufacture and consumption tax at the rate of 8 reis (0·42*d.*) per kilogramme net weight.

Sec. 1. The tax referred to in this article will be computed per the indivisible weight of 100 kilogrammes, on the raw materials entering the manufactory, at the rate of 40 per cent. for palm oil, 70 per cent. for cocoa and coprah oils, and of 50 per cent. for all other concrete oils on the net weight of the respective raw materials.

Sec. 2. The recovery of this tax will be effected on the clearance of the article; the entrance of the said raw materials into the manufactories being dependent upon the exhibition of the proper receipt of the amount of tax paid.

Sec. 3. For the intents and purposes of the provisions set forth in Article 2 of this law, all fatty oils of a vegetable nature, which are usually in a solid or pasty condition in this climate—with the exception of unrefined palm-oil—will be considered products similar to those upon which the tax of 8 reis (0·42*d.*) per kilogramme is levied.

Art. 9. Candle lights, of any shape or kind, for consumption in this country, and manufactured here, are to pay a manufacture and consumption tax of 40 reis (2·13*d.*) per kilogramme net weight, recovered in the same manner as laid down for the fixed oils for food in the law of 10th May 1892.

All manufactured articles of any kind or shape for lighting purposes and composed of solid fatty matters, as well as stearine in paste or any analogous composition, susceptible of being used at once for lighting purposes, will be considered to be similar products to the candle lights referred to in this article for the intents and purposes of the second article of this law.

Art. 10. Any merchandise liable to the payment of the tax on manufacture or consumption, that may be exported or lodged in any general stores or dépôts will be exempted from the payment of the tax in question.

Sec. 1. All merchandise referred to in this article, if once exported, will, on re-importation, be considered for all intents and purposes as foreign merchandise, unless it should be proved that it has not gone out of the stores of the Custom-house of destination, in which case it will have to pay the tax on manufacture and consumption on entry of the same, irrespective of the state or condition in which such merchandise is to be found.

Sec. 2. The provision contained in the latter part of the preceding paragraph is applicable to any merchandise which may have been lodged in any general store or dépôt and then entered for consumption.

Art. 11. With a view to the proper fiscal control of the taxes imposed by this law, the Government are at liberty, if it should be necessary, to appoint permanent fiscal officers, or to order direct inquiries to be made in order to ascertain the amount of production.

Sec. 1. The total amount of the tax corresponding to the amount of production, as ascertained by direct inquiry, is to be paid annually at the end of every quarter.

Sec. 2. The expenses on account of fiscal supervision will be defrayed by the manufactory.

Art. 12. The Government are empowered to draw up the necessary regulations for carrying this law into effect in the form of a regular code containing the whole of the regulations concerning taxes on manufacture and consumption.

Art. 13. No exemptions from the payment of the taxes on manufacture and consumption are admissible except those set forth in this law.

Art. 14. Any sugar lying on the 27th of March 1896, inclusive, in any Custom-house in Portugal or in the adjacent islands, or else on board any ship bound to any Portuguese port, or else at sea on its way thereto, will be liable to the payment of the tax imposed in this law should its clearance not be applied for up to the 30th of June of this year inclusive.

Art. 15. Any laws contrary thereto are hereby revoked.

Regulation for Provisional Imports.

A despatch, dated the 9th May last, has been received at the Foreign Office from Sir H. G. Macdonell, stating that the "Official Journal," of the 8th May, published a Royal Decree,

dated the 30th April, in virtue of which all' provisional imports are allowed the precise term of one year within which they must be re-exported, or else the sums deposited as security for the re-exportation of the same will be entered as public receipts.

Differential Treatment in favour of Portuguese Shipping.

A despatch, dated 25th May last, has been received at the Foreign Office from Sir H. G. Macdonell transmitting copy and a translation of a law of the 21st May, by virtue of which has been established a differential treatment in favour of Portuguese shipping in Portugal and in her Colonies, as follows:—

1. The duty on cargo (Law 16th September 1890) is reduced to one-half.
2. The importation of sailing vessels in a seaworthy state of the tonnage of more than 300 cubic metres is free of duty.
3. All Portuguese ships on long voyages will carry the Portuguese Post Office mails and parcels, and *ipso facto* will be entitled to the privileges in Nos. 1–7 inclusive of Article 76 of the law of 7th July 1880.
4. A drawback will be granted for the total amount of duty on coal supplied to Portuguese steamers.
5. Portuguese consular officials are not to levy a higher fee than 10 reis (0·53*d.*) per net ton of any Portuguese vessel, as per Moorson's system of admeasurement, either bound to or calling at any Portuguese port.

Alteration in the Duty on Paraffin.

A despatch, dated 25th May last, has been received from Sir H. G. Macdonell, enclosing copy and translation of a new law regulating the import duty on paraffin, as follows:—

Art. 1. The words "paraffin purified" are struck out from the text of Article 149 of the Import Customs Tariff for Portugal and the adjacent islands, and these words are inserted in Article 99 of the said tariff, so that paraffin, either purified or not, presented for clearance after the publication of this law, will have to pay an import duty of 2 reis. (0·10*d.*) per kilogramme.

Art. 2. Any laws contrary thereto are hereby revoked.

Commercial Declaration between Holland and Portugal.

A despatch, dated 27th May last, has been received at the Foreign Office from Sir H. G. Macdonell, Her Majesty's Minister at Lisbon, stating that the ratifications of the Dutch-Portuguese Commercial Declaration, signed at Lisbon 5th July 1894, were exchanged at the Hague on the 22nd May last, and it was then stipulated that the Declaration should come into force on 8th June 1896.

SPAIN.

Modification of Customs Tariff.

A despatch, dated 13th May last, has been received from Sir H. Drummond Wolff, Her Majesty's Ambassador at Madrid, enclosing copy and translation of a Royal Decree which has been published in the "Official Gazette," extending the provisions of Article 2 of the Royal Order of November 21, 1895, to the articles specified. The following is a translation of this decree, which is applicable only to the Custom-house at Barcelona.

Sole Article.—Article 2 of the Royal Decree of November 21 last, shall, in so far as relates to goods classed under Nos. 5, 251, and 252 of the import tariff in force, which respectively comprise stones and earths employed in industry, arts, and building, and not included under other numbers, be amplified as follows:—Cement, lime, and plaster, and guano and other natural or artificial manures, shall pay at the rate of 5 centimes per 100 kilogrammes the duty established for the purpose of constructing the new Custom-house at Barcelona.

Sir H. Drummond Wolff also forwards translation, of which the following is a copy, of a Royal Order relative to the classification in the tariff of medicated cotton.

Royal Order.

His Majesty the King, &c., &c. having heard the opinion of his Direction-General, and in conformity with information received from the Council of Customs and Customs tariffs has been pleased to order that a portion of the change solicited be carried into effect, and that the clause which in the repertory of the Customs tariff refers to carbolised cotton, be modified as follows:—Carbolised and salicylic cotton, and cotton impregnated with iodoform, or the like prepared with any other substance and destined for medical purposes, shall be dutiable according to No. 119; whereas cotton, raw, washed, desiccated, or sterilised, shall be classed as heretofore (No. 129).

Certificates of Origin.

With reference to a notice on page 572 of the "Board of Trade Journal" for May last concerning certificates of origin for Spain, a despatch, dated 31st May last, has been received from Sir G. F. Bonham, Her Majesty's Chargé d'Affaires, enclosing copy and translation of a note received from the Spanish Government containing the official confirmation of the information published in the above note.

ROUMANIA.

Export and Import Dues at Roumanian Ports.

A despatch, dated 13th May last, has been received at the Foreign Office from Sir G. H. Wyndham, Her Majesty's

Minister at Bucharest, reporting that a Bill has been voted by the Legislative Chambers which was laid before them by Monsieur Stoicesco, the Minister of Public Works, for the application to the maritime ports of Roumania, viz., Constantza and Mangalia, of a half per cent. tax on exports and imports, such as is already levied at the Danube ports. This measure, it is stated, is necessitated by the works to be undertaken at the ports of Constantza and Mangalia, and to protect the Danubian ports, and especially Braila and Galatz, from any commercial harm which might come to them since the opening of the bridge over the Danube at Cernavoda.

With reference to the above, a further despatch, dated 21st May last, has been received at the Foreign Office from Sir G. H. Wyndham, stating that the tariff in force for levying the half per cent. tax at the Danube ports will also apply to the maritime ports, and that there seems to be no prospect of this tax being abolished.

TURKEY.

Import Dues on Coal.

The "Bulletin Commercial" (Brussels) reports that it has received a communication from the Belgian Embassy at Constantinople to the effect that there had lately arisen a question of establishing on the Bosphorus a series of dépôts for foreign coal.

The Council of State having also looked into the affair, it was recognised that the establishment of dépôts on the Bosphorus was the cause of protests on the part of the inhabitants. The idea, therefore, will be abandoned for the present, and the existing dépôts will be maintained until such a time as it will be possible to organise, as in other countries, floating stores where colliers will load or discharge according to their requirements. The actual dépôts, will, however, be placed under the permanent control of Custom-house officers, who will not permit the storage of any other goods.

Foreign coal, the free entry of which is not granted according to present arrangements, will continue to pay a uniform import duty of 8 per cent. *ad valorem*, 7 per cent. of which will be re-imbursed in case of re-exportation. Moreover, it has been agreed that the duty of 8 per cent. will not be leviable from the moment of the arrival of the coal, but only 1 per cent., and the agents will have the right of delivering the coal to ships without the payment of other dues; the permit to unload will be also handed over without charge. The remaining 7 per cent. will only be exacted on coal for internal consumption.

EGYPT.

New Tax on Shipping at Alexandria.

A despatch, dated 5th April last, has been received at the Foreign Office from Lord Cromer, Her Majesty's Agent and Consul-General at Cairo, with reference to a proposed tax on shipping at Alexandria.

Lord Cromer, in his despatch, states that, by a Khedival Decree, dated the 7th January 1890, which obtained the consent of the Powers, it was determined that, as soon as the new pass into Alexandria harbour had been enlarged to a breadth of 300 feet and to a depth of 30 feet, a special tax on shipping entering and leaving the port of one millième and a quarter (5 paras) should be levied on all vessels except ships of war, until the capital sum of £E. 33,000, with interest calculated at 5 per cent., should be reimbursed to the Egyptian Government, the new pass being at the same time lighted at the Government expense, and the present pilotage dues being suppressed.

According to a decree, dated the 2nd April, and published in the "Official Gazette" of April 4th, 1896, it is notified that the new pass being now open to navigation under the conditions specified, the special tax of one millième and a quarter was to be levied on and after the 1st of May 1896, on which date the decree of April 27th, 1881, respecting the pilotage dues, was to cease to have effect.

UNITED STATES.

Customs Decisions.

The following decisions relating to the classification of articles in the Customs tariff and the application of the Customs laws of the United States have recently been given by the Customs authorities in that country:—

Cotton curtain trimmings consisting of pieces of white cotton cloth, 5 inches wide and about 5 yards long, one edge finished in double-fold cotton cloth, in various colours, about $\frac{1}{2}$ inch in width, and attached by hemstitching, are dutiable at 60 per cent. *ad valorem*, under paragraph 373, Act of October 1, 1890.

Lace-bordered handkerchiefs are entitled to entry at 40 per cent. *ad valorem*, under paragraph 258.

Piano wire is assessed for duty at 35 per cent. *ad valorem*, under the provision of paragraph 177 for articles or wares composed wholly of metal, and not as tinsel wire.

Domestic coverings (carboys, iron drums, &c.) containing foreign goods, if of American manufacture, are free of duty as American manufacture, under paragraph 387.

Lanoline is dutiable at 25 per cent. *ad valorem*, under paragraph 58, as a medicinal proprietary preparation, nonalcoholic.

Rattan baskets are assessed for duty, as manufactures of wood, at 35 per cent., under paragraph 230, Act of October 1890.

Seaweed carrageen or Irish Moss is liable to duty, as sea moss, at 10 per cent., under paragraph 69.

Chemical glassware, viz., Kyeldahl flasks, Norys flasks, Leyden jars, test tubes, Chevalier's creamometers, and other vessels of glass, some with glass stoppers and others without the same, are dutiable, if holding more than 1 pint, at three-fourths of one cent per pound. Articles of the above description holding less than one pint, including chemical thermometers, are dutiable at 40 per cent. *ad valorem*, under the respective provisions of paragraphs 88 and 90 of the Act of August 28, 1894, as other bottle glassware (than vials holding not more than 1 pint) and articles or vessels of glass, engraved.

Soda-ash mixed with soap, soda-ash being the component material of chief value, is dutiable, under paragraph 67, at one-fourth of one cent per pound, the rate provided for soda-ash.

Cotton-cloth-lined envelope paper is assessed for duty at 35 per cent., under paragraph 264.

Broken clay pipe stems are dutiable at 30 per cent., under paragraph 86.

Dress trimmings composed of cotton, and known commercially as crochet trimmings and also as crochet gimps, are to pay duty at 45 per cent. *ad valorem*, under paragraph 263.

Dress trimmings composed in chief value of cotton, ornamented respectively with metal and glass beads, and known commercially as beaded trimmings, are dutiable at 35 per cent., under paragraph 354.

Regulations for Unloading Ships.

A despatch, dated 22nd May last, has been received from Sir Julian Pauncefote, Her Majesty's Ambassador at Washington, enclosing copy of an Act to amend the regulations which fix the time for the unloading, in any port of the United States, of a ship's cargo imported from any foreign port, as follows:—

"Whenever any merchandise shall be imported into any port of the United States from any foreign port, in any vessel, at the expiration of 10 working days, if the vessel is less than 500 tons register, and within 15 working days if it is of 500 tons register and less than 1,000, and within 20 working days if it is of 1,000 tons register and less than 1,500, and within 25 working days if it is of 1,500 tons register and upward, not including legal holidays and days when the condition of the weather prevents the unloading of the vessel with safety to its cargo, after the time within which the report of the master of any vessel is required to be made to the collector of the district, if there is found any merchandise other than has been reported for some other district or some foreign port, the collector shall take possession thereof; but with the consent of the owner or consignee of any merchandise, or with the consent of the owner or master of the vessel in which the same may be imported, the merchandise may be taken possession of by the collector after one day's notice to the collector of

the district. All merchandise so taken shall be delivered pursuant to the order of collector of the district, for which a certificate or receipt shall be granted."

NICARAGUA.

New Law as to Consular Certificates.

With reference to the notice on page 576 of the May number of the "Board of Trade Journal" a further despatch, dated 14th April last, has been received at the Foreign Office from Mr. J. F. Roberts, Her Majesty's Chargé d'Affaires at Guatemala, enclosing copy of the new decree with regard to consular certificates for goods entering Nicaragua, copy of the form to be used in such certificates, and table of consular fees in connection with same.

Decree.

1st. No invoice will be accepted in the Custom-house without a consular certificate as to what the goods are, their weight, and whence they come.

2nd. The consular certificate referred to in the previous article will be extended by the respective Consuls of Nicaragua, in conformity with the form accompanying this law, but it will be quite indispensable for the importer or his commission agent to give a sworn declaration to the Consul in writing, that the classification and weight of goods as expressed in invoice are perfectly exact.

3rd. On examining goods in Custom-house, should the sworn declaration not exactly correspond with the merchandise, either in the class of goods or in the weight, the document will be immediately returned to the Consul that may have authorised it in order that he may take criminal proceedings against the person responsible.

4th. Besides doing as expressed in previous article, the chief officer of Custom-house will retain a legal copy of consular invoice which he will attach to the other legal documents in order to proceed according to the law of 5th January 1895, of fiscal defraudation.

5th. When from some just cause it should not be possible for the importer to present a properly certified invoice, he must request the Minister of Finance to authorise the employés in Custom-house to examine the goods by opening entirely every package, and when the necessary authorisation has been conceded, the goods will have to be examined on the exact day and hour appointed by the Ministry, and in the presence of two wholesale merchants of different localities, who will separately classify and weigh the goods and send on their closed report to the Ministry of Finance.

6th. When by order of Ministry of Finance an examination of goods by opening every package is to take place, besides the two

merchants, there should be present all the employés in Custom-house and two of the most important Government officials in the port, and on finishing the examination they will all sign an act on plain paper stating what has been done.

7th. Of this act and the policy formed by Custom-house authorities a legal copy will be sent to Ministry of Finance not later than 24 hours afterwards.

8th. The merchants called to classify goods will be paid by the Custom-house in accordance with the legal tariff of appraisers in judicial matters; but the amount will be added to the policy of goods registered, for the importer to pay in the Government Treasury.

9th. The present law is additional to the Customs Regulations or Laws for fiscal defraudations, and derogates all other dispositions that may oppose it.

Form.

The undersigned _____ of the house of _____ in this city, established in _____ of _____ nationality, who signs the preceding invoice, of which each folio is certified by the Nicaraguan Consul, and amounts to the sum of _____ solemnly declares and takes oath that the classification and weight of merchandise specified in the referred invoice is perfectly exact, and the goods as customary of this market, in which case he swears to the legitimacy of the expressed document in the form that is written, renouncing all claim that he may have had in the event of it being otherwise, and subjecting himself to the laws and tribunals of the Republic of Nicaragua

I, _____, Consul in this city for the Republic of Nicaragua, hereby certify that the foregoing declaration and protest have been given and sworn in my presence by Mr. _____, of _____ house of business in this city, residing at _____

In testimony of which I now sign and seal, giving the original to the person interested and keeping a copy signed by the same person.

the day of 189 .

Table of Consular Fees.

For invoices up to, and not exceeding	\$100	-	\$2.50
" " "	\$500	-	\$3.50
" " "	\$1,000	-	\$5.00
" " "	\$3,000	-	\$6.00
" more than \$3,000 and not exceeding	\$6,000	-	\$6.50

On every \$1,000 in excess of \$6,000	-	-	\$0.50
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Of these fees collected the proportion of 25 per cent. will be allowed the consular officer, and the other 75 per cent. will belong to the State for diplomatic expenses, and must be placed

to the order of the Minister of Finance at the end of every month.

The second part of section No. 2 in Article No. 122 of Consular Rules is hereby derogated.

UNITED STATES OF COLOMBIA.

Custom-house Regulations at Barranquilla.

According to a report of Mr. MacGregor, Her Majesty's Acting Vice-Consul at Barranquilla, the Custom-house regulations at that port are as follows :—

Each person is entitled to the entry free of duty of personal effects up to the weight of 150 kilos., including weight of trunks. All excess over this pays duty at the rate of 1 dol. 20 cts. per kilo. Colombian currency. Firearms for sporting purposes can be introduced under the general head of luggage, but not rifles or other military weapons.

The commercial traveller must present a regular invoice certified by the Colombian Consul at the port whence he comes. His sample cases are weighed, and he pays duty on them according to the national tariff.

The Government gives him four months to travel through the country, which term can be prolonged if required.

On leaving the country, if he presents the same samples with the same weight, and leaves through the same port by which he entered, he will receive back the original amount of duties which he paid on entering.

All kinds of effects, merchandise, &c. may be introduced into Colombia, excepting such as are forbidden by special law. Imports are permissible under any flag, as there are no differential duties.

All invoices must be made out in triplicate. They must be presented to the Columbian consular agent in the port of embarkation, or to his representative, to be certified by him. This certificate is necessary even though the contents of the invoice are free of import duty.

This certified invoice is indispensable, and the absence of it causes great trouble and expense to the consignee.

All packages must be numbered. The absence of this formality causes an augmentation of 5 per cent. on the duties.

There must be no discrepancy between the contents of a package and its description in the consular invoice. Such a discrepancy causes much expense and trouble.—(*Foreign Office Annual Series, No. 1721.*)

New Harbour Regulations at Colon.

A communication, dated 12th May last, has been received at the Foreign Office from Mr. Claude C. Mallet, Her Britannic Majesty's Consul at Panama, enclosing a copy of the following

harbour regulations of the Panama Railroad Company which were to come into effect on the 1st June 1896:—

1. All vessels entering the harbour of Colon will be charged light dues, and all vessels coming to wharf will be charged wharfage, in proportion to tonnage, as per printed rates.

2. No vessel will be allowed to hang at the outer buoys, as they are to be used solely for convenience in hauling in and out, and making sail.

3. Vessels will anchor in the harbour, and remain there until assigned berths by the company.

4. No chains are allowed to be used in making fast to the wharves, and vessels will be held liable for any damage done to the wharves, by unnecessary chafing, neglect, &c.

5. No coals, ashes, offal, or rubbish are to be thrown overboard by vessels at the wharves.

6. Masters of vessels will be governed by the directions of the Company in changing berths, hauling to buoys, anchoring in any part of the harbour, &c., and are requested to notify the company when wishing to haul, and also to give at least six hours' notice before leaving port.

7. All vessels to show a light in some conspicuous part of the vessel while at anchor in the harbour at night.

8. Any vessel arriving at this port with gunpowder or other explosives on board, will not be admitted to wharf or allowed to land the same until a report is made to the company and an arrangement entered into for the immediate disposal of the explosives.

9. To satisfy the company that the vessel is Colombian, the captain will present as proof a certificate given by the proper Custom-house officer who has issued the register.

BRITISH INDIA.

Tariff Modifications.

Notifications have recently been issued by the Finance and Commerce Department of the Government of India, according to which the following changes are introduced in various rates of import duty.

Instruments, apparatus, and appliances (Art. 64 of the Tariff Schedule) when imported by a passenger as part of his personal baggage, and in actual use by him in the exercise of his profession or calling, may be treated as personal effects on which import duty will not be levied.

Gum olibanum or frankincense is exempted from the import duty leviable thereon under No. 59 of Schedule IV. of the Indian Tariff Act, VIII. of 1894, as amended by Acts XVI. of 1894, and III. of 1896.

Water tanks are exempted from the import duty leviable thereon under the Indian Tariff Act, VIII. of 1894, as amended

by Acts XVI. of 1894 and III. of 1896, when imported by or under the orders of a railway company as defined in No. 93 of Schedule IV. (Import Tariff) of the said Act.

JAMAICA.

Customs Tariff Amendment Law.

A communication, dated 23rd April last, has been received at the Board of Trade from the Acting Colonial Secretary at Jamaica forwarding a copy of the Customs Tariff Amendment Law of 1896.

By this law (34 of 1896) all sugar imported is to pay duty of 1*d.* per lb.

The following articles are added to the free list, viz., bags and sacks made entirely of flax, hemp or jute; lymph or serum to be used for vaccination, or as a prophylactic or remedial agent.

No liquids shall be included under the term "wine" which contain more than 42 per cent. of proof spirit. Any liquid containing more than 42 per cent. of proof spirit shall be considered a spirituous compound.

The act provides for the payment of drawbacks on the export of preserved fruit and ginger in quantities of not less than 20 lbs. weight, provided the sugar used has been imported and paid duty. The drawback on preserved fruit is 1*d.* per lb. and on preserved ginger 3*d.* per lb.

XV.—EXTRACTS FROM DIPLOMATIC AND CONSULAR REPORTS.

IMPORTATION OF IRON INTO RUSSIA.

A despatch, dated April 28 last, has been received at the Foreign Office from Sir N. R. O'Connor, Her Majesty's Ambassador at St. Petersburg, enclosing a memorandum and statistical table drawn out by Mr. J. Michell, Her Majesty's Consul-General at St. Petersburg, with reference to the amount of iron imported into the Russian Empire during the last five years.

Mr. Michell states that the importation of iron from all countries into the Russian Empire under the categories both of bar and sheet iron, shows a visible increase, having in fact doubled in 1895 as compared with 1890. From Great Britain, the importation into Russia under the heads both of assorted, and bar and sheet iron appears also to have increased, especially under the latter class of this commodity, although not to the same extent as the importation of such iron from Germany and Belgium, particularly from the former country.

At Odessa, the importation of iron of both classes from all countries has also increased, the increase from Germany, and more particularly from Belgium being very marked. The figures for Great Britain show a falling off of assorted iron imported at Odessa in 1895 as compared with the year immediately preceding, as well as with that of 1890. Under the head of sheet iron, however, a considerable increase is apparent in 1894 and 1895, compared with 1890, and this is in all probability due to the increased activity displayed in the last few years in shipbuilding in the South of Russia.

It must be expected that a considerable diminution in the importation of bar, assorted, sheet and other iron from all countries will take place in the South of Russia in view of the increasing activity of ironworks already there existing, and the establishment of new works of a similar description by foreign, especially Belgian capitalists.

In order not to lose the market entirely, it would seem desirable that English Iron Companies and exporters should endeavour to secure orders at Odessa, Nicolaieff, and other ports of the Black Sea through the medium of specially appointed active agents well acquainted with the iron market of Southern Russia and with its requirements.

TABLE showing the AMOUNT of IRON in BARS, ASSORTED, SHEET, and other kinds, IMPORTED into the RUSSIAN EMPIRE during the period 1890-95, distinguishing the amounts imported at Odessa only :—

Years.	From Great Britain.		From Germany.		From Belgium.		From all Countries.	
	Iron in bars, assorted, &c.	Iron in sheets.	Iron in bars, assorted, &c.	Iron in sheets.	Iron in bars, assorted, &c.	Iron in sheets.	Iron in bars, assorted, &c.	Iron in sheets.
<i>A.—Imported into the Russian Empire.</i>								
1890	Pounds. 362,175	Pounds. 711,500	Pounds. 1,532,726	Pounds. 816,319	Pounds. 739,983	Pounds. 338,117	Pounds. 3,826,000	Pounds. 2,008,000
1891	171,770	239,544	829,828	353,340	353,402	330,793	2,487,000	1,247,000
1892	107,632	359,920	1,076,875	463,665	55,965	236,442	1,873,000	1,177,000
1893	212,664	636,962	1,340,686	833,752	318,986	616,545	2,952,000	2,343,000
1894	398,239	1,026,916	4,456,577	2,188,369	1,005,728	1,264,819	6,740,000	4,625,000
1895	*	*	*	*	*	*	7,152,408	5,957,255
<i>B.—Imported at Odessa only.</i>								
1890	Pounds. 173,908	Pounds. 189,785	Pounds. 12,318	Pounds. 3,833	Pounds. 242,276	Pounds. 127,450	Pounds. 429,698	Pounds. 328,305
1891	124,405	136,020	81	636	87,023	61,642	171,753	215,458
1892	22,878	60,600	84	1,483	8,564	29,059	31,526	148,103
1893	118,047	142,252	249	6,062	91,458	71,486	211,139	235,724
1894	233,508	273,191	737	2,935	524,966	293,565	769,637	574,725
1895	128,622	295,879	133,148	124,738	421,956	311,165	699,667	740,926

Note.—Poud = 36 lbs. avoirdupois.

* The Customs returns for the whole Empire for the year 1895 have not yet been published.

NEW GERMAN IRONWORKS IN SOUTH RUSSIA.

A despatch, dated May 30 last, has been received from Mr. G. Strachey, Her Majesty's Minister at Dresden, stating that the spirit of enterprise is now the rule in every department of German industrial activity.

As an example of this, the "Dresden Bank" has lately arranged with the great Chemnitz machinery house (Hartmanns) to open some extensive ironworks at Lugansk, in South Russia. The land is already purchased and the buildings are well advanced. It is intended to provide foundries with furnaces for the Martini-Siemens process, and there will be a branch for constructing railway apparatus and materials. The locality selected is described as abounding in coal and ores, as well as blast-furnaces, and as being well supplied with skilled labour.

The capital named is four millions of gold roubles ; the "International Bank" of St. Petersburg and some Berlin houses appear to be concerned in the undertaking. It is said that the new establishment is already provided with orders sufficient to guarantee its full employment for some years.

DEVELOPMENT OF SWEDISH MANUFACTURES.

In a report to the Foreign Office, dated 31st March last, Mr. M. S. Constable, Her Majesty's Consul at Stockholm, states that some interesting information has lately been published by the Swedish Board of Trade respecting the development within recent years of the native manufacturing industries :—

A considerable increase in production of iron and steel has occurred since 1891, the total value for the year 1893 being calculated at 2,777,777*l*.

The number of sawmills in Norrland is estimated at 225.

The wood-pulp industry has been increasing rapidly of late years. At the end of 1893 there were 93 factories in operation, producing in that year 96,000,000 kilos.

The development of the beet sugar industry has been very rapid and successful. In the year 1890 there were only four factories in existence, but others have been set up each successive year, so that at the present time there are 18 in operation, producing nearly 50,000 tons of raw sugar annually.

The following table serves to give a good idea of the recent development of Swedish manufacturing industries in all departments :—

Year.	Number of Factories.	Increase or Decrease.	Value of Output.	Increase or Decrease.
			£	£
1883	2,938	—	10,613,216	—
1884	2,924	— 14	10,631,847	+ 18,631
1885	2,976	+ 52	10,637,216	+ 5,369
1886	3,118	+ 142	10,545,932	— 91,288
1887	3,087	— 31	10,638,889	+ 92,957
1888	3,159	+ 72	11,611,603	+ 972,714
1889	3,254	+ 95	13,308,209	+ 1,696,606
1890	3,306	+ 52	14,995,483	+ 1,687,274
1891	3,436	+ 130	15,162,398	+ 166,915
1892	3,420	— 16	15,348,681	+ 186,283
1893	3,684	+ 264	16,436,938	+ 1,088,257

—(*Foreign Office Annual Series, No. 1704.*)

GRANT FOR THE GERMAN POST OFFICE.

A despatch, dated May 18th last, has been received at the Foreign Office from Sir F. Lascelles, Her Majesty's Ambassador

at Berlin, stating that it is announced in the "Reichsanzeiger," of the 18th May, that in the supplementary Budget, approved by the Bundesrath for submission to the Reichstag, provision is made for a grant for the Post Office, amounting to 1,288,000 marks (64,000*l.*), for the establishment of a new (fourth) cable between England and Germany.

OPENING OF INDUSTRIAL EXHIBITION AT NUREMBURG.

A despatch, dated the 17th May last, has been received at the Foreign Office from Sir Brooke Boothby, Her Majesty's Chargé d'Affaires at Munich, stating that the opening of the National Industrial Exhibition at Nuremberg by the Prince Regent of Bavaria took place on Thursday the 14th May. His Royal Highness, after spending two days at Nuremberg and making a thorough inspection of the exhibition, returned on the 16th May to Munich. The exhibition will remain open until the 15th of October next.

STRIKES OF WHARF LABOURERS IN GERMANY.

A communication, dated May 23 last, has been received at the Foreign Office from Mr. Wilfrid Powell, Her Majesty's Consul at Stettin, stating, with reference to a paragraph that appeared on p. 598 of the "Board of Trade Journal" to the effect that according to the "Reichs Anzeiger" the wharf labourers of Swinemünde had declared themselves in union with those on strike at Stettin, that this statement of the German journal was in error, the fact being that it was a proposal made by the Stettin strikers to which the Swinemünde wharf labourers refused to respond. The only point upon which the Swinemünde men were in agreement with the strikers at Stettin was that they would not go there to work. There has been no strike at Swinemünde this year.

OPENINGS FOR BRITISH TRADE IN DUTCH GUIANA.

Mr. Sydney A. Churchill, Her Majesty's Consul at Paramaribo (Dutch Guiana), in a recent report to the Foreign Office, states that there is a market in Surinam for machinery if it is properly looked after. Attention must be given to the requirements of the purchaser. It is alleged that the British manufacturer looks to the amount which the buyer has at his disposal rather than to the suitability of the article supplied to the requirements of the purchaser. Foreign makers inquire what the machine is required for, and offer valuable suggestions as to the most suitable plant for the purpose to be effected, even sending men out to the machine-employing district and studying local requirements. Very often a suggestion of great moment is evolved by the joint combination of

the practical experience of the manufacturer and the planter. Until British manufacturers show a disposition to personally visit their markets and study local requirements they will have to see foreigners successfully competing with them. At one of the biggest sugar estates, which is managed by an Englishman, it was said that all the machinery used was French, and that the manager would always recommend it because the manufacturer came out himself and made his plant a success before handing it over, remedying all defects which showed themselves, and suggesting various improvements which perfected the machinery he had supplied.

A great deal of attention has been given in recent years in Dutch Guiana to cocoa-drying machinery. Various systems have been tried experimentally. A manufacturer with a good, simple, and economical system might find some buyers in the market.

Surinam has had but one visit from a representative of a British commercial firm during 1894 and 1895; and that visit was from the chief partner in a well-known firm of general merchants of London and New York. German, American, French, and other commercial travellers are constantly visiting the market.

One of the most important items in the list of imports is that of cotton piece goods. The value of the importation in 1894 was about 53,000*l.* and in 1895 about 62,000*l.* Of these amounts about 4,000*l.* in 1894 and 10,000*l.* in 1895 came direct from England. British Guiana, in both years, figures for over 5,000*l.* in the totals; and the remainder comes almost entirely from Dutch ports, but nevertheless, Manchester is given as the place of origin of the goods. A little care and attention to the wants of this market would stimulate and improve the trade. Cheapness is the great essential.

There should be an opening in Dutch Guiana for piece goods printed in India, and the cooli-carrying ships should be a good medium to try the market with. Indian muslins and light silks, on which the Customs duty is 10 per cent., might also find a ready sale.—(*Foreign Office Annual Series*, No. 1708.)

NEW FRENCH CABLE TO AMERICA.

A communication, dated May 22 last, has been received at the Foreign Office from Mr. W. R. Hoare, Her Majesty's Consul at Brest, stating that on the return to Brest of the French transport "Vienne," now loading at Toulon for the northern dockyards, she is to be got ready to proceed to New York with a staff of hydrographers for the purpose of taking soundings for the new cable which is to be laid between Brest and New York, the transport "Charente," of Toulon, being the vessel selected to lay the cable.

THE FISHERIES OF THE BRETON COAST.

In a recent report to the Foreign Office, Mr. W. R. Hoare, Her Majesty's Consul at Brest, states that serious misgivings were entertained at first as to the prospects of the sardine fishery (a very important one considering that there are 130 preserving factories on the coast between Sables d'Olonne and Camaret) on account of the late arrival of the fish, which did not make their appearance on the coasts of Morbihan and Finistère till long after the usual time, and it was only towards the middle of August that they came in any abundance, running generally small, the fish of a suitable size for preserving not arriving till still later on. The season, which closed on November 30, was therefore a short one, the amount of packing done was not up to the average, and prices at the factories were 7 to 8 per cent. higher than in 1894.

The mackerel, whiting, and sprat fisheries were exceptionally good, and low prices prevailed for the most part of the season.

The principal fishing ports of Finistère are Concarneau, Douarnenez, and Audierne, and the value of the industry at these places according to the latest return (for 1894) was:—Douarnenez, 2,808,000 francs; Concarneau, 2,813,000 francs; and Audierne, 2,051,000 francs; a total of 7,672,000 francs out of 12,000,000 francs representing the aggregate value of all the fisheries of the neighbouring districts.

There are extensive oyster parks on these coasts, and cultivation of the bivalve makes good progress. Auray, Morbihan, Concarneau, Finistère; and Treguier, Côtes-du-Nord, deserve especial notice in this connection. In the vicinity of Lorient, too, some excellent oysters are to be found with clean, white, pearly shells and of good flavour. Large quantities of young oysters from the parks are bought by foreign dealers to lay down in their own native waters. Many of the oysters are rather small and thin, but there are some, the "Marennnes vertes," for instance, which are of good size and flavour. They, perhaps, would not be acceptable in England on account of their greenish tinge, against which there might be a prejudice, but they are certainly appreciated by many in France. Prices on the coast are very low compared with those paid for "seconds" and other descriptions in England; in Paris, however, and other cities, to which they are largely consigned, they are far from being so.

The retail prices in the Brest market vary from 60 centimes to 90 centimes per dozen.—(*Foreign Office Annual Series*, No. 1709.)

TUSCAN OLIVE OIL.

According to a report of Mr. W. Percy Chapman, Her Majesty's Consul at Leghorn, the olive groves throughout Tuscany gave promise of a splendid crop for the season 1895-96,

the trees having a magnificent show of fruit, which was, moreover, quite free from the olive-maggot, and otherwise perfectly sound.

But in the latter part of last November, and again early in December, north-east gales of extreme violence and bitter cold were experienced, which did immense damage, blowing down a great part of the fruit, besides uprooting many trees. Of the fruit still left on the trees, a considerable proportion then in a green state was injured by the icy winds, and subsequently shrivelled up.

Owing to these causes a crop of splendid promise has turned out to be a very indifferent one. The yield of oil may be put at not more than one-half of what otherwise might have been secured, while much of the product is of inferior quality, and the finer descriptions of oil are in relatively short supply.

Notwithstanding these circumstances the market opened at a low level of prices, and, in fact, the present outlook seems anything but satisfactory to Tuscan olive growers.—(*Foreign Office Annual Series*, No. 1700.)

IMPORTATION OF BRITISH MANUFACTURED GOODS INTO ITALY.

In a recent report to the Foreign Office, Mr. Charles A. Payton, Her Majesty's Consul at Genoa, states, with reference to the importation of manufactured goods, that it may be mentioned in the interest of British manufacturers, that, although the superiority of most British goods over those of Italian, German, and other makes is generally acknowledged, yet the pressure of heavy import duties, taxes, and financial troubles generally tend to prevent a really good market being found in Italy for British products for some time to come.

It is interesting to note that, although the recently opened works at Valcannonica near Brescia supply a very large proportion of the tin-plates used in the north of Italy, the works are understood to be chiefly in the hands of British manufacturers who have established them in Italy to avoid the heavy Italian import duty.

A certain quantity of plates were, however, imported from England, and used by the Italo-American Petroleum Company for making into petroleum tins for exportation, in which case the duty is returned.—(*Foreign Office Annual Series*, No. 1723.)

THE CORAL TRADE OF LEGHORN.

Mr. W. Percy Chapman, Her Majesty's Consul at Leghorn, in a report to the Foreign Office, states that Custom-house statistics show that 41,608 lbs. were exported from Leghorn in 1895, as against 34,900 lbs. in 1894. Assuming that these figures

are correct, it is certain that the leading coral merchants of the city have not shared in the apparent improvement in the trade, for they complain that the demand from both British India and Poland, the main outlets for their goods, has sensibly declined. The decrease in the export to Poland is put down to purely temporary causes, viz., bad harvests, which have prevented the peasantry indulging in their love of the article. As regards India, merchants have not been able to feel the full benefit of the slight improvement in the rupee, as the new 5 per cent. Customs duty on coral is found to be a severe handicap.

There has been some demand this year from the West of Africa, but only for a very dark-coloured coral which is scarce and difficult to find in large quantities. Merchants are hopeful that the opening up of Africa will prove a new market for corals.

Nearly the same quantity of coral has been fished in the Sciacca beds this year as last. The quality appears to have been somewhat inferior, but prices were not reduced. There were fewer boats engaged in the fishery than in 1894, and it seems as if there would be still fewer in 1896, owing to the losses sustained by the fishers, and the yearly increasing expenses of fishing.—(*Foreign Office Annual Series, No. 1700.*)

THE MILLENNIUM EXHIBITION AT BUDA-PESTH.

A report has been recently received at the Foreign Office from Her Majesty's Consul at Buda-Pesth, stating that great importance is attached to the Millennium Exhibition which was solemnly opened by His Majesty the King on the 2nd May last.

The original idea was that this Exhibition was not only to be a record of the progress made by Hungary in national industries and manufactures since the last Exhibition, which was held 11 years ago in Buda-Pesth, but that it was to represent the definite results which had been obtained by all the producing classes of Hungary up to the present day, as well as a display of the agricultural power of the country on the thousandth anniversary of its existence.

The State, the counties, every well-known public institution, and, in fact, all classes of society, combined in their attempt to attain the original object of the Exhibition, and to make it as representative and interesting as possible.

The architectural arrangements of the buildings, as well as the attractive formation of the gardens, together with the collection of many relics and trophies of past centuries, and last, but certainly not least, the almost countless numbers of restaurants and coffee-houses, unite in making the Exhibition a favourite resort for all classes, and especially for those who do not take an immediate interest in the exhibition of manufactures.

The section devoted to manufactures shows, in the first instance, the development of the branches of industry which are connected with agriculture.

Agricultural machines of all sorts, many of them copied from English models, pumps, fittings for waterworks, especially machines and plant for flour mills, and again, very practical and well-made fire-engines, machinery for electric works, &c., &c., may be especially mentioned.

One of the most interesting features of the Exhibition is the displaying of railway engines and carriages, the manufacture of which has reached a very high degree of perfection. It may be mentioned that the thousandth Hungarian railway engine left the factory two years ago—an interesting fact when it is known that the first locomotive built in Hungary only dates from the year 1873.

In the agricultural produce department all the countries are well represented. Besides the ordinary products, there is a large show of rice, hops, flax, hemp, &c.; and a large portion of this building is set aside for an exhibition of beehives, honey of all sorts, and, in fact, everything appertaining to apiculture.

Forestry takes a permanent place in the exhibition:—Gigantic trunks and logs reveal the existence of oak forests. A large quantity of this fine timber is exported to foreign countries partly in the form of logs, partly in the shape of staves.

Two buildings are devoted to sugar and flour manufactures, both of which are in a flourishing condition.

The silk-worm industry is to be seen in another building; but this has not yet reached a very advanced stage as the silk obtained from the cocoons is not manufactured in Hungary, but is practically all sent to France and Italy for that purpose.

Bosnia and Herzegovina show specimens of their home industries, confined chiefly to embroidering and such like.

Croatia-Slavonia have their pavilion with specimens of their industries, an important feature of which appears to be the tanning liquid extracted from oak waste wood. The manufacture of this extract was introduced into Slavonia by an English Company.

The exhibition on the whole, apart from the historical section, does not present any striking features except the railway engines and carriage department and the manufactures connected with agriculture.

No catalogue of the exhibition has as yet been published.

CREDITS VOTED BY ROUMANIAN CHAMBERS FOR RAILWAYS.

The Board of Trade have received through the Foreign Office a despatch from Sir G. H. Wyndham, Her Majesty's Minister at Bucharest, stating that a credit of 8,000,000 frs. (320,000*l.*) has been voted by the Roumanian Chambers for repairing the Cernavoda-Constantza line.

The preamble to this law states that this sum is required for the amelioration of the existing line, the building of additional stations and sheds for locomotives, as well as of store houses for grain in

order to facilitate its transport and shipment at the port of Constantza.

A credit of 15,600,000 frs. (624,000*l.*) has also been laid before the Chambers for the completion of the Galatz-Berlad line, which is 110 kilometres long and is intended to facilitate the export of the products of the north of Moldavia.

The following credits have also been voted for railway extension and improvement : 4,000,000 frs. (160,000*l.*) for the completion of the line Roshiori-Alexandria-Zimnicea, 6,000,000 frs. (240,000*l.*) for the purchase of railway carriages, 800,000 frs. (32,000*l.*) for sums paid for expropriation in the construction of the line from Dorohoi to Jassy, 1,270,000 frs. (50,800*l.*) for building the station at Rimnic-Sarat, 8,000,000 frs. (320,000*l.*) for the improvement of the lines in general and of the railway stations, 3,280,000 frs. (131,200*l.*) for the completion of the line Tirgu-Ocna-Comaneshti-Moineshti, with a branch line to the salt mines at Tirgu-Ocna, and 4,000,000 frs. (160,000*l.*) for the construction of the line Rimnic-Valcea-Calimaneshti-Riul-Vadului which is 62 kilometres long and is intended as a further means of junction with the Austro-Hungarian railway system, west of that already existing viâ Predeal.

BRITISH TRADE IN THE CYCLADES.

A report, dated April 20 last, has been received at the Foreign Office from Mr. W. H. Cottrell, Her Majesty's Consul at Syra, in which it is stated that the development of British trade in the Cyclades Islands rests mainly on the amount of energy and activity exhibited in competing with foreign markets; this, unfortunately, does not appear to extend much beyond the distribution of a few catalogues and circulars, and there is not even the shadow of a commercial traveller. If British manufacturers wish to retain their hold on these markets or open up new ones, it certainly would be advantageous for a smart traveller, able to speak French, to pay an occasional visit to that part of the world, not merely as a representative of a single firm and only one class of goods, but of various manufactures, such as oils, paints, dyes, chemicals, perfumery, cutlery, plated goods, hardware, lamps, rope, hats, yarns, cotton stuffs, silks, hides, machinery, earthenware, and glass; this could not fail to promote increase of business and profitable results.

In these days the exigencies of trade in various countries, whatever branch of it is concerned must be attended to, and the German trader, with his interminable energy and complaisance, should not be allowed to cover ground lost to British trade, and perhaps eventually secure the lead in the commercial competition which is becoming so keen and active everywhere.

It may be as well to repeat also, that those whose business is to deal with foreign countries should as far as possible employ

the metric system of weights and measures in their trade circulars and catalogues, and have translations made into the language of the country, or at least into French, which is well understood in the Levant.—(*Foreign Office Annual Series*, No. 1720.)

ESTABLISHMENT OF REGULAR STEAMSHIP LINE BETWEEN LIVERPOOL AND GALVESTON (U.S.A.)

A communication, dated May 2 last, has been received from Mr. Horace D. Nugent, Her Majesty's Consul at Galveston (Texas), reporting that, in consequence of the growing importance of that place as a port, owing to the successful system of jetties, whereby a channel of over 23 feet has been obtained direct to the sea, arrangements have recently been concluded for the calling there at regular intervals, of the vessels of the Harrison Packet Line.

These vessels will leave Liverpool at intervals of one month, and, after touching at various South American and West Indian ports, will come to Galveston and return hence with cargo and passengers to Liverpool.

The first of the fleet has recently sailed and will be followed, at regular intervals, by others.

Negotiations are also pending for the visiting Galveston of vessels of the West Indian and Pacific, and of the Hamburg and American Line—the former to run to Liverpool, and the latter to Hamburg.

Galveston has also been recently visited by agents on behalf of the White Star and North German Lloyd Lines, with a view of examining its facilities as a port, and it seems probable that the vessels of one or more of these lines, will, before long, run regularly to Galveston.

THE COAL TRADE OF MOBILE (U.S.A.)

The coal deposits of Alabama, which are a continuation of the great Apalachian coalfield, are situated in the northern part of the State, and underlie the counties of Bibb, Jefferson, St. Clair, Shilby, Tuskalooza, and Walker, extending with small deposits into Blount, Cherokee, Cullman, and Etowah. These measures are divided into three separate regions or fields, the aggregate area of which is estimated at about 8,600 square miles.

Mr. Barnewall, British Vice-Consul at Mobile (U.S.A.) in a recent report states that the coal-mining industry of Alabama has made rapid progress within a few years, rising from a total product of 13,200 tons for 1870 to 5,529,312 tons in 1892, which was the largest production as yet of the State. In the last year, owing to labour troubles and closing of mines, the total tonnage taken from the mines amounted to 4,381,195 tons, a decrease of 788,847 tons from the output of 1893.

The average price at the mines for the total product of 1894 was 99 c. per ton. The total amount of coke manufactured in Alabama in the year 1894 was 924,002 tons, a decrease of 140,850 tons. The largest producing county was Jefferson with 2,776,302 tons, and the smallest Blount with 8,000 tons.

The coal business of Mobile is yearly improving, especially in steam coal.

The large increase in the number of steamers which have entered Mobile in 1894, and the establishment of the Liverpool line, have tended to develop more activity in the sale of steam coal.

The fruit steamers use 2,500 tons a month, or about 30,000 tons a year. The home consumption in Mobile is about 7,000 tons.

The total receipts during the year 1894-95 were 156,996 tons, as compared with 104,340 tons during 1893-94, and this applies to Alabama coal alone. The importation of foreign coal was barely 2,200 tons.—(*Foreign Office Annual Series*, No. 1688.)

THE TOBACCO INDUSTRY OF SOUTH CAROLINA.

In a report to the Foreign Office, Mr. H. de Coëtlogon, Her Majesty's Consul at Charleston, states that a noteworthy development took place last year in the South Carolina tobacco-growing industry. A few years ago there was little or no tobacco grown in that State, and last year mention was made of some successful experimental tobacco growing that had been made in what is known as the Pee Dee section, embracing the counties of Darlington, Florence, Sumpter, Clarendon, and Williamsburg. What was only an experiment a year ago now appears to bid fair to become one of the staple agricultural products of the State. From recently published statistics it is reported that 11,865 acres of land in South Carolina were under cultivation in tobacco during the season of 1895, and that the total product was about 10,000,000 lbs. The actual sales up to January 15, 1896, were within a fraction of 4,000,000 lbs., which at current price realised for the planters the sum of 400,000 dol., equal to, say, 80,000*l.* sterling, for tobacco marketed up to the above-mentioned date, which had been grown upon light land. In addition to the tobacco sold in the local markets much of it was shipped away by planters direct to other States from the various railway stations in their districts, and it is also stated that at the close of the year 1895 at least one-third of the entire crop still remained in the hands of planters. This industry, however, is only in its infancy as a State crop, being but one year old, and it is probable that next season the article will be grown in a number of sections of the State situated in other counties than those already named, and that many more acres will be added to those now cultivated in the established tobacco-

growing regions. A few years ago not an acre of land was cultivated in tobacco in South Carolina, and not a dollar's worth was raised or sold so far as the obtainable records show, while to-day the large and rapidly increasing figures given speak for themselves. Tobacco farms, barns, pack-houses, factories, and warehouses have been established on a considerable scale, and the industry, already an important one, promises to increase rapidly in the future, and to become a prominent feature in the general trade of the State. The most reliable and best-posted tobacco men estimate that the total crop of tobacco made in South Carolina in 1895, 10,000,000 lbs. in quantity, was worth 1,000,000 dols. (200,000*l.*).—(*Foreign Office Annual Series*, No. 1711.)

AMERICAN COTTON CROP FOR 1895-96.

In a recent report to the Foreign Office, Mr. Horace D. Nugent, Her Majesty's Consul at Galveston, Texas, states that, during the summer of 1895 the weather was such as to lead experts to predict a very short cotton crop for the present season. These predictions have been more than verified, and it is doubtful whether the total crop of the United States for 1895-96 will reach 7,000,000 bales. A conservative estimate places the crop at from 6,750,000 to 7,000,000 bales.

Whilst the crop was short all over the United States, Texas, in particular, shows a large decrease. This State, as a rule, produces to-day about 30 per cent. of the total crop, and therefore taking that crop at 7,000,000, Texas production should be, under ordinary circumstances, 2,100,000 bales.

From present appearances, however, it is doubtful if Texas has produced this season more than 1,750,000 bales.

The shortness of the crop has, of course, greatly affected the receipts and exports at Galveston, but, in spite of this, Galveston has held her own as second cotton port in the United States, though the difference between the amount of cotton handled there and at New Orleans is more marked this season than it was last.—(*Foreign Office Annual Series*, No. 1687.)

THE TRADE OF THE MAGDALENA (U.S. COLOMBIA).

A despatch, dated 10th February last, has been received at the Foreign Office from Mr. G. Jenner, Her Majesty Minister at Bogota, enclosing a report of Mr. MacGregor, Acting Vice Consul at Barranquilla, on the geographical and commercial importance of the River Magdalena, in which it is stated that the river affords ample facilities for the efficacious and secure transportation of any conceivable amount of exports and imports applicable to a large portion of the Republic of Colombia.

The present amount of imports and exports is very considerable and every successive year brings an increase in quantity and total value.

It should be clearly understood that this increase in trade is not due to any extraordinary cause, but is merely the natural augment of the labouring forces of the country.

From the "fifties" to the "seventies" Barranquilla suffered much for want of accommodation for export and import. In the year 1870 a railroad was constructed between Barranquilla and the Bay of Savanilla, and in a great measure this railroad was and still is the mainstay of the commerce of the country.

But various difficulties presented themselves. The terminus first adopted in the Bay of Savanilla called Salgar, was found unsuitable for the prompt or even moderately prompt despatch, inasmuch as all disembarkations and shipments had to be effected by means of barges towed by tugboats to and from the sides of the vessels which were anchored in the roadstead at a considerable distance from the shore.

This was remedied by the construction of a pier stretching 4,000 feet into the sea where there is water sufficient for the largest class of sailing vessels or steamers to come alongside, and this is the present method of embarking and landing.

Much praise is due to the British company which has brought these railroad facilities for import and export to the present state.

Still if it is reflected that everything is moving onwards in progression, it must be seen that at the farthest in five or six years hence, the present facilities will be entirely inadequate for the commerce of the country.

In the year 1876 an extensive cattle trade sprung up between Barranquilla and the island of Cuba, which demanded cheap transport, and the result was that steamers and sailing vessels entered through the Bocas de Cenizas to carry on this trade, which lasted from 1876 to 1884. Steamers of the Royal Mail, Atlas, and various other lines entered and went out again after discharging and loading in Barranquilla, and during those years no accident of any importance occurred. At the conclusion of the trade with Cuba, and with a more energetic administration of the railroad, the traffic was resumed *viâ* Salgar.

The experience of the last 20 years proves that the commerce of Colombia is becoming more and more important year by year. This commerce is carried on by a most respectable body of men, composed of Colombians and foreigners, who enjoy the perfect confidence of the various European mercantile centres.

The vital question now is the taking of the necessary steps to insure a full and ample accommodation for both import and export trade.

The River Magdalena is navigable at all seasons of the year. It traverses the most important part of the country, and its navigation has been abundantly proved not only to be practicable, but also as safe as any navigation can be. But this river lacks one essential requisite, and that is the opening of its outlet direct to the sea.

By opening the Bocas de Cenizas to commerce, a secure and permanent outlet would be obtained for all time.

XVI.—GENERAL TRADE NOTES.

CIRCULAR TO GROWERS OF WOOL.

The following is a copy of a circular recently issued by the Council of the Bradford Chamber of Commerce to wool growers of the country :—

“Some years ago the Council of the Bradford Chamber of Commerce issued a circular calling the attention of wool growers to certain practices which prevailed in some parts in connection with the winding and getting-up of wool.

The Council believe that the circular has been the means of effecting considerable reform, but as several complaints have recently been made on the subject, they think it advisable again to call attention to the matter.

“The Council refer more particularly to the negligent or fraudulent winding of wool ; that is, the insertion of washed locks, dung, clags, and other substances in the fleeces. The Council consider it hardly necessary to point out that this is illegal, and that in several cases damages have been recovered for such practices both in the Assize Courts and the County Courts.

“Flock-masters should also be careful not to clip wool in wet weather or when the wool is not thoroughly dry, as if wound in that state it deteriorates greatly if kept for any length of time.

“Wool is sometimes shorn in places where there is chopped straw, chaff, or short grass. This should be avoided, as particles of these substances get mixed with the wool and cannot afterwards be separated, thus causing great difficulty and loss to spinners and manufacturers.

“Care should also be taken to avoid the use of tar for marking or treating sores on the sheep. As it is impossible to eliminate the tar in the process of manufacture, its presence causes great trouble and loss, and seriously depreciates the value of the wool.

“An improvement in these particulars would be of direct advantage to both seller and buyer. The Council therefore venture to hope that agriculturists, through their chambers of agriculture and otherwise, will aid them in their efforts to obtain the improvements suggested, and thus promote their mutual interests.”

BRITISH CHAMBER OF COMMERCE IN PARIS.

A copy has been received at the Board of Trade of the report presented at the twenty-second annual general meeting of the British Chamber of Commerce in Paris.

The report deals with the business of the Chamber during the year 1895, with especial reference to the metric system ; Anglo-French trade ; treaties and tariffs ; laws affecting foreigners in France ; dividend tax on foreign companies ; and French Customs circulars.

THE BELFAST LINEN TRADE.

According to the report of the Belfast Linen Trade Board, as published in the "Irish Textile Journal," the condition of the export trade in April was quite as good as in the previous month, and the Board of Trade returns for the four months of this year show an increase in value of yarn shipments from the United Kingdom equal to $19\frac{1}{2}$ per cent. Belgian spinners are specially firm in quotations for tow yarns. The value of foreign yarns imported shows a decline of 20·7 per cent. compared with the same period of last year.

A steady general demand has been maintained in the home trade, and though there was no exceptional briskness in the market, repeat orders came more regularly to hand, and the tone was good.

On French and German account the improved trade of late was kept up, the four months, according to official figures, showing a very substantial increase compared with the same period of last year. With the smaller market of Spain there was a falling off, but on Italian account values for the four months were slightly better.

There was a decrease in the season's trade of the United States compared with last year, the quantities for the four months showing a falling off of 17·6 per cent. This, however, was largely accounted for by the exceptional demand for scrim last spring. Advices of late are rather more cheerful, and prospects generally are now considered to be brighter.

The wretched state of affairs in Cuba still operates against business with the foreign West Indies. The volume of trade has dropped over one-fourth since 1894. Exports to Mexico, Columbia, and the Argentine showed considerable improvement on last year. With British East India, Australia, North America, and the smaller markets, the improvement was also progressing. Trade with Brazil was dull, the figures being considerably less than last year.

For the four months ended April 30th, the exports of linen piece-goods from the United Kingdom show a decrease in quantity equal to 7·3 per cent., but values are 1·6 per cent. better as compared with the corresponding period last year.

METHODS OF FRUIT PRESERVING.

According to the "Adelaide Observer," the bottling of fruit in hot water is a paying operation. The fruit, having been carefully sorted, is placed in bottles, which are ranged closely together in a tank, and are then filled up with water, the tank being also filled as far as the necks of the bottles. The water is then heated to boiling point, and the bottles are corked as soon as possible. Steam is sometimes substituted for water. The corks are afterwards covered with a piece of tinfoil, and a label is placed

on each bottle. Fruit thus treated will last for years, and will not deteriorate if skilfully bottled, provided it was in good condition. Another means of preserving fruit, which originated in America and is much in vogue in that country, is the system known as evaporation. This method can be applied to all hardy fruits, and these will, under this system, preserve a wholesome condition for a year or more. Apples are usually pared, cored, and sliced by an ingenious machine before being placed on the evaporator. Other fruits are evaporated whole. This process is of great advantage for utilizing windfalls and fruit not good enough for marketing. The evaporator is a large boxlike apparatus, fitted with wire trays, on which the fruit is placed; at the bottom of this is a furnace, in which either coal or wood can be used, the smoke being carried off by a pipe at the back. Above the furnace is the evaporating chamber, the admission of air being regulated by dampers. The air is heated by the furnace and passes through the fruit, which remains for three or four hours, after which time the operation is complete. Before using the fruit for cooking purposes it is essential to soak it in water for some hours.

RUSSIAN TOBACCO.

The tobacco crop in European Russia and the Caucasus, according to the "Journal de St. Petersburg," for 1895 was inferior to the preceding year's, although the extent of ground cultivated (about 52,000 hectares) remains about the same. The most extensive plantations are found in the Central and South-Western provinces, but these are only for the production of the commonest tobacco consumed by the peasants. In the provinces of the south, on the other hand, only the kind known as Turkish tobacco is cultivated.

In the Crimea, Bessarabia and the Caucasus these tobaccos are excellent, and used in the manufacture of cigarettes, either pure or mixed with Macedonian tobacco.

The total crop of 1895 amounted to 64,183,000 kilogs. as compared with 67,654,000 kilogs. in 1894 and 81,207,000 kilogs. in 1893.

Three-fourths of the total is of the common kind. Odessa is still the great centre of the tobacco import trade, though for some years past the manufacturers of St. Petersburg and other towns of the north import their material direct from abroad. The quantity imported at Odessa diminishes every year, but in 1895 it still amounted to 321,500 kilogs.

RAILWAY ACTIVITY IN RUSSIA.

The Russian "Region du Midi," quoted in a recent issue of the "Journal de St. Pétersbourg," states that the railways in Russia are just now passing through a sort of crisis, which more especially concerns the lines of the south.

It appears that the amount of rolling-stock is not in proportion with the demands of the traffic, coal and sulphur mine owners especially complaining of this defect; and this abnormal state of affairs is also prejudicial to other branches of trade, especially to the trade in grain, large quantities of which are delayed at the stations without being able to be transported to its destination.

This insufficiency of rolling-stock has been felt for some time, as the latter has not increased in proportion with the extension of the railway system or the demands of local transport. At Kharkow, for example, an important new line has been opened and is worked with old carriages and locomotives from other lines.

Ten years ago the railways of the Empire extended over 22,000 versts. Towards the close of 1895 the total length amounted to 34,000 versts, without reckoning railways since opened for traffic, such as that of Kharkow-Balaschow (736 versts), and others in process of construction. Under this last heading must be included over 2,000 versts of line which is being laid down at the expense of the Government, as well as 5,000 versts of Siberian railway and 3,723 versts of private lines. Finally, authorised projected lines amount to over 2,000 versts. While the number of lines increases, traffic on the old lines is growing in activity, following on the development of industries and the increase in population. Passenger traffic has also increased to an unanticipated extent. The chief object of the railway companies, then, should be the providing as soon as possible of rolling-stock in proportion with the progressive extension of the lines.

AMERICAN LOCOMOTIVES FOR RUSSIA.

According to the "Railway Age," in accordance with an order received in January last from the Russian Government by the Baldwin Locomotive Works, 32 locomotives were delivered in Russia for special use in the traffic connected with the coronation of the Czar.

These 32 engines are all of the same type—ten-wheelers with straight boilers, compounded on the Vaucrain 4-cylinder system. They are to use oil for fuel, the fuel tank having a capacity of 1,850 gallons. With the exception of the form of the tender and the front frame, with hand-rail and English buffers, and the screw couplings, the engines are more like American locomotives than is common with engines built for European service; the

springs are underhung and the equalizers placed below the frames. The principal dimensions are as follows :—

Cylinders	-	-	-	-	14 and 24 ins. by 26 ins.
Driving wheel diameters	-	-	-	-	72 ins.
Driving wheel base	-	-	-	-	14 ft.
Total wheel base, engine	-	-	-	-	26 ft. 3 ins.
Number of tubes, 2 ins. diameter	-	-	-	-	223
Length of firebox	-	-	-	-	80½ ins.
Width of firebox	-	-	-	-	36 ins.
Grate area	-	-	-	-	20 sq. ft.
Heating surface, flues	-	-	-	-	1,650 sq. ft.
Heating surface, total	-	-	-	-	1,769 sq. ft.
Weight in working order	-	-	-	-	140,000 lbs.
Weight on drivers	-	-	-	-	103,000 lbs.

The engines are built for a gauge of 5 ft.

In connection with this order it is worthy of note, as showing the growth of compound engines in favour, that the Baldwin Works have now turned out over 600 engines of the Vauclain compound type and have orders for over 100 more in hand. Among them are engines of almost every conceivable class for every kind of passenger and freight service.

THE DEVELOPMENT OF EASTERN SIBERIA.

The "Gazette de St. Pétersbourg," in a recent article on the natural wealth of Eastern Siberia, notices the ever-increasing tendency of foreign capitalists to obtain a share in the development of those parts, especially in gold mining and the fisheries at the mouths of the great Siberian rivers.

The working of the gold mines annually produces over 500 pouds of metal. From a technical point of view the work done leaves much to be desired, but due allowance must be made for the difficulties of the pioneers of this industry in Siberia, who, for lack of proper workmen, were obliged to engage anyone at hand, even escaped convicts.

Now the country is rapidly becoming more civilised, workmen are coming in, and ways of communication are being opened; the greatest difficulties, in short, have been overcome, and the working of the mines is in a fair way to become profitable.

Fisheries form also an important industry in these latitudes, and become more and more profitable as the supply of the American rivers diminish. But, independent of this industrial point of view, fish constitutes the sole food of the people of the Far East, the Toungruses, Orotchones and people of Kamschatka, and consequently the wholesale working of the Siberian rivers touches a point of vital interest to the people.

MANUFACTURE OF MACHINERY IN GERMANY.

According to the Austrian *Handels-Museum*, the reports of the Imperial German Statistical Office show that the German export of machinery has been steadily increasing of late years. It has risen, in round numbers, from 86 million kilogs. in 1892, 96 million kilogs. in 1893, 125 million kilogs. in 1894, to 136 million kilogs. in the year just past. Against this there is entered for the year 1895 an import of 46 million kilogs. of machinery of a value in round figures of 30 millions of marks. The lion's share in this import came from England—two-thirds, that is to say, between 20 and 24 millions of marks; then follow North America and Switzerland, with much smaller figures; Belgium, France, and Austria close the list. The machines from America were mostly those employed in small trades, and sewing machines, whilst the greater part of the spinning-jennies and other large machines used in the textile trades, which are now so actively encouraged in Germany, came from England. But whereas a short time ago all mechanical looms had to be imported from England, the Germans have been giving close attention to this class of manufacture, and are now more and more fitting up their factories with German looms. The Saxon manufacturers have driven the English, especially in machinery for carding wools and vicuna wools, quite out of the market in this trade. As soon as the problem was solved of making velvet and velvet ribbons on mechanical looms, this manufacture developed rapidly in the Lower Rhine provinces, spreading to the production, not only of machinery for this special branch, but for all kinds of work in the cotton, woollen, and leather trades.

TRADE OF STETTIN IN 1895.

The Austrian "Handel's Museum" for May 7 states that the timber trade of Stettin made a considerable step forward in 1895, as compared with the preceding year, without, however, reaching the normal point of prosperity. The beginning of the year brought important orders, especially of oak timber for England; and for three months after the opening of the port to navigation there was great activity in shipping the goods which had been received. But as the year went on the activity in the export trade declined, on account of the fact that the cargoes delivered in England at the order of the railways there failed to give satisfaction so far as dimensions and quality were concerned, and so interfered with autumn orders being given. The export of oak to France was brisker in 1895, and made up for the decline in the business with Belgium and Denmark. Prices remained, as a rule, on the level of 1894, because the smaller supply of raw timber kept the balance. The export of deal was lower in almost every department. France and Belgium bought scarcely any at all, and England only a few cargoes; on the other hand, there

was a considerable demand for home use, so that the total results in the deal business were encouraging and even satisfactory. Pine timber was scarcely ever so much asked for, and even the extensive supply could not lower the prices.

CHANGES IN GERMAN SHIPPING IN THE BALTIC

From a report of the French Consul-General at Dantzic, published in a recent issue of the "*Moniteur Officiel du Commerce*," it would appear that in the past few years great changes have been effected in the German shipping, principally in the Baltic ports. Steamers are being everywhere substituted for sailing vessels except for the coasting trade. Many of the German ship-owners have been selling their sailing vessels at any price, and constructing steamers drawing little water, which are better adapted for the rapid loading of wood and grain, and on which more profitable freight rates are obtainable.

DEVELOPMENT OF ITALIAN TRADE.

The Austrian "*Handel's Museum*" for May 7 remarks that the efforts which Italy has made for some time past to develop her trade in foreign countries have been successful so far as regards Eastern Asia. For some years past the superior business houses of Italy have formed themselves into a union, the object of which is the encouragement of export trade to Eastern Asia. This union consisted at first of 30 members, each of whom paid 1,000 lire to the common store. This entrance fee has been reduced to 600 lire. The union has created agencies in the principal trade centres, and in particular in Bombay, Singapore, Hongkong, Shanghai, Rangoon, and Yokohama. Similar offices will shortly be opened in Melbourne, Sydney, Manilla, Java, and Ceylon. The agents are empowered to trade with one another, as there may exist in one centre or another a special demand for a particular article. Hitherto the commercial world of Lombardy has chiefly made use of this new market; an attempt is now being made to interest the South Italian and Sicilian producers in the scheme, so as to gain fresh emperiums for the varieties of Italian produce. The advantage of this union consists, with other things, in the fact that the different agencies deal directly with the manufacturer, and that the committee only interferes in case of dispute. Much is expected from the new trade company, which was formed for Benadir, and which has obtained a species of charter from the Italian government. But this company will not begin its operations until the conditions of the Italian colonies in Africa have become more favourable than they are at present.

THE ITALIAN TRADE IN WINE.

According to the Austrian "Handel's Museum," the returns of the export of wine from Italy during late years have shown a considerable irregularity in that trade, and upon the whole a steady decrease. In particular, the export of wine in casks was in 1892, 2,417,000 hectols; in 1893, 2,328,993 hectols; 1894, 1,911,987 hectols; 1895, 1,675,023 hectols, that is to say, 741,977 hectols less than in 1892. This reduction is due to various causes, among them the greatly reduced demand from Austria-Hungary caused by the increase in the production of wines for home consumption in that country, as well as the greatly lessened export to Switzerland up to 1894, that country being on better terms with France, and now obtaining wine from Spain, &c. under more favourable conditions. On the other hand, the export of Italian wine to South America goes on increasing, the innumerable Italian settlements on that continent having a large consumption of wine, which they prefer to import from Italy. The following table will show the amounts of the export of wine in casks in the last two years to the principal countries where it has a market:—

	1895.	1894.
	Hectolitres.	Hectolitres.
To Austria-Hungary - - -	626,389	927,302
„ South America - - -	378,573	357,571
„ Switzerland - - -	245,142	225,570
„ Germany - - -	133,194	106,182

For all other countries, including Malta, Africa, North America and France, the figures are relatively small. It will be seen that the reduction of export to Austria-Hungary last year was enormous.

THE SHERRY VINTAGE OF 1895.

According to a report on the sherry vintage by Mr. Charles L. Adams, United States Consul at Cadiz, the quantity of "mosto" (new wine) has fallen far short of that obtained the previous year, which was also short. This year is, therefore, much below the average. At the beginning of the season, the vineyards generally presented a most healthy and flourishing condition, which caused reasonable expectation of an abundant yield. This, however, was not realised, as the vines during the summer were attacked by phylloxera and mildew, both of which the vineyard owners seemed powerless to check.

The class of grapes used for making sweet wines, called "Pedro Ximenez," appears to have suffered most from disease. In some vineyards the entire crop of this grape shrivelled up and fell from the vines considerably before maturity. The gathering and pressing of the crop generally was effected during very unfavourable weather, much rain having fallen at the time.

The official data for 1895 show 7,700 hectares (19,027 acres) of vineyard in cultivation, producing 15 hectolitres of "mosto" per hectare, a total production of "mosto" for 1895 of 115,500 hectolitres (3,051,163 gallons), being equal to about 26,000 butts.

The average annual production of new wines in the district in previous years, before the appearance of the phylloxera, was 34,000 to 36,000 butts, so that the last vintage shows a decrease of at least 8,000 butts, this decrease being due to the above-mentioned cause.

The quality of the 1895 vintage, on the other hand, is represented as being very good and much superior to that of several preceding years. This fact, and the comparatively small supply, have operated in giving an upward tendency to prices, and the general opinion is that the majority of producers profited, rather than lost, by the shortness of the crop. Another circumstance that has contributed to this satisfactory result has been the purchase of considerable quantities of young sherries for exportation to France for distillation into brandies, &c.

There has undoubtedly been a very short crop, but as regards quality it is impossible to speak with any degree of certainty until the "mostos" are racked from the lees next spring.

The production of sherry may increase in subsequent years, especially if the efforts to extirpate the phylloxera prove successful; but the prevailing impression is to the contrary, based as it is upon the belief that the vineyards abandoned because of that pest, will not again be replanted or cultivated.

GOLD MINING IN THE PHILIPPINES.

According to the "London and China Telegraph," a British company is working gold deposits at Mambulao, in the Island of Luzon, and promising reports of mining progress there appear from time to time in the "Comercio." That journal takes consequently the opportunity to describe at some length the gold resources of the Philippines. The particulars given show that gold is largely scattered over the islands, and that mining has been carried on there for the last three centuries, but the yield has ever been small. The natives carry on mining, but with such rough and inadequate appliances that the value of the gold won hardly comes at present to 100,000 dols. a year. The mining is almost wholly on the surface from the defective means available and refractoriness of the ores, and the miners often give up when they have reached a depth where the prospects point to more paying returns.

PHARMACEUTICAL EXHIBITION AT PRAGUE.

The "Journal de la Chambre de Commerce de Constantinople," states that the Pharmaceutical Society of Prague will celebrate its 25th anniversary in 1896 by an international exhibition of pharmacy, to be held from the 15th August to the 15th September 1896 at Prague, and to include the following groups:—Scientific apparatus and articles used in the pharmaceutical branch, and the literature having reference to same; machinery and various apparatus serving for the manufacture of pharmaceutical articles, pharmacy fittings, &c.; products and drugs used in pharmacy; manuscript books, statistics, and tables concerning the historical development of pharmacy; associations and corporations of the pharmaceutical branch; hygiene and the care of invalids.

The Government, local authorities, and corporations have promised their support, and considering the number of acceptances from the provinces and abroad, it may be anticipated that this exhibition will be a success.

There has been no important exhibition of pharmacy in Europe since 1883, and the forthcoming one at Prague in 1896 is to include all features of progress in the pharmaceutical branch and its ramifications.

Meetings of various pharmaceutical societies will be held at Prague during the exhibition.

For particulars apply to "Dr. Charles Fragner, Pharmacien, Président du Comité Exécutif, à Prague."

SERICULTURE IN RHODES.

According to the "Journal de la Chambre de Commerce de Constantinople," the silk industry in Rhodes, which 30 years ago was a very important one, at one time showed a serious falling off, on account of disease among the silkworms. From a minimum annual harvest of 40,000 kilos. of fresh cocoons, the production had fallen to the insignificant figure of 1,800 kilos. During this crisis, which lasted several years, the mulberry trees were uprooted, as no longer forming a source of revenue.

In the last 10 years, however, the industry has again somewhat developed, and the production of the last harvest amounted to from 14,000 to 15,000 kilos. of fresh cocoons, including an increase of from 10 to 15 per cent. on the previous crop. But the mulberry trees, not having been replanted, are scarce, and the worms sometimes die from lack of nourishment.

The municipality of the capital of the island have imported from Beyrout some young mulberry shoots, and distributed them at cost price. These, however, are too young to yield a sufficient quantity of leaves.

The importation into Rhodes of French silkworms' eggs dates only from a few years back, and already amount to about 80 per

cent. of the total quantity imported. The remainder come from Broussa, or are produced in the island itself.

The cocoons, usually sold fresh, are quoted at about 12 piastres the ocque, and are exported to France and Beyrout.

The system most in vogue amongst the importers of eggs is to supply these to the breeders, receiving in return so much per cent. of the production, and there is a very small amount of eggs which are sold in the strict acceptation of the word.

Notwithstanding the satisfactory results obtained, after careful experiments, the supplies of eggs hardly realise their value, the reason being that there are no breeders on a large scale; but in all the 44 villages of the island there are many peasant breeders, whom it is impossible to supervise in their dealings, and who attribute the small amount of cocoons supplied to the egg merchant to a disease amongst the worms.

THE OPIUM CROP IN ASIATIC TURKEY.

A Smyrna correspondent of the "Journal de la Chambre de Commerce de Constantinople" writes that, according to recent advices from the interior of the vilayet of Aïdin, the autumn sowings of opium have been three parts destroyed by the excessive cold, the winter sowings have not taken place at all, the ground having been under snow at the time, and the spring sowings, last hope of the cultivator, have been totally destroyed by the frosts of the early part of April.

Information so hard from the districts gives reason to suppose that the total crop of opium will not amount to more than 3,000 "couffes."

A "couffe" is a high basket weighing about 75 kilograms.

It is calculated that the stock of Turkish opium available does not exceed 7,450 "couffes," an amount which has to last until August 1897, by which time the crop of 1896-97 will be ready.

THE LIGHTHOUSE SERVICE OF THE UNITED STATES.

The "Iron Age" of New York states that the magnitude of the workings of the United States lighthouse service are well shown by Report of the board for 1895. There are 1014 light-houses and beacon lights distributed over the great coast line of the United States. An expenditure of 100,000 dols. for a single light of this kind is common in the undertakings of the service, and when Congressional appropriations are liberal, 10 to 25 new lights of this kind may be established in a single year. There is no estimate in the board's report of the aggregate value of light-houses and lighthouse sites, but the total is certainly far up in the millions. In addition to these stationary lights, there were under the control of the lighthouse establishment on June 30, 1895, the

following aids to navigation: Lightships in position, 41; lightships for relief, 4; electric-lighted buoys in position, 11; gas buoys in position, 12; fog signals operated by steam or caloric engines, 130; fog signals operated by clock work, 185; post lights on the Western rivers, 1411; post lights on other rivers, 382; day or unlighted beacons, 424; whistling buoys in position, 66; bell buoys in position, 96; other buoys in position, including pile buoys and stakes in fifth district and buoys in Alaskan waters, 4547. In the construction, care and maintenance of these aids to navigation there were employed during 1895: Steam tenders (vessels of seagoing kind), 32; steam launches, 4; sailing tenders, 2; light keepers, 1203; other employees, including crews of light ships and tenders, 1078; labourers in charge of Western river lights, 1154; labourers in charge of other river post lights, 210.

The general appropriations for the year 1896 illustrate the scope of the work. These appropriations refer simply to maintenance, and represent yearly expenditures of the board, apart from anything in the way of new construction, and apart also from the salaries of a large number of army and navy officers who direct affairs in the institution and who are paid by the army and navy departments: Supplies of lighthouses, 410,000 dols.; repairs of lighthouses, 600,000 dols.; salaries of keepers of lighthouses, 700,000 dols.; expenses of light vessels, 350,000 dols.; expenses of buoyage, 500,000 dols.; expenses of fog signals, 125,000 dols.; inspecting lights, 5,000 dols.; lighting of rivers, 350,000 dols.; survey of lighthouse sites, 1,000 dols.

THE JAPANESE HABUTAYE INDUSTRY.

According to a recent issue of the "Japan Weekly Mail," a telegram despatched by the Japanese Consul in Paris states that the French Government has determined to impose a 6-franc duty on bleached *habutaye* silk. That is a very serious matter, but the message is so meagre that it is not possible to obtain a clear idea of the case. The "Nichi Nichi" thinks that the step taken by the French Government must be attributed to either of the following motives, viz. (1) to encourage the weaving of *habutaye* in France; or (2) to restrict the import of bleached *habutaye* with a view to undertake its bleaching in France, and to confine the import to unbleached *habutaye* silk alone. The "Nichi Nichi" is inclined to give more credence to the latter alternative, seeing that bleached *habutaye* is specified, and because also it will be difficult to start the weaving industry in France to compete successfully with Japan. Still, considered from another standpoint, and admitting that bleaching is defective in Japan, no satisfactory reasons can be adduced to justify the imposition of duties on Japanese bleached *habutaye*; neither can the "Nichi Nichi" bring itself to think that the defects in bleaching are so grave as to necessitate the imposition of a restrictive tariff, or the

bleaching industry so important as to need artificial encouragement. The export of unbleached *habutaye* is attended by many inconveniences, so that out of a *habutaye* export of about 10 million yen annually, the greater portion consists of bleached goods. If the worst be true about the new impost, the Japanese weavers must strive to improve the quality of their work, reduce the cost of production, and secure a firmer hold of the American market, where the larger part of Japanese *habutaye* ultimately goes. But if the cause lies in the bleaching explanation, then the weavers must exert themselves to improve their dyeing, and place themselves in touch with the latest fashions. Their aim must be to dispense with France as a medium for furnishing their goods to America, for it is a well-known fact that *habutaye* sent to France mostly goes to America after having first gone through certain decorative processes. What is to be apprehended from the imposition of a restrictive tariff in France is not so much the disappearance of Japanese *habutaye* from the French market, as that it will be forced out of America. The reason why Japanese *habutaye* must pass through France before reaching America is that France being the centre of fashion is also the world's best dyer. It is on this account that superior silks always pass through the hands of French artists before reaching the real consumers. Japan must strive to improve in the art of dyeing, bringing it up to the standard of the French, and, by taking care to keep abreast of the prevailing fashion, she will be able to develop the *habutaye* industry and satisfy the demands and meet the tastes of her American customers.

THE SILK TRADE OF YOKOHAMA.

The "Japan Weekly Mail" states that, according to the "Yomiuri," over 30,000 bales of silk are stocked in Yokohama for which no buyers are forthcoming. Wholesale silk merchants are experiencing considerable trouble owing to depression in trade and the steady fall in prices since the beginning of the season. Besides this, local middlemen are unable to recover the sums advanced by them to silk producers to help them to carry on until the stock was disposed of; and the middlemen themselves are being pressed by their bankers to liquidate their own overdrafts and advances on silk. The prospect is anything but cheering, for at present business with America is entirely suspended, while the demand from Europe is very meagre. Wholesale merchants are therefore placed in a very difficult dilemma, for should they force local producers to refund the money advanced on crops, they will practically bring them to the verge of bankruptcy and probably ruin the silk industry for years.

ABANDONMENT OF JAPAN'S SPINNING ENTERPRISE AT
SHANGHAI.

It is stated in the "Japan Weekly Mail" that the Shanghai Spinning Company (the Mitsui's undertaking) recently held a council to decide whether, in view of the present delay in the negotiation of the new Treaty of Commerce between Japan and China, the enterprise should be continued. The directors reported that, under the circumstances, they thought it rather too risky to establish a factory at Shanghai. The shareholders present endorsed the directors' views, and most of them were of opinion that it would be prudent to abandon the Shanghai undertaking for awhile, and that, by reducing the original 30,000 spindles to 20,000, the enterprise could much better be started at home, as it would not be too late to establish a factory at Shanghai after the Treaty had been definitely arranged.

Commenting on this news, the "Oriental Economist" says that since last Fall, when public and Governmental opinion was firmly determined that Japanese merchants should not lose the opportunity of extending their commercial influence in China, various enterprises have been mooted, the most important being the starting of spinning factories there. The establishment of two spinning factories was decided upon, one being projected by the Mitsui firm, and the other by the capitalists of Osaka. Both purchased land at Shanghai, sent to England for plant, and have, moreover, designed the buildings. In the meantime, however, the negotiation of the new treaty progressed very tardily, while, at the same time, the situation in the Far East assumed an ominous aspect. The result is that the spinning enterprise undertaken with so much alacrity has to be abandoned.

"The Japan Weekly Mail," however, goes on to say that the delay in concluding the new commercial treaty between China and Japan has nothing whatever to do with the abandonment of Japanese cotton-spinning projects in Shanghai. The difficulty lies in quite another direction. The Chinese are at present in a position to impose any taxes they please on cotton carried from the interior to an open port for the purpose of being there manufactured into goods intended for consumption in China. Were the goods destined for export abroad, it is possible that raw cotton for their manufacture might be allowed to reach the mills on payment only of the transit duties conventionally leviable in the case of exports. But since the manufactures are to remain in China, their raw material can not claim any of the exceptions accorded specifically to exports. Under such circumstances, it is plainly within the competence of the Chinese local authorities to impose prohibitive taxes on cotton travelling from the place of production to foreign factories in Shanghai. Two obvious reasons might operate to encourage the imposition of such transit duties. One is that, though bound by the provisions of the Shimonoseki Treaty to permit foreign manufacturing enterprises in the settle-

ments, the Chinese are distinctly opposed to such an innovation, and would be glad to prevent it by interfering with the supply of raw material. The other is that certain influential Chinese in high positions are pecuniarily interested in securing their own cotton-spinning enterprises against local foreign competition. It would be over-sanguine, and altogether contrary to experience of Chinese ways, to anticipate spontaneous liberal action in this matter. Japanese capitalists seem, therefore, to be adopting a prudent course when they refrain from erecting factories and setting up costly plants of machinery before they know whether a supply of raw material will be procurable on reasonable terms.

What assurance European and American projectors of Shanghai mills have as to supplies of cotton, is not apparent. If they can procure the raw material at a reasonable price, so can the Japanese, for the latter now enjoy most-favoured-nation treatment in China. But certainly before sinking large sums of money in buildings and machinery, it seems prudent to ascertain definitely whether the difficulty about transit dues can be overcome. The Japanese projectors lose nothing by suspending their enterprise at this stage. The machinery ordered from England can be set up in Kiushu, where there is ample room for another factory, and the land acquired in Shanghai can be sold for more than its cost.

WOOLLEN MILLS IN INDIA.

According to a statement issued by the India Office, there were six woollen mills at work at the close of 1895—one in the 24-Parganas in Bengal, one at Cawnpore, one at Bangalore, one at Dhariwal in the Punjab, and two in the city of Bombay. The number of looms and spindles in 1895 was 531 and 17,244, and the value of the out-turn is reported at about Rx. 256,360. The fabrics made at present consist mainly of blankets, great-coats for the police and native soldiery, and materials for servants' clothing. Serges and other cloths and hosiery are also made of excellent quality in the Cawnpore and Dhariwal mills, but the manufacture of superior goods is as yet comparatively limited.

BREWERIES IN INDIA.

From a recent circular issued by the India Office it appears that the quantity of beer brewed during 1895 amounted to 6,238,877 gallons, of which more than half was bought by the Army Commissariat, the remainder being left for consumption by the civil population. The average purchases of the Commissariat for the last five years (1891-95) have amounted to about 2,908,000 gallons yearly, the total average production in the same period having been 5,494,000 gallons. The army therefore

consumes about 53 per cent. of the production and the civil population about 47 per cent. The quantity taken for the troops does not, however, represent the whole consumption of malt liquor by them, for they also consume imported beer to a substantial extent, though it is imported by the trade and no longer by Government.

The quantity of beer imported into India has averaged 2,908,406 gallons yearly in the last five years; and the quantity brewed in India in the same period has averaged, as noted above, 5,494,000 gallons, the local production being thus nearly double the quantity imported.

Twelve out of the 23 breweries are located at stations in the Himalayas from Murree to Darjeeling, and much of the beer is brewed there. A good deal is also brewed at Lucknow Rawalpindi, Poona, Bangalore, at and near Ootacamund, and at Quetta and Mandalay. Last year the quantity brewed in the plains exceeded 2,000,000 gallons, though in the beginning of the industry and for a considerable period thereafter it was supposed to be impossible to brew beer in the plains. The largest of the breweries is at Murree, where about 851,000 gallons were brewed last year. The next largest is at Poona, where the brewing of last year amounted to 757,000 gallons. More than one-third of the whole production is brewed in the Panjab.

The commencement of the industry dates from 1850, when a brewery was established at Masuri. This was followed by another at Kasauli in 1852. These were followed by the brewery at Murree in 1860 and at Simla in 1862. The capital employed in the industry (so far as it is known) is Rx. 170,000.

FAST STEAMSHIP SERVICE BETWEEN CANADA AND ENGLAND.

According to the "Canadian Gazette," the determination of the Canadian and Imperial Governments to do all in their power to perfect the link of steamship communication between England and Canada is again evidenced by the new invitation for tenders recently issued. The proposed service is to be a weekly one between "Liverpool, or such other port as may be agreed upon," and Quebec in summer and Halifax in winter. The vessels are also to call, if required, at one Irish port, and may, after landing the mails, proceed to Montreal in summer and St. John in winter, but "are in no case to call at any foreign port." The necessity of making a call at a French port is done away with by the conclusion of a contract for a fortnightly direct steamship service between Canada and France and Belgium which is also announced from Ottawa this week; and the stipulation that the steamers shall not proceed to United States ports is in accordance with the announced determination of the Canadian Government to keep the trade carried on by subsidised

steamers in Canadian hands. The vessels are to be built in compliance with the requirements of the Admiralty respecting armed cruisers, to be of not less tonnage than 8,500 gross register tons, and the average speed on the round trip from port to port across the Atlantic is to be not less than 20 knots an hour. This, of course, means a capacity considerably greater than 20 knots. The passenger trade is provided for by a stipulation for not less than 275 first-class, 200 second-class, and 1,000 steerage passengers, with accommodation equal to that given on the best vessels plying to and from New York, while to meet the growing requirements of the export provisions trade of Canada the vessels are to be fitted with cold storage accommodation. Tenders were to be submitted by June 10th, with a deposit of 20,000*l.* sterling as security, while the service is to begin on May 1st, 1898, and to run for 10 years.

It will be felt that the conditions laid down are adequate to secure the really high-class weekly service that has long been felt to be called for by the growing importance of Canadian relations with Great Britain, and by the position which Canada is rapidly assuming in the chain of inter-Imperial communications.

REVENUE RETURNS OF CAPE COLONY.

A communication, dated May 12 last, has been received from the Agent-General for the Cape of Good Hope, enclosing copy of the "Cape of Good Hope Government Gazette" containing comparative statements, from which it appears that the revenue received in the month of March last was 623,503*l.*, showing a net increase of 174,506*l.* on the corresponding amount for 1895. For the nine months, July—March 1895–96, it amounted to 5,025,354*l.*, showing a net increase of 1,125,015*l.* on the amount for the corresponding period of 1894–95.

TRADE OF THE CAPE.

A communication, dated June 1 last, has been received from the Agent-General for the Cape of Good Hope, enclosing extracts from the "Cape of Good Hope Government Gazette," of May 12 last, from which it appears that the value of the imports into the colony for the four months ended April 30, 1896, including specie, amounted to 6,799,336*l.*, as compared with 4,756,402*l.*, for the corresponding period of 1895.

With regard to the exports, the total value for the four months ended April 30, 1896, including specie, was 5,382,233*l.*, as compared with 4,941,890*l.*, in the corresponding period of 1895.

TRADE OF NEW SOUTH WALES FOR 1895.

The following details of the distribution of the trade of New South Wales for the year 1895 are taken from the Sydney "Trade Review":—

Countries.	Imports.	Exports.		
		Domestic Produce or Manufacture.	British, Foreign, and other Colonial Produce and Manufacture.	Total.
	£	£	£	£
Victoria - - -	1,499,592	2,733,643	347,293	3,080,941
Queensland - - -	4,011,776	244,624	1,167,217	1,411,841
South Australia - - -	911,170	2,186,732	132,961	2,319,693
Western Australia - - -	32,332	49,406	95,009	144,415
Tasmania - - -	303,270	44,497	108,126	152,623
New Zealand - - -	563,528	177,366	299,918	477,284
Total Australasian Colonies	7,321,668	5,436,268	2,150,529	7,586,797
United Kingdom - - -	6,420,107	7,061,767	2,311,881	9,373,648
Other British Possessions - -	611,021	157,418	205,085	362,503
Foreign Countries - - -	1,639,619	3,800,670	811,167	4,611,837
Grand Total - - -	15,992,415	16,456,123	5,478,662	21,934,785

Excess of exports, 5,942,370*l*.

Imports from the United States totalled 624,268*l*., and exports thereto 683,606*l*.

ARTESIAN BORING IN AUSTRALIA.

With reference to recent experiments in Artesian boring for water in Australia, the "Australasian" states that the Gil Gil bore is down 2,900 ft., the flow being 2,000,000 gall. per diem. The Woolahbras bore has reached a depth of 1,200 ft. Three bores have been completed on the Boatman Estate, on a country which four years ago only supported a few head of horses and cattle. The first bore is 1,511 ft. deep, and has a flow of 4,200,000 gall. daily. It supplies eight miles of creeks, and waters 60,000 sheep. The second bore is 1,900 ft. deep, has a flow of 3,000,000 gall. daily, and supplies 300 square miles of hitherto waterless country. The third bore also has a flow of 3,000,000 gall.

XVII.—PROCEEDINGS AND DELIBERATIONS OF THE CHAMBERS OF COMMERCE OF THE UNITED KINGDOM.

Birmingham.—A monthly meeting of the Birmingham Chamber of Commerce was held on June 3 last. Mr. T. Walsh in the chair. Mr. Wright, of the Wright and Butler Lamp Manufacturing Company, Limited, attended the Council to suggest the advisability of appointing a representative to give evidence before the Select Committee of the House of Commons on the petroleum lamp question.—Mr. Wright said that the lamp trade in the district was seriously menaced, and traders were anxious that some decisive action should be taken to bring influence upon the Select Committee in London on the question of petroleum and petroleum lamps. The traders had put the Walsall Chamber of Commerce in action, and they were sending a witness; the Birmingham lamp trade was sending two witnesses, the London Chamber of Commerce two witnesses, the London lamp manufacturers three; the Liverpool houses were also to be represented; and it was felt that if, in addition to the witnesses from the Birmingham lamp trade, the Chamber of Commerce would put its influence into the movement, it would further add to the weight of the testimony it was desired to bring forward to effect the object they had in view. That object was the removal, as far as possible, from the minds of the Committee, of misconceptions founded on statements that had been made to them by so-called expert witnesses, which, in local opinion, were not founded on fact.

After some further discussion, it was decided that a representative of the Chamber should be appointed to give evidence.

Blackburn.—The monthly meeting of the Blackburn Chamber of Commerce was held on June 3 last, Mr. H. Harrison in the chair.

The president reviewed the action of the Chamber on the question of trade reports from consuls with a view to the improvement of home trade. After careful consideration, a committee of the Chamber was appointed to deal with the matter and approach the Foreign Office on the question. After considering the matter they felt they could not ask for anything further than was done at present. They were anxious to secure the appointment of commercial attachés to all consular stations to report all phases of trade. Personally, he thought the consular reports were very well done and covered almost every phase of the work. The reports were exact and unbiassed, and that being so they hesitated

to ask the Foreign Office to do more. It was finally decided to forward the resolution of the Chamber asking for the appointment of commercial attachés to all consular stations.

With regard to the commercial mission to China, the president stated that the arrangements for the completion of the mission were now practically settled. It only remained for Lord Salisbury to consent to a proposal respecting the appointment of a consul. The mission would start at the latter end of August. The amount promised to the fund at present amounted to 4,093*l.*, and the amount paid, 2,617*l.* That amount made the mission a success financially, and, providing they secured the gentleman they desired, the mission in every phase of its work would prove signally successful.

This was all the business.

Bradford.—A meeting of the Council of the Bradford Chamber of Commerce, was held on May 20 last, Mr. T. A. Duncan in the chair.

It appeared from the minutes of the Weavers' Wages List (Dress Goods) Committee that on the 4th May a meeting was held to consider alterations in the dress goods list suggested by the Trades Council Committee, and that most of the suggested alterations were approved. On the 7th May the Coatings Committee considered alterations suggested by the Trades Council in the coating list. Some of the suggested alterations were agreed to, and on others a compromise was proposed.

It appeared from the minutes of the Tariff Committee that, in reply to a letter from the Foreign Office asking for particulars as to the samples which it had been urged should be collected and sent home by British Consuls, it had been resolved to suggest that, so far as Bradford was concerned, the Consuls should be requested to forward samples of articles of important consumption, composed wholly or partly of wool, whether yarns or woven fabrics (*a*) produced in the country itself, (*b*) imported from foreign countries; together with information as to price, duty, widths, and other particulars, and the quantity or value of the goods imported or produced; and to send annually a return with respect to the increase or decline of British trade in such goods.

The secretary stated that a letter had been received from the South African Mohair-growers' Association, announcing the formation of that association, the principal object of which was to encourage the growth of a longer staple.

Mr. A. Crabtree said that this association was the outcome of letters sent by the Chamber to South Africa last year, and it was to him a very gratifying circumstance that an attempt should so soon have been made by the mohair-growers to do what was requested. The length which they fixed was six inches instead of seven. His representative, who would otherwise have been invited to the meeting at which the association was formed, was in England at the time, but he was now on his way to the Cape,

and would no doubt get them to adopt the length of seven inches.

A correspondence with the Postmaster-General on the subject of foreign parcel post rates was read, and a long discussion was raised on a motion hostile to the Truck Bill, which motion, however, was rejected by a large majority.

Bristol.—The monthly meeting of the Council of the Bristol Chamber of Commerce was held on 2nd June last, Mr. Mark Whitwill, junior, in the chair.

The Railways Committee of the Chamber reported on the subject of railway connections at Exeter. It was stated that the Great Western and London and South-Western Railway Companies had been approached upon the subject in the following terms:—

“Representations having been made to the Council of this Chamber with respect to the great inconvenience experienced by travellers for the want of better connections in the train service at St. David’s, Exeter, the Council, in the interests of the public, venture to suggest that some mutual arrangement should be arrived at between your companies upon this matter, so as to remedy the cause of complaint; and the suggestion which this Council would respectfully submit is that both companies should be prepared to make a concession upon one train per day up and down, so as to enable passengers to proceed upon their journey without such long detention. As an illustration, the Council would point out that the Great Western Railway 9.8 down train is timed to reach St. David’s at 11.3, and the South-Western train for Launceston leaves at 10.54, or nine minutes before the Great Western Railway train arrives, necessitating waiting until 1.52 for the next train—a waste of two and a half hours. The Council trusts, therefore, that your companies at no distant date will endeavour by some mutual concessions to bring about improvements in these and other connections at Exeter.”

Letters of acknowledgment were read from the Great Western and the London and South-Western Railway Companies, and a promise made by each that the subject should have careful consideration.

Other matters under consideration were passenger fares between Bristol and Ireland; signal stations in the Bristol Channel; extension of the free delivery boundary; and the registration of firms.

A copy has been received at the Board of Trade of the Annual Report for 1895–96 of the Bristol Incorporated Chamber of Commerce and Shipping, containing report of the council, trade reviews, and proceedings at the annual meeting of the members, with Appendix containing report of the annual banquet of the Chamber, statistics of the trade of the port, memorials and petitions, list of members, &c.

Leeds.—The monthly meeting of the Council of the Leeds Chamber of Commerce was held on May 27 last, Mr. J. Peate, president, in the chair. The correspondence read by the secretary

included a communication from the secretary of Lloyd's on the subject of telegraphic communication with lighthouses and light-vessels. The Plymouth Chamber of Commerce had expressed their views on the matter, which were that accidents would be better prevented if there was communication by telephone or telegraph between lighthouses and the mainland. Lloyd's seemed to attach greater importance to the erection of more lighthouses. It was agreed to support the opinion of the Plymouth Chamber. A communication from the London Chamber was read with reference to the proposed vocabulary of code words, which will be brought before the International Telegraphic Convention at Buda-Pesth. The secretary said that on the Continent there was an objection to the compulsory application of the proposed vocabulary, and a good many of the Chambers of Commerce in this country were willing to co-operate with London. The chairman remarked that it seemed to be a question for experts to pronounce an opinion on.

The next subject introduced was the Truck Bill now before Parliament. The secretary read its provisions, which, he said, dealt with deductions and payments in respect of fines, damaged goods, and materials. The United Cotton Manufacturers' Association had written asking the Chamber to co-operate with them in their view that the Bill ought not to apply to the textile trades. In the course of a discussion, the chairman remarked that it was the practice in the textile trades to impose fines for bad workmanship. He thought that to do away with fines would be a mistake. Under the Bill, each workman would be required to sign a contract with his employer—to his mind an entirely unnecessary proceeding. They found in practice that a fine acted, in certain circumstances, as a deterrent, and went much further than a remonstrance. But in the textile trades, fines amounted in the aggregate to a very small amount. The whole Bill seemed to him unnecessary, so far as the textile trades were concerned. The matter was referred to the Parliamentary Committee. Two resolutions passed by the Associated Chambers were read, the first suggesting that good might be done by collecting and exhibiting specimens of foreign goods imported into English markets, with the cost price attached to each; and the other urging Chambers of Commerce to keep in touch with the local county councils, in order to secure adequate representation on such educational authorities as might be appointed. The council resolved to take no action in regard to either matter.

Liverpool.—The monthly meeting of the Council of the Liverpool Chamber of Commerce was held on the 3rd June last, Mr. Charles McArthur in the chair.

Mr. F. C. Danson was elected president for the ensuing year.

From the proceedings of the General Trade Committee it appeared that correspondence had taken place with the Foreign Office on the subject of pressure to be put upon the Brazilian Government to complete the new quarantine stations promised

some time back, and especially the station for North Brazil ; also that a letter from the agents of Messrs. T. and J. Harrison, at Pernambuco, had been put before the same committee, which gave details as to the site and works at the new station of Tamandare, 45 miles south of Pernambuco, from which it appeared that satisfactory progress was there being made, and that vessels would in future be spared the *détour* to Ilha Grande so long complained of.

A memorandum of suggestions by the committee of the Liverpool Stock Exchange, in reference to the proposed assimilation and extension of trustees' investment powers, had been laid by a representative of that body before the council of the Chamber, with a request that the Chamber would assist the movement by memorialising the Lord Chancellor in favour of the embodiment in a Bill to be laid before Parliament of such of the suggestions as were approved. It was understood from the Liverpool Stock Exchange that their memorandum had been approved by the Stock Exchanges of Manchester, Sheffield, Leeds, Birmingham, Glasgow, and Dublin. A copy of the memorandum had been sent to each member of the council of the Chamber, accompanied by a request for any observations they might have to make upon the proposal contained therein. The views of the Law Society had also been asked for, and that society had just furnished the Chamber with a copy of a memorial on the subject forwarded on the 28th ultimo to the Lord Chancellor. It was the intention of the Commercial Law Committee of the Chamber to compare the proposals of the Law Society with those of the Stock Exchange, and to address the Lord Chancellor on the subject at an early date.

It was reported that the Salt Union, who had made a communication to the Chamber on the subject of the dues charged at Liverpool on salt, and to the desirability of their reduction, had been asked for a further detailed statement showing how it compared with other ports in the matter of dues on salt ; and they had been informed that such a statement, if furnished, would be laid by the Chamber before the Mersey Docks and Harbour Board, with a request that the question should be reconsidered.

Other matters referred to were : the insolvency laws of British Guiana, trade with Madagascar, commercial education, and the cost of the dock police.

A meeting of the committee of the African Trade Section of the Liverpool Chamber of Commerce was recently held, Mr. Alfred L. Jones in the chair.

A letter was read from the Colonial Office, dated 29th May, acknowledging receipt of a letter from the committee, making suggestions respecting the continuation of the second section of the Lagos Railway, and saying that the views expressed in the committee's letter will be borne in mind when the question of the extension of the Lagos Railway from Otta is under consideration. Correspondence with the Colonial Office on the subject of the industrial training of natives for railway services

was laid before the committee, which included a statement from the Government to the effect that practical difficulty would arise in having natives trained in this country by reason of the climate, and arrangements had in consequence been made with the Government of Ceylon that lads from the Gold Coast Colony should be trained at first in the technical school of the colony, and afterwards in the Government factory, the railway workshops, and in the Public Works Department.

A letter was laid before the committee from a firm interested in the Sierra Leone trade, which stated that on the 1st August next it is the intention of the Governor of Sierra Leone to raise the duty on cotton goods from $7\frac{1}{2}$ per cent. to 10 per cent. *ad valorem*. The letter went on to say that this will have the effect, as in the case of the recent increase in the duty on spirits, of driving the trade from the colony into the neighbouring French possessions, and will necessitate further heavy taxation in order to enable sufficient revenue to be raised to meet the expenditure necessary for the government of the colony.

A communication on the subject of the bar at Lagos and the possibility of dredging operations being successful, which was laid before the committee, was postponed for further consideration, and this concluded the important business of the meeting.

North Staffordshire.—The annual meeting of the North Staffordshire Chamber of Commerce was held on May 20 last. The annual report was of great length, and described with much detail the work of the Chamber during the past 12 months. It stated that although the year had not shown any remarkable advancement in trade, the year's results were not unsatisfactory. Locally the statistics pointed to a slow but steady improvement in all directions, and the Council trusted that that general upward tendency might be fully maintained, and North Staffordshire receive its full share of the increase. The business transacted during the year had been of an important character, bearing in many instances directly upon the interests of local manufacturers and traders, who would be able to see that the support they had accorded the Chamber had been justified by results.

The report also drew attention to the increased charges of the North Staffordshire Railway Company, and described the action of the Chamber to secure an amendment of the law relating to glass and earthenware measures, as also steps taken with regard to the Factory and Workshops Bill, railway rates on earthenware, and fire insurance rates.

At the monthly meeting of the Chamber on the same date, the minutes of the Railway, Canal, and Telephone Committee were read and adopted. These minutes stated that at a meeting held on the 20th of April, the committee considered the complaint made by the Colliery Owners' Association against the North Staffordshire Railway Company that the latter claimed payment of the full rate on ironstone traffic, after allowing a rebate of *5d.* for from 10 to 12 years. The association also complained of the way their

group rate was arrived at, and questioned whether the rates charged traders on other systems did not amount to an undue preference as compared with local rates. It was arranged that additional information should be obtained, to be laid before a future meeting.

Nottingham.—A meeting of the Council of the Nottingham Chamber of Commerce was held on the 1st June last, Mr. G. Wigley in the chair.

A communication from the Associated Chambers of Commerce of the United Kingdom asked the Council's opinion as to samples of goods manufactured in other countries being exhibited in the locality. The chairman intimated that the committee had adopted the resolution contained in the communication. The terms of the resolution were as follow: "That in the opinion of this Association it is advisable for individual Chambers of Commerce to consider whether good would not be done to the trades in their immediate vicinity by collecting and exhibiting specimens, with the cost price attached to each, of foreign goods imported into the English market, so that manufacturers and workpeople might judge whether those goods could not be manufactured by them rather than imported from foreign countries."

The consuls at Canton and Amoy (China) had forwarded a report as to the lace and hosiery goods imported into China from foreign countries, accompanied by samples and prices, this having been done at the request of the Chamber, conveyed through the Foreign Office. It was resolved, after consideration by the Foreign Trade Committee, that the patterns should be submitted for exhibition to members of the Chamber in a room at that exchange, and at some future time the same articles should be submitted for inspection to manufacturers and others interested.

Wolverhampton.—The annual meeting of the Wolverhampton Chamber of Commerce was held on May 13 last, Mr. A. McBean presiding.

The chairman, in moving the adoption of the report, said that a great improvement had occurred in trade during the past 12 months, and most of the local branches of industry had benefited. He was of opinion that the trade had come to stay. The Australian and Argentine markets had improved, and South Africa, despite recent unfortunate events, had provided some good orders. The home trade had also expanded, but they had, however, to record that foreign trade had increased in a still greater ratio. It was a hard thing that though England had established extensive colonies, and had pioneered trade in many other parts, other nations were enjoying to her prejudice so much of the trade with those countries. He thought England ought to have some sort of preference, particularly with her colonies, and it was to those colonies that she ought to look rather than to other countries for the maintenance of her over-

sea trade. He trusted that the railway companies would reduce their transit charges, and he felt that, unless they altered their policy, they would do much to strangle trade.

Mr. S. T. Mander seconded the motion. He said the recent Board of Trade returns showed that in connection with the increase in every branch of foreign trade our exports to those countries which were our most formidable rivals had increased to a considerable extent, while our exports to Germany had increased to a far greater extent than the exports to any other country. The Germans were our greatest rivals, but the returns showed that we were carrying the war into the enemy's camp, and successfully competing with them in their own land. There was no greater object lesson for English manufacturers than the history of Germany, and by a study of the German methods England might learn how to increase her trade. The reasons why Germany had progressed so greatly in industry and commerce were: (1) the people were exceptionally frugal, and secured a comfortable standard of living at a comparatively low outlay; (2) the co-operation of the German Government with the traders, partly by means of their excellent consular staff, and partly through the State purchase of railways and the reduction of the transit rates for merchandise; (3) the wonderful degrees of attention given by the Government to the subject of education. During the past century they had reared up what Matthew Arnold had described as the most perfect system of education in the world. This system combined intellectual culture with commercial and industrial training, and was accountable for no small share of the commercial advantage now enjoyed by that country. He was glad that the English commercial supremacy had been challenged, as it was only by adopting improved methods that England could successfully compete with her rivals.

Mr. W. W. Walker, in supporting the motion, said that some months ago he expressed the opinion that the home of the gavanised iron trade would soon be found in Germany, and he was now inclined to go a little deeper into the subject. It was an exploded fallacy to call German sheets rubbish, as they were almost as good as English sheets, and could be produced at 1*l.* a ton less than in this country. The Germans could purchase spelter cheaper because the bulk of it came from Silesia; chemicals were also cheaper as was also wood for packing cases. The price of wire nails was easier, and labour was between 30 and 40 per cent. lower than in England; while over-sea freights were in favour of Germany. Reckoning all the items, he concluded that there was a saving of 2*l.* per ton in favour of the German manufacturer.

XVIII.—RECENT TRADE BLUE BOOKS.

1. *Board of Agriculture. Agricultural Returns for Great Britain showing the Acreage and Produce of Crops, Prices of Corn, and Number of Live Stock with Agricultural Statistics for the United Kingdom, British Possessions and Foreign Countries, 1895. (C.—8073.) Price 1s. 6d.*

This is an annual statement issued by the Board of Agriculture. In the report which is prefixed to the return, it is stated that the statistical information included under this title has been considerably extended, and is now embodied for more convenient reference, in a single volume, which takes the place of three annual publications laid before Parliament separately in former years. Thus consolidated the returns show the acreage of crops and number of live stock as collected from the occupiers of land in Great Britain in June by officers of the Inland Revenue: the annual estimates of the produce of the principal crops obtained during the autumn and transmitted to the Board of Agriculture in November: and the complete statistics of the prices of British corn for the calendar year ending 31st December 1895, ascertained under the Corn Returns Act, 1882. These official records are accompanied by comparative figures for earlier years, and, as on former occasions, they are supplemented by statements respecting the crops and live stock in Ireland, the Isle of Man, and the Channel Islands, supplied respectively by the Registrar-General for Ireland, and the Lieutenant-Governors of the Isle of Man, Jersey, and Guernsey, which enable a general view of the agricultural position of the United Kingdom to be presented.

Summaries of Colonial and Foreign Agricultural Statistics, up to such dates as can be obtained, are also incorporated in the volume, while the incidental data respecting the imports, exports, and values of various forms of agricultural produce—in the collection of which the Board of Agriculture again acknowledge the co-operation of the India Office, the Board of Trade, and the Commissioners of Her Majesty's Customs—have been extended so as to embrace, where practicable, the figures for 1895.

In advance of the final tables, now brought together in the annual volume, the more important features of each separate section of the Agricultural Returns have been separately placed in the hands of the public, as completed, in seven successive instalments.

In addition to the ordinary subjects of investigation, the Agricultural Returns of 1895 give the results of a further endeavour to show the extent of woods and plantations in Great Britain which lie outside the scope of the annual inquiries. The approximate estimate on this occasion shows a total of 2,726,000

acres, whereof 132,000 acres are returned as planted in the last 15 years.

The aggregate area occupied exhibits on the whole but little variation from the results recorded in 1891, when information of this character was last collected, the total then given being 2,695,000 acres. England and Wales show some increase, and Scotland a decrease, when compared with the totals of the year referred to, but it is necessary again to repeat the caution, formerly given, that details of this class cannot be regarded as strictly comparative. In all cases where investigations are only made after an interval of some years, changes necessarily occur, both in the persons making and in those collecting the returns, which leave much room for varying interpretations of the instructions and for discrepancies from which annual estimates are more generally exempt. In particular, in this inquiry, it has to be remembered that opinions vary as to the extent of planting to which the term "wood" can be properly applied while the totals are affected, not only by actual extensions or reductions of woodland area, but by numerous corrections in both directions in the figures of former estimates, where the measurements have been ascertained with more exactness than before. It is believed that the areas of woodland estimated for 1895 may be accepted as a closer approximation than has been previously available, much care and attention having been given to the matter both by the collecting officers and by the owners and agents of important estates.

Of the entire measured surface of land and water in Great Britain covering 56,772,000 acres, woods and hill pastures together cover 15,000,000 acres, or 27 per cent. The cultivated area actually accounted for in the occupiers' schedules in 1895 as under crops or grass, temporary or permanent, is little more than twice this total, or 32,578,000 acres. The area not brought into account under either of these groups includes all the territory occupied for non-agricultural purposes or covered by fences, water, roads, railways, towns, and buildings, as well as the surfaces incapable of profitable occupation, and those held in plots below the acre limit of the holdings whence returns are collected.

A coloured map of Great Britain in agricultural divisions, appears in the present volume and in that of 1894, to indicate how widely the surface character of these areas varies not only as regards the relative share of territory falling into the several categories just indicated, but also as respects the proportion of the 32,578,000 acres of cultivated land which is returned as under permanent pasture and arable land. The totals so far as distinguished in 1895 may be shown as under :—

Divisions.	Total Area of Land and Water.	Returned as under Woods and Plantations.	Approximate Area of Mountain and Heath Land used for Grazing.	Permanent Pasture.	Arable Land.
	Acres.	Acres.	Acres.	Acres.	Acres.
England (<i>see Note</i>) :—					
I. Eastern and North-Eastern Counties.	7,410,000	232,000	70,000	1,962,000	4,312,000
II. South-Eastern and East Midland Counties.	7,215,000	593,000	118,000	3,053,000	2,612,000
III. West Midland and South-Western Counties.	8,111,000	455,000	329,000	3,831,000	2,561,000
IV. Northern and North-Western Counties.	9,808,000	386,000	1,587,000	4,399,000	2,115,000
Wales—V. - - -	4,774,000	182,000	1,099,000	1,979,000	860,000
Scotland :—					
VI.—Eastern Counties -	6,545,000	457,600	2,127,000	521,000	2,174,000
VII. Western Counties -	12,909,000	421,000	7,276,000	866,000	1,333,000
Great Britain - - -	56,772,000	2,726,000	12,606,000	16,611,000	15,967,000

Note.—Division I. (a.) Eastern Counties, viz., Bedford, Hunts, Cambridge, Suffolk, Essex, Herts, Middlesex, London.

(b.) North-Eastern Counties, viz., Norfolk, Lincoln, York (East Riding).

Division II. (a.) South-Eastern Counties, viz., Kent, Surrey, Sussex, Berks, Hants.

(b.) East Midland Counties, viz., Notts, Leicester, Rutland, Northampton, Bucks, Oxford, Warwick.

Division III. (a.) West Midland Counties, viz., Salop, Worcester, Gloucester, Wilts, Monmouth, Hereford.

(b.) South-Western Counties, viz., Somerset, Dorset, Devon, Cornwall.

Division IV. (a.) Northern Counties, viz., Northumberland, Durham, York (North Riding), York (West Riding).

(b.) North-Western Counties, viz., Cumberland, Westmoreland, Lancashire, Cheshire, Derby, Stafford.

2. *Emigration and Immigration. Copy of Statistical Tables relating to Emigration and Immigration from and into the United Kingdom in the Year 1895, and Report to the Board of Trade thereon.* 130. Price 6d.

These are the usual tables together with the report thereon which are prepared and issued annually by the Statistical Department of the Board of Trade. The present issue gives the details for the year 1895, with comparative tables for previous years.

3. *General Abstract of Marriages, Births, and Deaths registered in England and Wales in the Year 1895.* (C.—8047.) Price 1d.

This is a return issued by the Registrar-General of Births, Deaths, and Marriages, and is dated the 18th March 1896. It contains statements showing the marriages, births, and deaths registered in England, and in the registration divisions and counties, in the year 1895.

In a note it is stated that the number of births and deaths contained in these tables are obtained from the quarterly returns furnished by the local registrars, and will probably be found to differ slightly from the revised figures to be hereafter presented to Parliament in the 58th Annual Report of the Registrar-General.

4. *Patent Office, Thirteenth Report of the Comptroller-General of Patents, Designs, and Trade Marks, with Appendices for the Year 1895.* (C. 8090.) Price 2½d.

This annual report is issued by the Comptroller of the Patent Office in pursuance of the requirements of the 102nd section of the Patents, Designs, and Trade Marks Act, 1883 (46 & 47 Vict. c. 57).

The following statement shows the total number of applications for patents, designs, and trade Marks and amount of gross receipts and expenditure in each year, from 1884 to 1895 inclusive.

Years.	Patents.	Designs.		Trade Marks.	Gross Receipts.	Gross Expenditure.
		Single.	Sets.			
	No.	No.	No.	No.	£	£
1884	17,110	19,515	238	7,104	103,827	64,123
1885	16,101	20,388	337	8,026	88,509	77,739
1886	17,176	23,717	324	10,677	106,754	*109,567
1887	18,059	25,734	309	10,586	124,279	81,577
1888	19,089	25,923	316	13,315	149,623	83,924
1889	21,004	24,370	335	11,316	172,820	79,286
1890	21,309	22,235	318	10,258	192,606	83,240
1891	22,878	21,673	277	10,787	203,520	*103,180
1892	24,179	19,269	258	9,101	199,859	*96,822
1893	25,107	19,174	306	8,675	†174,878	*95,103
1894	25,386	21,230	1,025	8,013	181,855	*96,092
1895	25,065	20,507	910	8,272	187,012	*100,671

* Including expenditure on account of new offices and buildings.

† The diminution in the gross receipts was caused by a reduction in the amount of renewal fees in respect of patents.

It is stated in the report that in 1895, for the first time since the year 1885, the number of applications for patents showed no increase upon that of the preceding year, but on the contrary, a decrease of 321. This decrease, however, was entirely due to a falling off of 676 in the number of applications accompanied by *provisional* specifications, those accompanied by *complete* specifications having increased in even more than the usual proportion, by 355.

Five hundred and ninety-one, or 2·3 per cent. of the total number of applications, were made by women during the year; about 184 being for inventions connected with articles of dress.

The number of readers frequenting the Patent Office Library during the year was 108,285, or a decrease of 5,089 as compared with the previous year. 33,240 persons availed themselves of its use after 4 o'clock.

The gradual re-construction of the office has been advanced during the year by the completion of a new block of buildings on the south side of Staple Inn. Alterations in the adjacent block will next be taken in hand.

Reports of 55 patent cases tried in courts of law in the United Kingdom during 1895 were published in accordance with the

requirement of section 40 (1) of the Act, as well as 12 reports of trade mark cases, and 6 reports of design cases.

The number of designs applied for during the year amounted to 20,507, exclusive of 910 "sets" of designs. In the previous year 21,230 single designs and 1,025 "sets" were applied for. The term "set" includes any number of articles ordinarily on sale together, irrespective of the varieties of the size or arrangement in which the particular design may be shown on each separate article.

The total number of applications for the registration of trade marks during the year (including 77* applications to the Cutlers' Company of Sheffield) was 8,272, as compared with 8,013 applications in the previous year.

5. *Suez Canal. Returns of Shipping and Tonnage, 1893, 1894, and 1895. (Commercial No. 4, 1896.) (C.—8020.) Price 1½d.*

This is the annual return of the British directors of the Suez Canal. It states that the net tonnage for the past year shows an increase of 408,208 tons, as compared with that of 1894, and of 788,315 tons as compared with that of 1893.

The amount of dues has risen from 73,776,827 fr. in 1894 to 78,103,717 fr. in 1895, being an increase of 4,326,890 fr.

The number of vessels which passed through the canal was 3,341 in 1893, 3,352 in 1894, and 3,434 in 1895, of which 2,405 in 1893, 2,386 in 1894, and 2,318 in 1895 carried the British flag.

Although the number of British ships has gradually fallen, owing to the larger size of the vessels, the actual tonnage has increased, having risen from 5,752,934 in 1893 to 5,996,796 in 1894, and 6,062,587 in 1895.

The per-centage of British vessels and their tonnage has somewhat declined in 1895, but there has, however, been a slight increase in the number of German, Dutch, French, and Italian vessels, owing, no doubt, in the case of the two latter countries, to the passage of transports conveying troops to Madagascar and Massowah.

A table is given showing the shipping divided into the various classes of merchant vessels, mail steamers, war ships, and transports, Government chartered vessels, and vessels in ballast. Of 2,557 merchant vessels and vessels in ballast, of a net tonnage of 6,145,425 tons, passing through the canal, 2,023, of a net tonnage of 5,142,011 tons, were British, that is, fully 79 per cent. of the number and nearly 83 per cent. of the tonnage; and 221, or 8·6 per cent., were German vessels, whose tonnage was 7·1 per cent. of the whole. France, Holland, Norway, and Sweden combined furnishing a total of 9·5 per cent. of the vessels and 7·5 per cent. of the tonnage of the carrying trade to

* Of this number, 16 were in respect of old corporate marks, for which no fee is charged.

the East through the Suez Canal. These figures also show a slight falling off in British vessels as regards per-centage, but an increase in number and tonnage. The number and tonnage of vessels in ballast, 25 of 40,435 tons against 49 of 92,244 tons in 1894, indicates apparently an improvement in trade with the East.

In the years 1881-91 the annual net tonnage ranged from 4,136,779 tons to 8,698,777 tons, and the transit receipts from 51,274,352 fr. to 83,422,101 fr.; the average of the net tonnage was 6,179,843 tons, and of the transit receipts was 63,459,028 fr.; while in 1895 the net tonnage amounted to 8,448,383 tons, and the transit receipts to 78,103,717 fr. The mean net tonnage per vessel also rose from 1,517 tons in 1881 to 2,460 tons in 1895, being 62 tons per vessel in excess of 1894, and 168 tons more than in 1893.

The mean duration of passage for all vessels navigating the canal shows a decrease from 19 hrs. 55 mins. in 1894 to 19 hrs. 18 mins. in 1895. In 1894 the per-centage of vessels navigating by night was 94·8 per cent., at which figure it remained during 1895, the number being 3,266 in 1895, and 3,180 in 1894. The British directors again call attention to the satisfactory manner in which the duties of the transit department are carried on, notwithstanding the difficulties presented by the night service.

In the year 1870, 26,758 passengers were carried through the canal; in 1880 the number had risen to 98,900, and in 1890 to 161,352; in 1895 it amounted to 216,936.

6. *Agricultural Produce (Ireland). Return of the Average Prices of Agricultural Produce in Ireland, as collected by the Irish Land Commissioners Year by Year from 1881 to 1895 inclusive.* 136. Price $\frac{1}{2}$ d.

This is a return to the order of the House of Commons, dated the 10th of March 1896, for a statement showing the average prices of agricultural produce in Ireland as collected by the Irish Land Commission for each of the years from 1881 to 1895 inclusive. The return gives the average prices during the period named of wheat, oats, barley, hay, potatoes, butter, eggs, flax, beef, mutton, &c.

7. *Sea Fisheries (England and Wales). Tenth Annual Reports of the Inspectors (for 1895) (C.—8065.) Price 9d.]*

This volume contains the annual reports issued by the Fisheries Department of the Board of Trade, which have been prepared by the inspectors of fisheries for England and Wales. In addition to the reports of the inspectors the return now issued contains statements relating to the annual returns and accounts of local fisheries committees, particulars relating to apprentices to the sea fishing service for the last five years, number of applicants examined, and certificates of competency and of service issued under Merchant Shipping Acts, statements showing the number of fishermen who died at sea when serving on board fishing boats

belonging to England, Scotland, and Ireland respectively during the year 1895, accounts of fish delivered and of fish seized and condemned at Billingsgate, Shadwell, and Farringdon Markets during the year 1895, besides accounts of formal investigations into casualties to fishing vessels held by inspectors during the past year.

8. *Salmon and Freshwater Fisheries (England and Wales). Thirty-fifth Annual Report of the Inspectors of Fisheries (England and Wales) (for 1895). (C.—8064.) Price 1s. 9d.*

This paper contains the detailed reports for the year 1895 on the Salmon and Freshwater Fisheries of England and Wales by the inspectors under the Fisheries Acts. In the appendices which are attached to the reports are given in full special reports on inquiries instituted during the year by the inspectors, together with the abstracts of returns of license duties, annual returns and annual accounts of Boards of Conservators, returns of water bailiffs, prosecutions, &c., tables of limits of fishery districts, close seasons, rod license duty, &c.

XIX.—QUARANTINE NOTICES.

The following information with regard to quarantine, received from Her Majesty's representatives abroad, has been communicated to the Board of Trade through the Secretary of State for Foreign Affairs :—

NETHERLANDS.—The "Official Journal" of 21st May last contains a notice declaring Alexandria to be infected with cholera, and imposing seven days' quarantine on all arrivals from that port.

PORTUGAL.—Ceara and its province declared free from yellow fever from 1st May last.

GREECE.—Ten days' quarantine on arrivals at Athens from Egyptian ports which left after 10th May last. The quarantine is to be undergone at Deloz.

JAPAN.—With reference to the regulations made by Her Britannic Majesty's Chargé d'Affaires in Japan on the 20th day of June 1894 to secure the observance by British subjects of the Imperial Japanese Government regulations for the inspection of vessels arriving from localities infected with the pest, Her Britannic Majesty's Envoy Extraordinary and Minister Plenipotentiary in Japan has received a note from his Excellency the Japanese Minister for Foreign Affairs, stating that the said regulations for the inspection of vessels will now be put in force at the ports of Yokohama, Kobe, Nagasaki, Kuchinotsu, Shimonoseki, and Moji, in the case of all vessels arriving directly, or otherwise, from Hong Kong and ports on the China coast.

The said inspection will take place :—

For vessels proceeding to the port of Kobe at Wada Point ;

For vessels proceeding to the port of Yokohama at Nagahama ;
and

For vessels proceeding to the port of Moji at Shimonoseki.

When it is necessary to enforce disinfection in the case of vessels bound for the port of Kuchinotsu, in Nagasaki Prefecture, or for any other special port of export, they shall proceed to the nearest disinfecting station.

The following information has been received at the Board of Trade through the Secretary of State for the Colonies :—

MALTA.—Passengers arriving from any port of the Mediterranean, east of Gibraltar, shall not be permitted to land at Malta unless they declare on oath to the Port Authority that they have not resided or been in Egypt for 21 days previous to their arrival.

The importation of all fruits from Egypt is forbidden.

Vessels communicating with Egyptian ports will be subject to seven days' quarantine from date of departure, and vessels with passengers from Egyptian ports—21 days on board after arrival.

XX.—POST OFFICE NOTICES.

MAILS FOR THE WEST AND SOUTH-WEST COASTS
OF AFRICA.

The following table shows the places for which Mails will be despatched by the Mail Packets leaving Liverpool for the West and South-West Coasts of Africa during the latter part of the month of June 1896 :—

Wednesday, 17th June.—Madeira, Teneriffe, Goree, Dakar, Rufisque, Bathurst, Isles do Los, Conakry, *Sierra Leone, *Sherbro, Monrovia, Sinoe, Grand Bassa, Kroo Coast, Half Jack, Lahou, Cape Lahou, Grand Lahou, Drewin, Grand Bassam, Assinie, Axim, *Cape Coast Castle, Elmina, Salt Pond, *Accra, Pram Pram, Addah, Quitta, Jellah Coffee, Lome, Little Popo, Grand Popo, New Calabar, Abonema, Bakana, and Degama.

Saturday, 20th June.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Bonny, Old Calabar, Opobo, Benin, Warree, New Calabar, Bakana, Brass, and Liberia (Supplementary Mail).

Saturday, 27th June.—Grand Canary, Sierra Leone, Sherbro, Cape Coast Castle, Accra, Lagos, Forcados, Benin, Sapelli, Warree, Brass, and Akassa.

In London the correspondence should be posted on the previous day in each case.

Correspondence for Senegal (Goree, Dakar, Rufisque), the Congo (Banana, Boma), and Portuguese possessions on the West and South-West Coasts of Africa is not sent via Liverpool unless specially so superscribed.

The Packets in question carry Parcel Mails for Bathurst, Sierra Leone, Monrovia, Axim, Cape Coast Castle, Salt Pond, Winnebah, Accra, Addah, Quitta, Lagos, Benin, Bonny, Brass, New Calabar, Old Calabar, Opobo, and Warree.

The Parcel Mails are made up at Liverpool on the afternoon of the day before the departure of each Packet.

* Correspondence for Sierra Leone, Sherbro, Cape Coast Castle, and Accra will not be sent by the steamer of 17th of June, unless specially so superscribed as the direct packet leaving on the following Saturday is due at those ports some days earlier.

XXI.—STATISTICAL TABLES.

I.—FOREIGN TRADE OF THE UNITED KINGDOM.

STATEMENT of the IMPORTS into and of the EXPORTS from the UNITED KINGDOM during the Month and Five Months ended 31st May 1896, compared with the corresponding periods of the year 1895.

MONTH ENDED 31ST MAY.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Month ended 31st May		Increase.	Decrease.
	1896.	1895.		
	£	£	£	£
I. Animals, living (for food) - -	958,327	827,605	130,722	—
II. (A.) Articles of food and drink, duty free - - - -	10,628,933	11 815,409	—	1,186,476
(B.) Articles of food and drink, dutiable - - - -	1,716,187	1,734,648	—	18,461
Tobacco, dutiable - - - -	330,166	265,697	64,469	—
III. Metals - - - -	1,767,629	1,572,963	194,666	—
IV. Chemicals, dyestuffs, and tanning substances - - - -	461,232	552,232	—	91,050
V. Oils - - - -	581,425	704,954	—	123,529
VI. Raw materials for textile manufactures - - - -	5,615,257	5,704,849	—	89,592
VII. Raw materials for sundry industries and manufactures - - - -	3,483,085	3,471,679	11,406	—
VIII. Manufactured articles - - - -	6,608,402	6,851,996	—	243,594
IX. (A.) Miscellaneous articles - - - -	1,141,078	1,168,445	—	27,367
(B.) Parcel post - - - -	58,267	81,559	—	23,292
Total Value - - - -	33,349,988	34,752,086	—	1,402,098

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURE.

	£	£	£	£
I. Animals, living - - - -	78,369	86,728	—	8,359
II. Articles of food and drink - - - -	819,453	783,774	35,679	—
III. Raw materials - - - -	1,546,614	1,795,211	—	158,597
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - - - -	7,628,614	7,661,279	—	32,665
(B.) Metals and articles manufactured therefrom (except machinery) - - - -	2,951,548	2,460,717	490,831	—
(C.) Machinery and millwork - - - -	1,467,166	1,344,837	122,329	—
(D.) Apparel and articles of personal use - - - -	663,047	543,261	119,786	—
(E.) Chemicals and chemical and medicinal preparations - - - -	707,812	808,410	—	100,598
(F.) All other articles, either manufactured or partly manufactured - - - -	2,804,901	2,830,800	—	25,899
(G.) Parcel post - - - -	145,403	119,727	25,676	—
Total Value - - - -	18,812,927	18,344,744	468,183	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

Total Value - - - -	£	£	£	£
	4,770,958	5,215,785	—	444,827

I.—FOREIGN TRADE OF THE UNITED KINGDOM—*cont.*

FIVE MONTHS ENDED 31ST MAY.

I. IMPORTS FROM FOREIGN COUNTRIES AND BRITISH POSSESSIONS.

	Five Months ended 31st May		Increase.	Decrease.
	1896.	1895.		
I. Animals, living (for food) - - -	£ 4,414,091	£ 3,015,887	£ 1,398,204	£ —
II. (A.) Articles of food and drink, duty free - - -	57,051,896	54,432,422	2,619,474	—
(B.) Articles of food and drink, dutiable - - -	9,285,575	9,533,656	—	248,081
Tobacco, dutiable - - -	1,575,700	1,137,298	438,402	—
III. Metals - - -	3,383,210	7,207,655	1,175,555	—
IV. Chemicals, dyestuffs, and tanning substances - - -	3,677,450	3,230,726	446,724	—
V. Oils - - -	3,391,971	3,130,653	261,318	—
VI. Raw materials for textile manufactures and manufactures - - -	36,940,177	37,223,890	—	283,713
VII. Raw materials for sundry industries and manufactures - - -	16,073,570	14,176,707	1,896,863	—
VIII. Manufactured articles - - -	34,027,193	30,648,330	3,378,863	—
IX. (A.) Miscellaneous articles - - -	6,043,512	5,638,926	404,586	—
(B.) Parcel post - - -	439,385	454,466	—	15,081
Total Value - - -	181,303,730	169,830,616	11,473,114	—

II. EXPORTS OF BRITISH AND IRISH PRODUCE AND MANUFACTURES.

	£	£	£	£
I. Animals, living - - -	313,955	253,272	60,683	—
II. Articles of food and drink - - -	4,018,619	3,697,541	321,078	—
III. Raw materials - - -	6,988,489	6,926,821	61,668	—
IV. Articles manufactured and partly manufactured, viz. :—				
(A.) Yarns and textile fabrics - - -	44,351,161	40,904,855	3,446,306	—
(B.) Metals and articles manufactured therefrom (except machinery) - - -	13,501,345	10,767,507	2,733,838	—
(C.) Machinery and millwork - - -	6,726,655	5,821,388	905,267	—
(D.) Apparel and articles of personal use - - -	4,228,306	3,426,009	802,297	—
(E.) Chemicals and chemical and medicinal preparations - - -	3,769,888	3,499,380	270,508	—
(F.) All other articles, either manufactured or partly manufactured - - -	13,932,774	12,527,858	1,404,876	—
(G.) Parcel post - - -	641,477	492,745	148,732	—
Total Value - - -	98,473,669	88,347,416	10,155,253	—

III. EXPORTS OF FOREIGN AND COLONIAL PRODUCE.

	£	£	£	£
Total Value - - -	25,269,813	24,965,581	304,232	—

II.—CORN PRICES.

RETURN of the QUANTITIES SOLD and AVERAGE PRICES of BRITISH CORN, IMPERIAL MEASURE, as received from the Inspectors and Officers of Excise during the under-mentioned periods.

Periods.	Wheat.	Barley.	Oats.
Quantities sold.			
	Qrs. bus.	Qrs. bus.	Qrs. bus.
Week ended 2nd May 1896	37,806 1	12,843 4	9,935 4
„ 9th „ „	35,439 6	4,335 1	9,324 0
„ 16th „ „	37,245 0	4,471 4	9,288 0
„ 23rd „ „	36,895 5	3,359 4	6,672 6
„ 30th „ „	32,765 0	1,463 7	8,149 2
May 1896	180,151 4	26,473 4	43,369 4
Corresponding month in 1895	180,362 0	19,119 1	41,152 0
„ „ 1894	148,800 1	6,216 1	18,535 5
Average Prices.			
	s. d.	s. d.	s. d.
Week ended 2nd May 1896	25 8	22 6	14 3
„ 9th „ „	25 7	21 0	14 4
„ 16th „ „	25 7	21 0	14 5
„ 23rd „ „	25 6	21 8	14 6
„ 30th „ „	25 4	21 5	14 10
May 1896	25 6	21 6	14 5
April 1896	24 10	21 10	13 6
March „	25 2	21 3	13 7
February „	26 2	22 2	13 11
January „	25 7	23 10	13 10
December 1895	24 11	23 11	13 11
November „	25 9	25 1	14 2
October „	24 6	25 5	13 5
September „	22 10	24 0	13 7
August „	24 2	20 3	15 7
July „	24 9	19 8	16 1
June „	25 9	19 7	16 0
May 1895	22 5	20 5	15 2
„ 1894	24 7	25 0	18 8
„ 1893	26 8	23 8	19 5
„ 1892	31 5	25 4	21 2

III.—EMIGRATION.—MAY 1896.

RETURN of the NUMBERS, NATIONALITIES, and DESTINATIONS of the PASSENGERS that left the UNITED KINGDOM for PLACES out of EUROPE during the Month ended 31st May 1896, and the Five Months ended 31st May 1896, compared with the corresponding Periods of the previous Year.

Nationalities.	United States.	British North America.	Australasia.	Cape of Good Hope and Natal.	All other Places.	Total.	Total for corresponding Month, 1895.
Month ended 31st May.							
English - -	4,557	1,536	530	1,381	654	8,658	9,370
Scotch - -	825	326	77	169	65	1,462	1,582
Irish - -	8,384	154	52	48	7	8,645	10,091
Total of British origin - - }	13,766	2,016	659	1,598	726	18,765	21,343
Foreigners - -	5,897	1,206	17	858	137	8,115	7,831
Nationalities not distinguished - }	35	9	—	—	153	199	246
Total - -	19,698	3,231	676	2,456	1,018	27,079	29,470
Total for corresponding Month 1895 - }	22,990	3,153	644	1,656	1,027	29,470	—
Five Months ended 31st May.							
English - -	17,344	5,116	3,012	7,600	4,004	37,076	36,753
Scotch - -	3,231	745	248	777	434	5,435	5,459
Irish - -	21,757	373	172	180	118	22,600	26,905
Total of British origin - - }	42,332	6,234	3,432	8,557	4,556	65,111	69,117
Foreigners - -	22,972	3,261	74	3,138	669	30,114	25,320
Nationalities not distinguished - }	58	27	3	—	1,086	1,174	1,127
Total - -	65,362	9,522	3,509	11,695	6,311	96,399	95,664
Total for corresponding Months 1895 - }	72,067	8,027	3,418	7,250	5,302	—	—

NOTE.—The above figures, being made up at the earliest possible date after the close of each month, are subject to correction in the Annual Returns.

IV.—ALIEN IMMIGRATION—MAY 1896.

RETURN of the NUMBER of ALIENS that arrived from the CONTINENT at PORTS† in the UNITED KINGDOM, during the Month and Five Months ended 31st May 1896, compared with the corresponding Periods of the previous Year.

(Compiled from the ALIEN LISTS received by the Customs under Act 6 Will. 4. c. 11. sect. 2.)

	Hamburg, Bremen, and Bremerhaven.		Rotterdam, Amsterdam, and Antwerp.		Gothenburg, Christiania, Arendal, and Christiansand.		Dieppe.		Other Continental Ports.		Total.	
	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.	1896.	1895.
Month ended 31st May.												
Aliens not stated to be en route to America: *												
Arrived at London -	1,204	1,244	168	212	130	154	—	—	191	117	1,693	1,727
" Grimsby -	169	126	65	49	3	5	—	2	19	18	256	200
" Hull -	148	187	20	46	44	58	—	—	158	97	370	338
" Tyne Ports -	32	26	43	24	149	237	—	—	175	235	399	522
" Leith -	72	68	29	38	6	—	—	—	20	20	127	126
" Newhaven -	—	—	—	—	—	—	945	761	—	—	945	761
" Other Ports -	144	99	28	77	119	112	—	—	194	229	485	547
Total -	1,769	1,700	353	446	451	566	945	763	757	746	4,275	4,221
Aliens en route to America:												
Arrived at Grimsby -	31	122	82	237	1,136	724	—	—	139	175	1,388	1,258
" Hartlepool, } West -	68	233	—	—	—	—	—	—	—	—	68	233
" Hull -	104	82	2	50	1,445	1,183	—	—	1,635	1,246	3,186	2,561
" Leith -	126	493	44	96	8	—	—	—	30	64	208	653
" Other Ports -	—	10	25	42	16	9	—	—	455	335	496	396
Total -	329	940	153	425	2,605	1,916	—	—	2,259	1,820	5,346	5,101
Total of Aliens en route and of Aliens not stated to be en route to America * -	2,098	2,640	506	871	3,056	2,182	945	763	3,016	2,566	9,621	9,322
Five Months ended 31st May.												
Aliens not stated to be en route to America: *												
Total No. -	5,977	4,286	1,947	1,884	2,260	2,177	3,507	3,182	3,396	2,515	17,087	14,044
Aliens en route to America: Total No. -	1,441	1,957	470	1,228	9,736	7,071	—	—	9,145	6,993	20,792	17,279
Total of Aliens en route and of Aliens not stated to be en route to America * -	7,418	6,273	2,417	3,112	11,996	9,248	3,507	3,182	12,541	9,508	37,879	31,323

* The distinction made in this Return between "Aliens not stated to be en route to America," and "Aliens en route to America," is due to the fact that a large number of Aliens who arrive from Continental ports are reported to be on the way to America, and it is considered desirable to record this fact. But it is not thereby implied that the "Aliens not stated to be en route to America" come to this country for settlement, there being in fact a large emigration of foreigners from this country, while many of the Aliens arriving from Continental ports return to the Continent. (See Annual Report on Emigration and Immigration, No. 130, Sess. 1896, and Monthly Emigration Returns.)

† The ports from which Alien Lists are received are Aberdeen, Belfast, Bristol, Dover, Dublin, Folkestone, Glasgow, Goole, Grangemouth, Granton, Greenock, Grimsby, Harwich, Hull, Kirkcaldy, Leith, Liverpool, London, Middlesbrough, Newcastle, Newhaven, North Shields, South Shields, Southampton, Sunderland, and West Hartlepool. The lists received from Dover, Folkestone, Harwich, Newhaven, and Southampton show only deck passengers, and persons who, after landing, proceed by train as third-class passengers.

‡ The number of sailors included with the Aliens who arrived at ports in the United Kingdom not en route to America in the month of May 1896 was 886, and 971 in the same month of 1895; in the five months ended May 1896 the number was 4,317, and in the same months of 1895 the number was 3,787.

V.—BANKRUPTCY.—ENGLAND AND WALES.

NUMBER of RECEIVING ORDERS GAZETTED in the under-mentioned Periods and in the under-mentioned Principal Trades and Occupations.

	May		Five Months ended May	
	1896.	1895.	1896.	1895.
Total gazetted - - - -	No. 348	No. 374	No. 1,773	No. 1,933
Number gazetted in principal trades and occupations :—				
Publicans and hotel keepers, &c. -	21	24	134	157
Grocers, &c. - - - -	25	27	134	134
Farmers - - - -	15	23	104	135
Builders - - - -	19	13	101	93
Butchers - - - -	8	16	48	60
Bakers - - - -	13	14	48	56
Boot and shoe manufacturers and dealers - - - -	7	16	38	58
Decorators, painters, plumbers, &c. -	5	8	36	31
Greengrocers, fruiterers, &c. - -	6	7	35	38
Drapers, haberdashers, &c. - -	6	9	34	37
Tailors, &c. - - - -	6	11	30	53
Clerks, commercial and general -	6	3	27	22
Agents, commission and general -	7	1	24	27
Confectioners - - - -	3	2	22	13
Auctioneers - - - -	4	5	21	21
Carpenters and joiners - - - -	4	2	21	13
Corn, flour, seed, hay, and straw merchants and dealers - -	4	—	20	14
Jewellers, watchmakers, importers, silversmiths, &c. - -	3	2	19	14
Coal and coke merchants and dealers -	6	2	19	26
Saddlers and harness makers - -	6	3	19	21
Solicitors - - - -	1	5	18	18
Tobacconists, &c. - - - -	6	4	18	15
Fishmongers, poulterers, &c. - -	5	2	17	17
Clothiers, outfitters, &c. - -	1	2	16	20
Merchants - - - -	4	3	16	15
Cabinet makers and upholsterers -	—	4	15	9
Travellers, commercial, &c. - -	2	4	15	14
Schoolmasters and schoolmistresses -	1	4	14	10
Furniture dealers and makers - -	3	2	14	10
Millers - - - -	3	2	13	10
Provision merchants, &c. - -	1	6	12	20
Restaurant, coffee, and eating-house keepers - - - -	2	1	12	11
Carriers, carmen, lightermen, and hauliers - - - -	3	3	11	15
Dairymen, cowkeepers, &c. - -	3	3	11	10
Chemists, druggists, and chemical manufacturers - - - -	3	—	11	9
Printers and publishers - - - -	2	2	10	12
Milliners, dressmakers, &c. - -	3	1	10	5
General dealers - - - -	3	2	10	16
Wine and spirit merchants, &c. - -	4	2	10	18
Engineers and founders, &c. - -	4	3	10	12
Lodging-house keepers - - - -	2	1	9	9
Clerks in holy orders - - - -	2	—	9	9

	May		Five Months ended May	
	1896.	1895.	1896.	1895.
Number gazetted in principal trades and occupations— <i>cont.</i>	No.	No.	No.	No.
Blacksmiths, farriers, &c. - - -	2	1	9	6
Brokers, stock and share - - -	—	1	8	3
Gardeners, florists, &c. - - -	1	1	8	10
Ironmongers - - - -	3	3	8	14
Timber merchants and wood dealers -	2	2	7	4
Hairdressers - - - -	3	1	7	8
Wheelwrights - - - -	4	2	7	11
Cattle and pig dealers - - - -	1	—	7	4
Officers in Army - - - -	1	—	7	5
China, glass, and earthenware, &c. dealers - - - -	1	1	6	9
Contractors - - - -	1	—	6	4
Stationers - - - -	1	2	6	8
Carriage, &c. builders - - - -	1	4	6	2
Hosiers, gloves, &c. - - - -	2	1	5	10
Stone, marble, and monument masons -	3	—	5	3
Architects and surveyors - - -	1	3	4	6
Curriers, tanners, and leather merchants	1	1	4	8
Fishing net, smack owners, and masters	1	4	3	11
Cab, coach, and omnibus proprietors -	—	1	2	3
Directors and promoters of public companies - - - -	—	—	1	9

VI.—EXPORT OF LIVE ANIMALS FROM IRELAND TO GREAT BRITAIN.

RETURN of the NUMBER of ANIMALS exported from IRELAND to GREAT BRITAIN during the Month ended 31st May 1896, and during the first Five Months of the Year 1896, compared with the corresponding Periods of the Year 1895.

Animals.	May		Five Months ended May	
	1896.	1895.	1896.	1895.
	No.	No.	No.	No.
Cattle - - - - -	49,229	55,071	226,163	234,125
Sheep - - - - -	82,292	41,285	126,055	76,231
Swine - - - - -	36,257	25,178	293,297	280,155
Goats - - - - -	1,259	887	2,031	1,848
Horses - - - - -	5,810	4,111	16,714	10,952
Mules or Jennets - - - -	3	1	11	2
Asses - - - - -	645	210	943	312
Total - - - - -	175,495	126,743	665,214	603,625

VII.—FISHERY STATISTICS.—ENGLAND AND WALES.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the ENGLISH and WELSH COASTS from the fishing grounds during the Month and Five Months ended 31st May 1896, compared with the corresponding Periods of the Year 1895.

	May		Five Months ended May	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Brill	1,531	1,490	8,022	7,942
Soles	8,827	6,700	36,828	35,908
Turbot	6,758	6,099	30,037	30,028
Prime fish, not separately distinguished	5,210	2,241	13,492	9,846
Total prime fish	22,326	16,548	88,379	83,724
Cod	35,903	40,738	256,702	264,057
Haddock	183,215	181,506	1,018,562	994,731
Hake	16,752	12,419	36,426	28,030
Halibut	14,800	13,743	38,011	36,263
Herrings	6,804	19,297	38,486	47,153
Ling	7,500	10,300	49,249	53,237
Mackerel	119,444	134,499	231,848	191,855
Pilchards	—	—	6,101	270
Plaice	58,722	59,090	267,516	268,821
Sprats	13	—	49,225	43,134
Fish not separately distinguished, except shell fish	95,724	92,928	456,203	439,063
Total	561,203	580,868	2,536,708	2,455,347
Shell fish :—	No.	No.	No.	No.
Crabs	1,108,998	1,169,005	3,084,290	1,967,972
Lobsters	116,357	99,148	287,829	168,755
Oysters	1,812,000	1,690,000	8,109,000	5,620,000
Other shell fish	Cwts.	Cwts.	Cwts.	Cwts.
	40,837	56,767	222,574	225,108
VALUE.				
	£	£	£	£
Brill	3,466	3,505	22,576	22,353
Soles	44,596	45,673	232,487	245,996
Turbot	20,107	22,966	121,437	128,372
Prime fish, not separately distinguished	16,206	7,643	45,850	32,510
Total prime fish	84,375	79,787	422,350	429,231
Cod	17,071	19,044	166,091	173,135
Haddock	81,124	92,245	572,763	569,359
Hake	12,743	9,763	31,426	24,779
Halibut	23,993	22,962	76,638	72,944
Herrings	2,742	8,403	9,919	14,795
Ling	4,159	5,681	32,370	35,115
Mackerel	85,974	94,604	178,607	173,223
Pilchards	—	—	1,763	65
Plaice	61,195	56,465	325,029	309,223
Sprats	6	—	7,456	3,680
Fish not separately distinguished, except shell fish	68,045	61,805	321,868	293,721
Total	441,430	450,731	2,146,480	2,099,270
Shell fish :—				
Crabs	11,064	10,943	29,360	18,833
Lobsters	4,677	4,038	12,068	7,357
Oysters	4,359	4,060	27,230	15,904
Other shell fish	12,817	14,659	60,389	55,482
Total	32,917	33,720	129,047	97,576
Total value of fish landed	474,347	484,451	2,275,527	2,196,846

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

VIII.—FISHERY STATISTICS.—SCOTLAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the SCOTCH COASTS during the Month and Five Months ended 31st May 1896, compared with the corresponding Periods of the Year 1895.

	May		Five Months ended May	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles (Lemon Soles) - - - -	1,979	2,026	7,683	6,447
Turbot - - - - -	335	464	1,789	1,409
Cod - - - - -	83,616	72,695	317,745	246,965
Eel - - - - -	1,173	1,376	8,912	8,372
Flounder, Plaice, Brill - - -	5,035	6,120	42,991	33,263
Haddock - - - - -	57,209	73,939	505,880	464,970
Halibut - - - - -	8,408	7,733	20,516	18,374
Herrings - - - - -	250,434	263,347	330,423	375,097
Ling - - - - -	28,717	42,466	81,755	117,942
Mackerel - - - - -	485	25	768	91
Saith (Coal Fish) - - - -	32,338	11,788	56,412	35,222
Skate - - - - -	8,731	8,661	39,868	42,121
Sparling - - - - -	—	5	92	49
Sprats - - - - -	—	—	29,932	1,130
Torsk (Tusk) - - - - -	4,196	4,312	7,455	7,000
Whiting - - - - -	2,490	3,118	18,597	14,074
Fish not separately distinguished, } except shell fish - - - - -	4,521	4,104	22,367	19,875
Total - - - - -	489,637	501,979	1,543,188	1,393,201
Shell Fish:—	No.	No.	No.	No.
Crabs - - - - -	701,073	737,927	2,096,099	1,090,023
Lobsters - - - - -	82,362	85,656	267,125	227,108
Oysters - - - - -	2,000	1,100	114,462	154,100
Clams - - - - -	198	490	9,841	9,841
Mussels - - - - -	11,103	7,851	120,909	84,887
Other shell fish - - - - -	6,337	8,392	32,016	26,790
VALUE.				
	£	£	£	£
Soles (Lemon Soles) - - - -	3,380	3,464	15,576	13,625
Turbot - - - - -	694	1,161	5,323	4,448
Cod - - - - -	22,483	20,114	106,013	90,057
Eel - - - - -	487	593	4,546	4,926
Flounder, Plaice, Brill - - -	5,004	5,946	44,245	31,957
Haddock - - - - -	23,666	26,308	190,314	188,993
Halibut - - - - -	7,189	6,654	22,104	20,157
Herrings - - - - -	48,236	64,546	72,889	93,927
Ling - - - - -	7,218	9,487	23,378	30,969
Mackerel - - - - -	94	6	299	32
Saith (Coal Fish) - - - - -	2,617	1,065	5,645	4,055
Skate - - - - -	1,502	1,526	10,092	9,557
Sparling - - - - -	—	30	275	204
Sprats - - - - -	—	—	1,755	85
Torsk (Tusk) - - - - -	533	592	1,027	987
Whiting - - - - -	1,089	1,233	7,423	6,991
Fish not separately distinguished, } except shell fish - - - - -	939	914	4,776	4,507
Total - - - - -	125,081	143,644	515,680	505,477
Shell Fish:—				
Crabs - - - - -	3,361	4,104	9,756	6,143
Lobsters - - - - -	3,176	4,038	11,594	11,133
Oysters - - - - -	11	6	461	750
Clams - - - - -	30	84	1,369	1,325
Mussels - - - - -	703	609	8,253	5,216
Other shell fish - - - - -	1,519	1,448	6,895	5,990
Total - - - - -	8,800	10,339	38,328	30,566
Total value of fish landed	133,881	153,980	554,008	536,043

NOTE.—The above figures are subject to correction in the Annual Returns.

IX.—FISHERY STATISTICS.—IRELAND.

STATEMENT of the TOTAL QUANTITY and VALUE of the FISH returned as landed on the IRISH COASTS during the Month and Five Months ended 31st May 1896, compared with the corresponding Periods of the Year 1895.

	May		Five Months ended May	
	1896.	1895.	1896.	1895.
QUANTITY.				
	Cwts.	Cwts.	Cwts.	Cwts.
Soles - - - - -	185	215	640	866
Turbot - - - - -	101	106	367	444
Total prime fish - - -	286	321	1,007	1,310
Cod - - - - -	1,892	3,334	23,174	28,902
Haddock - - - - -	1,051	1,886	12,689	13,309
Hake - - - - -	184	80	804	1,251
Herrings - - - - -	3,619	7,618	5,436	11,747
Ling - - - - -	1,913	3,770	11,010	17,999
Mackerel - - - - -	126,144	71,032	162,487	103,323
Sprats - - - - -	—	—	—	—
Whiting - - - - -	931	883	5,623	5,550
Fish not separately distinguished, except shell fish - - - - -	5,548	4,633	17,761	15,287
Total - - - - -	141,568	93,567	239,991	198,743
Shell fish:—	No.	No.	No.	No.
Crabs - - - - -	38,663	42,321	66,782	59,583
Lobsters - - - - -	15,169	16,931	35,901	33,916
Oysters - - - - -	—	—	395,900	153,050
Other shell fish - - - -	Cwts.	Cwts.	Cwts.	Cwts.
	1,544	1,911	9,634	7,681
VALUE.				
	£	£	£	£
Soles - - - - -	697	933	2,819	3,342
Turbot - - - - -	335	292	1,393	1,393
Total prime fish - - -	1,032	1,225	4,212	5,240
Cod - - - - -	887	1,560	10,433	11,647
Haddock - - - - -	674	1,080	7,771	8,239
Hake - - - - -	100	41	455	653
Herrings - - - - -	1,544	3,121	1,886	3,871
Ling - - - - -	819	1,379	4,422	6,025
Mackerel - - - - -	38,738	30,630	64,848	50,477
Sprats - - - - -	—	—	—	—
Whiting - - - - -	496	472	3,164	3,027
Fish not separately distinguished, except shell fish - - - - -	2,639	2,254	9,033	7,479
Total - - - - -	46,929	41,771	106,224	96,663
Shell fish:—				
Crabs - - - - -	149	187	277	222
Lobsters - - - - -	505	623	1,224	1,231
Oysters - - - - -	—	—	777	230
Other shell fish - - - -	252	386	1,643	1,267
Total - - - - -	906	1,146	3,921	3,000
Total value of fish landed -	47,835	42,917	110,145	99,663

NOTE.—The above figures are subject to correction in the Annual Returns. The values given are the actual values returned by the local officers at each place.

X.--COTTON RETURNS.—MAY 1896.

RETURN of the NUMBER of BALES of COTTON IMPORTED and EXPORTED, FORWARDED from PORTS to INLAND TOWNS, and RETURNED to PORTS during the Month and Five Months ended May 1896, compared with the corresponding Months of the Year 1895.

	Month of May		Five Months ended May	
	1896.	1895.	1896.	1895.
IMPORTS.				
	No.	No.	No.	No.
American - - -	163,177	245,820	1,301,950	1,943,307
Brazilian - - -	463	401	24,851	3,160
East Indian - - -	15,100	4,527	80,715	13,282
Egyptian - - -	18,612	25,109	179,982	179,707
Miscellaneous - - -	2,893	7,070	15,931	26,168
Total - - -	200,245	282,927	1,603,429	2,165,624
EXPORTS.				
	No.	No.	No.	No.
American - - -	19,825	24,567	79,865	101,091
Brazilian - - -	1,340	400	3,690	5,958
East Indian - - -	3,710	3,010	21,564	13,837
Egyptian - - -	5,589	5,478	48,401	40,358
Miscellaneous - - -	536	3,857	10,683	12,905
Total - - -	31,000	36,542	164,203	174,149
FORWARDED from PORTS to INLAND TOWNS.				
	No.	No.	No.	No.
American - - -	205,792	259,156	1,104,150	1,259,207
Brazilian - - -	3,804	2,279	24,904	6,020
East Indian - - -	5,646	1,813	22,748	7,958
Egyptian - - -	25,981	13,453	142,175	133,699
Miscellaneous - - -	6,908	7,520	45,594	51,033
Total - - -	248,131	284,221	1,339,571	1,457,917
FORWARDED from INLAND TOWNS to PORTS.				
	No.	No.	No.	No.
American - - -	427	476	1,225	1,934
Brazilian - - -	—	—	7	—
East Indian - - -	—	—	4	—
Egyptian - - -	—	—	49	10
Miscellaneous - - -	166	81	406	412
Total - - -	593	557	1,691	2,356

XI.—PERIODICAL RETURNS OF IMPORTS AND EXPORTS.

STATEMENT of the IMPORTS and EXPORTS into and from the under-mentioned COUNTRIES in the latest Month for which RETURNS have been received, with Aggregates for the Period of the Year, including such latest Month.

Note.—Franc = $9\frac{6}{10}d.$; Milreis = 4s. 6d.; Lira = $9\frac{6}{10}d.$; Gulden = 1s. 8d.; Drachma = $9\frac{6}{10}d.$; Lew = $9\frac{6}{10}d.$; L. Egyptian = 1l. 0s. 6½d.; Dollar = 4s. 2d.

I.—IMPORTS.

Name of Country.	Latest Month.	Value for the Month.		Aggregate for Period of the Year, including latest Month.	
		1896.	1895.	1896.	1895.
France - - - April -	Frs. -	333,600,000	311,772,000	1,381,087,000	1,214,030,000
	(1895)	(1895)	(1894)	(1895)	(1894)
Portugal - - - Nov. -	Milreis -	3,792,000	2,795,000	36,603,000	33,078,000
	(1896)	(1896)	(1895)	(1896)	(1895)
Italy - - - April -	Lire -	96,600,000	100,368,000	370,230,000	355,328,000
Austria-Hungary - April -	Gulden -	67,711,000	60,204,000	255,440,000	232,514,000
Greece - - - Feb. -	Drchms. 10,054,000	5,124,000	18,083,000	11,725,000	
Bulgaria - - - Jan. -	Lew -	2,977,000	3,037,000	—	—
	(1895)	(1895)	(1894)	(1895)	(1894)
Egypt - - - Dec. -	L. Egypt. 923,000	755,000	8,246,000	9,119,000	
	(1896)	(1896)	(1896)	(1895)	(1895)
United States - Mar. -	Dollars -	66,401,000	69,295,000	197,538,000	195,150,000
	(1895)	(1895)	(1894)	(1895)	(1894)
Mexico* - - - Dec. -	„ 3,357,000	2,739,000	18,870,000	16,626,000	
	(1896)	(1896)	(1896)	(1895)	(1895)
British India† - Mar. -	Rupees 6,68,59,614	5,90,56,455	69,33,06,842	70,16,74,378	

II.—EXPORTS.

France - - - April -	Frs. -	324,177,000	290,499,000	1,158,267,000	1,075,391,000
	(1895)	(1895)	(1894)	(1895)	(1894)
Portugal - - - Nov. -	Milreis -	2,375,000	1,765,000	24,791,000	21,759,000
	(1896)	(1896)	(1895)	(1896)	(1895)
Italy - - - April -	Lire -	93,254,000	90,187,000	353,210,000	308,750,000
Austria-Hungary - April -	Gulden 61,861,000	60,453,000	237,243,000	227,635,000	
Greece - - - Feb. -	Drchms. 5,018,000	3,427,000	10,089,000	9,210,000	
Bulgaria - - - Jan. -	Lew -	6,691,000	5,156,000	—	—
	(1895)	(1895)	(1894)	(1895)	(1894)
Egypt - - - Dec. -	L. Egypt. 2,156,000	1,374,000	12,674,000	11,884,000	
	(1896)	(1896)	(1896)	(1895)	(1895)
United States - Mar. -	Dollars -	75,542,000	65,162,000	240,103,000	202,375,000
	(1895)	(1895)	(1894)	(1895)	(1894)
Mexico* - - - Dec. -	„ 8,614,000	6,802,000	49,683,000	39,299,000	
	(1896)	(1896)	(1896)	(1895)	(1895)
British India† - Mar. -	Rupees 11,41,77,450	9,61,35,425	1,14,27,43,357	1,08,81,49,990	

The above figures are subject to revision in the Annual Returns.

NOTE.—The figures are those of the “special” imports and exports, except in the case of Bulgaria, the United States, Mexico, and British India, where the figures are “general.” “Special” means, in the case of imports, imports for home consumption; in the case of exports, exports of domestic produce and manufacture only.

* The aggregate figures are for the financial year commencing 1st July.

† The aggregate figures are for the financial year commencing 1st April.

XII.—FOREIGN TRADE OF INDIA.

IMPORTS and EXPORTS into and from BRITISH INDIA.

	Imports from Foreign Countries.		Exports to Foreign Countries.*	
	Twelve Months ended 31st March		Twelve Months ended 31st March	
	1895-6.	1894-5.	1895-6.	1894-5.
	R.	R.	R.	R.
I.—Animals—living - - -	30,85,846	26,90,436	16,10,528	13,77,610
II.—Articles of food and drink -	8,73,77,005	8,33,65,800	30,23,49,889	28,84,16,906
III.—Metals and manufactures of—				
A.—Hardware and cutlery (including plated-ware)	1,42,23,072	1,23,33,972	11,72,260	3,79,365
B.—Metals - - -	6,77,33,582	5,06,17,630		
C.—Machinery and mill-work	3,23,71,933	2,44,24,323		
D.—Railway plant and rolling-stock (other than Government stores) -	1,52,03,845	1,55,69,686		
IV.—Chemicals, drugs, medicines and narcotics, and dyeing and tanning materials -	2,16,06,437	1,86,62,942	15,56,43,180	15,42,89,896
V.—Oils - - -	3,13,73,322	2,22,12,221	73,87,072	77,50,157
VI.—Raw materials and unmanufactured articles - -	4,85,03,608	4,42,09,468	42,21,54,144	40,66,90,340
VII.—Articles manufactured and partly manufactured—				
A.—Cotton yarn - -	2,97,10,904	2,35,12,542	6,73,03,363	5,67,20,237
B.—Cotton piece goods -	22,35,88,236	29,38,32,367	1,56,17,639	1,43,16,536
C.—Other articles - -	11,85,25,052	10,42,37,986	12,22,89,985	10,71,33,235
TOTAL - -	69,33,06,842	70,16,74,378	1,09,55,33,060	1,03,75,74,332

* Exports of Indian Produce and Manufacture only.

XXII.—LIST OF DIPLOMATIC AND CONSULAR REPORTS.

ANNUAL SERIES.

REPORTS of the Annual Series, 1896, have been issued from Her Majesty's Diplomatic and Consular Officers at the following places, and may be obtained from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C.

No.	Place.	Price.	No.	Place.	Price.
1657	Réunion - -	1d.	1692	Guayaquil - -	1d.
1658	Tokio - -	1½d.	1693	Wenchow - -	1d.
1659	Maranham - -	1d.	1694	Piræus - -	3d.
1660	Copenhagen - -	1d.	1695	Tokio - -	3d.
1661	Berlin - -	1½d.	1696	Marseilles - -	1d.
1662	Tehran - -	2½d.	1697	Manila - -	1d.
1663	Salonica - -	1½d.	1698	Jerusalem - -	1d.
1664	Manila - -	1½d.	1699	Cherbourg - -	2d.
1665	Florence - -	5½d.	1700	Leghorn - -	1½d.
1666	Dakar - -	1½d.	1701	Boston - -	1½d.
1667	Havre - -	2d.	1702	Kiungchow - -	1d.
1668	Rouen - -	2d.	1703	Naples - -	2½d.
1669	Corfu - -	½d.	1704	Stockholm - -	2d.
1670	Calais - -	1d.	1705	Corunna - -	2d.
1671	Tehran - -	1½d.	1706	Rio de Janeiro - -	2½d.
1672	Barcelona - -	2d.	1707	San José - -	1d.
1673	Amsterdam - -	1d.	1708	Paramaribo - -	2d.
1674	Bordeaux - -	2½d.	1709	Brest - -	1½d.
1675	Warsaw - -	1d.	1710	Montevideo - -	½d.
1676	Havana - -	1½d.	1711	Charleston - -	2½d.
1677	Berlin - -	1d.	1712	Baltimore - -	1d.
1678	Beira - -	1½d.	1713	Tripoli - -	1d.
1679	Saigon - -	1d.	1714	Callao - -	½d.
1680	Trebizond - -	1d.	1715	Ningpo - -	1d.
1681	Vera Cruz - -	1½d.	1716	Dunkirk - -	1½d.
1682	Patras - -	1d.	1717	Batoum - -	2d.
1683	La Rochelle - -	1½d.	1718	Hankow - -	1½d.
1684	Madrid - -	1½d.	1719	Foochow - -	3½d.
1685	Belgrade - -	2d.	1720	Syra - -	½d.
1686	Algiers - -	5d.	1721	Panama - -	1d.
1687	Galveston - -	2½d.	1722	Batavia - -	1½d.
1688	New Orleans - -	2d.	1723	Genoa - -	3d.
1689	Suakin - -	1d.	1724	Cagliari - -	2½d.
1690	Pernambuco - -	1d.	1725	Chicago - -	7½d.
1691	Guatemala - -	1½d.			

LIST OF DIPLOMATIC AND CONSULAR REPORTS—*cont.*

The following Reports from Her Majesty's Representatives abroad on subjects of general and commercial interest have also been issued, and may be obtained from the above-mentioned sources:—

No.	—	Price.
387	<i>Portugal</i> .—Report on the East Coast Route to Mashonaland -	1 <i>d.</i>
388	<i>Austria-Hungary</i> .—Report on the Iron Industry in Styria and the Betler Ironworks in Hungary - - -	5 <i>d.</i>
389	<i>Belgium</i> .—Report on the Output of Coal, Iron, and Steel in Belgium in 1894-95 - - - -	$\frac{1}{2}$ <i>d.</i>
390	<i>Japan</i> .—Report on the Railways of Japan (with Plans) -	13 $\frac{1}{2}$ <i>d.</i>
391	<i>Egypt</i> .—Report on the Commercial Relations of Great Britain and Egypt - - - - -	2 $\frac{1}{2}$ <i>d.</i>
392	<i>Germany</i> .—Report on the Wine Trade of Germany, 1893-95 -	1 <i>d.</i>
393	<i>Mexico</i> .—Report on the Cultivation of Coffee in Mexico -	1 <i>d.</i>
394	<i>Switzerland</i> .—Report on the various Institutions and Agricultural Development of Switzerland - - -	1 $\frac{1}{2}$ <i>d.</i>
395	<i>Germany</i> .—Report on the Reduction of the Railway Tariff for Coal from the Silesian Mines - - -	$\frac{1}{2}$ <i>d.</i>
396	<i>Austria-Hungary</i> .—Report on the Treatment of Habitual Drunkards in Austria and the "Curatel" Procedure -	$\frac{1}{2}$ <i>d.</i>
397	<i>Brazil</i> .—Report on the Productions, Commerce, and Finances, of the States of Amazonas and Para - - -	2 $\frac{1}{3}$ <i>d.</i>

XXIII.—LIST OF COLONIAL OFFICE REPORTS.

The following Reports relating to Her Majesty's Colonial Possessions have been issued, and may be purchased from Messrs. Eyre and Spottiswoode, East Harding Street, Fleet Street, E.C. :—

ANNUAL.

No.	Colony.	Year.	No.	Colony.	Year.
47	British Bechuanaland	1890-92	106	Gambia	1893
48	Malta	1890 and 1891	107	Hong Kong	"
49	Gibraltar	"	108	St. Vincent	"
50	Falkland Islands	"	109	Gibraltar	"
51	Leeward Islands	"	110	Bahamas	"
52	Grenada	1890 and 1891	111	Turks and Caicos Islands	"
53	St. Lucia	"	112	Leeward Islands	"
54	Labuan	"	113	Barbados	"
55	British Guiana	"	114	Malta	"
56	Zululand	"	115	Zululand	"
57	Mauritius	"	116	British Honduras	"
58	Lagos	"	117	Trinidad and Tobago	"
59	Straits Settlements	"	118	Falkland Islands	"
60	Ceylon	"	119	Rodrigues	"
61	Natal	1891-92	120	Mauritius	"
62	Basutoland	"	121	Strait Settlements	"
63	St. Helena	1891	122	Labuan	"
64	Sierra Leone	"	123	Basutoland	1893-94
65	Jamaica	1891-92	124	St. Lucia	1893
66	Trinidad and Tobago	1891	125	St. Helena	"
67	Newfoundland	"	126	Ceylon	"
68	British New Guinea	1891-92	127	Fiji	"
69	Victoria	1891	128	Grenada	"
70	Rodrigues	"	129	Sierra Leone	"
71	Bermuda	1892	130	British Bechuanaland	1893-94
72	Fiji	1891	131	British New Guinea	1892-93 and 1893-94
73	British Honduras	"	132	Lagos	1893
74	Turks and Caicos Islands	1890	133	British Guiana	1893-94
75	Gibraltar	1892	134	Jamaica	"
76	Leeward Islands	"	135	Newfoundland	1893
77	Bahamas	"	136	Gold Coast	"
78	Trinidad and Tobago	"	137	Zululand	1894
79	Malta	"	138	Bermuda	"
80	Gambia	"	139	Bahamas	"
81	Straits Settlements	"	140	Barbados	"
82	Grenada	"	141	Turks and Caicos Islands	"
83	Barbados	"	142	Malta	"
84	St. Vincent	"	143	Gambia	"
85	Hong Kong	"	144	Windward Islands	"
86	St. Lucia	"	145	Trinidad and Tobago	"
87	Falkland Islands	"	146	Gibraltar	"
88	Gold Coast	"	147	Falkland Islands	"
89	Basutoland	1892-93	148	Hong Kong	"
90	Ceylon	1892	149	Straits Settlements	"
91	Zululand	"	150	Lagos	"
92	Mauritius	"	151	Seychelles	"
93	Labuan	"	152	Basutoland	1894-95
94	British Honduras	"	153	Fiji	1894
95	Lagos	"	154	St. Helena	"
96	Sierra Leone	"	155	Ceylon	"
97	Fiji	"	156	Mauritius	"
98	Rodrigues	"	157	Labuan	"
99	St. Helena	"	158	Gold Coast	"
100	British Bechuanaland	1892-93	159	British Guiana	1894-95
101	British Guiana	"	160	Sierra Leone	1894
102	Newfoundland	1892	161	Jamaica	1894-95
103	Jamaica	1892-93	162	British Honduras	1894
104	Victoria	1892	163	British Bechuanaland	1894-95
105	Bermuda	1893	164	Newfoundland	1894

MISCELLANEOUS.

No.	Colony.	Subject.
1	Gold Coast	Economic Agriculture.
2	Zululand	Forests.
3	Sierra Leone	Geology and Botany.
4	Canada	Advantages to Emigrants.

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